

2022

Statewide Airfield Pavement Management Program



Airport Pavement Evaluation Report

BCT - Boca Raton Airport | *District 4*



AVIATION



Florida Department of Transportation

Statewide Airfield Pavement Management Program

Airport Pavement Evaluation Report

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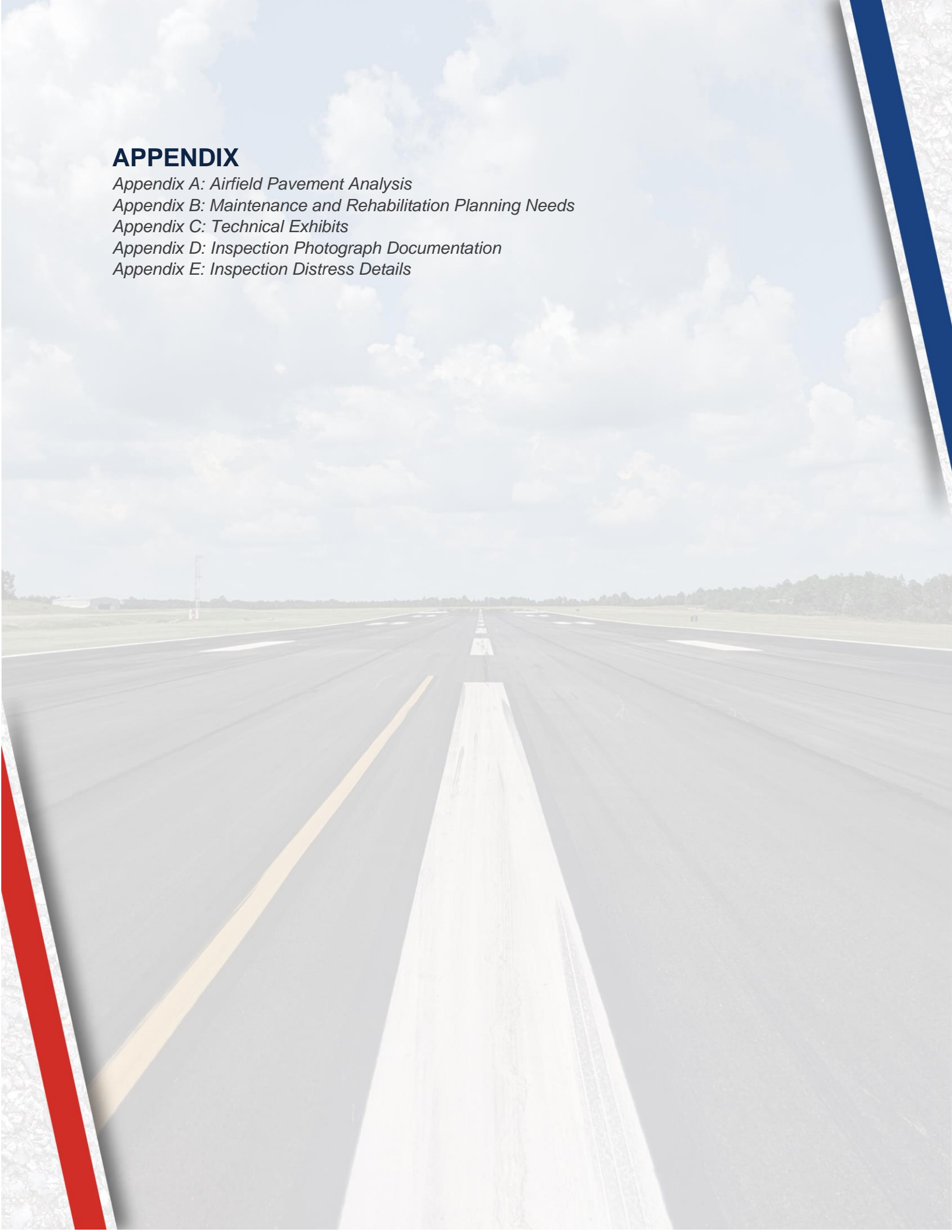
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Executive Summary



Executive Summary

Program Background

The FDOT Aviation Office (AO) has a mission to provide a safe and secure air transportation system that ensures the mobility of people and goods, enhances economic prosperity, and preserves the quality of our environment and communities. As part of ongoing efforts in fulfilling this mission, the Aviation Office is executing a System Update to the Statewide Airfield Pavement Management Program (SAPMP). The scope of the SAPMP encompasses 95 public-use airport facilities distributed throughout the seven (7) participating FDOT Districts. Boca Raton Airport's System Update results are presented in this report and can be utilized by FDOT and the Federal Aviation Administration (FAA) to identify, prioritize, and schedule pavement maintenance, repair, and major rehabilitation projects.

Pavement condition was assessed utilizing the pavement condition index (PCI) methodology as defined in FAA Advisory Circular 150/5380-7B "Airport Pavement Management Program (PMP)" using the procedures documented in ASTM D5340-20 "Standard Test Method for Airport Pavement Condition Index Surveys".

The PCI methodology provides a means for systematically assessing pavement condition and provides an indication of the degree of maintenance, repair, rehabilitation, or reconstruction efforts required to sustain functional pavement conditions. Pavement deterioration, in accordance with ASTM D5340-20, is characterized in terms of distinct distress types, distress severity levels, and quantity of distress. This information is utilized to calculate a PCI value ranging from 0 to 100, which provides an indication of the overall condition of the pavement, with "100" indicating a pavement in new condition and "0" indicating a failed pavement section. This is graphically depicted in **Figure E.1**.

Figure E.1: PCI Rating

Color	Range	Condition Rating
	86-100	Good
	71-85	Satisfactory
	56-70	Fair
	41-55	Poor
	26-40	Very Poor
	11-25	Serious
	0-10	Failed

Current Pavement Conditions

In August 2022, approximately 1.5 million square feet of pavement was assessed as part of the airside pavement network PCI survey at Boca Raton Airport (BCT). In general, airfield pavements at BCT are in Good condition with an area-weighted PCI of 87. The area-weighted average PCI values of the runways, taxiways, and aprons are 88, 85, and 87, respectively. **Figure E.2** and **Table E.1** summarize the current PCI values for BCT.

Figure E.2: Current Condition Summary – Branch-Level

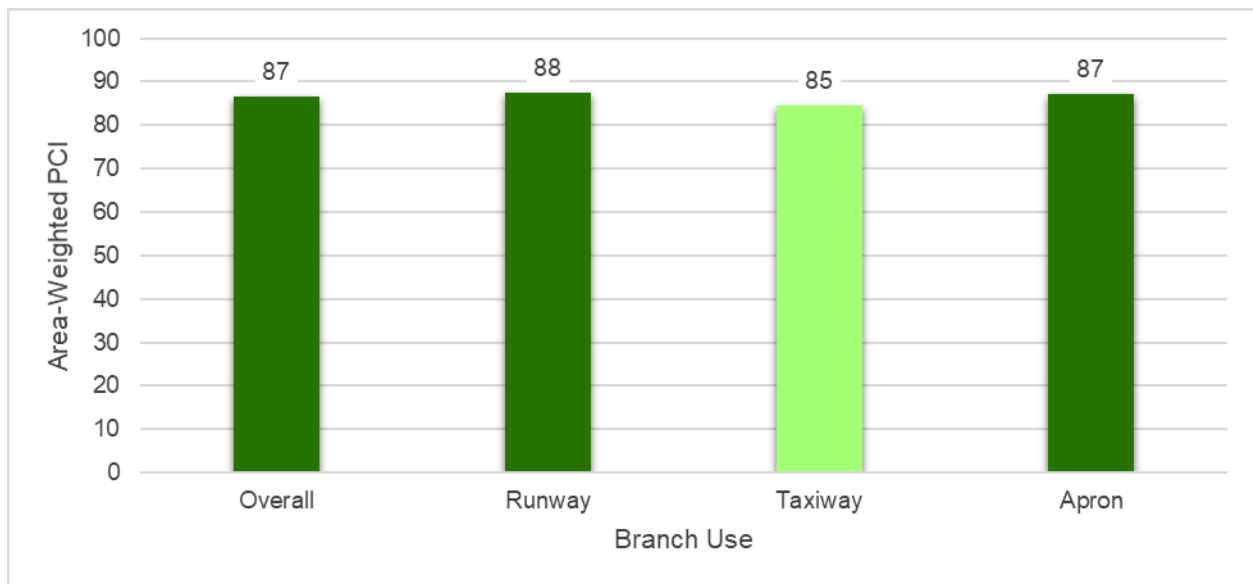


Table E.1: Pavement Condition Index Summary (Current PCI Survey) – Section Level

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	PCI	Condition Rating
BCT	RW 5-23	Runway	6105	520,000	86	Good
BCT	RW 5-23	Runway	6106	72,700	89	Good
BCT	RW 5-23	Runway	6107	35,000	91	Good
BCT	RW 5-23	Runway	6110	260,000	90	Good
BCT	RW 5-23	Runway	6111	36,350	83	Satisfactory
BCT	RW 5-23	Runway	6112	17,500	94	Good
BCT	TW A	Taxiway	130	8,671	81	Satisfactory
BCT	TW B	Taxiway	125	14,426	100	Good
BCT	TW C	Taxiway	120	14,426	100	Good
BCT	TW E	Taxiway	116	14,729	70	Fair
BCT	TW F	Taxiway	115	14,426	100	Good
BCT	TW G	Taxiway	110	8,671	84	Satisfactory
BCT	TW H	Taxiway	111	7,946	78	Satisfactory
BCT	TW P	Taxiway	105	193,060	82	Satisfactory
BCT	TW P	Taxiway	106	29,080	86	Good
BCT	TW P	Taxiway	107	14,241	86	Good
BCT	TW P	Taxiway	108	10,940	79	Satisfactory

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	PCI	Condition Rating
BCT	TW P	Taxiway	112	12,673	82	Satisfactory
BCT	TW P	Taxiway	113	4,000	81	Satisfactory
BCT	TW P	Taxiway	131	12,673	80	Satisfactory
BCT	TW P1	Taxiway	205	4,102	88	Good
BCT	TW P10	Taxiway	240	8,665	89	Good
BCT	TW P11	Taxiway	250	8,665	93	Good
BCT	TW P2	Taxiway	210	2,572	72	Satisfactory
BCT	TW P3	Taxiway	215	1,488	74	Satisfactory
BCT	TW P4	Taxiway	225	8,391	100	Good
BCT	TW P5	Taxiway	230	8,400	87	Good
BCT	TW P6	Taxiway	220	8,315	85	Satisfactory
BCT	TW P7	Taxiway	222	4,994	89	Good
BCT	TW P8	Taxiway	221	5,090	87	Good
BCT	TW P9	Taxiway	260	3,165	81	Satisfactory
BCT	AP CUSTOMS	Apron	4105	82,167	91	Good
BCT	AP RU 23	Apron	5110	21,414	88	Good
BCT	AP RU 5	Apron	5105	26,544	79	Satisfactory
BCT	AP RU 5	Apron	5115	11,787	77	Satisfactory

Forecasted Pavement Conditions

Table E.2 provides section-level details for PCI forecasts. Pavement condition forecasts should be used for planning purposes only, as the actual condition of sections is subject to sensitivities in changes of traffic and maintenance frequency.

The estimation of forecasted PCI values gives no assurance of future pavement conditions as PCI values represent an engineering estimation to be used as a planning tool. Forecasted PCI data should not be the sole metric for determining the year in which a project should be planned. Design-level planning should be undertaken by the responsible engineer prior to the development of airfield design plans.

Table E.2: Forecasted PCI Values 2023-2032 – Section-Level

Network ID	Branch ID	Section ID	Current PCI	2023	2024	2025	2026	2027	2028	2029	2030	2031	2032
BCT	RW 5-23	6105	86	84	83	81	79	78	76	74	72	71	69
BCT	RW 5-23	6106	89	87	86	84	82	81	79	77	75	74	72
BCT	RW 5-23	6107	91	89	88	86	84	83	81	79	77	76	74
BCT	RW 5-23	6110	90	88	87	85	83	82	80	78	76	75	73
BCT	RW 5-23	6111	83	81	80	78	76	75	73	71	69	68	66
BCT	RW 5-23	6112	94	92	91	89	87	86	84	82	80	79	77
BCT	TW A	130	81	79	78	76	75	73	72	71	69	68	67
BCT	TW B	125	100	98	95	93	91	89	87	85	83	82	80
BCT	TW C	120	100	98	95	93	91	89	87	85	83	82	80
BCT	TW E	116	70	69	68	67	66	66	65	64	63	63	62
BCT	TW F	115	100	98	95	93	91	88	86	84	83	81	79
BCT	TW G	110	84	82	81	79	77	76	74	73	71	70	69

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Network ID	Branch ID	Section ID	Current PCI	2023	2024	2025	2026	2027	2028	2029	2030	2031	2032
BCT	TW H	111	78	77	75	74	72	71	70	69	67	66	65
BCT	TW P	105	82	80	79	77	76	74	73	71	70	69	68
BCT	TW P	106	86	84	82	81	79	77	76	74	73	71	70
BCT	TW P	107	86	84	82	81	79	77	76	74	73	71	70
BCT	TW P	108	79	78	76	74	73	72	70	69	68	67	66
BCT	TW P	112	82	80	79	77	76	74	73	71	70	69	68
BCT	TW P	113	81	79	78	76	75	73	72	71	69	68	67
BCT	TW P	131	80	79	77	75	74	73	71	70	69	68	67
BCT	TW P1	205	88	86	84	82	81	79	77	76	74	73	71
BCT	TW P10	240	89	87	85	83	81	80	78	76	75	73	72
BCT	TW P11	250	93	91	89	87	85	83	81	79	78	76	75
BCT	TW P2	210	72	71	70	68	67	66	65	64	63	63	62
BCT	TW P3	215	74	73	71	70	69	68	67	66	65	64	63
BCT	TW P4	225	100	98	95	93	91	88	86	84	83	81	79
BCT	TW P5	230	87	85	83	81	80	78	76	75	74	72	71
BCT	TW P6	220	85	83	81	80	78	76	75	74	72	71	70
BCT	TW P7	222	89	87	85	84	82	80	79	77	76	75	73
BCT	TW P8	221	87	85	84	82	80	79	77	76	75	73	72
BCT	TW P9	260	81	79	78	76	75	73	72	71	69	68	67
BCT	AP CUSTOMS	4105	91	89	87	85	83	81	79	77	75	74	72
BCT	AP RU 23	5110	88	86	84	82	79	77	75	73	71	69	66
BCT	AP RU 5	5105	79	77	75	73	70	68	66	64	62	60	57
BCT	AP RU 5	5115	77	75	74	72	70	69	67	66	65	63	62

Major Rehabilitation Planning 2023-2032

Localized maintenance and repair policies identified within this report are categorized as preventive or stopgap based on FDOT SAPMP and FAA maintenance policies and recommendations. Major rehabilitation is identified within the FDOT SAPMP as a major construction activity that results in a reset of a pavement section's PCI to a value of 100. Major rehabilitation activities can include mill and Asphalt Concrete (AC) overlay, Portland cement concrete (PCC) pavement repair and slab replacement, and full-depth reconstruction. It is recommended that the Airport use this report as a planning tool for future project development and prioritization. Localized maintenance, repair, and major rehabilitation recommendations should be considered as planning-level only. Final localized maintenance, repair, and major rehabilitation recommendations are subject to change based on Airport prioritization and further design-level evaluations.

Due to FAA Order 5100.38D Change 1 Airport Improvement Program (AIP) Handbook (February 26, 2019), a substantial update to the FDOT SAPMP policy on identifying major rehabilitation work has been incorporated in this System Update. In previous System Updates, major rehabilitation had been identified for pavement sections below a PCI Value of 65; however, based on the thresholds identified by the FAA in the AIP Handbook, major rehabilitation will now be identified for pavement sections below a PCI value of 70.

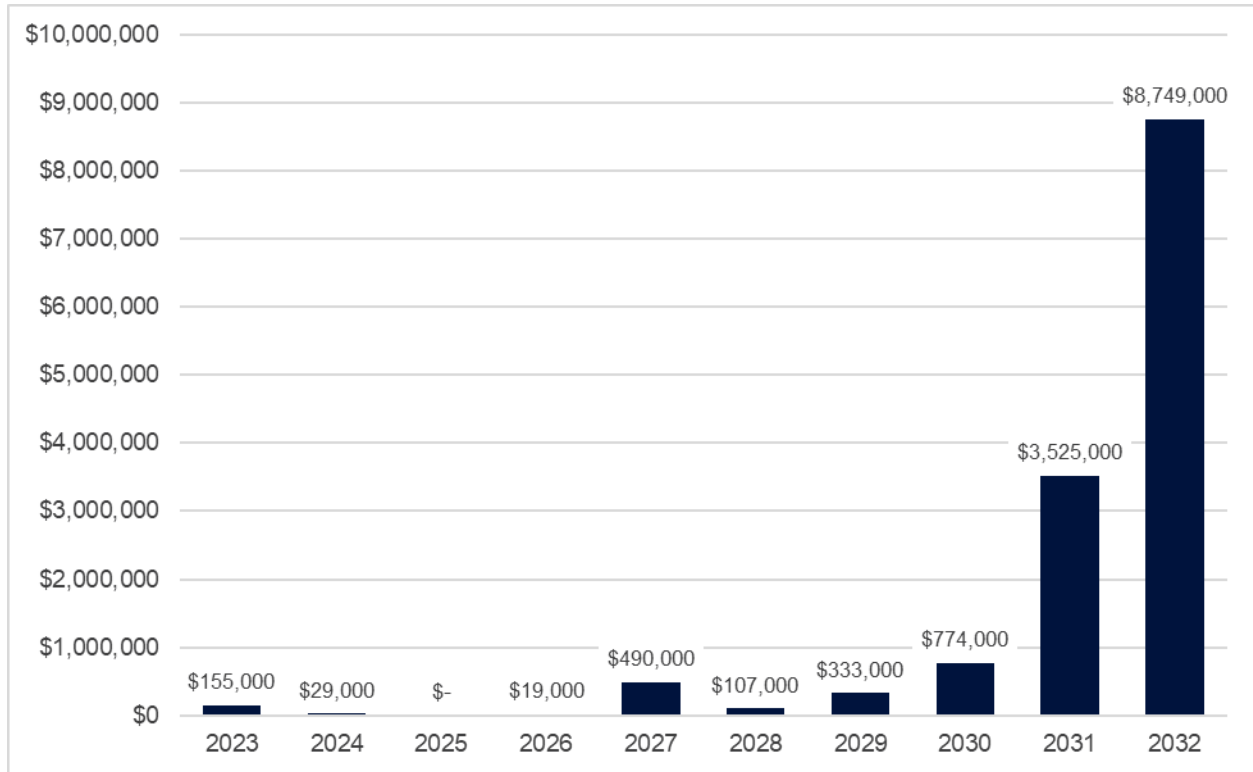
The results of the maintenance, repair, and major rehabilitation analysis identified approximately \$14.18M in major rehabilitation needs for the 10-year forecast period. Year 1 major needs are \$0.16M and localized maintenance needs for Year 1 are \$0.06M.

Table E.3: Major Rehabilitation Planning 2023-2032

Program Year	Network ID	Branch ID	Section ID	Surface	Area (SF)	PCI Before	Rehabilitation Type	Planning Cost Estimate
2023	BCT	TW E	116	AC	14,729	69	AC Rehabilitation	\$ 155,000
2024	BCT	TW P2	210	AAC	2,572	70	AC Rehabilitation	\$ 29,000
2026	BCT	TW P3	215	AAC	1,488	69	AC Rehabilitation	\$ 19,000
2027	BCT	AP RU 5	5105	AAC	26,544	68	AC Rehabilitation	\$ 339,000
2027	BCT	AP RU 5	5115	AC	11,787	69	AC Rehabilitation	\$ 151,000
2028	BCT	TW H	111	AAC	7,946	70	AC Rehabilitation	\$ 107,000
2029	BCT	TW P	108	AAC	10,940	69	AC Rehabilitation	\$ 154,000
2029	BCT	TW P	131	AAC	12,673	70	AC Rehabilitation	\$ 179,000
2030	BCT	RW 5-23	6111	AAC	36,350	69	AC Rehabilitation	\$ 538,000
2030	BCT	TW A	130	AAC	8,671	69	AC Rehabilitation	\$ 129,000
2030	BCT	TW P	113	AAC	4,000	69	AC Rehabilitation	\$ 60,000
2030	BCT	TW P9	260	AAC	3,165	69	AC Rehabilitation	\$ 47,000
2031	BCT	TW P	105	AAC	193,060	69	AC Rehabilitation	\$ 2,995,000
2031	BCT	TW P	112	AAC	12,673	69	AC Rehabilitation	\$ 197,000
2031	BCT	AP RU 23	5110	AAC	21,414	69	AC Rehabilitation	\$ 333,000
2032	BCT	RW 5-23	6105	AAC	520,000	69	AC Rehabilitation	\$ 8,471,000
2032	BCT	TW G	110	AAC	8,671	69	AC Rehabilitation	\$ 142,000
2032	BCT	TW P6	220	AAC	8,315	70	AC Rehabilitation	\$ 136,000

**All planning cost values have been rounded up to the nearest thousand dollars.*

Figure E.3: 10-Year Major Rehabilitation Needs by Program Year





Chapter 1: Introduction



Chapter 1 – Introduction

The State of Florida has 128 public airports, 100 of which are recognized as part of the Federal Aviation Administration’s (FAA) National Plan of Integrated Airport Systems (NPIAS). These public-use airports are vital to Florida’s economy as well as the economy of the United States. The Florida Airport System (FAS) provides opportunities for the State to capitalize on an increasingly global marketplace. Florida’s system of commercial service and general aviation airports are important to businesses throughout the State as air travel is essential to tourism, Florida’s most prominent industry.

1.1 Background

In 1992, the Florida Department of Transportation (FDOT) established the Statewide Airfield Pavement Management Program (SAPMP) to provide program managers, District Aviation Offices, and Airport operators with a system to proactively manage airfield pavement infrastructure within the FAS. The SAPMP includes network-level Pavement Condition Index (PCI) surveys for Airport facilities that are categorized as General Aviation (GA), Reliever (RL), and Primary/Commercial (PR). Currently, the SAPMP includes 95 participating public-use airports with pavement facilities and provides its users with comprehensive data to better manage their pavement assets.

There are millions of square feet of pavement infrastructure at airports across a network of runways, taxiways, aprons, and other areas. This pavement infrastructure is vital to the support and safety of aircraft operations. Timely maintenance, repair, and major rehabilitation of pavement infrastructure allows the Airport to operate safely, efficiently, and economically without excessive down time.

Airports participating in the Airport Improvement Program (AIP) Grant Program are required by the FAA to develop and implement a pavement maintenance program in order to be eligible for funding, per FAA Advisory Circulars 150/5380-6C “Guidelines and Procedures for Maintenance of Airport Pavements” and 150/5380-7B “Airport Pavement Management Program (PMP)”. The AIP program requires detailed assessments of airfield pavements at least once a year for a pavement management program. The frequency of the detailed inspections may be extended to every three years if the pavement is assessed according to the PCI survey procedure described in ASTM D5340-20 “Standard Test Method for Airport Pavement Condition Index Surveys”.

In general, adherence to the FAA Advisory Circulars is mandatory for projects funded with federal grant monies through the AIP program and with revenue from the Passenger Facilities Charges (PFC) Program. Further information is detailed in FAA Grant Assurance No. 11 “Pavement Maintenance,” No. 34 “Policies, Standards, and Specifications,” and PFC Assurance No. 9 “Standards and Specifications.” The FDOT performs the SAPMP System Updates for the benefit of participating public-use and publicly-owned airports through the Aviation Office (AO).

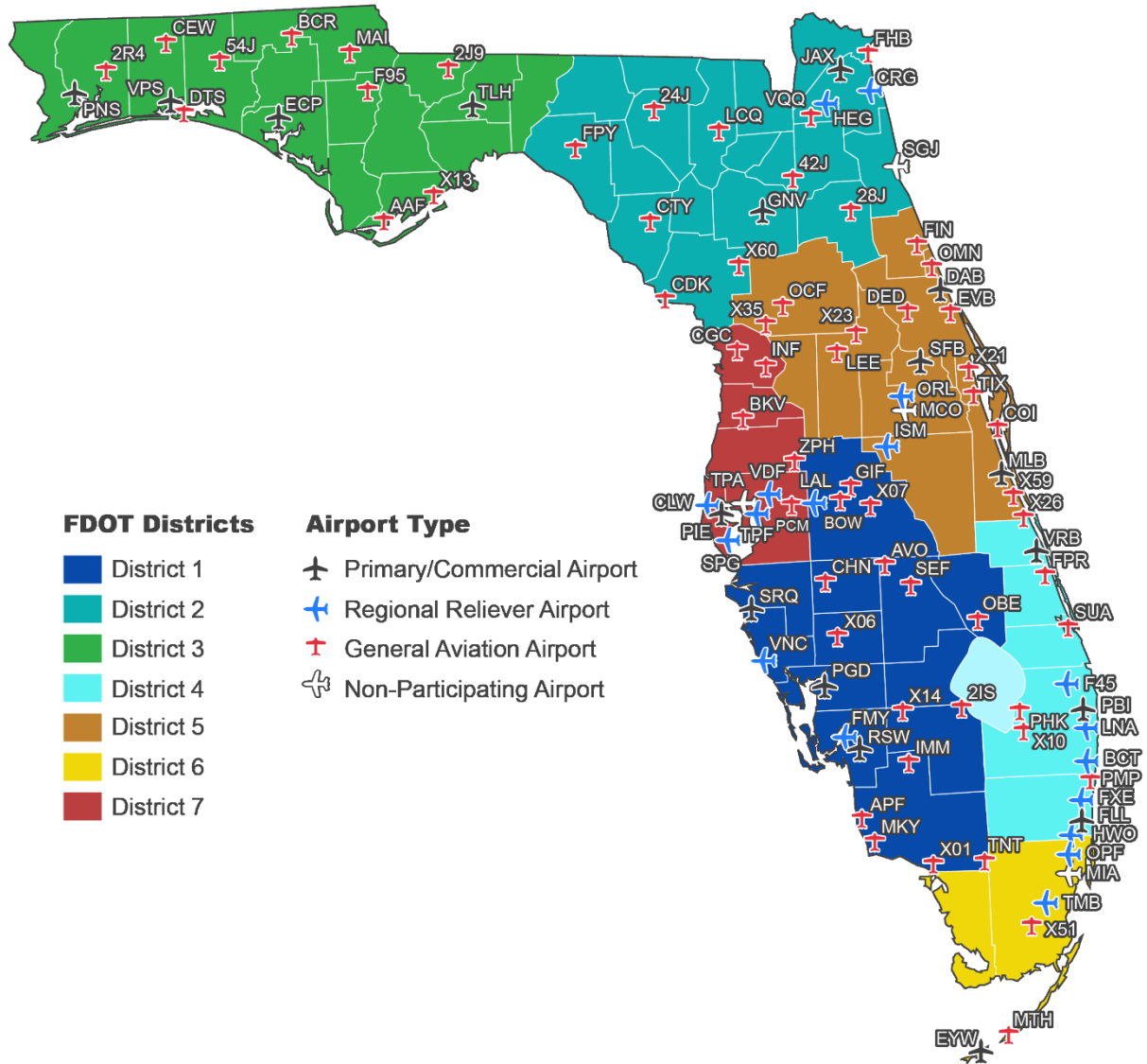
The SAPMP addresses the requirements of maintaining an effective pavement management program for participating airports at the network level. Network-level management of pavement assets provides insight for short-term and long-term budget needs, understanding of the overall condition of the network (current and future), and knowledge of the pavement facilities that are

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under consideration for projects. A network-level evaluation can support the identification of maintenance, repair, and major rehabilitation needs and budgetary planning-level opinions of probable construction costs.

Figure 1.1: Florida Aviation System (Facilities with Pavement) and FDOT Districts



1.2 Stakeholders

The SAPMP is performed for the benefit of the stakeholders. The table below outlines the primary stakeholders of the FDOT SAPMP and their role in the program.

Table 1.2: FDOT SAPMP Stakeholders

Role	Description
FAA Orlando Airports District Office (Orlando ADO)	Key Stakeholder: local ADO Program Manager personnel that oversees the grant administration of AIP grant with Planning Agency Sponsor (Florida Department of Transportation).
Florida Department of Transportation (FDOT)	Key Stakeholder: the FDOT is the "Sponsor" for the AIP grant agreement. Specifically, the Aviation Office (AO) provides development and operations support for the Florida Airport System.
FDOT District Offices	The seven (7) FDOT District Offices, specifically the Aviation representatives, provide essential support to the SAPMP System Update and the AO Program Manager (AO-PM). Each District supports the SAPMP's ongoing efforts by providing local construction cost information throughout the State, which is used as the basis of development for maintenance, repair, and major rehabilitation opinions of probable construction costs for planning purposes.
Participating Public-Use and Publicly-Owned Airports	The airports are the end-user and primary beneficiary of the SAPMP. The SAPMP provides a specific Airport Pavement Evaluation Report that meets the requirements of the FAA AC 150/5380-7B. Individual participating airports are provided a final Airport Pavement Evaluation Report by the Consultant that is specific to each airport's airfield PCI assessment.
Aviation Office Program Manager (AO-PM)	FDOT AO Airport Engineering Manager: oversees and manages the overall Program System Update.

1.3 General Scope of Work

The SAPMP is limited to performing tasks in adherence to the key elements of an effective pavement management program on a statewide level. The primary tasks undertaken to update the FDOT SAPMP include, but are not limited to:

- » Research and evaluation of existing record documentation;
- » Establishment of a pavement system inventory;
- » Development of a pavement network definition map and supplemental GIS model;
- » Functional pavement evaluations via the PCI assessment method;
- » Customization of PAVER™ software including prioritization, policies, and performance models;
- » Analysis of condition data; and
- » Maintenance, repair, and rehabilitation planning.

1.4 FDOT SAPMP Objectives

The SAPMP enables the FDOT AO and FAA to monitor pavement conditions at airports in the Florida Airport System. The SAPMP provides objective condition information needed to make informed decisions regarding the significant capital investment that the public-use airport pavement infrastructure represents.

Airport staff are responsible for making decisions regarding the timing and type of maintenance and rehabilitation activities that should be completed in order to maintain an acceptable operational condition and adequate load-carrying capacity. Utilizing the SAPMP will help Airport staff better understand the relative condition of their pavement facilities and when those facilities should be rehabilitated. The data collected from the SAPMP can be used for project programming for the next 10 years. This report summarizes the data collection, analysis, program update, and implementation of the FDOT SAPMP.

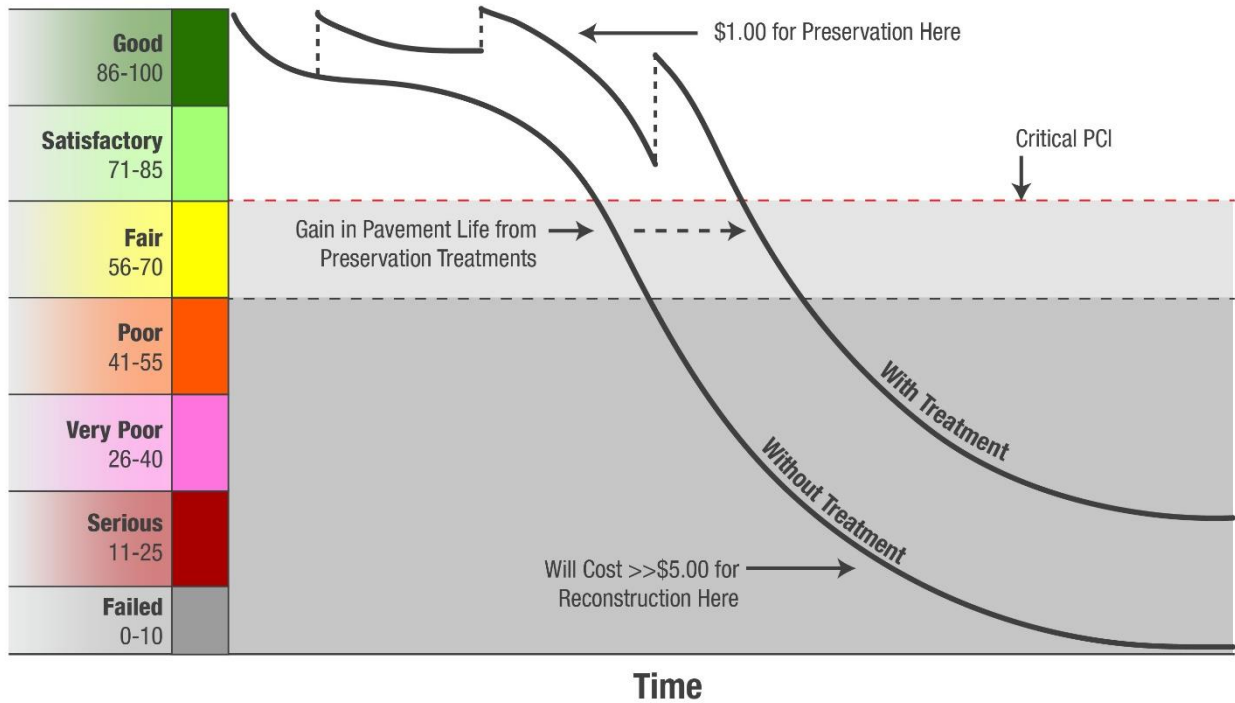
A comprehensive SAPMP provides information that assists with the project programming process. The primary objectives of the FDOT SAPMP consist of the following:

- » Assist airports in meeting the requirements of Public Law 103-305;
- » Assist airports in complying with FAA Grant Assurances 11 and 19;
- » Provide airports with functional pavement condition in accordance with ASTM D5340-20 (current) and with the FAA AC 150/5380-7B (current) based on visual assessment efforts;
- » Provide airports with planning-level guidance on maintenance, repair, and rehabilitation in accordance with the FAA AC 150/5380-6C (current) based on pavement conditions and distress data in terms of type, severity, and extent; and
- » Provide airports, FDOT Districts, FDOT AO, and the FAA Airports District Office with long-term, planning-level forecasts of pavement performance and rehabilitation budgetary needs (e.g., maintenance, repair, and major reconstruction) through reports.

From a pavement management perspective, one of the most valuable aspects of the PCI methodology is the ability to save money by effectively prioritizing the rehabilitation of pavement assets before they reach critical condition. Critical PCI values are assigned to deterioration models for pavement assets based on their respective use and rank. The concept of critical PCI will be further discussed in **Chapter 5**, but it is used as a benchmark to help identify pavement assets that should receive rehabilitation. In doing so, the PCI methodology can help create a proactive maintenance and rehabilitation (M&R) strategy to effectively address pavement projects before the cost of these projects increases significantly.

With M&R costs escalating over time, the consequences of inadequate maintenance practices can result in an inefficient allocation of funding. If maintenance is conducted before a significant decline in pavement condition occurs, substantial repair and/or rehabilitation costs may be avoided or delayed. **Figure 1.4** illustrates how the cost of pavement repairs can significantly increase if M&R activities are delayed.

Figure 1.4: Pavement Life and the Effect of Treatments



FAA Eligibility Thresholds: ☐ >70: Routine Maintenance ☐ 55-70: Rehabilitation Eligible ☐ <55: Reconstruction Eligible

*Figure is for conceptual purposes only – unit costs are not specific to airfield pavements



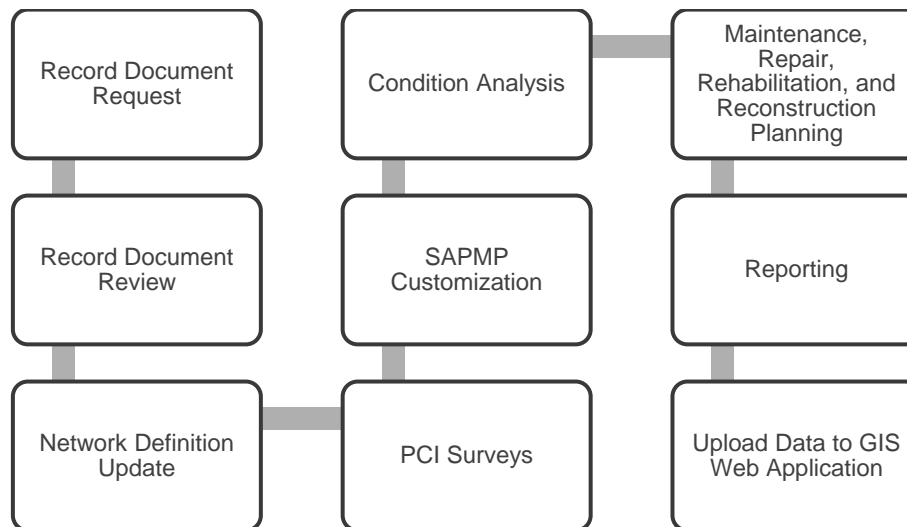
Chapter 2: Methodology



Chapter 2 – Methodology

An effective pavement management program incorporates both the regular collection of pavement condition information and communication of information to appropriate sponsors. This chapter of the report defines the specific methods utilized as part of the SAPMP System Update to meet the requirements of an effective pavement management system as defined by the FAA AC 150/5380-7B. **Figure 2** summarizes the overall process for the FDOT SAPMP.

Figure 2: FDOT SAPMP General Process



2.1 Airfield Pavement Database

This SAPMP utilizes PAVER™ 7.0 software as its airfield pavement database. The PAVER™ software application was developed by the U.S. Army Construction Engineering Research Laboratory and sponsored by the FAA, Federal Highway Administration, U.S. Army, U.S. Air Force, and U.S. Navy to meet the objectives of an effective pavement management system. The PAVER™ database includes a network-level inventory of the participating airport's eligible airfield pavement facilities. PAVER™ can achieve the following pavement management objectives:

- » Create a manageable inventory system;
- » Analyze the current condition of pavements in accordance with ASTM D5340-20;
- » Develop pavement performance models to forecast conditions; and
- » Generate maintenance, repair, and major rehabilitation recommendations based on budgetary scenarios.

PAVER™ inventory management is based on a tiered organizational structure consisting of networks, branches, sections, and samples, with the sample being the smallest unit of management. Critical elements of an effective pavement management program are maintained within the network-level PAVER™ database and typically consist of pavement inventory

characteristics, pavement structure, work history, historic condition records, and analytical customization.

2.2 Airfield Pavement Record Keeping (Historical Records Research)

In accordance with the FAA AC 150/5380-7B, it is a best practice that airports maintain records of all airfield construction and maintenance (routine, emergency, and proactive) related to the pavement facilities. These records should consist of:

- » Location and limits of work;
- » Types and severities of repaired distresses;
- » Work type and cost; and
- » Supporting documents (e.g., contract documents, construction drawings, specifications, bid tabulations, repair products, and photograph records).

As part of the SAPMP, participating airport's staff was asked to provide documentation regarding the historical work performed at the Airport, including construction drawings and bid tabulations. This information is used to identify location, limits, type of work, pavement cross-sections, and representative material costs.

Updated historical data collected during this task was entered into the PAVER™ database. This database includes the following fields for historical information:

- » Date of last construction/rehabilitation
- » Work type performed
- » Comments for documenting pavement cross-section
- » Pavement surface type
- » Section area (limits of work)

The SAPMP PAVER™ database accuracy is limited to the record documentation provided by the participating airports. Airport Sponsors should rely on this information as a planning tool and defer to final as-built plans, record drawings, and/or engineer's construction report for pavement construction records.

2.3 Airfield Pavement Structure

A pavement is a prepared surface designed to provide a continuous, smooth ride at a certain speed and to support an estimated amount of traffic for a certain number of years. A pavement structure is composed of constructed layers consisting of subgrade, subbase, base, structural, and surface courses. For the FDOT SAPMP, two (2) predominant pavement types are classified for evaluation and analysis: Asphalt Concrete (AC) and Portland cement concrete (PCC). Composite Structures, known as Whitetopping Pavements consisting of PCC on AC, are also present at limited airports in Florida and are evaluated separately.

2.3.1 Asphalt Concrete

Asphalt concrete is a pavement comprised of aggregate mixture with an asphalt cement binder. The FDOT SAPMP categorizes three (3) Asphalt Concrete surface types: Asphalt Concrete (AC), Asphalt Concrete overlaid on Asphalt Concrete (AAC), and Asphalt Concrete overlaid on Portland cement concrete (APC).

Asphalt Concrete (AC)

A flexible pavement section consisting of aggregate mixture with asphalt cement binder layered on engineered base course material that is layered on subbase and subgrade soil material.

Asphalt Concrete Overlaid on Asphalt Concrete (AAC)

A flexible pavement section consisting of aggregate mixture with asphalt cement binder layered on an existing flexible AC pavement section. Airfield pavement sections are considered to be AAC when a pavement rehabilitation includes a pavement milling and resurfacing operation or a direct overlay of Asphalt Concrete without surface preparation.

Asphalt Concrete Overlaid on Portland Cement Concrete (APC)

A flexible pavement section consisting of aggregate mixture with asphalt cement binder layered on an existing PCC pavement section. This unique pavement composition may result in distinct pavement distress manifestations known as reflective joint cracking.

2.3.2 Portland Cement Concrete

Portland cement concrete is a pavement comprised of aggregate mixture with a Portland cement binder. The FDOT SAPMP categorizes Portland cement concrete (PCC) as the primary rigid pavement section.

Portland Cement Concrete (PCC)

A rigid pavement section composed of Portland cement concrete placed on a granular or treated base course that is supported on a compacted subgrade. The concrete surface provides a texture of nonskid qualities, prevents the infiltration of surface water into the subgrade, and provides structural support for airplane loading. Rigid pavement construction requires the layout of appropriately designed joints. Concrete overlays built in accordance with the FAA Advisory Circular 150/5320-6F "Airport Pavement Design and Evaluation" are recognized as PCC pavement.

2.3.3 Composite Structure – Whitetopping Pavement

Whitetopping pavement is a composite pavement comprised of relatively thin PCC overlaid on an existing AC pavement structure. There are three (3) types of Whitetopping Pavements: Conventional (WT), Thin (TWT), and Ultra-Thin (UWT).

Conventional Whitetopping (WT)

A composite pavement structure consisting of a modified PCC overlaid on an existing AC pavement section. The modified PCC layer is typically greater than 6 inches in thickness.

Thin Whitetopping (TWT)

A composite pavement structure consisting of modified PCC overlaid on an existing AC pavement section. The modified PCC layer is typically between 4 and 6 inches in thickness.

Ultra-Thin Whitetopping (UWT)

A composite pavement structure consisting of a modified PCC overlaid on an existing AC pavement section. The modified PCC layer is typically between 2 and 4 inches in thickness.

2.4 Airfield Pavement Traffic

A pavement section is typically designed to meet the needs of the user (airlines, air cargo, general aviation, and/or military) in providing a safe, smooth, operational surface. Pavement deterioration generally occurs gradually from aircraft loading and environmental conditions.

This System Update does not involve a study or analysis of BCT's aircraft fleet mix or traffic operations. However, it is strongly recommended that the Airport incorporate the requirements of the FAA AC 150/5320-6F when developing design-level rehabilitation activities; this AC provides guidance on incorporation of aircraft traffic fleet mix data.

2.5 Pavement Management Program Network Definition Terminology

To facilitate an effective pavement management program, a pavement network must be established and subdivided into smaller, manageable working units. Sectioning of the pavement network was established in a prior System Update and was revised during this SAPMP to account for work that has been performed on the airfield since the previous Update. Information from historic records is used to help define the limits of the smaller working units. A critical input for a pavement inventory and network definition is the date of last major construction or rehabilitation, as this type of work will reset the section PCI to a value of 100.

The following sections define the common terms used in pavement management systems and cover their application for this SAPMP System Update.

2.5.1 Pavement Network Identification

Establishing the pavement network is the first step in organizing pavements into a structure for pavement management. The network is the starting point of the hierarchy of pavement management organization. A network typically consists of one or more pavement *branches*, which have one or more pavement *sections*. For example, a network can be all the pavements within an Airport's airfield or all the pavements in a statewide program. For the FDOT SAPMP, a network represents an individual Airport's airfield pavement facilities maintained by the Airport.

2.5.2 Pavement Branch Identification

A pavement branch, also known as a facility, is a logical unit of generally identifiable pavement within a network that has a distinct functional classification. For example, within an airfield, each runway, taxiway, or apron is considered a branch. Each branch contains at least one section but may contain more if pavement feature characteristics are distinct throughout the branch.

2.5.3 Pavement Section Identification

A pavement section, or feature, is a subdivision of a branch and has consistent characteristics throughout its length or area. These characteristics include structural composition (pavement layer material type and thickness), construction history, age, traffic type, traffic frequency, and pavement condition. A section is the basic management unit of a pavement network and is the level at which maintenance, repair, or major rehabilitation treatments are considered.

2.5.4 Pavement Sample Unit Identification

A pavement sample unit is an arbitrarily defined subdivision of a pavement section that has a standard size range of 20 contiguous slabs (± 8 slabs) for PCC pavement and 5,000 contiguous square feet ($\pm 2,000$ SF) for AC. A sample unit is the smallest subdivision of a pavement network and is analyzed during field assessments to establish condition ratings.

2.5.5 Terminology Summary

Below is a summary table, **Table 2.5.5**, with definitions and examples of common SAPMP terminology.

Table 2.5.5: SAPMP Terminology

SAPMP Terminology	Common Definition	Airport Example
Network	Totality of pavement assets maintained by the Airport.	"Tallahassee International Airport – Airfield Pavements"
Branch Name	Commonly defined asset name as established by Airport and by use.	"Runway 18-36"
Branch ID	Codified shorthand name for commonly defined asset established for database identification.	"RW 18-36" RW, Branch Use, "Runway" "Runway 18-36", Runway Facility
Section ID	Codified identification for pavement asset that is distinct by pavement composition, work history, aircraft loading, or condition.	"6105"
Sample Unit	A numeric identification of an area of pavement ($5,000 \pm 2,000$ SF of AC or 20 ± 8 slabs of PCC) that has been inspected in accordance with ASTM D5340-20.	"300"

2.6 Airfield PCI Survey Methodology

In adherence to the FAA AC 150/5380-7B, the FDOT SAPMP utilizes the PCI survey method to collect pavement distress data and analyze the condition. The PCI survey procedure is a visual statistical sampling of pavements for recording primary distress types (e.g., cracking and deformation), associated severities, and quantities as defined by the ASTM D5340-20. This effort is the primary means of obtaining and recording pavement distress data. The PCI survey consists primarily of visual assessments of pavement surfaces for signs of distress and deterioration resulting from loading (aircraft) and environmental influences.

Overall, a visual pavement condition survey provides an indication of the cause and rate of deterioration of a pavement section from a functional point of view and can help identify if any underlying structural deficiencies are present. Although a visual PCI survey does not predict the remaining structural life of a pavement section or its ability to support loads, it does assess the rating of the operational surface. Functional condition, determined by the PCI method, can provide a cost-effective means to plan for pavement rehabilitation projects. Timely application of pavement rehabilitation may lead to the extension of functional life of individual pavement sections. This method varies from structural evaluation; functional condition is limited to visually observed distresses and indicative modes of pavement deterioration. A formal structural evaluation analyzes subsurface conditions, material characteristics, and qualitative pavement structure attributes. A structural evaluation may consist of subsurface geotechnical exploration, falling weight deflectometer testing, petrographic testing, material coring, and/or flexural testing.

2.6.1 Pavement Distress Types

For each sample, the severity and quantity of defined distresses are recorded and then analyzed in accordance with the ASTM D5340-20 standard, which identifies 17 AC distress types and 16 PCC distress types. **Tables 2.6.1 (a)** and **2.6.1 (b)** identify these distresses and their common causes or mechanisms.

Table 2.6.1 (a): Pavement Distress Types – Asphalt Concrete

Distress Mechanism	Distress Type
Load	Alligator Cracking Rutting
Climate/Durability	Block Cracking Joint Reflection Cracking Longitudinal and Transverse Cracking (LT) Raveling Shoving Weathering
Construction/Material	Bleeding Corrugation Depression Polished Aggregate Slippage Cracking Swelling
Other	Jet Blast Erosion Oil Spillage Patching and Utility Cut Patching

Table 2.6.1 (b): Pavement Distress Types – Portland Cement Concrete

Distress Mechanism	Distress Type
Load	Corner Break Longitudinal, Transverse, and Diagonal Cracking (LTD) Pumping Shattered Slab/Intersecting Cracks
Climate/Durability	Blowup Durability "D" Cracking Joint Seal Damage Popouts
Construction/Material	Alkali Silica Reaction (ASR) Scaling Shrinkage Cracking
Other	Corner Spalling Joint Spalling Large Patching and Utility Cut Settlement or Faulting Small Patching

2.6.2 PCI Survey Procedures

PCI surveys are conducted on sample units defined in previous System Updates. Sample units are subject to change at the discretion of field personnel and/or to major pavement rehabilitation treatments. Furthermore, access to sample units based on accessibility or operational impacts may affect the overall sampling rate effort at each airport. **Tables 2.6.2 (a) and (b)** define the sampling criteria used by the FDOT SAPMP. A higher sampling rate may be utilized to achieve greater statistical confidence, should the Airport have the available resources to perform PCI survey independent of the FDOT SAPMP.


Table 2.6.2 (a): Recommended Sampling Rates for Asphalt Concrete

Number of Total Sample Units in Section	Runway Sampling Rate	Taxiways, Aprons, and Others Sampling Rate
1 - 4	1	1
5 - 10	2	1
11 - 15	3	2
16 - 30	5	3
31 - 40	7	4
41 - 50	8	5
51 or more	20% but ≤ 20	10% but ≤ 10


Table 2.6.2 (b): Recommended Sampling Rates for Portland Cement Concrete

Number of Total Sample Units in Section	Runway Sampling Rate	Taxiways, Aprons, and Others Sampling Rate
1 - 3	1	1
4 - 6	2	1
7 - 10	3	2
11 - 15	4	2
16 - 20	5	3
21 - 30	7	3
31 - 40	8	4
41 - 50	10	5
51 or more	20% but ≤ 20	10% but ≤ 10

The FDOT SAPMP is limited to select sample units for each section identified in each airport's Airfield Pavement Network Definition. The intent is to perform a limited amount of sample unit PCI surveys to reasonably reflect the functional condition. Due to the limited sampling criteria, there may be instances of pavement distress and deterioration outside of the inspected sample units that were not observed.



Chapter 3: Airfield Pavement System Inventory



Chapter 3 – Airfield Pavement System Inventory

This chapter discusses the inventory data collected from the Airport and summarizes network-level characteristics of the Airport's airfield pavements. At the start of each FDOT SAPMP System Update, all airports are asked to review the existing Airfield Pavement Network Definition Exhibit for accuracy. Furthermore, participating airports are asked to provide documentation of any recent or anticipated construction related to their airfield pavements.

3.1 Airfield Pavement Network Information

3.1.1 Previous and/or Anticipated Airfield Pavement Construction

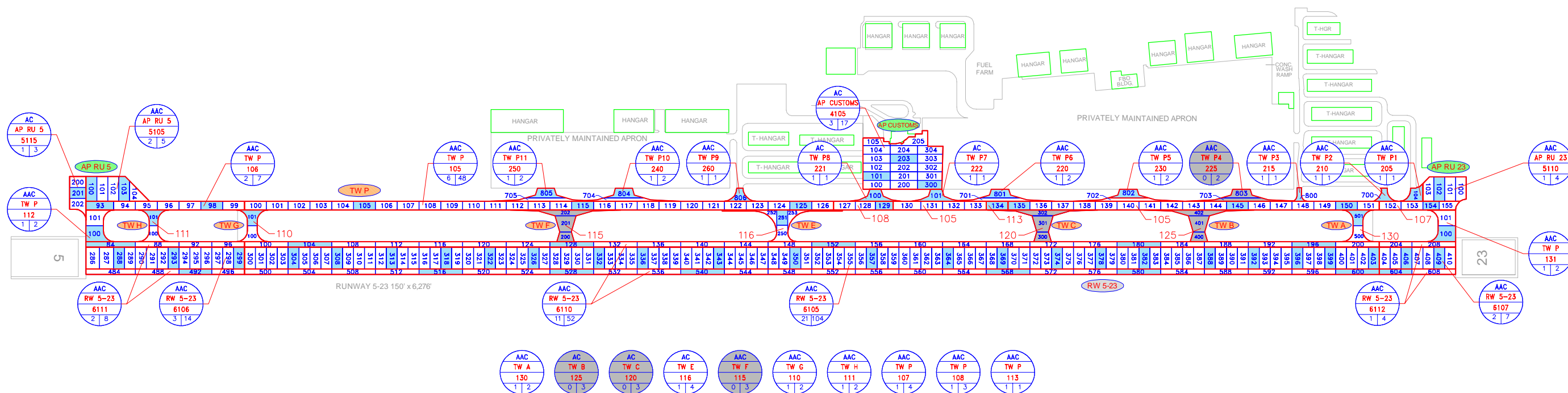
Based on information provided by the Airport, **Table 3.1.1** summarizes recent or anticipated airfield pavement construction projects since 2017.

Table 3.1.1: Summary of Previous and/or Anticipated Airfield Pavement Construction

Construction Year	Location	Work Type / Pavement Section
2018	TW P8, AP CUSTOMS	Complete Reconstruction - AC
	TW P7	New Construction - AC
2022	TW B, TW C	New Construction - AC 4" P-401, 12" P-211
	TW F, TW P4	Mill and Overlay Variable 2" Mill and P-401 Overlay, Fillet Widening 4" P-401, 12" P-211

The Airport provided a combination of record drawings, reports, and staff input, which aided in developing the construction history of the Airport's pavements since inception. Major rehabilitation and construction activities performed in the last 24 months, or anticipated in the next 24 months, are assumed to restore the PCI to 100. These activities include pavement overlay, mill and overlay, new construction, and/or complete reconstruction. These pavements were not formally subject to a PCI assessment and actual conditions may vary. Furthermore, any localized maintenance or repair performed in the assessment areas that would improve the PCI are considered in the condition analysis.

Figure 3.1.1 (a), the Airfield Pavement Network Definition Exhibit, provides details of the PCI assessment efforts. The Exhibit identifies pavement facilities, surface types, section definitions, and sample unit delineations. **Figure 3.1.1 (b)**, the Airfield Pavement System Inventory Exhibit, provides details of the work history updates communicated by the Airport. The Exhibit provides the approximate limits of recent and/or anticipated construction on the airfield pavement facilities. The limits are based on documentation provided by the Airport and, if constructed, are confirmed during field surveys.

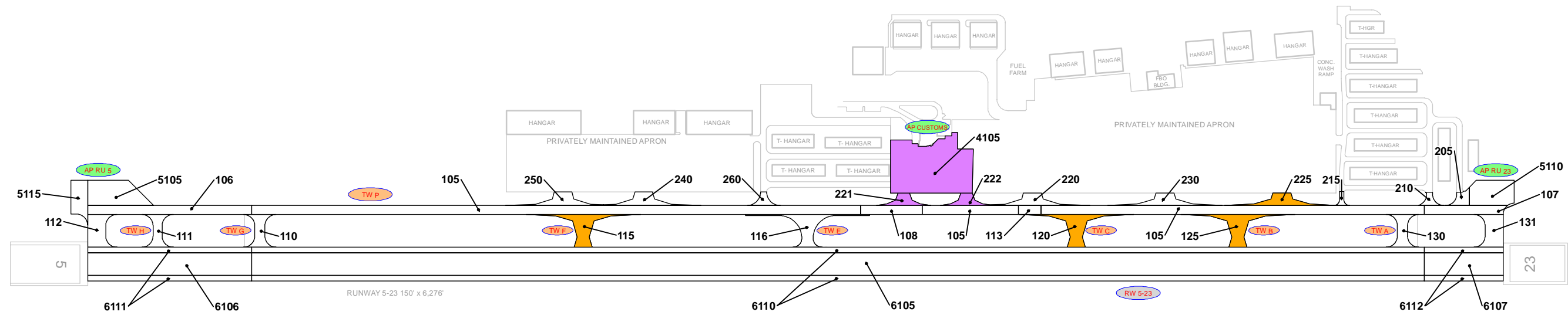


LEGEND

- RW 13-31 — TYPICAL RUNWAY BRANCH ID
- TW A — TYPICAL TAXIWAY BRANCH ID
- AP S — TYPICAL APRON BRANCH ID
- AAC — PAVEMENT SURFACE TYPE
- AP MAIN — PAVEMENT BRANCH ID
- 4105 — SECTION NUMBER
- 10 100 — NUMBER OF SAMPLE UNITS IN SECTION
NUMBER OF SAMPLE UNITS TO BE INSPECTED
- AAC
AP MAIN
4105
0 100 — SECTION NOT INSPECTED DUE TO RECENT CONSTRUCTION. SEE SYSTEM INVENTORY MAP FOR CONSTRUCTION DATES.
- 100 — INSPECTED SAMPLE UNITS.

TOTAL SAMPLES INSPECTED = 74
AC: 74 PCC: 0

RUNWAY LENGTHS DEPICTED IN THIS DRAWING ARE FOR PAVEMENT MANAGEMENT PURPOSES ONLY AND MAY NOT MATCH PUBLISHED RUNWAY LENGTHS. DRAWING NOT TO SCALE.



CONSTRUCTION YEAR	LOCATION	WORK TYPE / PAVEMENT SECTION
2018	TW P8, AP CUSTOMS	Complete Reconstruction - AC
	TW P7	New Construction - AC
2022	TW B, TW C	New Construction - AC 4" P-401, 12" P-211
	TW F, TW P4	Mill and Overlay Variable 2" Mill and P-401 Overlay, Fillet Widening 4" P-401, 12" P-211

The diagram illustrates the naming convention for project years using colored shapes and arrows. It shows three examples of branch IDs: 'RW 13-31' (runway), 'TW A' (taxiway), and 'AP S' (apron). Below these, a legend titled 'PROJECT YEAR' maps colors to years from 2017 to 2026.

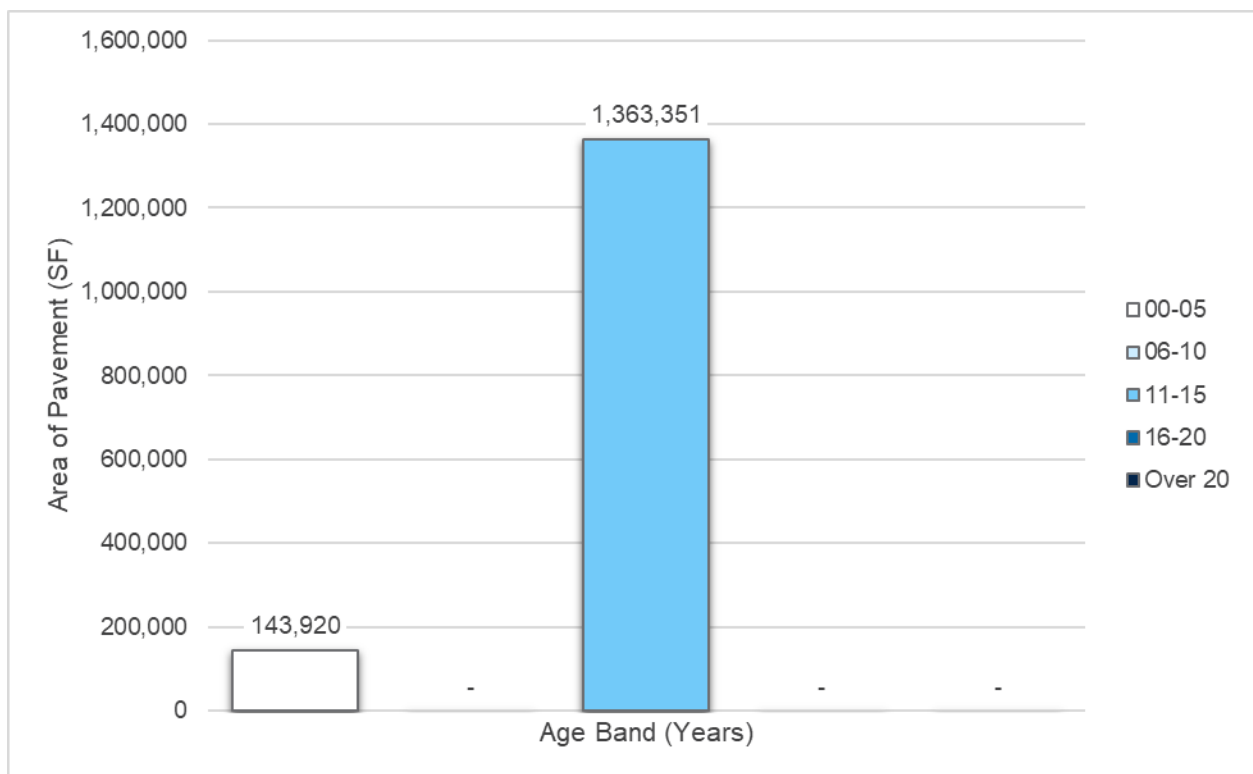
Color	Year
Green	2017
Orange	2022
Purple	2018
Blue	2023
Red	2019
Light Red	2024
Cyan	2020
Light Green	2025
Dark Red	2021
Dark Blue	2026

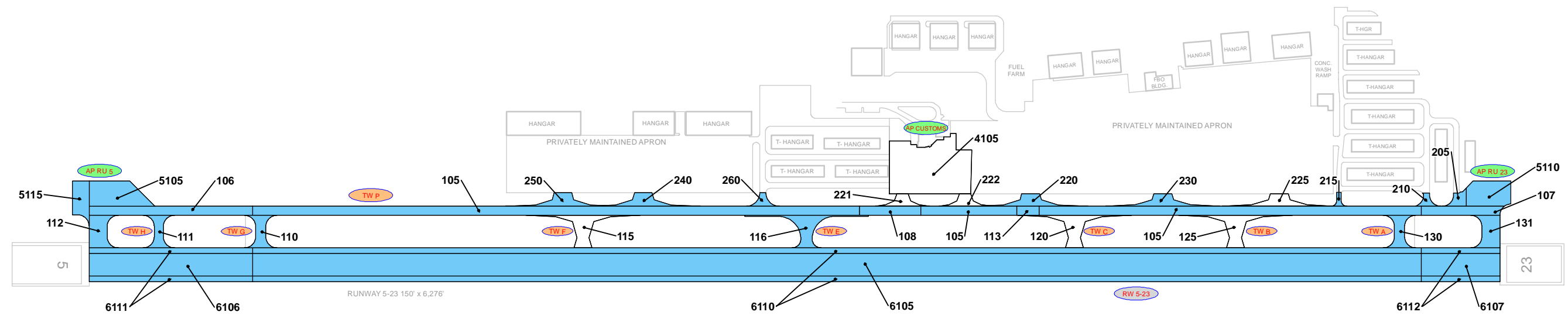
RUNWAY LENGTHS DEPICTED IN THIS DRAWING ARE FOR PAVEMENT MANAGEMENT PURPOSES ONLY AND MAY NOT MATCH PUBLISHED RUNWAY LENGTHS. DRAWING NOT TO SCALE.

3.1.2 Estimated Pavement Age

Standard pavement design practice considers a design life of 20 years. Design inputs typically require subgrade soil conditions, pavement layer material characteristics, and anticipated loading (aircraft fleet mix) for the design-life period. Based on the review of historic airfield pavement construction activities, **Figure 3.1.2 (a)** summarizes the age of the pavement sections since the last major construction activity has occurred. **Figure 3.1.2 (b)** provides the approximate limits of those age ranges on the airfield pavement facilities. This is intended to be a rough estimate based on interpretation of the limited data available at the time of report. The estimation of pavement age is based on information requested from the Airport.

Figure 3.1.2 (a): Age of Pavements at PCI Survey





LEGEND

TYPICAL RUNWAY BRANCH ID

TYPICAL TAXIWAY BRANCH ID

TYPICAL APRON BRANCH ID

AGE AT INSPECTION

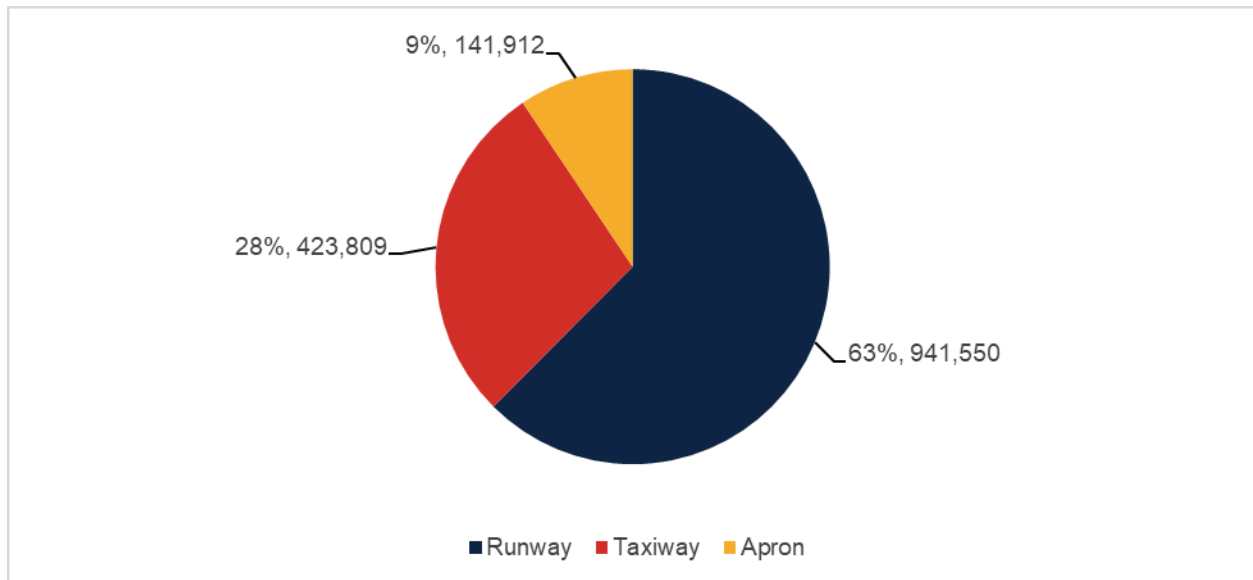
	0-5 Years
	6-10 Years
	11-15 Years
	16-20 Years
	> 20 Years

RUNWAY LENGTHS DEPICTED IN THIS DRAWING ARE FOR PAVEMENT MANAGEMENT PURPOSES ONLY AND MAY NOT MATCH PUBLISHED RUNWAY LENGTHS. DRAWING NOT TO SCALE.

3.1.3 Functional Use

Pavements are subject to variations in aircraft loading patterns based on use and overall operations. This is termed “functional use” or “branch use.” For this SAPMP System Update, the following categories of pavement functional use are identified: runway, taxiway, taxilane, and apron. **Figure 3.1.3** summarizes pavement functional use by area and excludes paved shoulders.

Figure 3.1.3: Airfield Pavement Branch Use by Area (SF)

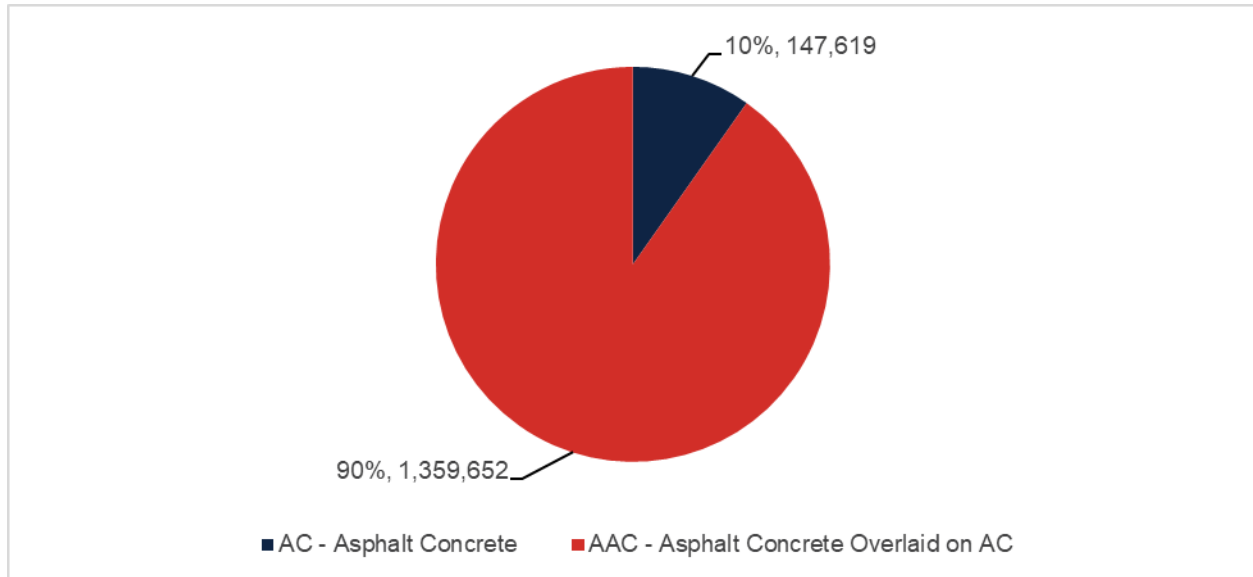


3.1.4 Pavement Surface Type

The airfield pavement facility surface types within the SAPMP include four (4) common types of pavement: Asphalt Concrete (AC), Asphalt Concrete overlaid on Asphalt Concrete (AAC), Asphalt Concrete overlaid on Portland cement concrete (APC), and Portland cement concrete (PCC).

Based on the record documentation incorporated within the SAPMP database and as observed during airfield pavement field assessments, pavement surface types have been assigned to the various pavement sections. **Figure 3.1.4** summarizes the applicable pavement types observed at BCT.

Figure 3.1.4: Airfield Pavement Surface Type by Area (SF)



3.1.5 Pavement System Inventory Details

The pavement inventory scope includes updates to existing pavement geometry and the development of an AutoCAD model with spatial projection for use within GIS. **Appendix C** includes the Airfield Pavement Network Definition Exhibit and the Airfield Pavement System Inventory Exhibit, which visually summarize the results of the airfield pavement system inventory analysis.

Table 3.1.5 displays the section-level pavement inventory data, which is based on record documentation provided by the airports and from previous System Updates. The information presented relies on the accuracy and the adequacy of data provided. In some cases, characteristics such as pavement area may be estimated based on aerial interpretation of spatially-projected imagery. Additionally, if the last construction date is unknown, a date of January 1 of the estimated year was assigned to the section. The accuracy of data is appropriate for this network-level planning document. Should the Airport perform rehabilitation work, it is recommended that project-level investigations be performed to support the data accuracy needed for design and construction.

Table 3.1.5: Pavement System Inventory Details

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	Surface Type	Estimate of Last Construction Date
BCT	RW 5-23	Runway	6105	520,000	AAC	1/1/2010
BCT	RW 5-23	Runway	6106	72,700	AAC	1/1/2010
BCT	RW 5-23	Runway	6107	35,000	AAC	1/1/2010
BCT	RW 5-23	Runway	6110	260,000	AAC	1/1/2010
BCT	RW 5-23	Runway	6111	36,350	AAC	1/1/2010
BCT	RW 5-23	Runway	6112	17,500	AAC	1/1/2010
BCT	TW A	Taxiway	130	8,671	AAC	1/1/2010
BCT	TW B	Taxiway	125	14,426	AC	7/1/2022

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	Surface Type	Estimate of Last Construction Date
BCT	TW C	Taxiway	120	14,426	AC	7/1/2022
BCT	TW E	Taxiway	116	14,729	AC	1/1/2010
BCT	TW F	Taxiway	115	14,426	AAC	7/1/2022
BCT	TW G	Taxiway	110	8,671	AAC	1/1/2010
BCT	TW H	Taxiway	111	7,946	AAC	1/1/2010
BCT	TW P	Taxiway	105	193,060	AAC	1/1/2010
BCT	TW P	Taxiway	106	29,080	AAC	1/1/2010
BCT	TW P	Taxiway	107	14,241	AAC	1/1/2010
BCT	TW P	Taxiway	108	10,940	AAC	1/1/2010
BCT	TW P	Taxiway	112	12,673	AAC	1/1/2010
BCT	TW P	Taxiway	113	4,000	AAC	1/1/2010
BCT	TW P	Taxiway	131	12,673	AAC	1/1/2010
BCT	TW P1	Taxiway	205	4,102	AAC	1/1/2010
BCT	TW P10	Taxiway	240	8,665	AAC	1/1/2010
BCT	TW P11	Taxiway	250	8,665	AAC	1/1/2010
BCT	TW P2	Taxiway	210	2,572	AAC	1/1/2010
BCT	TW P3	Taxiway	215	1,488	AAC	1/1/2010
BCT	TW P4	Taxiway	225	8,391	AAC	7/1/2022
BCT	TW P5	Taxiway	230	8,400	AAC	1/1/2010
BCT	TW P6	Taxiway	220	8,315	AAC	1/1/2010
BCT	TW P7	Taxiway	222	4,994	AC	2/24/2018
BCT	TW P8	Taxiway	221	5,090	AC	2/24/2018
BCT	TW P9	Taxiway	260	3,165	AAC	1/1/2010
BCT	AP CUSTOMS	Apron	4105	82,167	AC	2/24/2018
BCT	AP RU 23	Apron	5110	21,414	AAC	1/1/2010
BCT	AP RU 5	Apron	5105	26,544	AAC	1/1/2010
BCT	AP RU 5	Apron	5115	11,787	AC	1/1/2010

A wide-angle photograph of an asphalt runway stretching into the distance under a bright blue sky with scattered white clouds. The runway has a central white dashed line and side yellow lines. The image is framed by a red diagonal bar on the left and a blue diagonal bar on the right.

Chapter 4: Airfield Pavement Condition Analysis

A close-up, low-angle view of the runway pavement, showing a concrete slab with a yellow dashed line and a yellow chevron marking. The image is framed by a red diagonal bar on the left and a blue diagonal bar on the right.

Chapter 4 – Airfield Pavement Condition Analysis

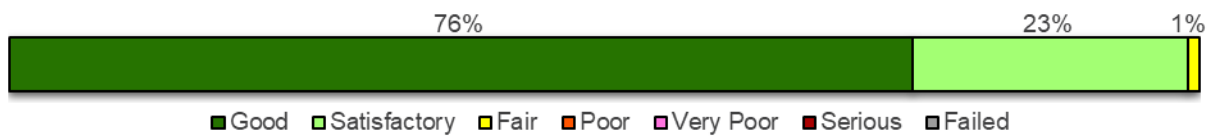
The Pavement Condition Index (PCI) provides insight to possible causes of deterioration to help support pavement maintenance and rehabilitation planning. Distress type, severity, and extent are required in the computation of a PCI value. The PCI method of pavement condition evaluation is strictly a visual review of surface condition, also referred to as a functional evaluation. Further evaluation of pavement conditions may be necessary, such as structural evaluation, for design-and/or project-level determination of pavement rehabilitation needs.

4.1 Airfield Pavement Condition Index

4.1.1 Network-Level Analysis

The following figure, **Figure 4.1.1**, summarizes the network-level pavement condition analysis based on the most recent survey results. On a network level, approximately 99% of inspected pavements are in Good or Satisfactory condition and the remaining 1% of inspected pavements are in Fair condition.

Figure 4.1.1: Current Condition – Overall Network



4.1.2 Branch-Level Analysis

The following **Figures 4.1.2 (a)-(d)** summarize branch-level pavement conditions according to the most recent PCI assessment results.

Figure 4.1.2 (a): Current Condition Summary – Branch-Level

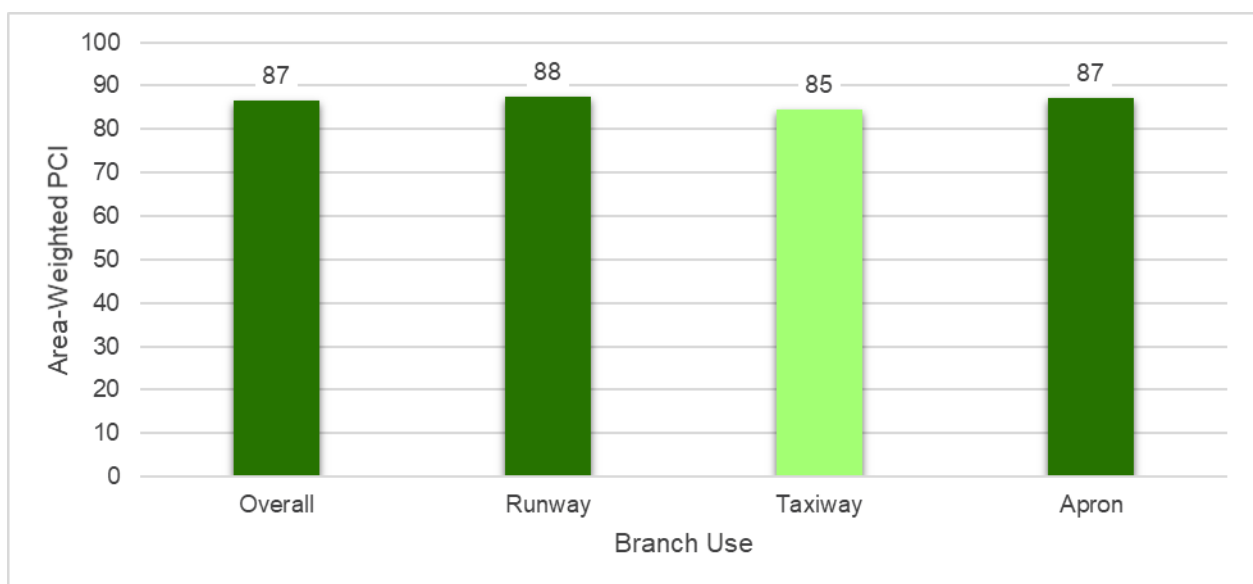


Figure 4.1.2 (b): Current Condition – Runway

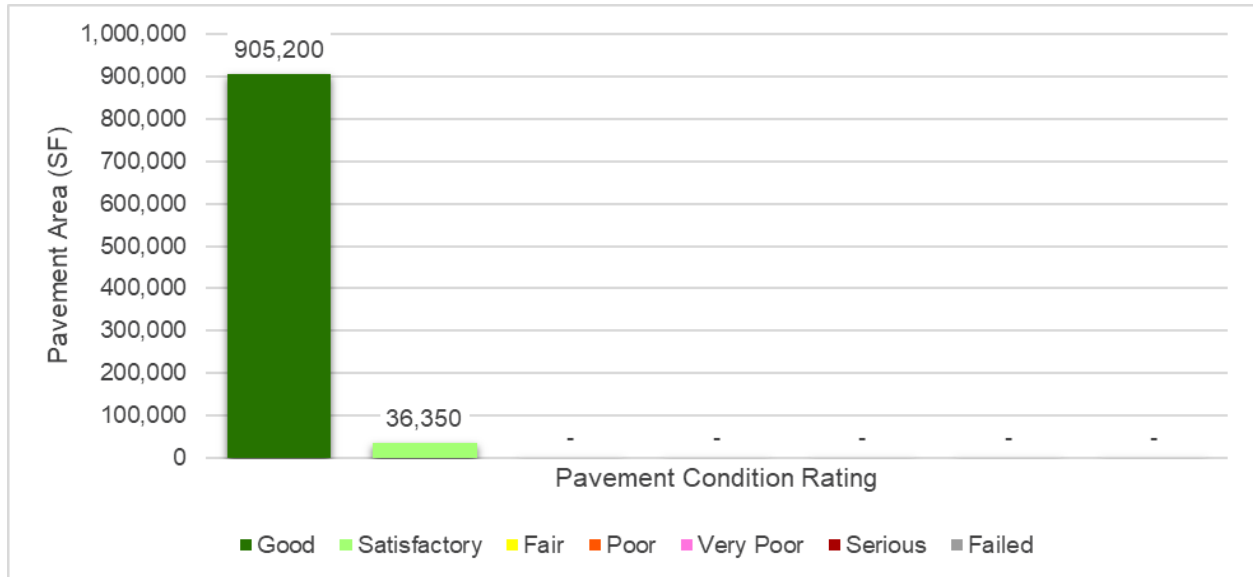


Figure 4.1.2 (c): Current Condