

**STATE OF FLORIDA
DEPARTMENT OF TRANSPORTATION
AVIATION OFFICE**

**Statewide Airfield Pavement
Management Program**

**Williston Municipal Airport–X60
(General Aviation)
Williston, Florida
(District 2)**



May 2012

TABLE OF CONTENTS

	PAGE NO.
Executive Summary	iii
1. Introduction.....	1
2. Network Definition and Pavement Inventory	10
3. Pavement Condition.....	14
4. Pavement Condition Prediction	20
5. Maintenance Policies and costs	21
6. Pavement Rehabilitation Needs Analysis	26
7. Maintenance and Rehabilitation Plan	30
8. Visual Aids.....	31
9. Recommendations.....	32

LIST OF FIGURES

Figure 1-1: Pavement Life Cycle.....	4
Figure 1-2: PCI Rating Scale	6
Figure 2-1: Pavement Area by Surface Type.....	12
Figure 3-1: Network PCI Distribution by Rating Category	16
Figure 3-1a: Condition Rating Summary.....	17
Figure 3-2: Percentage of Pavement Area within Each PCI Range by Pavement Use.....	18
Figure 4-1: Predicted PCI by Pavement Use	20
Figure 6-1: Budget Scenario Analysis	29

LIST OF TABLES

Table I: Condition Summary by Branch.....	iv
Table II: Condition Summary by Pavement Use	iv
Table III: Condition Summary by Pavement Rank.....	v
Table IV: Immediate Major M&R Needs	v
Table V: 10-Year M&R Costs under Unlimited Funding Scenario	vi
Table 1-1: Sampling Rate for FDOT Condition Surveys	5
Table 2-1: Construction Since Last Inspection & Anticipated Construction Activity	11
Table 2-2: Pavement Area by Pavement Use	11
Table 2-3: Branch and Section Inventory	13
Table 3-1: Pavement Distresses for Asphalt Concrete Surfaces.....	14
Table 3-2: Pavement Distresses for Portland Cement Concrete Surfaces	15
Table 3-3: Condition by Pavement Use.....	17
Table 5-1: Routine Maintenance Activities for Airfield Pavements	22
Table 5-2: Critical PCI for General Aviation Airports	23
Table 5-3: FDOT Minimum Service Level PCI for General Aviation Airports.....	23
Table 5-4: M&R Activities for General Aviation Airports	23
Table 5-5: Maintenance Unit Costs for FDOT	24
Table 5-6: M&R Activities and Unit Costs by Condition for General Aviation Airports...	25
Table 6-1: Summary of Immediate Major M&R Needs Option No. 1	26

TABLE OF CONTENTS

	<u>PAGE NO.</u>
Table 6-2: Summary of Immediate Major M&R Needs Option No. 2	27
Table 6-3: Summary of Year 1 Maintenance Activities	28
Table 7-1: M&R Costs under Unlimited Funding Scenario	30

APPENDICES

Appendix A	Network Definition Map System Inventory Map Pavement Inventory Table Work History Report
Appendix B	2012 Condition Map Pavement Condition Index Table
Appendix C	Branch Condition Report Section Condition Report
Appendix D	Pavement Condition Prediction Table Predicted PCI by Pavement Use Graph
Appendix E	Year 1 Maintenance Activities Table
Appendix F	Major M&R Plan by Year under Unlimited Funding Scenario Table
Appendix G	10-Year M&R Map
Appendix H	Photographs
Appendix I	PCI Re-inspection Report

EXECUTIVE SUMMARY

In 2010, the Florida Department of Transportation (FDOT) Aviation Office selected a Consultant team consisting of Kimley-Horn and Associates and their Subconsultants, AMEC Environment & Infrastructure, Inc. and All About Pavements, Inc., to provide services in support of FDOT in the continuing evaluation and updating of the existing Statewide Airfield Pavement Management Program (SAPMP) to be completed over fiscal years 2011 and 2012.

The tasks required to achieve this objective at Williston Municipal Airport included:

- Obtain recent construction history from the Airport to update the Pavement Inventory CADD drawings from the previous SAPMP update,
- Perform a visual Pavement Condition Index (PCI) survey of the airfield pavements at the Airport,
- Update the MicroPAVER database to analyze the PCI field data and determine the current condition of the airfield pavements,
- Predict the future deterioration of the pavements,
- Develop a 10-year M&R plan to address the pavement needs at Williston Municipal Airport, and
- Provide the estimated costs associated with the suggested immediate and future M&R activities

During February 2012, the PCI survey was performed at Williston Municipal Airport. The results of the survey indicate that, based on a numerical scale of 0 to 100, the overall area-weighted average PCI of the airfield pavements in 2012 is 59, representing a Fair overall network condition.

Table I below summarizes the overall condition summary by network branch.

Table I: Condition Summary by Branch

Branch Name	Area Weighted PCI	PCI Range	Average Condition Rating	FDOT Minimum Service Level	MicroPAVER Minimum PCI	Action Required
Apron	91	91	Good	60	65	
Hangar Apron	93	91-98	Good	60	65	
Apron at T-Hangars	76	44-90	Satisfactory	60	65	
Runway 14-32	35	14-66	Very Poor	75	65	X
Runway 5-23	44	39-47	Poor	60	65	X
Runway 5-23 WT	87	87-90	Good	60	65	
Taxiway Alpha	71	66-98	Satisfactory	65	65	
Taxiway Bravo	96	96	Good	65	65	
Taxiway Charlie	93	87-97	Good	65	65	
Taxiway Delta	92	92	Good	65	65	
Taxiway D-1	100	100	Good	65	65	
Taxiway Echo	100	100	Good	65	65	
Taxiway Foxtrot	12	7-100	Failed	65	65	X
Taxiway Golf	2	2	Failed	65	65	X

Note: WT = whitetopping.

Tables II and III below illustrate the area-weighted PCI computed individually for each pavement use and rank, respectively.

Table II: Condition Summary by Pavement Use

Use	Average Area-Weighted PCI	Condition Rating
Runway	36	Fair
Taxiway	57	Fair
Apron	86	Good
All (Weighted)	59	Fair

Use	Area-Weighted PCI	Condition Rating
Runway 5-23	85 - 93	Good

Note: This runway has whitetopping pavement.

Table III: Condition Summary by Pavement Rank

Rank*	Average Area-Weighted PCI	Condition Rating
Primary	59	Fair
Tertiary	47	Poor
All (Weighted)	59	Fair

*The pavement rank for the airport pavement network is listed on Table 2-3.

The immediate M&R needs, or needs that have been programmed to be completed in the first year of the 10-year M&R plan based on an unlimited budget at Williston Municipal Airport, include: Apron at T-Hangars, Runway 14-32, Runway 5-23, Taxiway Foxtrot, and Taxiway Golf. Asphalt pavement conditions in these areas justify either mill and overlay rehabilitation activity or full pavement reconstruction. Portland Cement Concrete pavement conditions in Runway 5-23, Runway 14-32, and Taxiway Foxtrot would benefit from PCC restoration or full PCC pavement reconstruction. The immediate needs are summarized in Table IV below.

Table IV: Immediate Major M&R Needs

Branch Name	Section ID	Surface Type	Section Area (ft ²)	Major M&R Costs*	PCI Before M&R	M&R Activity	PCI After M&R
Apron at T-Hangars	4315	AC	4,001	\$10,406.61	63	Mill and Overlay	100
Apron at T-Hangars	4316	AC	3,259	\$20,499.11	43	Mill and Overlay	100
Runway 14-32	6205	PCC	30,300	\$279,426.68	36	Reconstruction	100
Runway 14-32	6210	PCC	15,000	\$138,330.04	36	Reconstruction	100
Runway 14-32	6215	AC	430,000	\$2,704,700.21	43	Mill and Overlay	100
Runway 14-32	6220	AC	215,000	\$2,928,300.95	13	Reconstruction	100
Runway 14-32	6235	PCC	30,000	\$111,210.07	59	PCC Restoration	100
Runway 5-23	6105	PCC	30,000	\$188,700.01	46	PCC Restoration	100
Runway 5-23	6110	PCC	15,000	\$116,340.03	38	Reconstruction	100
Taxiway Foxtrot	550	AC	185,509	\$2,526,633.40	6	Reconstruction	100
Taxiway Foxtrot	555	PCC	11,250	\$153,225.05	0	Reconstruction	100
Taxiway Golf	450	AC	94,786	\$1,290,985.74	1	Reconstruction	100
Total				\$10,468,757.90	26		100

* Costs are adjusted for inflation.

A forecast of Major M&R needs for a 10-year period, starting from 2012, was developed using an unlimited budget. The analysis identified ongoing maintenance needs and major M&R during that interval. The results of this analysis are provided in Table V below.

Table V: 10-Year M&R Costs under Unlimited Funding Scenario

Year	Preventative	Major M&R	Total Year Cost
2011	\$39,246.89	\$10,468,757.89	\$10,508,004.78
2012	\$11,732.55	\$218,985.37	\$230,717.92
2013	\$17,621.76	\$0.00	\$17,621.76
2014	\$24,147.07	\$0.00	\$24,147.07
2015	\$33,812.70	\$0.00	\$33,812.70
2016	\$39,569.57	\$62,379.87	\$101,949.44
2017	\$55,172.48	\$0.00	\$55,172.48
2018	\$80,344.55	\$0.00	\$80,344.55
2019	\$123,523.73	\$0.00	\$123,523.73
2020	\$152,570.01	\$0.00	\$152,570.01
Total	\$577,741.31	\$10,750,123.13	\$11,327,864.44

Note: Costs are adjusted for inflation.

The implementation of the 10-Year Major M&R Plan is expected to provide an improvement in the overall condition of the airfield pavement, where the area-weighted PCI would increase from 59 in 2012 to 80 in 2021. Appendix F lists the Major M&R for the 10-Year program. Appendix G graphically depicts the program activity.

It is important to note that although preventative and some major M&R activities would have to be conducted over several years, the area-weighted PCI value for all Williston Municipal Airport pavements in 2021 may remain near 80. The airport manager should realize that what is most important is that the pavement repair work (preventative and major M&R) that has been identified for Williston Municipal Airport is conducted at some point in the 10-year plan.

1. INTRODUCTION

The State of Florida has more than 100 public airports that are vital to the Florida economy as well as the economy of the United States. There are millions of square yards of pavement for the runways, taxiways, aprons and other areas of these airports that support aircraft operations. The timely and proper maintenance and rehabilitation (M&R) of these pavements allows the airports to operate efficiently, economically and without excessive down time.

In order to support the planning, scheduling, and design of the M&R activities based on pavement evaluation and pavement management performance trends, the Florida Department of Transportation (FDOT) Aviation Office implemented the Statewide Airfield Pavement Management Program (SAPMP) in 1992.

In 2010, the FDOT Aviation Office selected a Consultant team consisting of Kimley-Horn and Associates and their Subconsultants, AMEC Environment & Infrastructure, Inc. and All About Pavements, Inc., to provide services in support of FDOT in the continuing evaluation and updating of the existing SAPMP to be completed over fiscal years 2011 and 2012.

This report discusses the work performed, a summary of the findings, results, and recommendations for M&R planning associated with the update to the SAPMP. It also describes the procedures used to ensure that the appropriate engineering and scientific standards of care, quality, budget, and schedule requirements are implemented during the performance of the SAPMP.

1.1 Purpose

This Florida Airport Pavement Evaluation Report is intended to:

- Describe, briefly, the SAPMP and the roles and responsibilities of the program's participants;
- Provide background information on pavement management principles, objectives, and benefits to this airport;
- Outline the procedures used to collect, evaluate and report pavement inspection results at this airport;
- Present the findings from the pavement inspection;
- Analyze and discuss the needs for Maintenance and Rehabilitation (M&R) activities and associated costs for this airport.

1.2 FDOT Statewide Airfield Pavement Management Program

In 1992, the FDOT implemented the SAPMP to improve the knowledge of pavement conditions at public airports in the State system, identify maintenance needs at individual airports, automate information management, and establish standards to address future needs. The 1992 SAPMP provided valuable information for establishing and performing pavement M&R.

In 1992/1993, and 1998/1999, the FDOT Aviation Office participated in the development of a proprietary software pavement management system and developed and populated a pavement management database that provided valuable information for establishing M&R policies, estimating M&R costs, and developing recommendations for performing routine pavement

maintenance. This system, AIRPAV, was implemented, and initial condition surveys were performed in 1992 and 1993. The SAPMP was updated with additional surveys in 1998 and 1999.

In 2004, the FDOT Aviation Office undertook a project to update the pavement management system software utilized for the SAPMP. This project involved a review of the AIRPAV software and other available pavement management system software. As a result of this review, MicroPAVER was selected as the software for the update project. Data from the 1998/1999 condition surveys were converted to the MicroPAVER system, and the inventory of the pavement systems and drawings of the pavements were updated to reflect maintenance, rehabilitation, and construction activities since 1998/1999. The pavements were inspected between 2006 and 2008, and an updated M&R program was developed based on the new condition of the airfield pavements. As part of the update, procedures for the inspection and collection of pavement data were developed, and a website (www.floridaairportpavement.com) was created for the input of data under secure procedures.

Currently, airports using the AIP Grant Program are required by the Federal Aviation Administration (FAA) to develop a pavement maintenance program (FAA/AC 150/5380-6B “Guidelines and Procedures for Maintenance of Airport Pavements”) using trained personnel to perform a detailed inspection of airfield pavements. The inspections are required to be performed at least once a year or every 3 years if pavement inspection is characterized in the form of a Pavement Condition Index (PCI) survey (such as ASTM D 5340 “Standard Test Method for Airport Pavement Condition Index Surveys”, (2004 edition)). The 2004 edition was utilized in lieu of the 2010 edition to maintain database integrity and benefit of pavement performance curves from the previous inspections.

In 2010, the FDOT Aviation Office selected a team consisting of the Consultant and their Subconsultants to provide services in support of FDOT in the continuing evaluation and updating of the existing SAPMP to be completed over fiscal years 2011 and 2012.

1.3 Organization

1.3.1 Aviation Office Program Manager Role

The Aviation Office Airport Engineering Manager serves as the Aviation Office Program Manager (AO-PM) monitoring the work of the Consultant. The AO-PM has review and approval authority for each program task and also manages the day-to-day details of the SAPMP and the updates.

1.3.2 Consultant Role

The Consultant (Kimley-Horn and Associates, Inc.) and their Subconsultants (AMEC Environment & Infrastructure, Inc. Engineering and Consulting and All About Pavements, Inc.) provide technical and administrative assistance to the AO-PM during the execution of this program, which involves the continuing evaluation of airport pavements and updating of the SAPMP based upon procedures outlined in FAA Advisory Circular 150/5380-6B “Guidelines and Procedures for Maintenance of Airport Pavements” and ASTM D 5340 “Standard Test Method for Airport Pavement Condition Index Surveys” (2004).

1.3.3 Airport Role

The airports are the ultimate client for each of the field inspections and reports. Individual airports will be provided final deliverables prepared by the Consultant that have been reviewed and approved by the AO-PM. The airport should provide a current Airport Layout Plan (ALP) to the Consultant and, if they participated in the previous SAPMP update, indicate any construction activity that has been performed since the previous inspections.

1.4 Pavement Types and Pavement Management

1.4.1 Pavement basics

A pavement is a prepared surface designed to provide a continuous smooth ride at a certain speed and to support an estimated amount of traffic for a certain number of years. Pavements are constructed of a combination of subgrade soils, subbases, bases and surfacing. There are mainly two types of pavements;

- Flexible pavement, composed of an asphalt concrete (AC) surface, and
- Rigid pavement composed of a Portland Cement Concrete (PCC) surface.

Both pavement types use a combination of layered materials and thicknesses in order to support the traffic loads and protect the underlying natural subgrade soil. Flexible pavements (AC) dissipate the load from layer to layer until the load magnitude is small enough to be supported by the subgrade soil. In rigid pavements (PCC), the Portland Cement Concrete supports most of the load, and the base or subbase layer is mainly constructed to provide a smooth and continuous platform for the construction of the concrete surface.

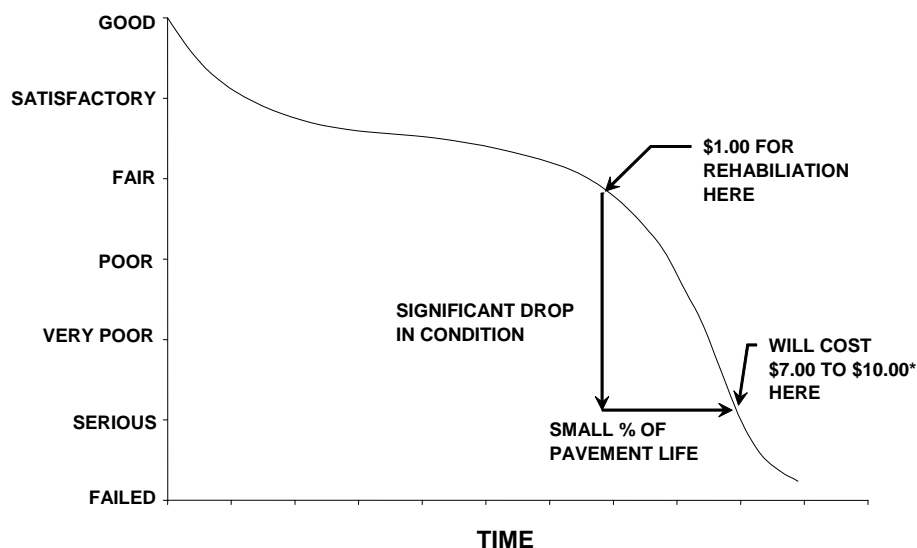
A small percentage of the airport pavements in Florida are composed of asphalt concrete surface over Portland Cement Concrete (APC). This pavement type is known as “composite” pavement.

Due to the different nature of the pavement types and their materials, flexible and rigid pavements have different distresses and failure mechanisms. Understanding the mechanics and failure modes of both pavement types will assist engineers in making adequate and long lasting repairs or rehabilitation to the pavement structures.

1.4.2 Pavement Management System Concept

The SAPMP utilized a Pavement Management System (PMS) to develop the M&R recommendations discussed in this report. A PMS is a tool to assist engineers, planners and managing agencies in making decisions when planning pavement M&R. The management of pavements involves scheduling pavement maintenance and rehabilitation before pavements deteriorate to a condition where reconstruction (the most expensive alternative) is the only solution. Figure 1-1 below, taken from FAA/AC 5380-7A “Airport Pavement Management Program”, illustrates how a pavement generally deteriorates and the relative cost of rehabilitation at various times throughout its life. Note that during the first 75 percent of a pavement’s life, it performs relatively well. After that, however, it begins to deteriorate rapidly. The number of years a pavement stays in “good” condition depends on how well it is maintained. As the illustration demonstrates, the cost of maintaining the pavement above a critical condition before rapid deterioration occurs is much less compared to maintaining pavements after substantial deterioration has occurred.

Figure 1-1: Pavement Life Cycle



Source: FAA/AC 150/5380-7A "Airport Pavement Management Program"
 *Modified to reflect current construction costs.

Pavements deteriorate at an accelerated rate with increasing traffic and limited M&R resources. Planned maintenance and rehabilitation, essentially preventing pavements from reaching deteriorated conditions, helps managers/owners/agencies maximize the use of their budgets and prolong the life of the pavements. A PMS provides a tool to schedule and plan maintenance and rehabilitation based on engineering information and existing and predicted conditions of pavements.

There are several components or elements that are essential to a PMS. The first steps in the implementation of a PMS are to know and clearly identify what needs to be managed, the limits of the managing agency's responsibilities and the condition of the existing pavements. Once the cause and the extent of pavement problems are known, the appropriate maintenance and/or rehabilitation can be planned. By using local unit costs and expected yearly budgets, a multi-year M&R plan can be determined.

1.4.3 Pavement Inspection Methodology for the SAPMP

Pavement condition assessment is one of the primary decision variables in any airport PMS. Pavement condition assessments generally include visual surveys in accordance with ASTM D 5340, "Standard Test Method for Airport Pavement Condition Index Surveys" and structural evaluation. Pavement condition surveys assess the functional condition of the pavement surface. Typically, most problems within a pavement structure will eventually reflect to the pavement surface. The structural condition and relative support of the pavement layers can be assessed utilizing non-destructive deflection testing (NDT) as well as other in-depth engineering evaluation or sampling and testing methods.

For the Statewide Aviation Pavement Management Program update, only visual surveys were performed. Further structural and geotechnical testing should be conducted to determine the appropriate rehabilitation methods during the design process.

In preparation of the PCI surveys, the airfield pavements are divided into sample units as established in FAA AC 150/5380-6B and ASTM D 5340. Further discussion of how the airport pavements are divided and subdivided into units by construction and use can be found in Section 2 “Network Definition and Pavement Inventory” of this report.

Sample unit sizes are approximately 5000 ± 2000 square feet for AC-surfaced pavements and 20 ± 8 slabs for PCC-surfaced pavements. Prior to conducting the field inspections, the sampling plan was developed based on previous sampling and modified based on the available knowledge of Branches, Sections, use patterns, construction types and history. The sampling rate used for the FDOT Statewide Airfield Pavement Management Program is provided in Table 1-1 below.

Table 1-1: Sampling Rate for FDOT Condition Surveys

AC Pavements			PCC Pavements		
N	n		N	n	
	Runway	Others		Runway	Others
1-4	1	1	1-3	1	1
5-10	2	1	4-6	2	1
11-15	3	2	7-10	3	2
16-30	5	3	11-15	4	2
31-40	7	4	16-20	5	3
41-50	8	5	21-30	7	3
≥51	20% but ≤20	10% but ≤10	31-40	8	4
			41-50	10	5
			≥51	20% but ≤20	10% but ≤10

Where N = total number of sample units in Section
 n = number of sample units to inspect

The sample units to inspect are determined by a systematic random sampling technique. This means that the locations are determined such that they are distributed evenly throughout the Section. In the case when nonrepresentative distresses are observed in the field, additional sample units were added.

The distress quantities and severity levels from the sample units are used to compute the PCI value for each Section. PCI values range from 0 to 100. As Figure 1-2 below indicates, MicroPAVER provides a rating scale that relates PCI to pavement condition. A PCI between 0 and 10 is considered ‘Failed’ pavement, and a PCI between 86 and 100 is considered ‘Good’ pavement, with five other conditions for PCI values between 11 and 85.

Figure 1-2: PCI Rating Scale

	PCI	Condition Rating
	86 – 100	Good
	71 – 85	Satisfactory
	56 – 70	Fair
	41 – 55	Poor
	26 – 40	Very Poor
	11 – 25	Serious
	0 – 10	Failed

1.5 Definitions

Aviation Office - The Aviation Office is charged with responsibility for promoting the safe development of aviation to serve the people of the State of Florida. The Aviation Office Program Manager (AO-PM) has review and approval authority for each program task of the SAPMP.

Base Course - Base Course is a layer of manufactured material, usually crushed rock (aggregate) or stabilized material (asphalt or concrete or Florida Limerock), immediately beneath the surface course of a pavement, which provides support to the surface course.

Branch - A Branch designates pavements that have common usage and functionality, such as an entire runway, taxiway, or apron.

Branch ID - A short form identification for the pavement Branch. In this report, Branch includes the common designation for the item e.g. RW 18-36.

Category - The Category classifies the airport according to the type and volume of aircraft traffic, as follows:

- GA – for general aviation or community airports;
- RL – for regional relievers or small hubs;
- PR – for primary (certified under Part 139 requirements).

Critical PCI - The PCI value considered to be the threshold for M&R decisions. PCI above the Critical generate economical activities expected to preserve and prolong acceptable condition. M&R for PCI values less than Critical make sense only for reasons of safety or to maintain a pavement in operable condition. A pavement section is expected to deteriorate very quickly once it reaches the Critical PCI and the unit cost of repair increases significantly.

Distress Type - A distress type is a defined visible defect in pavement evidenced by cracking, vertical displacement or deterioration of material. In PCI technology, 16 distinct distress types for asphalt surfaced and 15 for Portland Cement Concrete surfaced pavements have been described and rated according to the impact their presence has on pavement condition.

Florida DOT (FDOT) - Florida Department of Transportation was represented in this project by the Office of Aviation.

Global M&R - Global M&R is defined as activities applied to entire pavement Sections with the primary objective of slowing the rate of deterioration. These activities are primary for asphalt surfaced pavements, e.g. surface treatments.

Localized M&R (Maintenance and Repair) - Localized M&R is a temporizing activity performed on existing pavement to extend its serviceability and/or to improve rideability. Localized M&R can be applied either as a safety (stop-gap) measure or preventive measure. Common localized maintenance methods include crack sealing, joint sealing, and patching.

Major M&R (e.g. Rehabilitation) - Activities performed over the entire area of a pavement Section that are intended to restore and/or maintain serviceability. This includes asphalt overlays, milling and replacing asphalt pavement, reconstruction with asphalt, reconstruction with Portland Cement Concrete (PCC) pavements, and PCC overlays.

MicroPAVER - A commercially available software subsidized by FAA and agencies in the US Department of Defense developed to support engineered management of pavement assets using a condition based approach. This software has the functionality such that, if properly implemented, maintained, and operated, it meets the pavement management program requirements described by the FAA in Advisory Circular 150/5380-7A.

Minimum Condition Level - A threshold PCI value established by FDOT to represent the targeted minimum pavement condition that is desirable in the Florida Airport System. These values were established with consideration of pavement function and airport type. For instance, runways have higher minimum condition levels than aprons, and Primary airports have higher minimum condition levels than General Aviation airports.

Network Definition - A Network Definition is a Computer-Aided Drafting & Design (CADD) drawing which shows the airport pavement outline with Branch and Section boundaries. This drawing also includes the PCI sample units and is used to identify those sample units to be surveyed, i.e. the sampling plan. The Network Definition for the airport is in Appendix A along with a table of inventory data.

Pavement Condition Index (PCI) - The Pavement Condition Index is a number which represents the condition of a pavement segment at a specific point in time. It is based on visual identification and measurement of specific distress types commonly found in pavement which has been in service for a period of time. The definitions and procedures for determining the PCI are found in ASTM D 5340, published by ASTM International.

Pavement Evaluation - A systematic approach undertaken by trained and experienced personnel intended for determination of the condition, serviceability, and best corrective action for pavement. Techniques to standardize pavement evaluation include the Pavement Condition Index procedures.

Pavement Management System (PMS) - A Pavement Management System is a broad function that uses pavement evaluation and pavement performance trends as a basis for planning, programming, financing, and maintaining a pavement system.

Pavement Surface Type - The surface of pavement is identified as one of four types:

- AC – for asphalt surface pavements;
- PCC – for Portland Cement Concrete pavements;
- AAC – for asphalt surface pavements that have had an asphalt overlay at some point in their construction history;
- APC – for composite pavements, which consist of asphalt over Portland Cement Concrete pavement.
- PAC – for composite pavements, which consist of Portland Cement Concrete over asphalt pavement.

Rank - Pavement rank in MicroPAVER determines the priority to be assigned to a pavement Section when developing an M&R plan. Pavement Sections are ranked as follows according to their use:

- P – for Primary pavements, such as primary runways, primary taxiways, and primary aprons;
- S – or Secondary pavements, such as secondary runways, secondary taxiways, and secondary aprons;
- T – for Tertiary pavements such as “T” hangars and slightly used aprons.

Reconstruction - Reconstruction includes removal of existing pavement, preparation of subgrade, and construction of new pavement with new or recycled materials. Reconstruction is indicated when distress types evident at the surface indicate failure in the pavement structure or subgrade of a type, and to an extent, not correctable by less extensive construction.

Rehabilitation - Rehabilitation represents construction using existing pavement for a foundation. Rehabilitation most commonly consists of an overlay of existing pavement with a new asphalt or concrete surface. Recently, technology has expanded the options to include recycling of existing pavement and incorporating engineering fabrics or thin layers of elasticized materials to retard reflection of distress types through the new surface.

Sample Unit - Uniformly sized portions of a Section as defined in ASTM D 5340. Sample units are a means to reduce the total amount of pavement actually surveyed using statistics to select and survey enough area to provide a representative measure of Section PCI. Sample Unit sizes are $5,000 \pm 2,000$ square feet for AC-surfaced pavements and 20 ± 8 slabs for PCC-surfaced pavements.

Section - Sections subdivide Branches into portions of similar pavement. Sections are prescribed by pavement structure, age, condition, and use. Sections are identified on the airport Network Definition. They are the smallest unit used for determining M&R requirements based on condition.

Section ID - A short form identification for the pavement Section that maintains the original AirPAV identification where 100 series through 3000 series Sections are taxiways, 4000 and 5000 series Sections are aprons (the 5000 series represent run-up aprons and turnarounds), and 6000 series Sections are runways.

Statewide Airfield Pavement Management Program (SAPMP) – The Statewide Airfield Pavement Management Program is a program implemented in 1992 by the Florida Department of Transportation to plan, schedule, and design the maintenance and rehabilitation activities

necessary for the airfield pavement on Florida's public airports to allow the airports to operate efficiently, economically, and without excessive down time.

System Inventory - A System Inventory is a Computer-Aided Drafting & Design (CADD) drawing which shows the airport pavement outline and identifies airfield construction activities since the last inspection. The System Inventory for the airport is included in Appendix A.

Use - In MicroPAVER, Use is the term for the function of the pavement area. This is either Runway, Taxiway, or Apron for purposes of the FDOT Statewide Aviation Pavement Management System.

2. NETWORK DEFINITION AND PAVEMENT INVENTORY

Williston Municipal Airport (X60) is located approximately 2 miles southwest of Williston city, Florida and serves as a basic utility airport for Levy County. The airport facility includes two intersecting runways: Runway 5-23 with a length of 6,668 ft. and a width of 100 ft. and Runway 14-32 with a length of 4,399 and a width of 100 ft. Runway 5-23 has a concrete surface with 92% of the surface constructed of thin whitetopping approximately 5 feet by 5 feet slab ranging from 4 to 5 inches in thickness while Runway 14-32 has an asphalt surface. The whitetopping construction was completed in 2006. This airport is designated as a General Aviation airport and is located in District 2 of the Florida Department of Transportation.

It is important to note that the aforementioned runway data in addition to the remaining airfield pavement facilities geometric dimensions may vary slightly from the geometry used in the condition and M & R analysis based on field measurements.

The airfield was opened on January 1, 1942 as an US Army Air Forces installation and was part of the Army Air Force School of Applied Tactics. It was put on standby status and placed under the control of the 4318th Army Air Force Base Unit on May of 1944. In 1974 the airport was acquired by the City of Williston and opened for public use.

2.1 Network Definition

The pavements within the network are defined in MicroPAVER in terms of manageable units that help to organize the data into similar groups. An organizational hierarchy is used to establish these units.

2.1.1 Branch Section Identification

The airport pavement network is subdivided into separate Branches (runways, taxiways, or aprons) that have distinctly different uses. Branches are then further divided into Sections with similar pavement construction and performance that may share other common attributes.

Sections are manageable units used to organize the data collection and are treated individually during the rehabilitation planning stage. A pavement rank, consisting of primary, secondary, and tertiary levels, is assigned to each Section based on their level and type of use. The pavement rankings that were designated for each Section in the previous SAPMP update were again used for this update.

As discussed in Section 1.4.3 “Pavement Inspection Methodology for the SAPMP”, the sections are sub-divided into sample units, which are the smallest subdivision in a pavement network, only for the purpose of conducting the pavement condition survey.

2.1.2 System Inventory and Network Definition Update

The System Inventory and Network Definition drawings are used to identify changes in the network since the most recent update from the 2006/2008 inspections and also to plan the field inspection activities for the 2012 survey. Prior to the field inspection process, the System Inventory drawing was updated from the previous inspection with notes indicating recent construction projects on the various Sections of pavement throughout the airfield. This System Inventory drawing is used to update the Network Definition drawing.

The Network Definition drawing shows the airport pavement outline with Branch and Section boundaries. This drawing also includes the PCI sample units and is used to identify those sample units to be surveyed, i.e. the sampling plan. The previous airport configuration and history was compared with the current airport configuration, and the existing network branch, section and sample unit designations were revised to match the current configuration. This drawing serves not only as a primary guide for the airfield inspectors but also as an important historical record.

Due to recent and anticipate construction history; pavement area sections may have been consolidated or created which will affect the total number of sample units to be inspected based on the ASTM 5340 criteria.

The updated System Inventory and Network Definition drawings for Williston Municipal Airport are provided in Appendix A. Table 2-1 below lists the recent construction projects at the airport.

Table 2-1: Construction Since Last Inspection & Anticipated Construction Activity

Construction Year	Location	Work Type / Pavement Section
2005	Runway 5-23	New 100' wide grooved concrete runway
2009	Taxiways Charlie and Delta	Milled and asphalt overlaid
2009	Taxiways Bravo, D-1, and Echo	Milled and new asphalt laid
2009	Parking Ramp	Milled and asphalt overlaid
2012	Taxiway Alpha Extension	New asphalt construction
2014	Taxiway Foxtrot	Asphalt

2.2 Pavement Inventory

The detailed pavement inventory was updated to reflect the network definition update and field inspection results. The total number of sample units designated to be inspected at the airport is 97 sample units.

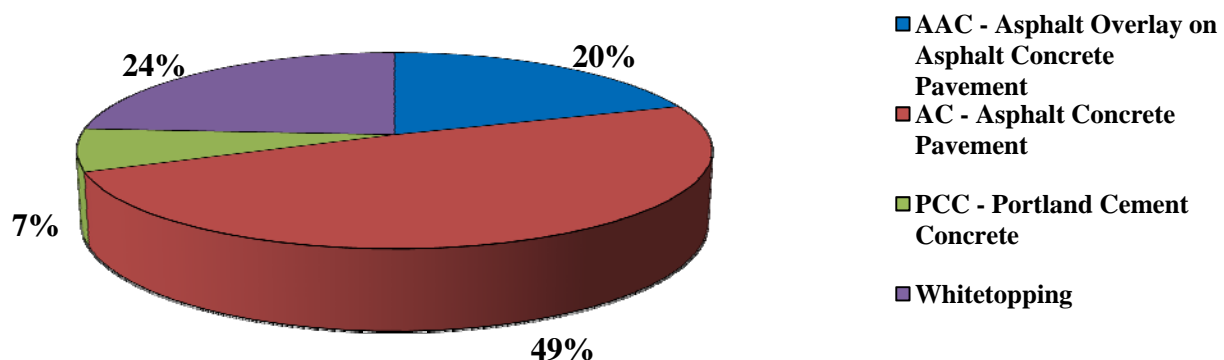
The total airfield pavement area in 2012 at Williston Municipal Airport is 2,184,209 square feet. The breakdown of pavement area for each pavement use is provided in Table 2-2.

Table 2-2: Pavement Area by Pavement Use

Use	Area (ft²)	% of Total Area
Runway	1,298,050	59%
Taxiway	720,546	33%
Apron	165,613	8%
All (Weighted)	2,184,209	100%

Figure 2-1 presents the breakdown of the pavement area at Williston Municipal Airport by surface type.

Figure 2-1: Pavement Area by Surface Type



Details of pavement Branch and Section information including Branch name (which indicates pavement use), Branch ID, Section ID, section area, rank, surface type, last construction date, number of samples inspected, and number of samples in each Section are given in Table 2-3 below. A more detailed Pavement Inventory Table may be found in Appendix A of this report.

Table 2-3: Branch and Section Inventory

Branch Name	Branch ID	Section ID	True Area (ft ²)	Section Rank	Surface Type	Last Const. Date	Total Samples Inspected	Total Samples
Apron	AP	4105	106,379	P	AAC	1/1/2009	3	24
Hangar Apron	AP HANG	4205	4,300	P	AAC	1/1/2009	1	1
Hangar Apron	AP HANG	4210	6,400	P	AAC	1/1/2009	1	1
Apron at T-Hangars	AP T-HANG	4315	4,001	P	AC	1/1/1986	1	2
Apron at T-Hangars	AP T-HANG	4316	3,259	P	AC	1/1/1986	1	2
Apron at T-Hangars	AP T-HANG	4320	18,160	P	AC	1/1/2005	1	6
Apron at T-Hangars	AP T-HANG	4325	23,114	P	AC	1/1/2003	1	9
Runway 14-32	RW 14-32	6205	30,300	P	PCC	1/1/1942	2	6
Runway 14-32	RW 14-32	6210	15,000	P	PCC	1/1/1942	1	4
Runway 14-32	RW 14-32	6215	430,000	P	AC	1/1/1942	17	85
Runway 14-32	RW 14-32	6220	215,000	P	AC	1/1/1942	8	46
Runway 14-32	RW 14-32	6235	30,000	P	PCC	1/1/1942	2	6
Runway 14-32	RW 14-32	6240	15,000	P	PCC	1/1/1942	2	4
Runway 5-23	RW 5-23	6105	30,000	T	PCC	1/1/1942	1	6
Runway 5-23	RW 5-23	6110	15,000	P	PCC	1/1/1942	1	4
Runway 5-23	RW 5-23	6115	375,750	P	Whitetopping	1/1/2005	20	100
Runway 5-23	RW 5-23	6125	127,000	P	Whitetopping	1/1/2005	5	26
Runway 5-23	RW 5-23	6127	15,000	P	Whitetopping	1/1/2005	2	12
Taxiway Alpha	TW A	205	72,808	P	AC	1/1/1986	3	21
Taxiway Alpha	TW A	210	10,709	P	AAC	1/1/2009	1	3
Taxiway Alpha	TW A	215	6,477	P	AAC	1/1/2009	1	2
Taxiway Bravo	TW B	305	101,472	P	AAC	1/1/2009	4	29
Taxiway Charlie	TW C	105	62,453	P	AAC	1/1/2009	2	11
Taxiway Charlie	TW C	115	36,492	P	AAC	1/1/2009	1	10
Taxiway Delta	TW D	505	28,750	P	AC	12/25/1999	2	12
Taxiway D-1	TW D1	405	52,495	P	AAC	1/1/2009	2	14
Taxiway Echo	TW E	705	57,345	P	AAC	1/1/2009	3	16
Taxiway Foxtrot	TW F	550	185,509	P	AC	1/1/1942	5	49
Taxiway Foxtrot	TW F	555	11,250	P	PCC	1/1/1942	1	3
Taxiway Golf	TW G	450	94,786	P	AC	1/1/1942	3	24

Note: If a new construction, then survey date = last construction date and PCI is set to 100 by MicroPAVER.

Sections not surveyed due to reasons such as re-sectioning, no escort, not accessible at the time of survey.

3. PAVEMENT CONDITION

Pavement conditions were inspected in accordance with the methods outlined in FAA AC 150/5380-6B and ASTM D 5340-04 “Standard Practice for Airport Pavement Condition Index Surveys.” These procedures define distress type, severity and quantity for sampling areas within each section to determine the Pavement Condition Index (PCI).

3.1 Inspection Methodology

A PCI survey is performed by measuring the amount and severity of pavement distresses, which are caused by traffic load, climate, and other factors, observed within a sample unit. This data is imported into MicroPAVER, which calculates PCI values for the pavement sections. Tables 3-1 and 3-2 below list the pavement distress types and related causes for asphalt concrete (AC) and Portland Cement Concrete (PCC), respectively.

Table 3-1: Pavement Distresses for Asphalt Concrete Surfaces

Code	Distress	Mechanism
41	Alligator Cracking	Load
42	Bleeding	Construction Quality/ Mix Design
43	Block Cracking	Climate / Age
44	Corrugation	Load / Construction Quality
45	Depression	Subgrade Quality
46	Jet Blast	Aircraft
47	Joint Reflection - Cracking	Climate / Prior Pavement
48	Longitudinal/Transverse Cracking	Climate / Age
49	Oil Spillage	Aircraft / Vehicle
50	Patching	Utility / Pavement Repair
51	Polished Aggregate	Load
52	Weathering/Raveling	Climate / Load
53	Rutting	Load
54	Shoving	Pavement Growth
55	Slippage Cracking	Load / Pavement Bond
56	Swelling	Climate / Subgrade Quality
<i>Source: U.S. Army CERL, FDOT Airfield Inspection Reference Manual</i>		

Table 3-2: Pavement Distresses for Portland Cement Concrete Surfaces

Code	Distress	Mechanism
61	Blow-up	Climate
62	Corner Break	Load
63	Linear Cracking	Load
64	Durability Cracking	Climate
65	Joint Seal Damage	Climate
66	Small Patch	Pavement Repair
67	Large Patch/Utility Cut	Utility / Pavement Repair
68	Popout	Climate
69	Pumping	Load
70	Scaling/Crazing	Construction Quality
71	Faulting	Subgrade Quality
72	Shattered Slab	Load
73	Shrinkage Cracking	Construction Quality / Load
74	Joint Spalling	Load
75	Corner Spalling	Load
<i>Source: U.S. Army CERL, FDOT Airfield Inspection Reference Manual</i>		

Prior to conducting the inspections, Global Positioning System (GPS) coordinates were recorded using CADD at the centroid of each sample unit. The centroid is usually the geometric center of the area, but in cases where sample units are irregular in shape, this is the center of mass. These data are presented in a table on the updated Network Definition Map in Appendix A of this report.

Pavement condition inspections at Williston Municipal Airport were performed in February 2012. Data was recorded in the field in accordance with FAA Advisory Circular 150/5380-6B “Guidelines and Procedures for Maintenance of Airport Pavements” and ASTM D 5340 “Standard Test Method for Airport Pavement Condition Index Surveys” (2004).

After the completion of data collection, the data was imported into MicroPAVER, and PCI values were calculated for the pavement sections.

3.2 Pavement Condition Index Results

According to the 2012 survey, the overall area-weighted PCI at Williston Municipal Airport is 59, representing a Fair overall network condition.

The asphalt concrete of Runway 14-32 exhibited low to high severity weathering and raveling, low to medium severity block cracking, along with low to medium severity longitudinal and transverse cracking. The PCC pavement sections of Runway 14-32 and Runway 5-23 exhibited low to high severity joint seal damage, low to medium severity linear cracking. Runway 14-32 also exhibited low to high severity joint spalling.

The whitetopping sections of Runway 5-23 exhibited low severity joint spalling, corner spalling, and faulting. A total of 27 sample units were evaluated on the runway. The PCI of the

whitetopping ranged from 85 to 93 (Good Condition) based on the age and observed conditions. Due to the age of the whitetopping pavement and PCI ranging between 85 to 93 no immediate Major M&R is recommended for the whitetopping pavement. Preventive maintenance is suggested to correct the minor issues that were observed on the runway. Per the airport the whitetopping pavement typically does not require any maintenance during the first 10 years. Since distresses were observed during the current inspections, it is recommended that the airport continue to monitor the whitetopping pavement. A 10 year condition prediction analysis was not conducted for the whitetopping pavement. For future 10 year pavement condition prediction analysis for whitetopping pavement, FDOT will use the inspection data from the current inspections and from the next inspection cycle to determine the pavement deterioration rate.

Taxiways throughout the airfield exhibited low to high severity longitudinal and transverse cracking, low to high severity block cracking, and low to high severity weathering and raveling.

The asphalt pavement of the aprons exhibited low to medium severity block cracking, low to medium severity weathering and raveling, and low to medium severity patching.

Appendix B contains a table and a Condition Map which depicts the PCI results by Section, and Appendix C contains a table of PCI results by Branch. Appendix I includes detailed distress data generated by MicroPAVER for each inspected sample unit.

Figure 3-1 provides the PCI distribution by rating category for Williston Municipal Airport.

Figure 3-1: Network PCI Distribution by Rating Category

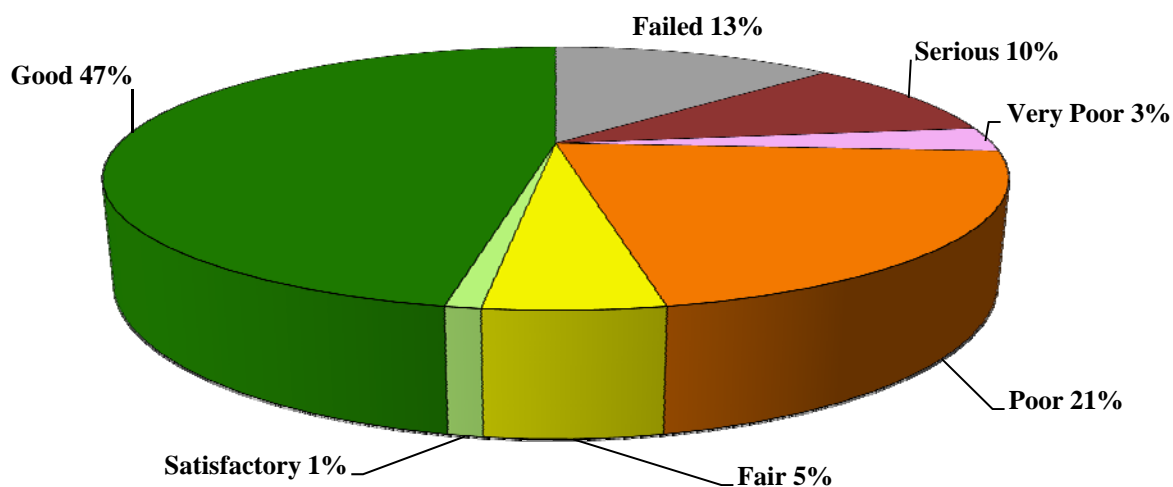


Figure 3-1a: Condition Rating Summary

Condition Rating	Total Area (ft ²)	Percent
Good	1,020,432	47%
Satisfactory	23,114	1%
Fair	121,809	5%
Poor	463,259	21%
Very Poor	60,300	3%
Serious	215,000	10%
Failed	280,295	13%

Approximately 48% of the network is in Good and Satisfactory condition while 5% of the network is in Fair condition, 24% of the network is in Poor and Very Poor condition, and 23% of the network is in Serious and Failed condition. Table 3-3 illustrates the area-weighted PCI computed individually for each pavement use.

Table 3-3: Condition by Pavement Use

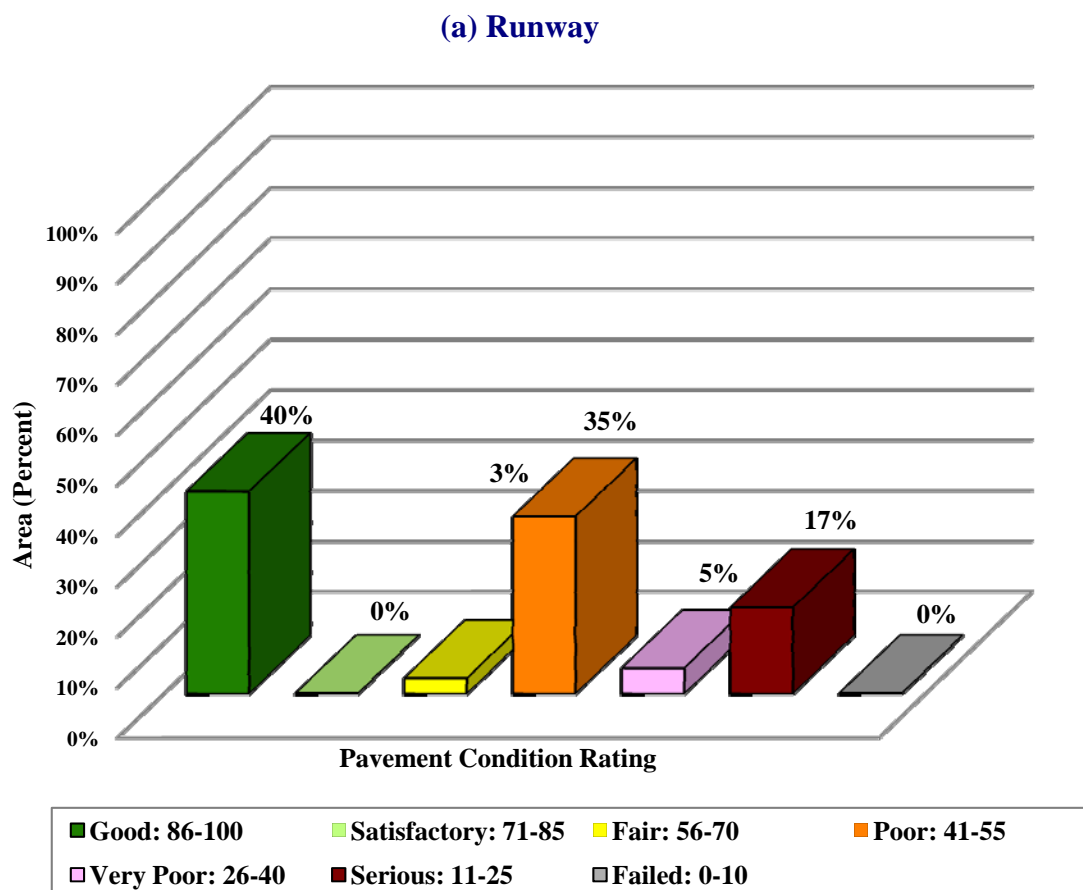
Use	Average Area-Weighted PCI	Condition Rating
Runway	36	Fair
Taxiway	57	Fair
Apron	87	Good
All (Weighted)	59	Fair

Use	Area-Weighted PCI	Condition Rating
Runway 5-23	85 - 93	Good

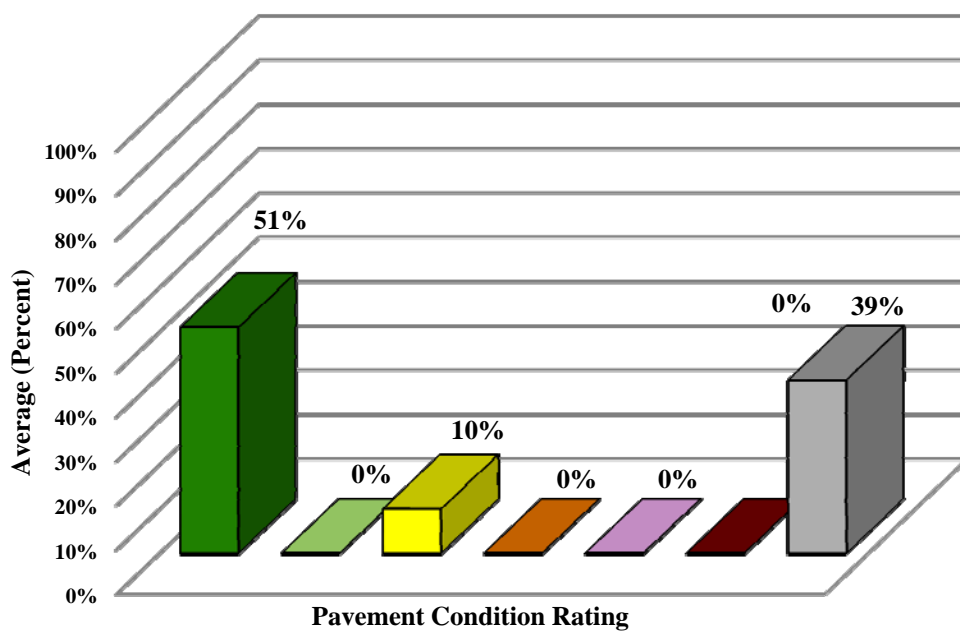
Note: This runway has whitetopping pavement.

Figure 3-2 presents the breakdown of PCI by range for each pavement use.

Figure 3-2: Percentage of Pavement Area within Each PCI Range by Pavement Use

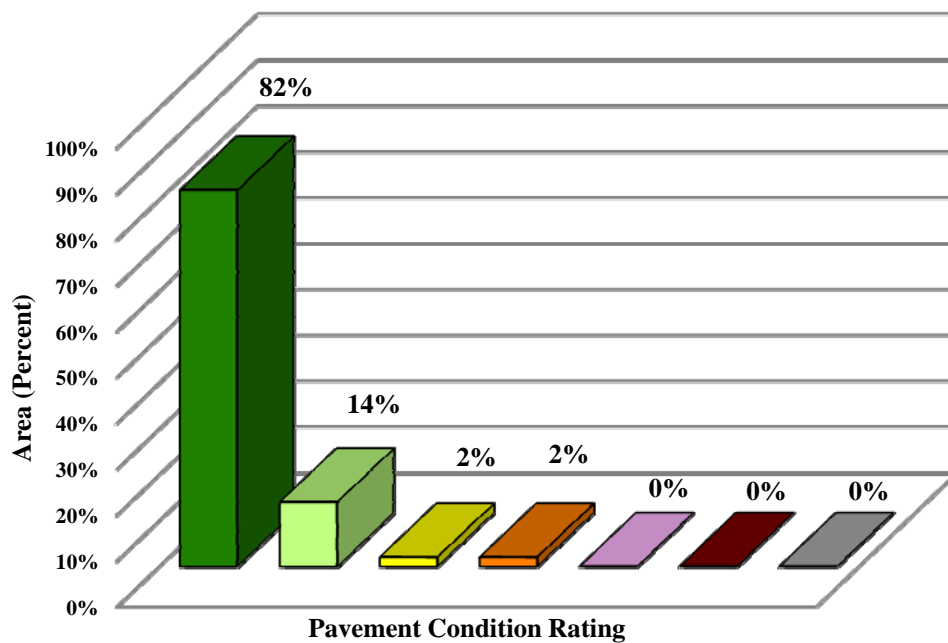


(b) Taxiway



Good: 86-100	Satisfactory: 71-85	Fair: 56-70	Poor: 41-55
Very Poor: 26-40	Serious: 11-25	Failed: 0-10	

(c) Apron

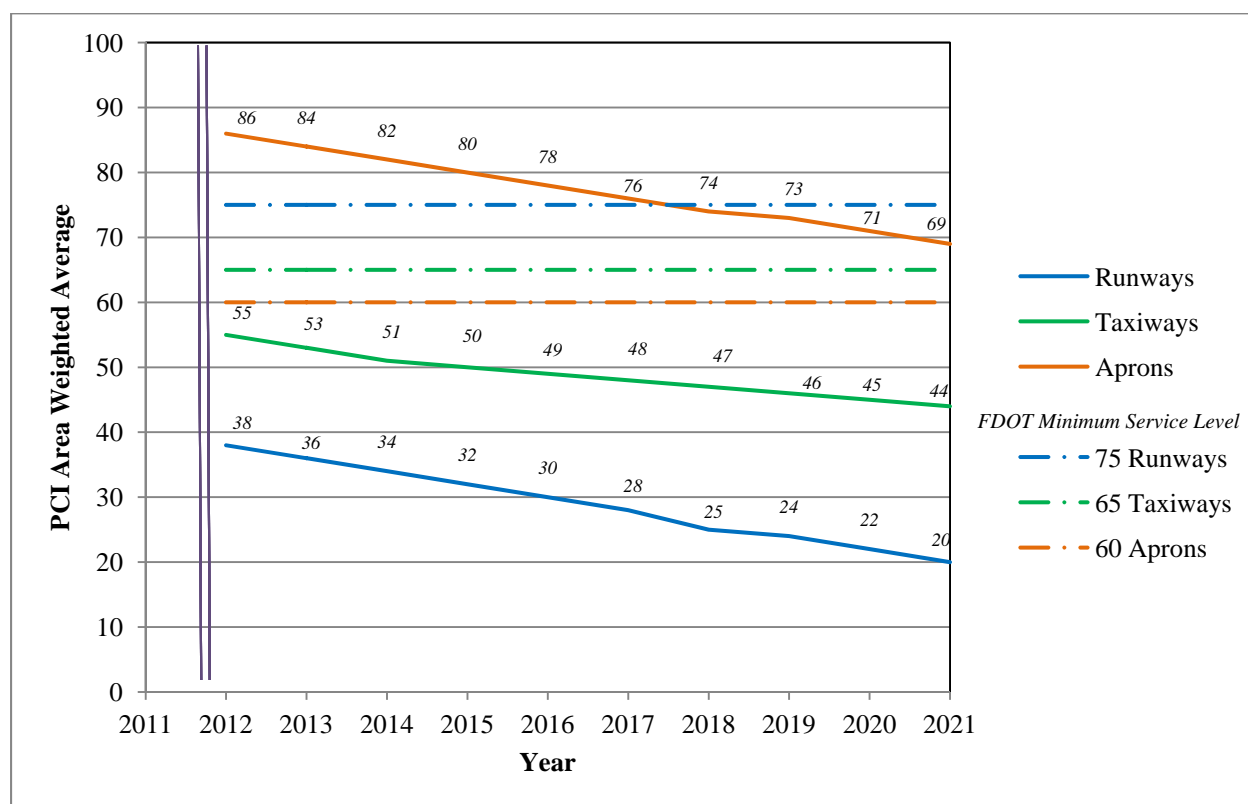


Good: 86-100	Satisfactory: 71-85	Fair: 56-70	Poor: 41-55
Very Poor: 26-40	Serious: 11-25	Failed: 0-10	

4. PAVEMENT CONDITION PREDICTION

Performance prediction models or deterioration curves for PCI were used to develop a condition forecast. The performance models were developed for combinations of variables such as pavement use (runway, taxiway or apron), surface type (AC or PCC) and airport category (GA, RL, or PR). Figure 4-1 illustrates the predicted performance of pavements at Williston Municipal Airport based on current condition, age since last construction and the deterioration model appropriate for the type of pavement. The figure presents the forecast for each pavement use and displays the FDOT minimum service level for General Aviation (GA) airports.

Figure 4-1: Predicted PCI by Pavement Use



Note: The above predicted PCI curve for the runway does not include Runway 5-23 which has whitetopping pavement.

Appendix D presents the tabular summary of the predicted Section PCI for each year from 2012 to 2021.

5. MAINTENANCE POLICIES AND COSTS

5.1 Policies

Maintenance and rehabilitation (M&R) policies are sets of rules used to develop repair recommendations for distresses encountered during the visual inspections.

Maintenance refers to repair-type activities that are applied to specific distress types on the pavement. These activities are preventative and/or corrective in nature and are recommended to help achieve the performance goal.

Table 5-1 provides the list of the maintenance activities used in MicroPAVER to treat specific distress types. MicroPAVER applies repairs to these distresses and adjusts the PCI based on specific rules. These repairs are used only in the first year of an analysis.

Rehabilitation is warranted when the pavement condition decreases below a critical point such that the deterioration is extensive or the rate of deterioration is so great that routine maintenance is no longer cost-efficient. This critical point is called “Critical PCI.” The critical PCI levels for different pavement and branch types established in the previous SAPMP update were used in this update for the development of the M&R plan for the airport. Sections above critical PCI levels receive routine maintenances while pavements predicted to deteriorate below their respective critical PCI level during the analysis period will be identified for Major M&R. Table 5-2 gives the critical PCI levels for General Aviation Airports.

The maintenance rehabilitation policy and activity costs have been updated based on the study of readily available construction cost data at the time of this study. The costs depicted in this report are intended for planning purposes.

Table 5-1: Routine Maintenance Activities for Airfield Pavements

Surface	Distress	Severity*	Work Type	Code	Work Unit
AC	Alligator Crack	M, H	Patching - AC Deep	PA-AD	SqFt
	Bleeding	N/A	No Localized M&R	NONE	N/A
	Block Crack	M, H	Crack Sealing – AC	CS-AC	SqFt
	Corrugation	L, M, H	Patching - AC Deep	PA-AD	SqFt
	Depression	M, H	Patching - AC Deep	PA-AD	SqFt
	Jet Blast	N/A	Patching - AC Deep	PA-AD	SqFt
	Joint Ref. Crack	M, H	Crack Sealing – AC	CS-AC	Ft
	L & T Crack	M, H	Crack Sealing – AC	CS-AC	Ft
	Oil Spillage	N/A	Patching - AC Shallow	PA-AS	SqFt
	Patching	M, H	Patching - AC Deep	PA-AD	SqFt
	Polished Agg.	N/A	No Localized M&R	NONE	N/A
	Raveling / Weathering	L	Surface Sealing - Rejuvenating	SS-RE	SqFt
		M	Surface Seal - Coal Tar	SS-CT	SqFt
		H	Microsurfacing	MI-AC	SqFt
	Rutting	M, H	Patching - AC Deep	PA-AD	SqFt
	Shoving	M, H	Grinding (Localized)	GR-LL	SqFt
	Slippage Crack	N/A	Patching - AC Shallow	PA-AS	SqFt
	Swelling	M, H	Patching - AC Deep	PA-AD	SqFt
PCC	Blow-Up	L, M, H	Patching - PCC Full Depth	PA-PF	SqFt
	Corner Break	M, H	Patching - PCC Full Depth	PA-PF	SqFt
	Linear Crack	M, H	Crack Sealing – PCC	CS-PC	Ft
	Durability Crack	H	Slab Replacement – PCC	SL-PC	SqFt
		M	Patching - PCC Full Depth	PA-PF	SqFt
	Jt. Seal Damage	M, H	Joint Seal (Localized)	JS-LC	Ft
	Small Patch	M, H	Patching - PCC Partial Depth	PA-PP	SqFt
	Large Patch	M, H	Patching - PCC Full Depth	PA-PF	SqFt
	Popouts	N/A	No Localized M&R	NONE	N/A
	Pumping	N/A	No Localized M&R	NONE	N/A
	Scaling	H	Slab Replacement – PCC	SL-PC	SqFt
	Faulting	M, H	Grinding (Localized)	GR-PP	Ft
	Shattered Slab	M, H	Slab Replacement – PCC	SL-PC	SqFt
	Shrinkage Crack	N/A	No Localized M&R	NONE	N/A
	Joint Spall	M, H	Patching - PCC Partial Depth	PA-PP	SqFt
	Corner Spall	M, H	Patching - PCC Partial Depth	PA-PP	SqFt

*L = Low, M = Medium, H = High

Table 5-2: Critical PCI for General Aviation Airports

Use	Critical PCI
Runway	65
Taxiway	65
Apron	65

It should be noted that critical PCI is not the same as Minimum PCI or Minimum Condition. The Minimum PCI is a value set by the user so pavement sections are rehabilitated before they fall below the set minimum. Table 5-3 gives the targeted, or desired, Minimum PCI values for runways, taxiways, and aprons of General Aviation Airports.

Table 5-3: FDOT Minimum Service Level PCI for General Aviation Airports

Minimum PCI		
Runway	Taxiway	Apron
75	65	60

Typical Major M&R activities range from overlays to reconstruction. Based on the critical PCI values in Table 5-2 the PCI trigger range when the likely activity would be a mill and resurface was 40 to 79 and reconstruction at a PCI of 39 or lower. One important concept of pavement management systems is that it is cost effective to maintain pavements that are already in good condition rather than wait for them to get worse and require more expensive rehabilitation.

Crack sealing and full-depth patching are the M&R activities recommended to repair pavements with PCI values between 80 and 90. MicroPAVER considers these as preventative M&R with their primary objective being to slow the rate of pavement deterioration. While the trigger PCI for mill and overlay has been set to 55, MicroPAVER also assigns mill and overlay to sections with a PCI greater than 55 if they exhibit some structural distress. Table 5-4 summarizes the M&R activities for General Aviation Airports based on PCI value.

Table 5-4: M&R Activities for General Aviation Airports

	Activity	PCI Range
Maintenance	Crack Sealing and Full-Depth Patching	80 and 90
Rehabilitation	Mill and Overlay (AC) or Concrete Pavement Restoration (PCC)	40 to 79
	Reconstruction	39 and less

5.2 Unit Costs

FDOT cost databases for airports and highway pavement maintenance and rehabilitation were updated from the previous SAPMP study based on current construction cost trends in order to determine meaningful costs for the program. Table 5-5 presents the unit costs summary.

5.3 M&R Activities

FDOT recognizes that although Mill and Overlay work is recommended for asphalt pavements within a PCI range from 40 to 79, it is conceivable that airports may not have adequate funding to perform this type of rehabilitation. Microsurfacing treatment is a maintenance/rehabilitation measure that can be used in lieu of asphalt pavement mill and overlay; however it should be understood that this measure is intended for short term pavement life extension. While the cost of microsurfacing is significantly lower than that of pavement mill and overlay, it is not intended to be a full rehabilitative measure for long term benefit.

Table 5-5: Maintenance Unit Costs for FDOT

Code	Name	Cost	Unit
GR-LL	Grinding (Localized for AC)	\$2.10	SqFt
PA-AL	Patching – AC Leveling	\$2.30	SqFt
PA-AS	Patching – AC Shallow	\$2.90	SqFt
PA-PF	Patching – PCC Full Depth	\$38.11	SqFt
PA-PP	Patching – PCC Partial Depth	\$19.06	SqFt
SL-PC	Slab Replacement – PCC	\$39.11	SqFt
CS-PC	Crack Sealing – PCC	\$4.24	Ft
UN-PC	Undersealing – PCC	\$3.40	Ft
CS-AC	Crack Sealing – AC	\$2.25	Ft
GR-PP	Grinding (Localized for PCC)	\$22.51	Ft
JS-LC	Joint Seal (Localized)	\$2.00	Ft
SH-LE	Shoulder Leveling	\$2.81	Ft
JS-SI	Joint Seal – Silicon	\$2.81	Ft
PA-AD	Patching – AC Deep	\$4.90	SqFt
OL-AT	Overlay – AC Thin	\$2.80	SqFt
SS-CT	Surface Seal – Coal Tar	\$0.40	SqFt
SS-FS	Surface Seal – Fog Seal	\$0.40	SqFt
SS-RE	Surface Seal – Rejuvenating	\$0.40	SqFt
ST-SB	Surface Treatment – Single Bitum.	\$0.30	SqFt
ST-SS	Surface Treatment – Slurry Seal	\$0.55	SqFt
ST-ST	Surface Treatment – Sand Tar	\$0.28	SqFt
MI-AC	Microsurfacing - AC	\$0.65	SqFt

The improvement in condition due to maintenance actions applied to specific distresses is only performed when an inspection was performed recently and only in the first year of the M&R analysis. In subsequent years, MicroPAVER calculates M&R costs based on expected unit costs for pavements in a range of PCIs. That is, for low PCI, it is expected that the repair would be significant (e.g. reconstruction) and therefore very costly.

Using available unit cost data, the Major M&R Cost by Condition table was set up as shown in Table 5-6. The cost assigned to each range of PCI is based on a Transportation Cost Report provided by Office of Planning Policy of FDOT where the unit costs of reconstruction and resurfacing of airfield pavements were included. These costs were then assigned to the appropriate PCI range to arrive at a cost per square foot necessary to restore pavements at that PCI level to new condition, i.e. a PCI of 100.

**Table 5-6: M&R Activities and Unit Costs by Condition for
General Aviation Airports**

	Activity	PCI Trigger	Cost/SqFt
Maintenance	Crack Sealing and Full-Depth Patching	90	\$0.06
		80	\$0.24
Rehabilitation	Mill and Overlay (AC) or Concrete Pavement Restoration (PCC)	70	\$3.00
		60	\$3.42
		50	\$6.29
		40	\$6.29
	Reconstruction	30	\$13.62
		20	\$13.62

A 3% inflation rate per year was applied to the unit costs during the M&R analysis.

6. PAVEMENT REHABILITATION NEEDS ANALYSIS

Maintenance and Rehabilitation (M&R) analyses were performed after the condition data were calculated and MicroPAVER was customized with the maintenance policies and cost settings described in the previous section.

The objective of the M&R analysis is to observe the effect of different fiscal scenarios on the network condition, over a period of ten years, starting from 2012. The analysis was conducted using an unlimited budget. An unlimited budget allows all M&R needs to be identified along with the associated cost regardless of priority.

Table 6-1 presents the M&R list of immediate needs for Major M&R, i.e. Year 1 of the forecast. The importance of this listing is that it points out the major activities triggered by the current condition of the pavements.

Table 6-1: Summary of Immediate Major M&R Needs Option No. 1

Branch Name	Section ID	Surface Type	Section Area (ft²)	Major M&R Costs*	PCI Before M&R	M&R Activity	PCI After M&R
Apron at T-Hangars	4315	AC	4,001	\$10,406.61	63	Mill and Overlay	100
Apron at T-Hangars	4316	AC	3,259	\$20,499.11	43	Mill and Overlay	100
Runway 14-32	6205	PCC	30,300	\$279,426.68	36	Reconstruction	100
Runway 14-32	6210	PCC	15,000	\$138,330.04	36	Reconstruction	100
Runway 14-32	6215	AC	430,000	\$2,704,700.21	43	Mill and Overlay	100
Runway 14-32	6220	AC	215,000	\$2,928,300.95	13	Reconstruction	100
Runway 14-32	6235	PCC	30,000	\$111,210.07	59	PCC Restoration	100
Runway 5-23	6105	PCC	30,000	\$188,700.01	46	PCC Restoration	100
Runway 5-23	6110	PCC	15,000	\$116,340.03	38	Reconstruction	100
Taxiway Foxtrot	550	AC	185,509	\$2,526,633.40	6	Reconstruction	100
Taxiway Foxtrot	555	PCC	11,250	\$153,225.05	0	Reconstruction	100
Taxiway Golf	450	AC	94,786	\$1,290,985.74	1	Reconstruction	100
Total				\$10,468,757.90	26		100

* Costs are adjusted for inflation.

FDOT recognizes that the costs attributed to the aforementioned ‘Major Activity’ of performing a pavement ‘Mill and Overlay’ may conflict with budgetary constraints. Table 6-2 presents an alternative minor rehabilitative activity to the mid-range performing pavements. The alternative activity is performing a ‘Microsurfacing/Slurry Seal’ to the pavement to retard the degradation of the facility until funding is available for a ‘Mill and Overlay’ activity.

Table 6-2: Summary of Immediate Major M&R Needs Option No. 2

Branch Name	Section ID	Surface Type	Section Area (ft²)	Major M&R Costs*	PCI Before M&R	M&R Activity	PCI After M&R
Apron at T-Hangars	4315	AC	4,001	\$2,600.65	63	Microsurfacing	100
Apron at T-Hangars	4316	AC	3,259	\$2,118.35	43	Microsurfacing	100
Runway 14-32	6205	PCC	30,300	\$279,426.68	36	Reconstruction	100
Runway 14-32	6210	PCC	15,000	\$138,330.04	36	Reconstruction	100
Runway 14-32	6215	AC	430,000	\$279,500.00	43	Microsurfacing	100
Runway 14-32	6220	AC	215,000	\$2,928,300.95	13	Reconstruction	100
Runway 14-32	6235	PCC	30,000	\$111,210.07	59	PCC Restoration	100
Runway 5-23	6105	PCC	30,000	\$188,700.01	46	PCC Restoration	100
Runway 5-23	6110	PCC	15,000	\$116,340.03	38	Reconstruction	100
Taxiway Foxtrot	550	AC	185,509	\$2,526,633.40	6	Reconstruction	100
Taxiway Foxtrot	555	PCC	11,250	\$153,225.05	0	Reconstruction	100
Taxiway Golf	450	AC	94,786	\$1,290,985.74	1	Reconstruction	100
Total				\$8,017,370.97	26		100

* Costs are adjusted for inflation.

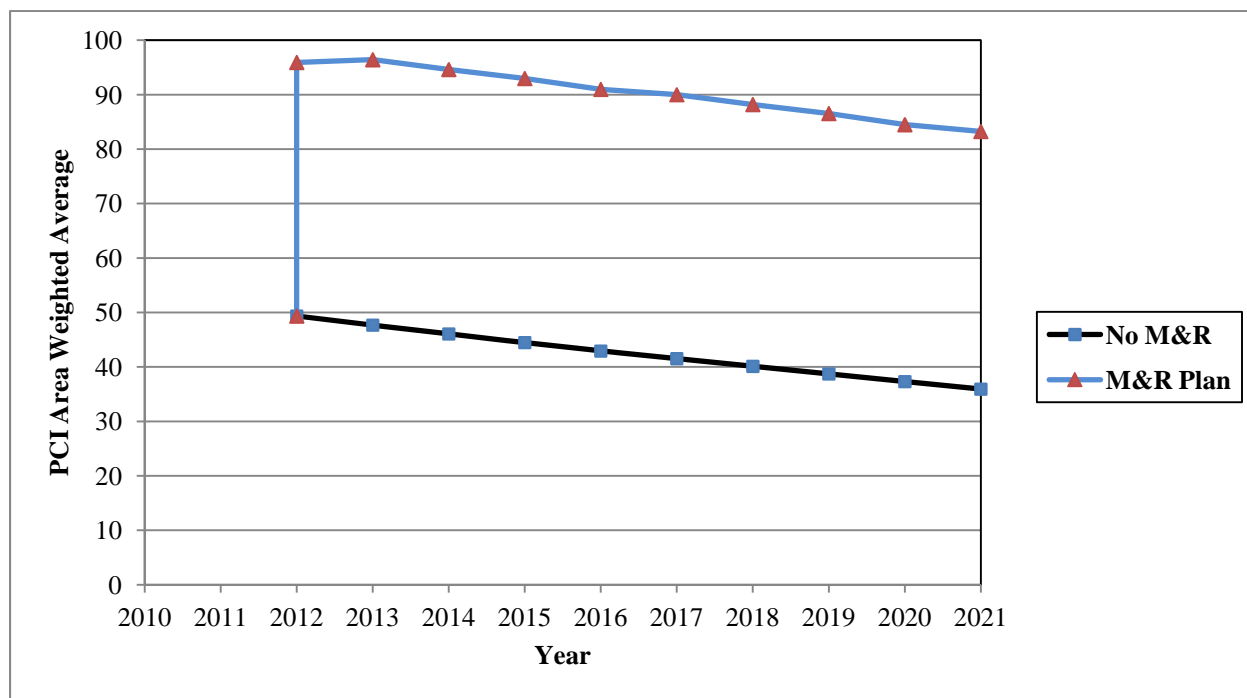
In addition to the immediate Major M&R needs, maintenance activities for pavement areas above critical PCI have been recommended by MicroPAVER for Year 1 and are shown in Table 6-3 below. The costs provided in Table 5-5 were used to calculate the costs associated with this work, which is intended to treat specific distress types. A more detailed table is provided in Appendix E.

Table 6-3: Summary of Year 1 Maintenance Activities

Branch Name	Branch ID	Section ID	Distress Description	Distress Severity	Work Description	Work Quantity	Work Unit	Unit Cost	Work Cost
Apron	AP	4105	PATCHING	M	Patching - AC Deep	19.50	SqFt	\$4.90	\$95.71
Apron	AP	4105	WEATH/RAVEL	L	Surface Seal - Rejuvenating	582.10	SqFt	\$0.40	\$232.84
Hangar Apron	AP HANG	4205	WEATH/RAVEL	L	Surface Seal - Rejuvenating	28.60	SqFt	\$0.40	\$11.46
Hangar Apron	AP HANG	4210	WEATH/RAVEL	L	Surface Seal - Rejuvenating	191.90	SqFt	\$0.40	\$76.77
Apron at T-Hangars	AP T-HANG	4320	WEATH/RAVEL	L	Surface Seal - Rejuvenating	1,089.60	SqFt	\$0.40	\$435.84
Apron at T-Hangars	AP T-HANG	4325	OIL SPILLAGE	N	Patching - AC Shallow	31.80	SqFt	\$2.90	\$92.32
Apron at T-Hangars	AP T-HANG	4325	WEATH/RAVEL	L	Surface Seal - Rejuvenating	23,113.80	SqFt	\$0.40	\$9,245.60
Runway 14-32	RW 14-32	6240	LINEAR CR	M	Crack Sealing - PCC	62.00	Ft	\$4.24	\$262.88
Taxiway Alpha	TW A	205	WEATH/RAVEL	L	Surface Seal - Rejuvenating	62,935.30	SqFt	\$0.40	\$25,174.32
Taxiway Alpha	TW A	205	WEATH/RAVEL	M	Surface Seal - Coat Tar	746.10	SqFt	\$0.40	\$298.46
Taxiway Alpha	TW A	205	L & T CR	M	Crack Sealing - AC	259.50	Ft	\$2.25	\$583.94
Taxiway Alpha	TW A	215	WEATH/RAVEL	L	Surface Seal - Rejuvenating	58.90	SqFt	\$0.40	\$23.55
Taxiway Bravo	TW B	305	WEATH/RAVEL	L	Surface Seal - Rejuvenating	533.00	SqFt	\$0.40	\$213.21
Taxiway Charlie	TW C	115	WEATH/RAVEL	L	Surface Seal - Rejuvenating	6,204.80	SqFt	\$0.40	\$2,481.92
Taxiway Delta	TW D	505	WEATH/RAVEL	L	Surface Seal - Rejuvenating	45.20	SqFt	\$0.40	\$18.08
Total =									\$39,246.90

The 10 year forecast results are shown in Figure 6-1, illustrating the effect on pavement condition (PCI) of doing no maintenance versus having unlimited funds and performing all M&R actions based on the policies.

Figure 6-1: Budget Scenario Analysis



The following network level observations can be made from the figure above:

- The PCI will deteriorate from an average of 59 in 2012 to an average of 44 in ten years if no M&R activities are performed. Specific pavement sections may be closer to critical condition as identified by the immediate needs in Table IV. Estimated PCI ratings are presented in Appendix D.
- The PCI will remain at or above an average of 80 through the 10-year analysis period under the unlimited budget scenario. A 2021 PCI average of 80 with this scenario is 36 PCI points higher than a “No M&R” scenario. The total cost for Major M&R over this 10-year period is about \$10.7 million.

7. MAINTENANCE AND REHABILITATION PLAN

The M&R analysis results include activities that likely exceed a typical annual budget level. These activities would need to be evaluated for feasibility and desirability based on the airport's future plans. In an effort to identify appropriate budget levels, the 10 year M&R analysis was evaluated to determine levels needed to address several specific areas: preventive maintenance, major activities for pavements in poor condition (Major M&R for PCIs less than Critical), and activities that would be desirable to preserve good pavement conditions where they exist (Major M&R for PCI greater than or equal to Critical).

Table 7-1 provides the summary results under the critical PCI unlimited funding scenario.

Table 7-1: M&R Costs under Unlimited Funding Scenario

Year	Preventative	Major M&R	Total Year Cost
2012	\$39,246.89	\$10,468,757.89	\$10,508,004.78
2013	\$11,732.55	\$218,985.37	\$230,717.92
2014	\$17,621.76	\$0.00	\$17,621.76
2015	\$24,147.07	\$0.00	\$24,147.07
2016	\$33,812.70	\$0.00	\$33,812.70
2017	\$39,569.57	\$62,379.87	\$101,949.44
2018	\$55,172.48	\$0.00	\$55,172.48
2019	\$80,344.55	\$0.00	\$80,344.55
2020	\$123,523.73	\$0.00	\$123,523.73
2021	\$152,570.01	\$0.00	\$152,570.01
Total	\$577,741.31	\$10,750,123.13	\$11,327,864.44

Note: Costs are adjusted for inflation.

Approximately 97% of the total Major M&R cost is required in the first year (2012). According to the 2012 inspections, the following pavement sections were in immediate need of Major M&R Activity:

- **Apron at T-Hangars** – Asphalt pavement mill and overlay.
- **Runway 14-32** – Asphalt pavement mill and overlay along with PCC restoration and reconstruction.
- **Runway 5-23** – PCC pavement restoration and reconstruction.
- **Taxiway Foxtrot** – Asphalt pavement reconstruction along with PCC reconstruction.
- **Taxiway Golf** – Asphalt pavement reconstruction.

The unlimited budget scenario provides the basis for estimating the total repair cost.

Appendix F provides details of M&R plan by year under the unlimited funding scenario, and the map of the 10-year M&R plan is provided in Appendix G. It is important to understand that the SAPMP is a network level tool and the M&R costs provided in this report are only for planning purposes.

8. VISUAL AIDS

8.1 System Inventory and Network Definition Drawings

The System Inventory and Network Definition CADD drawings, which show the airport pavement outline with Branch and Section boundaries and identify changes in the network pavement since the last inspection and the sampling plan, respectively, are included in Appendix A of this report.

8.2 Condition Map

A Condition Map that has been prepared based on data linked to the airport's shape file is included in Appendix B. The Condition Map graphically show the inventory and condition of the airport via color coding shown on the shape file. The coding provides a visual representation that illustrates the PCIs for each pavement section.

8.3 10-Year M&R Map

A 10-Year M&R Map that shows the summary of the M&R plan is attached in Appendix G.

8.4 Photographs

Selected digital photographs taken during the pavement inspection are provided in Appendix H to provide visual support to special pavement conditions or distress observed during the inspection of the airport.

9. RECOMMENDATIONS

Pavement condition inspections were performed at Williston Municipal Airport, and a 10-year M&R plan was developed based on the unlimited funding scenario.

The following recommendations were made based on the 2012 condition inspection and M&R analysis results:

- **Apron at T-Hangars** – Asphalt pavement mill and overlay.
- **Runway 14-32** – Asphalt pavement mill and overlay along with PCC restoration and reconstruction.
- **Runway 5-23** – PCC pavement restoration and reconstruction.
- **Taxiway Foxtrot** – Asphalt pavement reconstruction along with PCC reconstruction.
- **Taxiway Golf** – Asphalt pavement reconstruction.

Further evaluation of these features is necessary in order to develop repair plans and timing for future budgets since these needs cannot be addressed with typical annual expenditures.

APPENDIX A

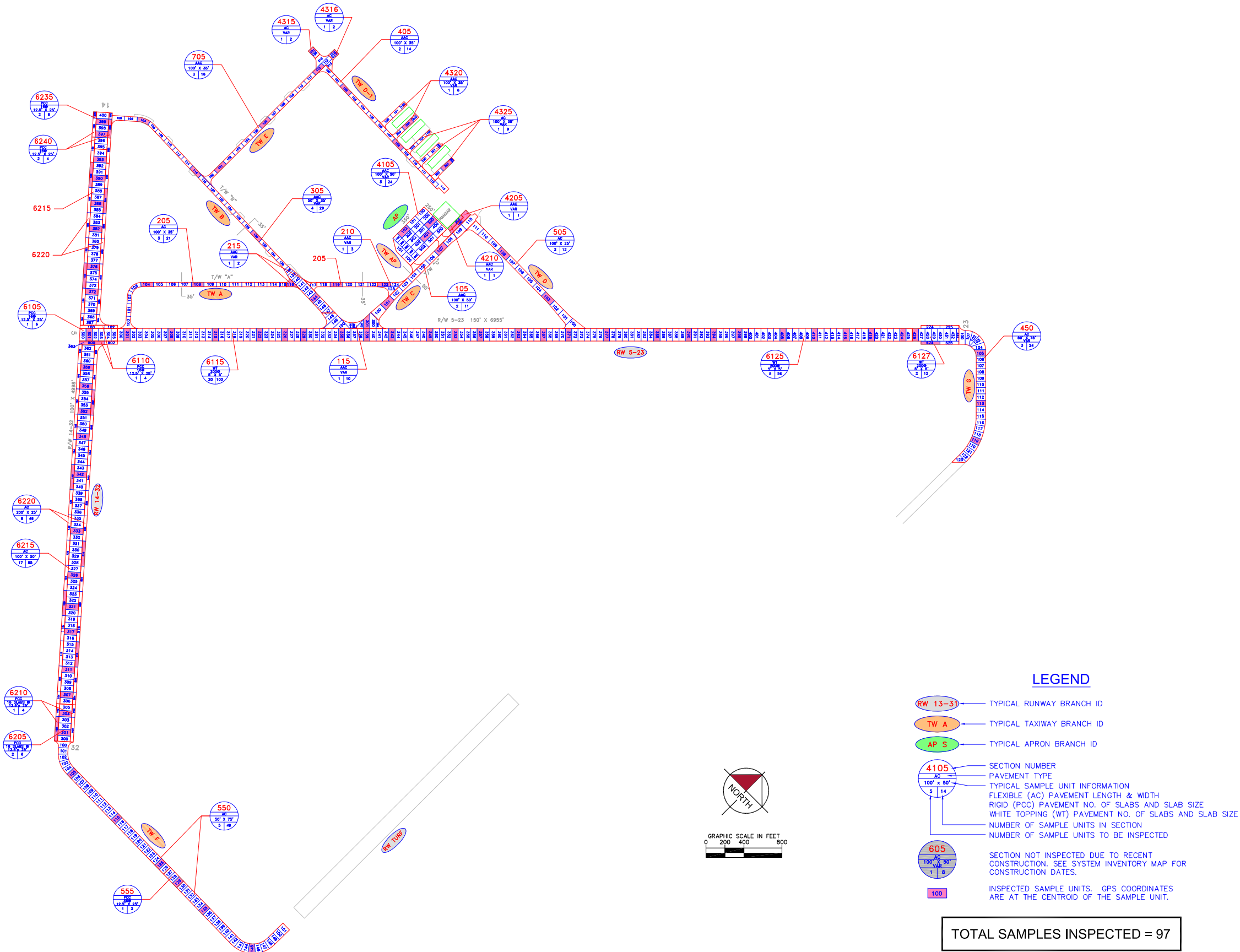
NETWORK DEFINITION MAP

SYSTEM INVENTORY MAP

PAVEMENT INVENTORY TABLE

WORK HISTORY REPORT

GPS COORDINATES - WILLISTON MUNICIPAL AIRPORT				
LOCATION	SECTION	SAMPLE	LATITUDE	LONGITUDE
AP	4105	102	29.358600	-82.473300
AP	4105	300	29.359100	-82.473000
AP	4105	401	29.358900	-82.472900
AP	4205	100	29.359600	-82.472500
AP	4210	200	29.359500	-82.472600
RW 14-32	6205	301	29.345600	-82.470500
RW 14-32	6205	304	29.345900	-82.470800
RW 14-32	6210	100	29.345600	-82.470700
RW 14-32	6215	307	29.346200	-82.471100
RW 14-32	6215	311	29.346700	-82.471500
RW 14-32	6215	317	29.347300	-82.472100
RW 14-32	6215	321	29.347700	-82.472600
RW 14-32	6215	326	29.348200	-82.473100
RW 14-32	6215	333	29.348900	-82.473800
RW 14-32	6215	342	29.349800	-82.474700
RW 14-32	6215	348	29.350500	-82.475400
RW 14-32	6215	352	29.350900	-82.475800
RW 14-32	6215	356	29.351300	-82.476200
RW 14-32	6215	359	29.351600	-82.476500
RW 14-32	6215	372	29.352800	-82.477800
RW 14-32	6215	376	29.353200	-82.478200
RW 14-32	6215	382	29.353900	-82.478800
RW 14-32	6215	386	29.354300	-82.479200
RW 14-32	6215	390	29.354700	-82.479600
RW 14-32	6215	393	29.355000	-82.479900
RW 14-32	6220	120	29.347600	-82.472800
RW 14-32	6220	140	29.349700	-82.474800
RW 14-32	6220	172	29.353000	-82.478200
RW 14-32	6220	188	29.354600	-82.479800
RW 14-32	6220	512	29.347000	-82.471600
RW 14-32	6220	528	29.348700	-82.473300
RW 14-32	6220	552	29.351100	-82.475800
RW 14-32	6220	584	29.354400	-82.479100
RW 14-32	6235	397	29.355400	-82.480400
RW 14-32	6235	399	29.355600	-82.480600
RW 14-32	6240	200	29.355600	-82.480800
RW 14-32	6240	596	29.355600	-82.480300
RW 5-23	6105	301	29.352100	-82.477000
RW 5-23	6110	500	29.352100	-82.476900
RW 5-23	6115	301	29.352700	-82.476400
RW 5-23	6115	308	29.353400	-82.475600
RW 5-23	6115	315	29.354100	-82.474800
RW 5-23	6115	322	29.354800	-82.474100
RW 5-23	6115	326	29.355200	-82.473600
RW 5-23	6115	329	29.355400	-82.473300
RW 5-23	6115	335	29.356000	-82.472600
RW 5-23	6115	339	29.356400	-82.472200
RW 5-23	6115	343	29.356800	-82.471700
RW 5-23	6115	349	29.357400	-82.471100
RW 5-23	6115	353	29.357800	-82.470600
RW 5-23	6115	357	29.358200	-82.470200
RW 5-23	6115	363	29.358800	-82.469500
RW 5-23	6115	367	29.359100	-82.469100
RW 5-23	6115	371	29.359500	-82.468600
RW 5-23	6115	377	29.360100	-82.468000
RW 5-23	6115	385	29.360900	-82.467100
RW 5-23	6115	390	29.361400	-82.466500
RW 5-23	6115	394	29.361800	-82.466100
RW 5-23	6115	398	29.362200	-82.465600
RW 5-23	6125	405	29.362800	-82.464900
RW 5-23	6125	410	29.363300	-82.464300
RW 5-23	6125	415	29.363800	-82.463800
RW 5-23	6125	419	29.364200	-82.463300
RW 5-23	6125	424	29.364700	-82.462800
RW 5-23	6127	624	29.365000	-82.462100
TW A	205	104	29.353800	-82.476900
TW A	205	108	29.354600	-82.476000
TW A	205	119	29.356700	-82.473600
TW A	210	123	29.357500	-82.472800
TW A	215	116	29.356000	-82.474300
TW B	305	104	29.356300	-82.479900
TW B	305	116	29.356300	-82.478100
TW B	305	130	29.356200	-82.475900
TW B	305	144	29.356200	-82.473700
TW C	105	101	29.357200	-82.472400
TW C	105	107	29.358900	-82.472400
TW C	115	301	29.356600	-82.472400
TW D	405	102	29.359900	-82.476900
TW D	405	108	29.359800	-82.475000
TW D	505	103	29.359800	-82.469600
TW D	505	108	29.359800	-82.471200
TW D-1	4315	101	29.359900	-82.478100
TW D-1	4320	201	29.360300	-82.475200
TW D-1	4325	301	29.360300	-82.473800
TW E	705	101	29.356700	-82.477700
TW E	705	106	29.358100	-82.477700
TW E	705	112	29.359800	-82.477700
TW E	4316	101	29.360300	-82.477700
TW F	550	105	29.345100	-82.469600
TW F	550	115	29.345100	-82.468000
TW F	550	125	29.345100	-82.466500
TW F	550	135	29.345100	-82.464900
TW F	550	145	29.345200	-82.463400
TW G	450	105	29.365600	-82.461100
TW G	450	113	29.364800	-82.460200
TW G	450	119	29.364200	-82.459600
NOTE: GEODETICS REPRESENT DECIMAL DEGREES WGS84 (DERIVED FROM NAD 83 FLORIDA STATE PLANES, EAST ZONE, US FOOT). ALL GPS COORDINATES ARE AT THE CENTROID OF THE SAMPLE UNITS.				



PLOTTED: May 3, 2012 - 3:37 PM, BY: Burton, George A

NUMBER	DATE	REVISIONS

CONSTRUCTION YEAR	LOCATION	WORK TYPE / PAVEMENT SECTION
2005	RUNWAY 5-23	NEW 100' WIDE GROOVED CONCRETE RUNWAY
2009	TAXIWAYS CHARLIE AND DELTA	MILLED AND ASPHALT OVERLAID
2009	TAXIWAYS BRAVO, D-1, AND ECHO	MILLED AND NEW ASPHALT LND
2009	PARKING RAMP	MILLED AND ASPHALT OVERLAID
2012	TAXIWAY ALPHA EXTENSION	NEW ASPHALT CONSTRUCTION
2014	TAXIWAY FOXTROT	ASPHALT

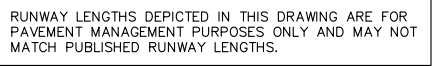


Table A-1: Pavement Inventory

Branch Name	Branch ID	Branch Use	Section ID	Length (ft)	Width (ft)	True Area (ft²)	Section Rank	Surface Type	Last Const. Date	Last Insp. Date	Total Samples
Apron	AP	APRON	4105	390	255	106,379	P	AAC	1/1/2009	2/15/2012	24
Hangar Apron	AP HANG	APRON	4205	180	20	4,300	P	AAC	1/1/2009	2/15/2012	1
Hangar Apron	AP HANG	APRON	4210	160	40	6,400	P	AAC	1/1/2009	2/15/2012	1
Apron at T-Hangars	AP T-HANG	APRON	4315	80	50	4,001	P	AC	1/1/1986	2/14/2012	2
Apron at T-Hangars	AP T-HANG	APRON	4316	65	50	3,259	P	AC	1/1/1986	2/14/2012	2
Apron at T-Hangars	AP T-HANG	APRON	4320	507	35	18,160	P	AC	1/1/2005	2/14/2012	6
Apron at T-Hangars	AP T-HANG	APRON	4325	709	35	23,114	P	AC	1/1/2003	2/14/2012	9
Runway 14-32	RW 14-32	RUNWAY	6205	303	100	30,300	P	PCC	1/1/1942	2/14/2012	6
Runway 14-32	RW 14-32	RUNWAY	6210	600	25	15,000	P	PCC	1/1/1942	2/14/2012	4
Runway 14-32	RW 14-32	RUNWAY	6215	4300	100	430,000	P	AC	1/1/1942	2/14/2012	85
Runway 14-32	RW 14-32	RUNWAY	6220	8600	25	215,000	P	AC	1/1/1942	2/14/2012	46
Runway 14-32	RW 14-32	RUNWAY	6235	300	100	30,000	P	PCC	1/1/1942	2/14/2012	6
Runway 14-32	RW 14-32	RUNWAY	6240	600	25	15,000	P	PCC	1/1/1942	2/14/2012	4
Runway 5-23	RW 5-23	RUNWAY	6105	300	100	30,000	T	PCC	1/1/1942	2/15/2012	6
Runway 5-23	RW 5-23	RUNWAY	6110	600	25	15,000	P	PCC	1/1/1942	2/15/2012	4
Runway 5-23	RW 5-23	RUNWAY	6115	5010	75	375,750	P	Whitetopping	1/1/2005	2/15/2012	100
Runway 5-23	RW 5-23	RUNWAY	6125	1270	100	127,000	P	Whitetopping	1/1/2005	2/15/2012	26
Runway 5-23	RW 5-23	RUNWAY	6127	150	100	15,000	P	Whitetopping	1/1/2005	2/15/2012	12
Taxiway Alpha	TW A	TAXIWAY	205	1990	35	72,808	P	AC	1/1/1986	2/15/2012	21
Taxiway Alpha	TW A	TAXIWAY	210	201	35	10,709	P	AAC	1/1/2009	2/14/2012	3
Taxiway Alpha	TW A	TAXIWAY	215	170	35	6,477	P	AAC	1/1/2009	2/14/2012	2
Taxiway Bravo	TW B	TAXIWAY	305	2365	35	101,472	P	AAC	1/1/2009	2/14/2012	29
Taxiway Charlie	TW C	TAXIWAY	105	1174	50	62,453	P	AAC	1/1/2009	2/15/2012	11
Taxiway Charlie	TW C	TAXIWAY	115	416	70	36,492	P	AAC	1/1/2009	2/14/2012	10

Table A-1: Pavement Inventory (Continued)

Branch Name	Branch ID	Branch Use	Section ID	Length (ft)	Width (ft)	True Area (ft²)	Section Rank	Surface Type	Last Const. Date	Last Insp. Date	Total Samples
Taxiway Delta	TW D	TAXIWAY	505	1150	25	28,750	P	AC	12/25/1999	2/15/2012	12
Taxiway D-1	TW D1	TAXIWAY	405	1384	35	52,495	P	AAC	1/1/2009	2/14/2012	14
Taxiway Echo	TW E	TAXIWAY	705	1384	35	57,345	P	AAC	1/1/2009	2/14/2012	16
Taxiway Foxtrot	TW F	TAXIWAY	550	2450	75	185,509	P	AC	1/1/1942	2/14/2012	49
Taxiway Foxtrot	TW F	TAXIWAY	555	150	75	11,250	P	PCC	1/1/1942	1/1/1942	3
Taxiway Golf	TW G	TAXIWAY	450	1173	75	94,786	P	AC	1/1/1942	2/14/2012	24

Note: If a new construction, then survey date = last construction date and PCI is set to 100 by MicroPAVER.

Sections not surveyed due to reasons such as re-sectioning, no escort, not accessible at the time of survey.

Date:04/24/2012

Work History Report

1 of 5

Pavement Database:

Network: X60 **Branch:** AP (APRON) **Section:** 4105 **Surface:** AAC
L.C.D.: 01/01/2009 **Use:** APRON **Rank:** P **Length:** 390.00 Ft **Width:** 255.00 Ft **True Area:** 106,379.00 SqF

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
01/01/2009	ML-OV	Mill and Overlay	\$0	0.00	True	ESTIMATE 1990 AC PAVEMENT WITH EMULSION SEAL
01/01/1990	IMPORTED	BUILT			True	

Network: X60 **Branch:** AP HANG (HANGAR APRON) **Section:** 4205 **Surface:** AAC
L.C.D.: 01/01/2009 **Use:** APRON **Rank:** P **Length:** 180.00 Ft **Width:** 20.00 Ft **True Area:** 4,300.00 SqF

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
01/01/2009	ML-OV	MILL and OVERLAY	\$0	0.00	True	ESTIMATE 1985 AC OVERLAY ASSUME: 1942 2" AC ON 6" LIME ROCK BASE
01/01/1985	IMPORTED	OVERLAY			True	
01/01/1942	IMPORTED	BUILT		2.00	True	

Network: X60 **Branch:** AP HANG (HANGAR APRON) **Section:** 4210 **Surface:** AAC
L.C.D.: 01/01/2009 **Use:** APRON **Rank:** P **Length:** 160.00 Ft **Width:** 40.00 Ft **True Area:** 6,400.00 SqF

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
01/01/2009	ML-OV	MILL and OVERLAY	\$0	0.00	True	ESTIMATE 1985 AC PAVEMENT
01/01/1985	IMPORTED	BUILT			True	

Network: X60 **Branch:** AP T-HANG (APRON AT T-HANGARS) **Section:** 4315 **Surface:** AC
L.C.D.: 01/01/1986 **Use:** APRON **Rank:** P **Length:** 80.00 Ft **Width:** 50.00 Ft **True Area:** 4,001.00 SqF

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
01/01/1986	INITIAL	Initial Construction	\$0	0.00	True	

Network: X60 **Branch:** AP T-HANG (APRON AT T-HANGARS) **Section:** 4316 **Surface:** AC
L.C.D.: 01/01/1986 **Use:** APRON **Rank:** P **Length:** 65.00 Ft **Width:** 50.00 Ft **True Area:** 3,259.00 SqF

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
01/01/1986	INITIAL	Initial Construction	\$0	0.00	True	

Network: X60 **Branch:** AP T-HANG (APRON AT T-HANGARS) **Section:** 4320 **Surface:** AC
L.C.D.: 01/01/2005 **Use:** APRON **Rank:** P **Length:** 507.00 Ft **Width:** 35.00 Ft **True Area:** 18,160.00 SqF

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
01/01/2005	INITIAL	Initial Construction	\$0	0.00	True	

Network: X60 **Branch:** AP T-HANG (APRON AT T-HANGARS) **Section:** 4325 **Surface:** AC
L.C.D.: 01/01/2003 **Use:** APRON **Rank:** P **Length:** 709.00 Ft **Width:** 35.00 Ft **True Area:** 23,114.00 SqF

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
01/01/2003	INITIAL	Initial Construction	\$0	0.00	True	

Network: X60 **Branch:** RW 14-32 (RUNWAY 14-32) **Section:** 6205 **Surface:** PCC
L.C.D.: 01/01/1942 **Use:** RUNWAY **Rank:** P **Length:** 303.00 Ft **Width:** 100.00 Ft **True Area:** 30,300.00 SqF

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
01/01/1942	IMPORTED	BUILT		8.00	True	1942: 8" PCC PAVEMENT

Date:04/24/2012

Work History Report

2 of 5

Pavement Database:

Network: X60 **Branch:** RW 14-32 (RUNWAY 14-32) **Section:** 6210 **Surface:** PCC
L.C.D.: 01/01/1942 **Use:** RUNWAY **Rank:** P **Length:** 600.00 Ft **Width:** 25.00 Ft **True Area:** 15,000.00 SqF

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
01/01/1942	IMPORTED	BUILT		8.00	True	1942: 8" PCC PAVEMENT

Network: X60 **Branch:** RW 14-32 (RUNWAY 14-32) **Section:** 6215 **Surface:** AC
L.C.D.: 01/01/1942 **Use:** RUNWAY **Rank:** P **Length:** 4,300.00 Ft **Width:** 100.00 Ft **True Area:** 430,000.00 SqF

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
01/01/1942	IMPORTED	BUILT		2.00	True	1942: 2" AC ON 6" LIME ROCK BASE

Network: X60 **Branch:** RW 14-32 (RUNWAY 14-32) **Section:** 6220 **Surface:** AC
L.C.D.: 01/01/1942 **Use:** RUNWAY **Rank:** P **Length:** 8,600.00 Ft **Width:** 25.00 Ft **True Area:** 215,000.00 SqF

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
01/01/1942	IMPORTED	BUILT		2.00	True	1942: 2" AC ON 6" LIME ROCK BASE

Network: X60 **Branch:** RW 14-32 (RUNWAY 14-32) **Section:** 6235 **Surface:** PCC
L.C.D.: 01/01/1942 **Use:** RUNWAY **Rank:** P **Length:** 300.00 Ft **Width:** 100.00 Ft **True Area:** 30,000.00 SqF

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
01/01/1942	IMPORTED	BUILT		8.00	True	1942: 8" PCC PAVEMENT

Network: X60 **Branch:** RW 14-32 (RUNWAY 14-32) **Section:** 6240 **Surface:** PCC
L.C.D.: 01/01/1942 **Use:** RUNWAY **Rank:** P **Length:** 600.00 Ft **Width:** 25.00 Ft **True Area:** 15,000.00 SqF

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
01/01/1942	IMPORTED	BUILT		8.00	True	1942: 8" PCC PAVEMENT

Network: X60 **Branch:** RW 5-23 (RUNWAY 5-23) **Section:** 6105 **Surface:** PCC
L.C.D.: 01/01/1942 **Use:** RUNWAY **Rank:** T **Length:** 300.00 Ft **Width:** 100.00 Ft **True Area:** 30,000.00 SqF

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
01/01/1942	IMPORTED	BUILT		8.00	True	1942: 8" PCC PAVEMENT

Network: X60 **Branch:** RW 5-23 (RUNWAY 5-23) **Section:** 6110 **Surface:** PCC
L.C.D.: 01/01/1942 **Use:** RUNWAY **Rank:** P **Length:** 600.00 Ft **Width:** 25.00 Ft **True Area:** 15,000.00 SqF

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
01/01/1942	IMPORTED	BUILT		8.00	True	1942: 8" PCC PAVEMENT

Network: X60 **Branch:** RW 5-23 (RUNWAY 5-23) **Section:** 6115 **Surface:** PAC
L.C.D.: 01/01/2005 **Use:** RUNWAY **Rank:** P **Length:** 5,010.00 Ft **Width:** 75.00 Ft **True Area:** 375,750.00 SqF

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
01/01/2005	OL-PCC	Overlay-PCC	\$0	0.00	True	ESTIMATE 1987 AC OVERLAY 1942: 2" AC ON 6" LIME ROCK BASE
01/01/1987	IMPORTED	OVERLAY			True	
01/01/1942	IMPORTED	BUILT		2.00	True	

Network: X60 **Branch:** RW 5-23 (RUNWAY 5-23) **Section:** 6125 **Surface:** PAC
L.C.D.: 01/01/2005 **Use:** RUNWAY **Rank:** P **Length:** 1,270.00 Ft **Width:** 100.00 Ft **True Area:** 127,000.00 SqF

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
01/01/2005	OL-PCC	Overlay-PCC	\$0	0.00	True	1942: 2" AC ON 6" LIME ROCK BASE
01/01/1942	IMPORTED	BUILT		2.00	True	

Date:04/24/2012

Work History Report

3 of 5

Pavement Database:

Network: X60 **Branch:** RW 5-23 (RUNWAY 5-23) **Section:** 6127 **Surface:** PAC
L.C.D.: 01/01/2005 **Use:** RUNWAY **Rank:** P **Length:** 150.00 Ft **Width:** 100.00 Ft **True Area:** 15,000.00 SqF

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
01/01/2005	OL-PCC	Overlay-PCC	\$0	0.00	True	SLURRY SEAL/SAND SEAL ON 1942: 2" AC ON 6" LIME ROCK BASE
01/01/1942	IMPORTED	OVERLAY			True	
01/01/1942	IMPORTED	BUILT		2.00	True	

Network: X60 **Branch:** TW A (TAXIWAY A) **Section:** 205 **Surface:** AC
L.C.D.: 01/01/1986 **Use:** TAXIWAY **Rank:** P **Length:** 1,990.00 Ft **Width:** 35.00 Ft **True Area:** 72,808.00 SqF

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
01/01/1986	IMPORTED	BUILT			True	ESTIMATE 1986 AC PAVEMENTT

Network: X60 **Branch:** TW A (TAXIWAY A) **Section:** 210 **Surface:** AAC
L.C.D.: 01/01/2009 **Use:** TAXIWAY **Rank:** P **Length:** 201.00 Ft **Width:** 35.00 Ft **True Area:** 10,709.00 SqF

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
01/01/2009	ML-OV	MILL and OVERLAY	\$0	0.00	True	
01/01/1986	INITIAL	Initial Construction	\$0	0.00	True	

Network: X60 **Branch:** TW A (TAXIWAY A) **Section:** 215 **Surface:** AAC
L.C.D.: 01/01/2009 **Use:** TAXIWAY **Rank:** P **Length:** 170.00 Ft **Width:** 35.00 Ft **True Area:** 6,477.00 SqF

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
01/01/2009	ML-OV	MILL and OVERLAY	\$0	0.00	True	
01/01/1986	INITIAL	Initial Construction	\$0	0.00	True	

Network: X60 **Branch:** TW B (TAXIWAY B) **Section:** 305 **Surface:** AAC
L.C.D.: 01/01/2009 **Use:** TAXIWAY **Rank:** P **Length:** 2,365.00 Ft **Width:** 35.00 Ft **True Area:**101,472.00 SqF

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
01/01/2009	ML-OV	MILL and OVERLAY	\$0	0.00	True	1942: 2" AC ON 6" LIME ROCK BASE
01/01/1942	IMPORTED	BUILT		2.00	True	

Network: X60 **Branch:** TW C (TAXIWAY C) **Section:** 105 **Surface:** AAC
L.C.D.: 01/01/2009 **Use:** TAXIWAY **Rank:** P **Length:** 1,174.00 Ft **Width:** 50.00 Ft **True Area:** 62,453.00 SqF

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
01/01/2009	ML-OV	MILL and OVERLAY	\$0	0.00	True	ESTIMATE 1982 AC OVERLAY 1942: 2" AC ON 6" LIME ROCK BASE
01/01/1982	IMPORTED	OVERLAY			True	
01/01/1942	IMPORTED	BUILT		2.00	True	

Network: X60 **Branch:** TW C (TAXIWAY C) **Section:** 115 **Surface:** AAC
L.C.D.: 01/01/2009 **Use:** TAXIWAY **Rank:** P **Length:** 416.00 Ft **Width:** 70.00 Ft **True Area:** 36,492.00 SqF

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
01/01/2009	ML-OV	MILL and OVERLAY	\$0	0.00	True	
01/01/1982	INITIAL	Initial Construction	\$0	0.00	True	

Network: X60 **Branch:** TW D (TAXIWAY D) **Section:** 505 **Surface:** AC
L.C.D.: 12/25/1999 **Use:** TAXIWAY **Rank:** P **Length:** 1,150.00 Ft **Width:** 25.00 Ft **True Area:** 28,750.00 SqF

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
12/25/1999	INITIAL	Initial Construction	\$0	0.00	True	

Date:04/24/2012

Work History Report

4 of 5

Pavement Database:

Network: X60 **Branch:** TW D1 (TAXIWAY D-1) **Section:** 405 **Surface:** AAC
L.C.D.: 01/01/2009 **Use:** TAXIWAY **Rank:** P **Length:** 1,384.00 Ft **Width:** 35.00 Ft **True Area:** 52,495.00 SqF

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
01/01/2009	ML-OV	MILL and OVERLAY	\$0	0.00	True	
01/01/1942	INITIAL	Initial Construction	\$0	0.00	True	

Network: X60 **Branch:** TW E (TAXIWAY E) **Section:** 705 **Surface:** AAC
L.C.D.: 01/01/2009 **Use:** TAXIWAY **Rank:** P **Length:** 1,384.00 Ft **Width:** 35.00 Ft **True Area:** 57,345.00 SqF

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
01/01/2009	ML-OV	MILL and OVERLAY	\$0	0.00	True	
01/01/1942	INITIAL	Initial Construction	\$0	0.00	True	

Network: X60 **Branch:** TW F (TAXIWAY F) **Section:** 550 **Surface:** AC
L.C.D.: 01/01/1942 **Use:** TAXIWAY **Rank:** P **Length:** 2,450.00 Ft **Width:** 75.00 Ft **True Area:**185,509.00 SqF

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
01/01/1942	INITIAL	Initial Construction	\$0	0.00	True	

Network: X60 **Branch:** TW F (TAXIWAY F) **Section:** 555 **Surface:** PCC
L.C.D.: 01/01/1942 **Use:** TAXIWAY **Rank:** P **Length:** 150.00 Ft **Width:** 75.00 Ft **True Area:** 11,250.00 SqF

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
01/01/1942	INITIAL	Initial Construction	\$0	0.00	True	

Network: X60 **Branch:** TW G (TAXIWAY G) **Section:** 450 **Surface:** AC
L.C.D.: 01/01/1942 **Use:** TAXIWAY **Rank:** P **Length:** 1,173.00 Ft **Width:** 75.00 Ft **True Area:** 94,786.00 SqF

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
01/01/1942	INITIAL	Initial Construction	\$0	0.00	True	

Summary:

Work Description	Section Count	Area Total (SqFt)	Thickness Avg (in)	Thickness STD (in)
BUILT	17	1,651,862.00	4.57	3.08
Initial Construction	13	532,347.00	.00	.00
Mill and Overlay	10	444,522.00	.00	.00
OVERLAY	4	457,503.00		
Overlay-PCC	3	517,750.00	.00	.00

STD = Standard Deviation

APPENDIX B

2012 CONDITION MAP PAVEMENT CONDITION INDEX TABLE

Table B-1: Pavement Condition Index

Branch Name	Branch ID	Branch Use	Section ID	True Area (ft ²)	Section Rank	Surface Type	Total Samples Inspected	Total Samples	PCI	PCI Category
Apron	AP	APRON	4105	106,379	P	AAC	3	24	91	Good
Hangar Apron	AP HANG	APRON	4205	4,300	P	AAC	1	1	98	Good
Hangar Apron	AP HANG	APRON	4210	6,400	P	AAC	1	1	91	Good
Apron at T-Hangars	AP T-HANG	APRON	4315	4,001	P	AC	1	2	64	Fair
Apron at T-Hangars	AP T-HANG	APRON	4316	3,259	P	AC	1	2	44	Poor
Apron at T-Hangars	AP T-HANG	APRON	4320	18,160	P	AC	1	6	90	Good
Apron at T-Hangars	AP T-HANG	APRON	4325	23,114	P	AC	1	9	72	Satisfactory
Runway 14-32	RW 14-32	RUNWAY	6205	30,300	P	PCC	2	6	37	Very Poor
Runway 14-32	RW 14-32	RUNWAY	6210	15,000	P	PCC	1	4	37	Very Poor
Runway 14-32	RW 14-32	RUNWAY	6215	430,000	P	AC	17	85	44	Poor
Runway 14-32	RW 14-32	RUNWAY	6220	215,000	P	AC	8	46	14	Serious
Runway 14-32	RW 14-32	RUNWAY	6235	30,000	P	PCC	2	6	60	Fair
Runway 14-32	RW 14-32	RUNWAY	6240	15,000	P	PCC	2	4	66	Fair
Runway 5-23	RW 5-23	RUNWAY	6105	30,000	T	PCC	1	6	47	Poor
Runway 5-23	RW 5-23	RUNWAY	6110	15,000	P	PCC	1	4	39	Very Poor
Runway 5-23	RW 5-23	RUNWAY	6115	375,750	P	Whitetopping	20	100	87	Good
Runway 5-23	RW 5-23	RUNWAY	6125	127,000	P	Whitetopping	5	26	90	Good
Runway 5-23	RW 5-23	RUNWAY	6127	15,000	P	Whitetopping	2	12	89	Good
Taxiway Alpha	TW A	TAXIWAY	205	72,808	P	AC	3	21	66	Fair
Taxiway Alpha	TW A	TAXIWAY	210	10,709	P	AAC	1	3	98	Good
Taxiway Alpha	TW A	TAXIWAY	215	6,477	P	AAC	1	2	95	Good
Taxiway Bravo	TW B	TAXIWAY	305	101,472	P	AAC	4	29	96	Good
Taxiway Charlie	TW C	TAXIWAY	105	62,453	P	AAC	2	11	97	Good
Taxiway Charlie	TW C	TAXIWAY	115	36,492	P	AAC	1	10	87	Good

Table B-1: Pavement Condition Index (Continued)

Branch Name	Branch ID	Branch Use	Section ID	True Area (ft²)	Section Rank	Surface Type	Total Samples Inspected	Total Samples	PCI	PCI Category
Taxiway Delta	TW D	TAXIWAY	505	28,750	P	AC	2	12	92	Good
Taxiway D-1	TW D1	TAXIWAY	405	52,495	P	AAC	2	14	100	Good
Taxiway Echo	TW E	TAXIWAY	705	57,345	P	AAC	3	16	100	Good
Taxiway Foxtrot	TW F	TAXIWAY	550	185,509	P	AC	5	49	7	Failed
Taxiway Foxtrot	TW F	TAXIWAY	555	11,250	P	PCC	1	3	100	Good
Taxiway Golf	TW G	TAXIWAY	450	94,786	P	AC	3	24	2	Failed

Note: If a new construction, then survey date = last construction date and PCI is set to 100 by MicroPAVER.

Sections not surveyed due to reasons such as re-sectioning, no escort, not accessible at the time of survey.

APPENDIX C

**BRANCH CONDITION REPORT
SECTION CONDITION REPORT**

Date: 4 /24/2012

Branch Condition Report

1 of 2

Pavement Database: NetworkID: X60

Branch ID	Number of Sections	Sum Section Length (Ft)	Avg Section Width (Ft)	True Area (SqFt)	Use	Average PCI	PCI Standard Deviation	Weighted Average PCI
AP (APRON)	1	390.00	255.00	106,379.00	APRON	91.00	0.00	91.00
AP HANG (HANGAR APRON)	2	340.00	30.00	10,700.00	APRON	94.50	3.50	93.81
AP T-HANG (APRON AT T-HANGARS)	4	1,361.00	42.50	48,534.00	APRON	67.50	16.52	76.20
RW 14-32 (RUNWAY 14-32)	6	14,703.00	62.50	735,300.00	RUNWAY	43.00	16.99	35.90
RW 5-23 (RUNWAY 5-23)	2	900.00	62.50	45,000.00	RUNWAY	43.00	4.00	44.33
TW A (TAXIWAY A)	3	2,361.00	35.00	89,994.00	TAXIWAY	86.33	14.43	71.90
TW B (TAXIWAY B)	1	2,365.00	35.00	101,472.00	TAXIWAY	96.00	0.00	96.00
TW C (TAXIWAY C)	2	1,590.00	60.00	98,945.00	TAXIWAY	92.00	5.00	93.31
TW D (TAXIWAY D)	1	1,150.00	25.00	28,750.00	TAXIWAY	92.00	0.00	92.00
TW D1 (TAXIWAY D-1)	1	1,384.00	35.00	52,495.00	TAXIWAY	100.00	0.00	100.00
TW E (TAXIWAY E)	1	1,384.00	35.00	57,345.00	TAXIWAY	100.00	0.00	100.00
TW F (TAXIWAY F)	2	2,600.00	75.00	196,759.00	TAXIWAY	53.50	46.50	12.32
TW G (TAXIWAY G)	1	1,173.00	75.00	94,786.00	TAXIWAY	2.00	0.00	2.00

Date: 4 /24/2012

Branch Condition Report

2 of 2

Pavement Database:

Use Category	Number of Sections	Total Area (SqFt)	Arithmetic Average PCI	Average PCI STD.	Weighted Average PCI
APRON	7	165,613.00	78.57	18.00	86.84
RUNWAY	8	780,300.00	43.00	14.85	36.38
TAXIWAY	12	720,546.00	78.33	34.23	57.85
All	27	1,666,459.00	67.93	30.52	50.68

STD = Standard Deviation

Date: 4 /24/2012

Section Condition Report

1 of 3

Pavement Database: FDOT NetworkID: X60

Branch ID	Section ID	Last Const. Date	Surface	Use	Rank	Lanes	True Area (SqFt)	Last Inspection Date	Age At Inspection	PCI
AP (APRON)	4105	01/01/2009	AAC	APRON	P	0	106,379.00	02/15/2012	3	91.00
AP HANG (HANGAR APRON)	4205	01/01/2009	AAC	APRON	P	0	4,300.00	02/15/2012	3	98.00
AP HANG (HANGAR APRON)	4210	01/01/2009	AAC	APRON	P	0	6,400.00	02/15/2012	3	91.00
AP T-HANG (APRON AT T-HANGARS)	4315	01/01/1986	AC	APRON	P	0	4,001.00	02/14/2012	26	64.00
AP T-HANG (APRON AT T-HANGARS)	4316	01/01/1986	AC	APRON	P	0	3,259.00	02/14/2012	26	44.00
AP T-HANG (APRON AT T-HANGARS)	4320	01/01/2005	AC	APRON	P	0	18,160.00	02/14/2012	7	90.00
AP T-HANG (APRON AT T-HANGARS)	4325	01/01/2003	AC	APRON	P	0	23,114.00	02/14/2012	9	72.00
RW 14-32 (RUNWAY 14-32)	6205	01/01/1942	PCC	RUNWAY	P	0	30,300.00	02/14/2012	70	37.00
RW 14-32 (RUNWAY 14-32)	6210	01/01/1942	PCC	RUNWAY	P	0	15,000.00	02/14/2012	70	37.00
RW 14-32 (RUNWAY 14-32)	6215	01/01/1942	AC	RUNWAY	P	0	430,000.00	02/14/2012	70	44.00
RW 14-32 (RUNWAY 14-32)	6220	01/01/1942	AC	RUNWAY	P	0	215,000.00	02/14/2012	70	14.00
RW 14-32 (RUNWAY 14-32)	6235	01/01/1942	PCC	RUNWAY	P	0	30,000.00	02/14/2012	70	60.00
RW 14-32 (RUNWAY 14-32)	6240	01/01/1942	PCC	RUNWAY	P	0	15,000.00	02/14/2012	70	66.00
RW 5-23 (RUNWAY 5-23)	6105	01/01/1942	PCC	RUNWAY	T	0	30,000.00	02/15/2012	70	47.00
RW 5-23 (RUNWAY 5-23)	6110	01/01/1942	PCC	RUNWAY	P	0	15,000.00	02/15/2012	70	39.00
TW A (TAXIWAY A)	205	01/01/1986	AC	TAXIWAY	P	0	72,808.00	02/15/2012	26	66.00
TW A (TAXIWAY A)	210	01/01/2009	AAC	TAXIWAY	P	0	10,709.00	02/14/2012	3	98.00
TW A (TAXIWAY A)	215	01/01/2009	AAC	TAXIWAY	P	0	6,477.00	02/14/2012	3	95.00
TW B (TAXIWAY B)	305	01/01/2009	AAC	TAXIWAY	P	0	101,472.00	02/14/2012	3	96.00
TW C (TAXIWAY C)	105	01/01/2009	AAC	TAXIWAY	P	0	62,453.00	02/15/2012	3	97.00
TW C (TAXIWAY C)	115	01/01/2009	AAC	TAXIWAY	P	0	36,492.00	02/14/2012	3	87.00
TW D (TAXIWAY D)	505	12/25/1999	AC	TAXIWAY	P	0	28,750.00	02/15/2012	13	92.00
TW D1 (TAXIWAY D-1)	405	01/01/2009	AAC	TAXIWAY	P	0	52,495.00	02/14/2012	3	100.00
TW E (TAXIWAY E)	705	01/01/2009	AAC	TAXIWAY	P	0	57,345.00	02/14/2012	3	100.00
TW F (TAXIWAY F)	550	01/01/1942	AC	TAXIWAY	P	0	185,509.00	02/14/2012	70	7.00

Date: 4 /24/2012

Section Condition Report

2 of 3

Pavement Database: FDOT NetworkID: X60

Branch ID	Section ID	Last Const. Date	Surface	Use	Rank	Lanes	True Area (SqFt)	Last Inspection Date	Age At Inspection	PCI
TW F (TAXIWAY F)	555	01/01/1942	PCC	TAXIWAY	P	0	11,250.00	01/01/1942	0	100.00
TW G (TAXIWAY G)	450	01/01/1942	AC	TAXIWAY	P	0	94,786.00	02/14/2012	70	2.00

Date: 4 /24/2012

Section Condition Report

3 of 3

Pavement Database: FDOT

Age Category	Average Age At Inspection	Total Area (SqFt)	Number of Sections	Arithmetic Average PCI	PCI Standard Deviation	Weighted Average PCI
0-02	0.00	11,250.00	1	100.00	0.00	100.00
03-05	3.00	444,522.00	10	95.30	4.10	95.17
06-10	8.00	41,274.00	2	81.00	9.00	79.92
11-15	13.00	28,750.00	1	92.00	0.00	92.00
26-30	26.00	80,068.00	3	58.00	9.93	65.00
over 40	70.00	1,060,595.00	10	35.30	20.37	28.17
All	31.00	1,666,459.00	27	67.93	30.52	50.68

APPENDIX D

PAVEMENT CONDITION PREDICTION TABLE PREDICTED PCI BY PAVEMENT USE GRAPH

Table D-1: Pavement Condition Prediction

Branch Name	Branch ID	Section ID	Current PCI	PCI Forecast									
				2012	2013	2014	2015	2016	2017	2018	2019	2020	2021
Apron	AP	4105	91	90	88	86	84	82	80	78	76	74	72
Hangar Apron	AP HANG	4205	98	97	95	93	91	88	86	84	82	80	78
Hangar Apron	AP HANG	4210	91	90	88	86	84	82	80	78	76	74	72
Apron at T-Hangars	AP T-HANG	4315	64	63	62	61	59	58	56	55	53	52	50
Apron at T-Hangars	AP T-HANG	4316	44	43	42	41	39	38	36	35	33	32	30
Apron at T-Hangars	AP T-HANG	4320	90	89	88	87	85	84	82	81	79	78	76
Apron at T-Hangars	AP T-HANG	4325	72	71	70	69	67	66	64	63	61	60	58
Runway 14-32	RW 14-32	6205	37	36	33	31	28	26	23	21	18	16	13
Runway 14-32	RW 14-32	6210	37	36	33	31	28	26	23	21	18	16	13
Runway 14-32	RW 14-32	6215	44	43	42	41	39	38	36	35	33	32	30
Runway 14-32	RW 14-32	6220	14	13	12	11	9	8	6	5	3	2	0
Runway 14-32	RW 14-32	6235	60	59	56	54	51	49	46	44	41	39	36
Runway 14-32	RW 14-32	6240	66	65	62	60	57	55	52	50	47	45	42
Runway 5-23	RW 5-23	6105	47	46	43	41	38	36	33	31	28	26	23
Runway 5-23	RW 5-23	6110	39	38	35	33	30	28	25	23	20	18	15
Taxiway Bravo	TW B	305	96	95	94	92	90	88	87	85	83	81	80
Taxiway Charlie	TW C	105	97	96	95	93	91	89	88	86	84	82	81
Taxiway Charlie	TW C	115	87	86	85	83	81	79	78	76	74	72	71
Taxiway Delta	TW D	505	92	91	90	88	86	84	83	81	79	78	76
Taxiway D-1	TW D1	405	100	99	98	96	94	92	91	89	87	85	84
Taxiway Echo	TW E	705	100	99	98	96	94	92	91	89	87	85	84
Taxiway Foxtrot	TW F	550	7	6	5	3	1	0	0	0	0	0	0
Taxiway Foxtrot	TW F	555	100	0	0	0	0	0	0	0	0	0	0
Taxiway Golf	TW G	450	2	1	0	0	0	0	0	0	0	0	0

Figure D-1: Predicted PCI by Pavement Use



APPENDIX E

YEAR 1 MAINTENANCE ACTIVITIES TABLE

Table E-1: Year 1 Maintenance Activities

Branch Name	Branch ID	Section ID	Distress Description	Distress Severity	Work Description	Work Quantity	Work Unit	Unit Cost	Work Cost
Apron	AP	4105	PATCHING	M	Patching - AC Deep	19.50	SqFt	\$4.90	\$95.71
Apron	AP	4105	WEATH/RAVEL	L	Surface Seal - Rejuvenating	582.10	SqFt	\$0.40	\$232.84
Hangar Apron	AP HANG	4205	WEATH/RAVEL	L	Surface Seal - Rejuvenating	28.60	SqFt	\$0.40	\$11.46
Hangar Apron	AP HANG	4210	WEATH/RAVEL	L	Surface Seal - Rejuvenating	191.90	SqFt	\$0.40	\$76.77
Apron at T-Hangars	AP T-HANG	4320	WEATH/RAVEL	L	Surface Seal - Rejuvenating	1,089.60	SqFt	\$0.40	\$435.84
Apron at T-Hangars	AP T-HANG	4325	OIL SPILLAGE	N	Patching - AC Shallow	31.80	SqFt	\$2.90	\$92.32
Apron at T-Hangars	AP T-HANG	4325	WEATH/RAVEL	L	Surface Seal - Rejuvenating	23,113.80	SqFt	\$0.40	\$9,245.60
Runway 14-32	RW 14-32	6240	LINEAR CR	M	Crack Sealing – PCC	62.00	Ft	\$4.24	\$262.88
Taxiway Alpha	TW A	205	WEATH/RAVEL	L	Surface Seal - Rejuvenating	62,935.30	SqFt	\$0.40	\$25,174.32
Taxiway Alpha	TW A	205	WEATH/RAVEL	M	Surface Seal - Coat Tar	746.10	SqFt	\$0.40	\$298.46
Taxiway Alpha	TW A	205	L & T CR	M	Crack Sealing – AC	259.50	Ft	\$2.25	\$583.94
Taxiway Alpha	TW A	215	WEATH/RAVEL	L	Surface Seal - Rejuvenating	58.90	SqFt	\$0.40	\$23.55
Taxiway Bravo	TW B	305	WEATH/RAVEL	L	Surface Seal - Rejuvenating	533.00	SqFt	\$0.40	\$213.21
Taxiway Charlie	TW C	115	WEATH/RAVEL	L	Surface Seal - Rejuvenating	6,204.80	SqFt	\$0.40	\$2,481.92
Taxiway Delta	TW D	505	WEATH/RAVEL	L	Surface Seal - Rejuvenating	45.20	SqFt	\$0.40	\$18.08
Total =									\$39,246.90

APPENDIX F

MAJOR M&R PLAN BY YEAR UNDER UNLIMITED FUNDING SCENARIO TABLE

Table F-1: Major M&R Plan by Year under Unlimited Funding Scenario

Year	Branch Name	Section ID	Surface Type	Section Area (ft ²)	Major M&R Costs*	PCI Before M&R	M&R Activity	PCI After M&R
2012	Apron at T-Hangars	4315	AC	4,001	\$10,406.61	63	Mill and Overlay	100
2012	Apron at T-Hangars	4316	AC	3,259	\$20,499.11	43	Mill and Overlay	100
2012	Runway 14-32	6205	PCC	30,300	\$279,426.68	36	Reconstruction	100
2012	Runway 14-32	6210	PCC	15,000	\$138,330.04	36	Reconstruction	100
2012	Runway 14-32	6215	AC	430,000	\$2,704,700.21	43	Mill and Overlay	100
2012	Runway 14-32	6220	AC	215,000	\$2,928,300.95	13	Reconstruction	100
2012	Runway 14-32	6235	PCC	30,000	\$111,210.07	59	PCC Restoration	100
2012	Runway 5-23	6105	PCC	30,000	\$188,700.01	46	PCC Restoration	100
2012	Runway 5-23	6110	PCC	15,000	\$116,340.03	38	Reconstruction	100
2012	Taxiway Foxtrot	550	AC	185,509	\$2,526,633.40	6	Reconstruction	100
2012	Taxiway Foxtrot	555	PCC	11,250	\$153,225.05	0	Reconstruction	100
2012	Taxiway Golf	450	AC	94,786	\$1,290,985.74	1	Reconstruction	100
2013	Runway 14-32	6240	PCC	15,000	\$44,403.33	62	PCC Restoration	100
2013	Taxiway Alpha	205	AC	72,808	\$174,582.04	64	Mill and Overlay	100
2017	Apron at T-Hangars	4325	AC	23,114	\$62,379.87	64	Mill and Overlay	100
Total					\$10,750,123.14	30		100

* Costs are adjusted for inflation.

APPENDIX G

10-YEAR M&R MAP

APPENDIX H

PHOTOGRAPHS



Runway 14-32, Section 6215, Sample Unit 326 – Low and moderate severity (43) Block Cracking; low severity (48) Longitudinal / Transverse Cracking, and low severity (52) Weathering and Raveling.



Runway 5-23, Section 6115, Sample Unit 315 – Overview of sample unit. Joint spalls observed on sample unit.



Runway 5-23, Section 6115, Sample Unit 349 – Corner spall observed in whitetopping.



Taxiway Alpha, Section 205, Sample Unit 108 – Low severity (48) Longitudinal / Transverse Cracking; and low to medium severity (52) Weathering and Raveling.



Apron, Section 4105, Sample Unit 102 – Low severity (48) Longitudinal / Transverse Cracking.



Taxiway Fox Trot, Section 550, Sample Unit 135 – Medium to high severity (43) Block Cracking; low to medium severity (48) Longitudinal / Transverse Cracking; and medium to high severity (52) Weathering and Raveling.

APPENDIX I

PCI RE-INSPECTION REPORT

Re-inspection Report

FDOT

Report Generated Date: 4/24/2012

Site Name:

Network: X60 Name: WILLISTON MUNICIPAL AIRPORT

Branch: AP Name: APRON Use: APRON Area: 106,379.00SqFt

Section: 4105 of 1 From: - To: - Last Const.: 1/1/2009
Surface: AAC Family: FDOT-GA-AP-AAC Zone: Category: Rank: P
Area: 106,379.00SqFt Length: 390.00Ft Width: 255.00Ft
Shoulder: Street Type: Grade: 0.00 Lanes: 0
Section Comments:

Last Insp. Date: 2/15/2012 Total Samples: 24 Surveyed: 3

Conditions: PCI: 91.00

Inspection Comments:

Sample Number: 102 Type: R Area: 5,000.05SqFt PCI = 89

Sample Comments:

48 L & T CR	L	13.00 Ft	Comments:
56 SWELLING	L	37.00 SqFt	Comments:
45 DEPRESSION	L	36.00 SqFt	Comments:

Sample Number: 300 Type: R Area: 5,000.05SqFt PCI = 88

Sample Comments:

50 PATCHING	L	4.00 SqFt	Comments:
56 SWELLING	L	46.00 SqFt	Comments:
50 PATCHING	M	1.00 SqFt	Comments:
45 DEPRESSION	L	10.00 SqFt	Comments:

Sample Number: 401 Type: R Area: 5,000.05SqFt PCI = 94

Sample Comments:

56 SWELLING	L	24.00 SqFt	Comments:
52 WEATH/RAVEL	L	100.00 SqFt	Comments:

Re-inspection Report

FDOT

Report Generated Date: 4/24/2012

Site Name:

Network: X60 Name: WILLISTON MUNICIPAL AIRPORT

Branch: AP HANG Name: HANGAR APRON Use: APRON Area: 10,700.00SqFt

Section: 4205 of 2 From: - To: - Last Const.: 1/1/2009
Surface: AAC Family: FDOT-GA-AP-AAC Zone: Category: Rank: P
Area: 4,300.00SqFt Length: 180.00Ft Width: 20.00Ft
Shoulder: Street Type: Grade: 0.00 Lanes: 0
Section Comments:

Last Insp. Date: 2/15/2012 Total Samples: 1 Surveyed: 1
Conditions: PCI: 98.00
Inspection Comments:

Sample Number: 100 Type: R Area: 6,944.12SqFt PCI = 98
Sample Comments:
52 WEATH/RAVEL L 56.00 SqFt Comments:

Re-inspection Report

FDOT

Report Generated Date: 4/24/2012

Site Name:

Network: X60 Name: WILLISTON MUNICIPAL AIRPORT

Branch: AP HANG Name: HANGAR APRON Use: APRON Area: 10,700.00SqFt

Section: 4210 of 2 From: - To: - Last Const.: 1/1/2009
Surface: AAC Family: FDOT-GA-AP-AAC Zone: Category: Rank: P
Area: 6,400.00SqFt Length: 160.00Ft Width: 40.00Ft
Shoulder: Street Type: Grade: 0.00 Lanes: 0
Section Comments:

Last Insp. Date: 2/15/2012 Total Samples: 1 Surveyed: 1
Conditions: PCI: 91.00
Inspection Comments:

Sample Number: 200	Type: R	Area: 3,957.35SqFt	PCI = 91
Sample Comments:			
49 OIL SPILLAGE	L	8.00 SqFt	Comments:
45 DEPRESSION	L	10.00 SqFt	Comments:
52 WEATH/RAVEL	L	120.00 SqFt	Comments:

Re-inspection Report

FDOT

Report Generated Date: 4/24/2012

Site Name:

Network: X60 Name: WILLISTON MUNICIPAL AIRPORT

Branch: AP T-HANG Name: APRON AT T-HANGARS Use: APRON Area: 48,534.00SqFt

Section: 4315 of 4 From: - To: - Last Const.: 1/1/1986
Surface: AC Family: FDOT-GA-AP-AC Zone: Category: Rank: P
Area: 4,001.00SqFt Length: 80.00Ft Width: 50.00Ft
Shoulder: Street Type: Grade: 0.00 Lanes: 0
Section Comments:

Last Insp. Date: 2/14/2012 Total Samples: 2 Surveyed: 1

Conditions: PCI: 64.00

Inspection Comments:

Sample Number: 101 Type: R Area: 2,250.00SqFt PCI = 64

Sample Comments:

48	LONGITUDINAL/TRANSVERSE CRACKING	L	114.03 Ft	Comments:
52	WEATHERING/RAVELING	L	2,237.98 SqFt	Comments:
52	WEATHERING/RAVELING	M	12.00 SqFt	Comments:

Re-inspection Report

FDOT

Report Generated Date: 4/24/2012

Site Name:

Network: X60 Name: WILLISTON MUNICIPAL AIRPORT

Branch: AP T-HANG Name: APRON AT T-HANGARS Use: APRON Area: 48,534.00SqFt

Section: 4316 of 4 From: - To: - Last Const.: 1/1/1986
Surface: AC Family: FDOT-GA-AP-AC Zone: Category: Rank: P
Area: 3,259.00SqFt Length: 65.00Ft Width: 50.00Ft
Shoulder: Street Type: Grade: 0.00 Lanes: 0
Section Comments:

Last Insp. Date: 2/14/2012 Total Samples: 2 Surveyed: 1
Conditions: PCI: 44.00
Inspection Comments:

Sample Number: 101 Type: R Area: 2,400.00SqFt PCI = 44

Sample Comments:

43 BLOCK CRACKING	M	2,199.98 SqFt	Comments:
52 WEATHERING/RAVELING	L	1,149.99 SqFt	Comments:

Re-inspection Report

FDOT

Report Generated Date: 4/24/2012

Site Name:

Network: X60 Name: WILLISTON MUNICIPAL AIRPORT

Branch: AP T-HANG Name: APRON AT T-HANGARS Use: APRON Area: 48,534.00SqFt

Section: 4320 of 4 From: - To: - Last Const.: 1/1/2005
Surface: AC Family: FDOT-GA-AP-AC Zone: Category: Rank: P
Area: 18,160.00SqFt Length: 507.00Ft Width: 35.00Ft
Shoulder: Street Type: Grade: 0.00 Lanes: 0
Section Comments:

Last Insp. Date: 2/14/2012 Total Samples: 6 Surveyed: 1
Conditions: PCI: 90.00 |
Inspection Comments:

Sample Number:	201	Type:	R	Area:	3,500.00SqFt	PCI = 90
Sample Comments:						
50	PATCHING		L	0.50	SqFt	Comments:
52	WEATHERING/RAVELING		L	210.00	SqFt	Comments:

Re-inspection Report

FDOT

Report Generated Date: 4/24/2012

Site Name:

Network: X60 Name: WILLISTON MUNICIPAL AIRPORT

Branch: AP T-HANG Name: APRON AT T-HANGARS Use: APRON Area: 48,534.00SqFt

Section: 4325 of 4 From: - To: - Last Const.: 1/1/2003
Surface: AC Family: FDOT-GA-AP-AC Zone: Category: Rank: P
Area: 23,114.00SqFt Length: 709.00Ft Width: 35.00Ft
Shoulder: Street Type: Grade: 0.00 Lanes: 0
Section Comments:

Last Insp. Date: 2/14/2012 Total Samples: 9 Surveyed: 1
Conditions: PCI: 72.00
Inspection Comments:

Sample Number: 301 Type: R Area: 3,500.00SqFt PCI = 72
Sample Comments:
49 OIL SPILLAGE N 2.00 SqFt Comments:
52 WEATHERING/RAVELING L 3,499.97 SqFt Comments:

Re-inspection Report

FDOT

Report Generated Date: 4/24/2012

Site Name:

Network: X60 Name: WILLISTON MUNICIPAL AIRPORT

Branch: RW 14-32 Name: RUNWAY 14-32 Use: RUNWAY Area: 735,300.00SqFt

Section: 6205 of 6 From: - To: - Last Const.: 1/1/1942
Surface: PCC Family: FDOT-GA-PCC Zone: Category: Rank: P
Area: 30,300.00SqFt Length: 303.00Ft Width: 100.00Ft
Shoulder: Street Type: Grade: 0.00 Lanes: 0
Section Comments:

Last Insp. Date: 2/14/2012 Total Samples: 6 Surveyed: 2

Conditions: PCI: 37.00

Inspection Comments:

Sample Number: 301 Type: R Area: 14.52 Count PCI = 35

Sample Comments:

65 JT SEAL DMG	H	16.00	Count	Comments:
70 SCALING	L	7.00	Count	Comments:
63 LINEAR CR	M	6.00	Count	Comments:
74 JOINT SPALL	M	1.00	Count	Comments:
74 JOINT SPALL	L	1.00	Count	Comments:
63 LINEAR CR	L	1.00	Count	Comments:
73 SHRINKAGE CR	L	1.00	Count	Comments:
66 SMALL PATCH	L	1.00	Count	Comments:
75 CORNER SPALL	L	1.00	Count	Comments:

Sample Number: 304 Type: R Area: 14.52 Count PCI = 40

Sample Comments:

65 JT SEAL DMG	M	0.00	Count	Comments:
74 JOINT SPALL	H	1.00	Count	Comments:
65 JT SEAL DMG	H	16.00	Count	Comments:
74 JOINT SPALL	M	2.00	Count	Comments:
63 LINEAR CR	M	3.00	Count	Comments:
70 SCALING	L	12.00	Count	Comments:

Re-inspection Report

FDOT

Report Generated Date: 4/24/2012

Site Name:

Network: X60 Name: WILLISTON MUNICIPAL AIRPORT

Branch: RW 14-32 Name: RUNWAY 14-32 Use: RUNWAY Area: 735,300.00SqFt

Section: 6210 of 6 From: - To: - Last Const.: 1/1/1942
Surface: PCC Family: FDOT-GA-PCC Zone: Category: Rank: P
Area: 15,000.00SqFt Length: 600.00Ft Width: 25.00Ft
Shoulder: Street Type: Grade: 0.00 Lanes: 0
Section Comments:

Last Insp. Date: 2/14/2012 Total Samples: 4 Surveyed: 1
Conditions: PCI: 37.00
Inspection Comments:

Sample Number: 100 Type: R Area: 14.18 Count PCI = 37

Sample Comments:

63 LINEAR CR	M	8.00	Count	Comments:
65 JT SEAL DMG	H	16.00	Count	Comments:
70 SCALING	L	6.00	Count	Comments:
63 LINEAR CR	L	4.00	Count	Comments:

Re-inspection Report

FDOT

Report Generated Date: 4/24/2012

Site Name:

Network: X60 Name: WILLISTON MUNICIPAL AIRPORT

Branch: RW 14-32 Name: RUNWAY 14-32 Use: RUNWAY Area: 735,300.00SqFt

Section: 6215 of 6 From: - To: - Last Const.: 1/1/1942
Surface: AC Family: FDOT-GA-RW-AC Zone: Category: Rank: P
Area: 430,000.00SqFt Length: 4,300.00Ft Width: 100.00Ft
Shoulder: Street Type: Grade: 0.00 Lanes: 0
Section Comments:

Last Insp. Date: 2/14/2012 Total Samples: 85 Surveyed: 17

Conditions: PCI: 44.00

Inspection Comments:

Sample Number: 307 Type: R Area: 4,999.94SqFt PCI = 48

Sample Comments:

43 BLOCK CR	M	450.00 SqFt	Comments:
43 BLOCK CR	L	3,200.00 SqFt	Comments:
48 L & T CR	L	336.00 Ft	Comments:
52 WEATH/RAVEL	L	3,800.00 SqFt	Comments:

Sample Number: 311 Type: R Area: 4,999.94SqFt PCI = 43

Sample Comments:

43 BLOCK CR	M	1,300.00 SqFt	Comments:
43 BLOCK CR	L	2,700.00 SqFt	Comments:
52 WEATH/RAVEL	L	2,900.00 SqFt	Comments:
50 PATCHING	L	0.25 SqFt	Comments:
48 L & T CR	L	29.00 Ft	Comments:

Sample Number: 317 Type: R Area: 5,000.05SqFt PCI = 46

Sample Comments:

43 BLOCK CR	M	800.00 SqFt	Comments:
43 BLOCK CR	L	2,150.00 SqFt	Comments:
48 L & T CR	M	31.00 Ft	Comments:
48 L & T CR	L	209.00 Ft	Comments:
52 WEATH/RAVEL	L	2,600.00 SqFt	Comments:

Sample Number: 321 Type: R Area: 4,999.94SqFt PCI = 44

Sample Comments:

43 BLOCK CR	M	1,250.00 SqFt	Comments:
43 BLOCK CR	L	2,200.00 SqFt	Comments:
48 L & T CR	L	110.00 Ft	Comments:
52 WEATH/RAVEL	L	3,600.00 SqFt	Comments:

Sample Number: 326 Type: R Area: 5,000.05SqFt PCI = 50

Sample Comments:

43 BLOCK CR	M	800.00 SqFt	Comments:
43 BLOCK CR	L	3,200.00 SqFt	Comments:
48 L & T CR	L	104.00 Ft	Comments:
52 WEATH/RAVEL	L	1,700.00 SqFt	Comments:

Sample Number: 333 Type: R Area: 4,999.94SqFt PCI = 44

Sample Comments:

43 BLOCK CR	M	2,000.00 SqFt	Comments:
43 BLOCK CR	L	2,400.00 SqFt	Comments:
48 L & T CR	L	90.00 Ft	Comments:
52 WEATH/RAVEL	L	1,550.00 SqFt	Comments:

Re-inspection Report

FDOT

Report Generated Date: 4/24/2012

Site Name:

Sample Number:	342	Type: R	Area:	5,000.05SqFt	PCI = 49
Sample Comments:					
43 BLOCK CR			M	1,100.00 SqFt	Comments:
43 BLOCK CR			L	1,900.00 SqFt	Comments:
52 WEATH/RAVEL			L	2,400.00 SqFt	Comments:
48 L & T CR			L	133.00 Ft	Comments:

Sample Number:	348	Type: R	Area:	5,000.05SqFt	PCI = 45
Sample Comments:					
43 BLOCK CR			M	1,150.00 SqFt	Comments:
43 BLOCK CR			L	2,600.00 SqFt	Comments:
52 WEATH/RAVEL			L	3,200.00 SqFt	Comments:
48 L & T CR			L	205.00 Ft	Comments:

Sample Number:	352	Type: R	Area:	4,999.94SqFt	PCI = 45
Sample Comments:					
43 BLOCK CR			M	2,200.00 SqFt	Comments:
43 BLOCK CR			L	1,450.00 SqFt	Comments:
52 WEATH/RAVEL			L	2,150.00 SqFt	Comments:
48 L & T CR			L	88.00 Ft	Comments:

Sample Number:	356	Type: R	Area:	4,999.94SqFt	PCI = 43
Sample Comments:					
43 BLOCK CR			M	2,750.00 SqFt	Comments:
43 BLOCK CR			L	1,550.00 SqFt	Comments:
52 WEATH/RAVEL			L	2,150.00 SqFt	Comments:
48 L & T CR			L	57.00 Ft	Comments:

Sample Number:	359	Type: R	Area:	5,000.05SqFt	PCI = 44
Sample Comments:					
43 BLOCK CR			M	2,150.00 SqFt	Comments:
43 BLOCK CR			L	1,500.00 SqFt	Comments:
52 WEATH/RAVEL			L	2,650.00 SqFt	Comments:
48 L & T CR			L	67.00 Ft	Comments:

Sample Number:	372	Type: R	Area:	5,000.05SqFt	PCI = 41
Sample Comments:					
43 BLOCK CR			M	1,500.00 SqFt	Comments:
43 BLOCK CR			L	2,800.00 SqFt	Comments:
50 PATCHING			L	0.20 SqFt	Comments:
52 WEATH/RAVEL			L	3,450.00 SqFt	Comments:
48 L & T CR			L	75.00 Ft	Comments:

Sample Number:	376	Type: R	Area:	5,000.05SqFt	PCI = 44
Sample Comments:					
43 BLOCK CR			M	1,100.00 SqFt	Comments:
43 BLOCK CR			L	3,400.00 SqFt	Comments:
48 L & T CR			L	54.00 Ft	Comments:
52 WEATH/RAVEL			L	3,100.00 SqFt	Comments:

Sample Number:	382	Type: R	Area:	5,000.05SqFt	PCI = 45
Sample Comments:					
43 BLOCK CR			M	1,400.00 SqFt	Comments:
43 BLOCK CR			L	2,900.00 SqFt	Comments:
52 WEATH/RAVEL			L	2,150.00 SqFt	Comments:
48 L & T CR			L	113.00 Ft	Comments:

Re-inspection Report

FDOT

Report Generated Date: 4/24/2012

Site Name:

Sample Number: 386	Type: R	Area:	5,000.05SqFt	PCI = 41
Sample Comments:				
43 BLOCK CR		M	2,200.00 SqFt	Comments:
43 BLOCK CR		L	2,700.00 SqFt	Comments:
52 WEATH/RAVEL		L	2,600.00 SqFt	Comments:
48 L & T CR		L	94.00 Ft	Comments:

Sample Number: 390	Type: R	Area:	5,000.05SqFt	PCI = 39
Sample Comments:				
43 BLOCK CR		M	3,100.00 SqFt	Comments:
43 BLOCK CR		L	1,800.00 SqFt	Comments:
52 WEATH/RAVEL		M	16.00 SqFt	Comments:
52 WEATH/RAVEL		L	2,900.00 SqFt	Comments:

Sample Number: 393	Type: R	Area:	5,000.05SqFt	PCI = 42
Sample Comments:				
43 BLOCK CR		M	2,900.00 SqFt	Comments:
43 BLOCK CR		L	2,100.00 SqFt	Comments:
52 WEATH/RAVEL		L	3,200.00 SqFt	Comments:

Re-inspection Report

FDOT

Report Generated Date: 4/24/2012

Site Name:

Network: X60

Name: WILLISTON MUNICIPAL AIRPORT

Branch: RW 14-32

Name: RUNWAY 14-32

Use: RUNWAY

Area: 735,300.00SqFt

Section: 6220 of 6 From: - To: - Last Const.: 1/1/1942
Surface: AC Family: FDOT-GA-RW-AC Zone: Category: Rank: P
Area: 215,000.00SqFt Length: 8,600.00Ft Width: 25.00Ft
Shoulder: Street Type: Grade: 0.00 Lanes: 0
Section Comments:

Last Insp. Date: 2/14/2012 Total Samples: 46 Surveyed: 8

Conditions: PCI: 14.00

Inspection Comments:

Sample Number: 120 Type: R Area: 5,000.05SqFt PCI = 14

Sample Comments:

43 BLOCK CR	M	2,200.00	SqFt	Comments:
43 BLOCK CR	L	2,800.00	SqFt	Comments:
52 WEATH/RAVEL	H	400.00	SqFt	Comments:
52 WEATH/RAVEL	M	4,600.00	SqFt	Comments:

Sample Number: 140 Type: R Area: 5,000.05SqFt PCI = 16

Sample Comments:

43 BLOCK CR	M	950.00	SqFt	Comments:
43 BLOCK CR	L	4,050.00	SqFt	Comments:
52 WEATH/RAVEL	H	400.00	SqFt	Comments:
52 WEATH/RAVEL	M	4,600.00	SqFt	Comments:

Sample Number: 172 Type: R Area: 5,000.05SqFt PCI = 6

Sample Comments:

43 BLOCK CR	M	4,200.00	SqFt	Comments:
43 BLOCK CR	L	800.00	SqFt	Comments:
52 WEATH/RAVEL	H	1,200.00	SqFt	Comments:
52 WEATH/RAVEL	M	3,800.00	SqFt	Comments:

Sample Number: 188 Type: R Area: 5,000.05SqFt PCI = 7

Sample Comments:

43 BLOCK CR	M	5,000.00	SqFt	Comments:
52 WEATH/RAVEL	H	1,800.00	SqFt	Comments:
52 WEATH/RAVEL	M	3,200.00	SqFt	Comments:

Sample Number: 512 Type: R Area: 5,000.05SqFt PCI = 37

Sample Comments:

43 BLOCK CR	M	5,000.00	SqFt	Comments:
52 WEATH/RAVEL	M	400.00	SqFt	Comments:
52 WEATH/RAVEL	L	4,600.00	SqFt	Comments:

Sample Number: 528 Type: R Area: 5,000.05SqFt PCI = 11

Sample Comments:

43 BLOCK CR	M	3,500.00	SqFt	Comments:
43 BLOCK CR	L	1,500.00	SqFt	Comments:
52 WEATH/RAVEL	H	400.00	SqFt	Comments:
52 WEATH/RAVEL	M	4,600.00	SqFt	Comments:

Sample Number: 552 Type: R Area: 5,000.05SqFt PCI = 16

Sample Comments:

43 BLOCK CR	M	1,500.00	SqFt	Comments:
43 BLOCK CR	L	3,500.00	SqFt	Comments:

Re-inspection Report

FDOT
Report Generated Date: 4/24/2012
Site Name:

52	WEATH/RAVEL	H	400.00	SqFt	Comments:
52	WEATH/RAVEL	M	4,600.00	SqFt	Comments:

Sample Number: 584 Type: R Area: 5,000.05SqFt PCI = 8

Sample Comments:					
52	WEATH/RAVEL	H	900.00	SqFt	Comments:
52	WEATH/RAVEL	M	4,100.00	SqFt	Comments:
43	BLOCK CR	M	3,500.00	SqFt	Comments:
43	BLOCK CR	L	1,500.00	SqFt	Comments:

Re-inspection Report

FDOT

Report Generated Date: 4/24/2012

Site Name:

Network: X60 Name: WILLISTON MUNICIPAL AIRPORT

Branch: RW 14-32 Name: RUNWAY 14-32 Use: RUNWAY Area: 735,300.00SqFt

Section: 6235 of 6 From: - To: - Last Const.: 1/1/1942
Surface: PCC Family: FDOT-GA-PCC Zone: Category: Rank: P
Area: 30,000.00SqFt Length: 300.00Ft Width: 100.00Ft
Shoulder: Street Type: Grade: 0.00 Lanes: 0
Section Comments:

Last Insp. Date: 2/14/2012 Total Samples: 6 Surveyed: 2

Conditions: PCI: 60.00

Inspection Comments:

Sample Number: 397 Type: R Area: 14.52 Count PCI = 71

Sample Comments:

65 JT SEAL DMG	H	16.00	Count	Comments:
75 CORNER SPALL	L	1.00	Count	Comments:
74 JOINT SPALL	M	1.00	Count	Comments:
74 JOINT SPALL	L	1.00	Count	Comments:
70 SCALING	L	9.00	Count	Comments:

Sample Number: 399 Type: R Area: 14.52 Count PCI = 50

Sample Comments:

65 JT SEAL DMG	H	16.00	Count	Comments:
74 JOINT SPALL	L	3.00	Count	Comments:
63 LINEAR CR	L	2.00	Count	Comments:
63 LINEAR CR	M	2.00	Count	Comments:
74 JOINT SPALL	M	1.00	Count	Comments:
75 CORNER SPALL	L	1.00	Count	Comments:
70 SCALING	L	6.00	Count	Comments:

Re-inspection Report

FDOT

Report Generated Date: 4/24/2012

Site Name:

Network: X60 Name: WILLISTON MUNICIPAL AIRPORT

Branch: RW 14-32 Name: RUNWAY 14-32 Use: RUNWAY Area: 735,300.00SqFt

Section: 6240 of 6 From: - To: - Last Const.: 1/1/1942
Surface: PCC Family: FDOT-GA-PCC Zone: Category: Rank: P
Area: 15,000.00SqFt Length: 600.00Ft Width: 25.00Ft
Shoulder: Street Type: Grade: 0.00 Lanes: 0
Section Comments:

Last Insp. Date: 2/14/2012 Total Samples: 4 Surveyed: 2

Conditions: PCI: 66.00

Inspection Comments:

Sample Number: 200 Type: R Area: 3.63 Count PCI = 40

Sample Comments:

65 JT SEAL DMG	H	4.00	Count	Comments:
63 LINEAR CR	M	1.00	Count	Comments:
63 LINEAR CR	L	3.00	Count	Comments:
73 SHRINKAGE CR	L	1.00	Count	Comments:
70 SCALING	L	2.00	Count	Comments:

Sample Number: 596 Type: R Area: 10.89 Count PCI = 74

Sample Comments:

65 JT SEAL DMG	H	12.00	Count	Comments:
74 JOINT SPALL	L	2.00	Count	Comments:
70 SCALING	L	8.00	Count	Comments:

Re-inspection Report

FDOT

Report Generated Date: 4/24/2012

Site Name:

Network: X60 Name: WILLISTON MUNICIPAL AIRPORT

Branch: RW 5-23 Name: RUNWAY 5-23 Use: RUNWAY Area: 562,750.00SqFt

Section: 6105 of 5 From: - To: - Last Const.: 1/1/1942
Surface: PCC Family: FDOT-GA-PCC Zone: Category: Rank: T
Area: 30,000.00SqFt Length: 300.00Ft Width: 100.00Ft
Shoulder: Street Type: Grade: 0.00 Lanes: 0
Section Comments:

Last Insp. Date: 2/15/2012 Total Samples: 6 Surveyed: 1

Conditions: PCI: 47.00

Inspection Comments:

Sample Number: 301 Type: R Area: 14.52 Count PCI = 47

Sample Comments:

65 JT SEAL DMG	M	16.00	Count	Comments:
63 LINEAR CR	M	5.00	Count	Comments:
70 SCALING	L	9.00	Count	Comments:
63 LINEAR CR	L	1.00	Count	Comments:

Re-inspection Report

FDOT

Report Generated Date: 4/24/2012

Site Name:

Network: X60 Name: WILLISTON MUNICIPAL AIRPORT

Branch: RW 5-23 Name: RUNWAY 5-23 Use: RUNWAY Area: 562,750.00SqFt

Section: 6110 of 5 From: - To: - Last Const.: 1/1/1942
Surface: PCC Family: FDOT-GA-PCC Zone: Category: Rank: P
Area: 15,000.00SqFt Length: 600.00Ft Width: 25.00Ft
Shoulder: Street Type: Grade: 0.00 Lanes: 0
Section Comments:

Last Insp. Date: 2/15/2012 Total Samples: 4 Surveyed: 1
Conditions: PCI: 39.00 |
Inspection Comments:

Sample Number: 500 Type: R Area: 14.82 Count PCI = 39

Sample Comments:

65 JT SEAL DMG	H	16.00	Count	Comments:
63 LINEAR CR	M	8.00	Count	Comments:
67 LARGE PATCH	L	1.00	Count	Comments:
70 SCALING	L	11.00	Count	Comments:

Re-inspection Report

FDOT

Report Generated Date: 4/24/2012

Site Name:

Network: X60 Name: WILLISTON MUNICIPAL AIRPORT

Branch: TW A Name: TAXIWAY A Use: TAXIWAY Area: 89,994.00SqFt

Section: 205 of 3 From: - To: - Last Const.: 1/1/1986
Surface: AC Family: FDOT-GA-TW-AC Zone: Category: Rank: P
Area: 72,808.00SqFt Length: 1,990.00Ft Width: 35.00Ft
Shoulder: Street Type: Grade: 0.00 Lanes: 0
Section Comments:

Last Insp. Date: 2/15/2012 Total Samples: 21 Surveyed: 3

Conditions: PCI: 66.00

Inspection Comments:

Sample Number: 104	Type: R	Area: 3,496.87SqFt	PCI = 60
Sample Comments: tw A			
48 L & T CR	M	32.00 Ft	Comments:
48 L & T CR	L	254.00 Ft	Comments:
52 WEATH/RAVEL	M	40.00 SqFt	Comments:
52 WEATH/RAVEL	L	3,060.00 SqFt	Comments:

Sample Number: 108	Type: R	Area: 3,499.99SqFt	PCI = 66
Sample Comments: tw A			
48 L & T CR	L	185.00 Ft	Comments:
52 WEATH/RAVEL	M	52.00 SqFt	Comments:
52 WEATH/RAVEL	L	2,600.00 SqFt	Comments:

Sample Number: 119	Type: R	Area: 3,499.99SqFt	PCI = 71
Sample Comments: tw a			
50 PATCHING	L	0.25 SqFt	Comments:
48 L & T CR	L	200.00 Ft	Comments:
52 WEATH/RAVEL	L	2,100.00 SqFt	Comments:

Re-inspection Report

FDOT

Report Generated Date: 4/24/2012

Site Name:

Network: X60 Name: WILLISTON MUNICIPAL AIRPORT

Branch: TW A Name: TAXIWAY A Use: TAXIWAY Area: 89,994.00SqFt

Section: 210 of 3 From: - To: - Last Const.: 1/1/2009
Surface: AAC Family: FDOT-GA-TW-AAC Zone: Category: Rank: P
Area: 10,709.00SqFt Length: 201.00Ft Width: 35.00Ft
Shoulder: Street Type: Grade: 0.00 Lanes: 0
Section Comments:

Last Insp. Date: 2/14/2012 Total Samples: 3 Surveyed: 1
Conditions: PCI: 98.00
Inspection Comments:

Sample Number: 123 Type: R Area: 3,500.00SqFt PCI = 98
Sample Comments:
50 PATCHING L 0.75 SqFt Comments:

Re-inspection Report

FDOT

Report Generated Date: 4/24/2012

Site Name:

Network: X60 Name: WILLISTON MUNICIPAL AIRPORT

Branch: TW A Name: TAXIWAY A Use: TAXIWAY Area: 89,994.00SqFt

Section: 215 of 3 From: - To: - Last Const.: 1/1/2009
Surface: AAC Family: FDOT-GA-TW-AAC Zone: Category: Rank: P
Area: 6,477.00SqFt Length: 170.00Ft Width: 35.00Ft
Shoulder: Street Type: Grade: 0.00 Lanes: 0
Section Comments:

Last Insp. Date: 2/14/2012 Total Samples: 2 Surveyed: 1
Conditions: PCI: 95.00
Inspection Comments:

Sample Number: 116	Type: R	Area: 3,300.00SqFt	PCI = 95
Sample Comments:			
45 DEPRESSION	L	12.00 SqFt	Comments:
52 WEATHERING/RAVELING	L	30.00 SqFt	Comments:

Re-inspection Report

FDOT

Report Generated Date: 4/24/2012

Site Name:

Network: X60 Name: WILLISTON MUNICIPAL AIRPORT

Branch: TW B Name: TAXIWAY B Use: TAXIWAY Area: 101,472.00SqFt

Section: 305 of 1 From: - To: - Last Const.: 1/1/2009
Surface: AAC Family: FDOT-GA-TW-AAC Zone: Category: Rank: P
Area: 101,472.00SqFt Length: 2,365.00Ft Width: 35.00Ft
Shoulder: Street Type: Grade: 0.00 Lanes: 0
Section Comments:

Last Insp. Date: 2/14/2012 Total Samples: 29 Surveyed: 4

Conditions: PCI: 96.00

Inspection Comments:

Sample Number: 104 Type: R Area: 3,499.89SqFt PCI = 94
Sample Comments: tw B
48 L & T CR L 12.00 Ft Comments:
50 PATCHING L 0.25 SqFt Comments:

Sample Number: 116 Type: R Area: 3,499.99SqFt PCI = 98
Sample Comments: tw b
50 PATCHING L 0.25 SqFt Comments:

Sample Number: 130 Type: R Area: 3,499.99SqFt PCI = 98
Sample Comments: tw b
50 PATCHING L 0.25 SqFt Comments:

Sample Number: 144 Type: R Area: 3,750.04SqFt PCI = 93
Sample Comments:
48 L & T CR L 24.00 Ft Comments:
52 WEATH/RAVEL L 40.00 SqFt Comments:

Re-inspection Report

FDOT

Report Generated Date: 4/24/2012

Site Name:

Network: X60 Name: WILLISTON MUNICIPAL AIRPORT

Branch: TW C Name: TAXIWAY C Use: TAXIWAY Area: 98,945.00SqFt

Section: 105 of 2 From: - To: - Last Const.: 1/1/2009
Surface: AAC Family: FDOT-GA-TW-AAC Zone: Category: Rank: P
Area: 62,453.00SqFt Length: 1,174.00Ft Width: 50.00Ft
Shoulder: Street Type: Grade: 0.00 Lanes: 0
Section Comments:

Last Insp. Date: 2/15/2012 Total Samples: 11 Surveyed: 2
Conditions: PCI: 97.00
Inspection Comments:

Sample Number: 101 Type: R Area: 5,700.89SqFt PCI = 98
Sample Comments: tw c
50 PATCHING L 0.25 SqFt Comments:

Sample Number: 107 Type: R Area: 5,000.05SqFt PCI = 96
Sample Comments: tw c
48 L & T CR L 22.00 Ft Comments:

Re-inspection Report

FDOT

Report Generated Date: 4/24/2012

Site Name:

Network: X60 Name: WILLISTON MUNICIPAL AIRPORT

Branch: TW C Name: TAXIWAY C Use: TAXIWAY Area: 98,945.00SqFt

Section: 115 of 2 From: - To: - Last Const.: 1/1/2009
Surface: AAC Family: FDOT-GA-TW-AAC Zone: Category: Rank: P
Area: 36,492.00SqFt Length: 416.00Ft Width: 70.00Ft
Shoulder: Street Type: Grade: 0.00 Lanes: 0
Section Comments:

Last Insp. Date: 2/14/2012 Total Samples: 10 Surveyed: 1
Conditions: PCI: 87.00
Inspection Comments:

Sample Number: 301 Type: R Area: 4,705.00SqFt PCI = 87
Sample Comments:
52 WEATHERING/RAVELING L 799.99 SqFt Comments:

Re-inspection Report

FDOT

Report Generated Date: 4/24/2012

Site Name:

Network: X60 Name: WILLISTON MUNICIPAL AIRPORT

Branch: TW D Name: TAXIWAY D Use: TAXIWAY Area: 28,750.00SqFt

Section: 505 of 1 From: - To: - Last Const.: 12/25/199
Surface: AC Family: FDOT-GA-TW-AC Zone: Category: Rank: P
Area: 28,750.00SqFt Length: 1,150.00Ft Width: 25.00Ft
Shoulder: Street Type: Grade: 0.00 Lanes: 0
Section Comments:

Last Insp. Date: 2/15/2012 Total Samples: 12 Surveyed: 2

Conditions: PCI: 92.00

Inspection Comments:

Sample Number: 103 Type: R Area: 5,033.10SqFt PCI = 99

Sample Comments:

52 WEATH/RAVEL L 15.00 SqFt Comments:

Sample Number: 108 Type: R Area: 5,000.05SqFt PCI = 85

Sample Comments: tw d

45 DEPRESSION L 120.00 SqFt Comments:

50 PATCHING L 0.25 SqFt Comments:

Re-inspection Report

FDOT

Report Generated Date: 4/24/2012

Site Name:

Network: X60 Name: WILLISTON MUNICIPAL AIRPORT

Branch: TW D1 Name: TAXIWAY D-1 Use: TAXIWAY Area: 52,495.00SqFt

Section: 405 of 1 From: - To: - Last Const.: 1/1/2009
Surface: AAC Family: FDOT-GA-TW-AAC Zone: Category: Rank: P
Area: 52,495.00SqFt Length: 1,384.00Ft Width: 35.00Ft
Shoulder: Street Type: Grade: 0.00 Lanes: 0
Section Comments:

Last Insp. Date: 2/14/2012 Total Samples: 14 Surveyed: 2
Conditions: PCI:100.00
Inspection Comments:

Sample Number: 102 Type: R Area: 3,500.00SqFt PCI = 100
Sample Comments:
<NO DISTRESSES>

Sample Number: 108 Type: R Area: 3,500.00SqFt PCI = 100
Sample Comments:
<NO DISTRESSES>

Re-inspection Report

FDOT

Report Generated Date: 4/24/2012

Site Name:

Network: X60 Name: WILLISTON MUNICIPAL AIRPORT

Branch: TWE Name: TAXIWAY E Use: TAXIWAY Area: 57,345.00SqFt

Section: 705 of 1 From: - To: - Last Const.: 1/1/2009
Surface: AAC Family: FDOT-GA-TW-AAC Zone: Category: Rank: P
Area: 57,345.00SqFt Length: 1,384.00Ft Width: 35.00Ft
Shoulder: Street Type: Grade: 0.00 Lanes: 0
Section Comments:

Last Insp. Date: 2/14/2012 Total Samples: 16 Surveyed: 3
Conditions: PCI: 100.00
Inspection Comments:

Sample Number: 101 Type: R Area: 3,500.00SqFt PCI = 100
Sample Comments:
<NO DISTRESSES>

Sample Number: 106 Type: R Area: 3,500.00SqFt PCI = 100
Sample Comments:
<NO DISTRESSES>

Sample Number: 112 Type: R Area: 3,500.00SqFt PCI = 100
Sample Comments:
<NO DISTRESSES>

Re-inspection Report

FDOT

Report Generated Date: 4/24/2012

Site Name:

Network: X60 Name: WILLISTON MUNICIPAL AIRPORT

Branch: TW F Name: TAXIWAY F Use: TAXIWAY Area: 196,759.00SqFt

Section: 550 of 2 From: - To: - Last Const.: 1/1/1942
Surface: AC Family: FDOT-GA-TW-AC Zone: Category: Rank: P
Area: 185,509.00SqFt Length: 2,450.00Ft Width: 75.00Ft
Shoulder: Street Type: Grade: 0.00 Lanes: 0
Section Comments:

Last Insp. Date: 2/14/2012 Total Samples: 49 Surveyed: 5

Conditions: PCI: 7.00

Inspection Comments:

Sample Number: 105 Type: R Area: 3,500.00SqFt PCI = 9

Sample Comments:

52 WEATHERING/RAVELING	M	3,099.97 SqFt	Comments:
52 WEATHERING/RAVELING	H	400.00 SqFt	Comments:
48 LONGITUDINAL/TRANSVERSE CRACKING	M	285.07 Ft	Comments:
48 LONGITUDINAL/TRANSVERSE CRACKING	H	120.03 Ft	Comments:
43 BLOCK CRACKING	M	699.99 SqFt	Comments:
43 BLOCK CRACKING	H	300.00 SqFt	Comments:

Sample Number: 115 Type: R Area: 3,500.00SqFt PCI = 3

Sample Comments:

48 LONGITUDINAL/TRANSVERSE CRACKING	M	345.09 Ft	Comments:
43 BLOCK CRACKING	M	1,899.98 SqFt	Comments:
43 BLOCK CRACKING	H	600.00 SqFt	Comments:
52 WEATHERING/RAVELING	M	2,969.98 SqFt	Comments:
52 WEATHERING/RAVELING	H	530.00 SqFt	Comments:

Sample Number: 125 Type: R Area: 3,500.00SqFt PCI = 5

Sample Comments:

52 WEATHERING/RAVELING	M	3,049.97 SqFt	Comments:
52 WEATHERING/RAVELING	H	450.00 SqFt	Comments:
43 BLOCK CRACKING	M	2,699.98 SqFt	Comments:
43 BLOCK CRACKING	H	550.00 SqFt	Comments:
48 LONGITUDINAL/TRANSVERSE CRACKING	M	180.05 Ft	Comments:

Sample Number: 135 Type: R Area: 3,500.00SqFt PCI = 8

Sample Comments:

43 BLOCK CRACKING	M	1,299.99 SqFt	Comments:
43 BLOCK CRACKING	H	450.00 SqFt	Comments:
52 WEATHERING/RAVELING	M	3,099.97 SqFt	Comments:
52 WEATHERING/RAVELING	H	400.00 SqFt	Comments:
48 LONGITUDINAL/TRANSVERSE CRACKING	L	27.01 Ft	Comments:
48 LONGITUDINAL/TRANSVERSE CRACKING	M	111.03 Ft	Comments:

Sample Number: 145 Type: R Area: 3,500.00SqFt PCI = 12

Sample Comments:

52 WEATHERING/RAVELING	M	2,799.98 SqFt	Comments:
52 WEATHERING/RAVELING	H	699.99 SqFt	Comments:
48 LONGITUDINAL/TRANSVERSE CRACKING	L	22.01 Ft	Comments:
48 LONGITUDINAL/TRANSVERSE CRACKING	M	273.07 Ft	Comments:
48 LONGITUDINAL/TRANSVERSE CRACKING	H	40.01 Ft	Comments:
43 BLOCK CRACKING	M	400.00 SqFt	Comments:

Re-inspection Report

FDOT

Report Generated Date: 4/24/2012

Site Name:

Network: X60 Name: WILLISTON MUNICIPAL AIRPORT

Branch: TW F Name: TAXIWAY F Use: TAXIWAY Area: 196,759.00SqFt

Section: 555 of 2 From: - To: - Last Const.: 1/1/1942
Surface: PCC Family: FDOT-GA-PCC Zone: Category: Rank: P
Area: 11,250.00SqFt Length: 150.00Ft Width: 75.00Ft
Shoulder: Street Type: Grade: 0.00 Lanes: 0
Section Comments:

Last Insp. Date 1/1/1942 Total Samples: 0 Surveyed: 0

Conditions: PCI:100.00 |

Inspection Comments: Construction/Major M&R inspection record.

Sample Number: Type: Area: 0.00
<NO SAMPLE RECORDS>

Re-inspection Report

FDOT

Report Generated Date: 4/24/2012

Site Name:

Network: X60 Name: WILLISTON MUNICIPAL AIRPORT

Branch: TW G Name: TAXIWAY G Use: TAXIWAY Area: 94,786.00SqFt

Section: 450 of 1 From: - To: - Last Const.: 1/1/1942
Surface: AC Family: FDOT-GA-TW-AC Zone: Category: Rank: P
Area: 94,786.00SqFt Length: 1,173.00Ft Width: 75.00Ft
Shoulder: Street Type: Grade: 0.00 Lanes: 0
Section Comments:

Last Insp. Date: 2/14/2012 Total Samples: 24 Surveyed: 3

Conditions: PCI: 2.00

Inspection Comments:

Sample Number: 105 Type: R Area: 3,750.00SqFt PCI = 6

Sample Comments:

52 WEATHERING/RAVELING	M	3,399.97 SqFt	Comments:
52 WEATHERING/RAVELING	H	350.00 SqFt	Comments:
43 BLOCK CRACKING	M	300.00 SqFt	Comments:
43 BLOCK CRACKING	H	1,099.99 SqFt	Comments:

Sample Number: 113 Type: R Area: 3,750.00SqFt PCI = 1

Sample Comments:

43 BLOCK CRACKING	M	999.99 SqFt	Comments:
43 BLOCK CRACKING	H	1,949.98 SqFt	Comments:
52 WEATHERING/RAVELING	M	2,949.98 SqFt	Comments:
52 WEATHERING/RAVELING	H	799.99 SqFt	Comments:

Sample Number: 119 Type: R Area: 3,750.00SqFt PCI = 0

Sample Comments:

43 BLOCK CRACKING	H	2,749.98 SqFt	Comments:
52 WEATHERING/RAVELING	M	2,749.98 SqFt	Comments:
52 WEATHERING/RAVELING	H	999.99 SqFt	Comments: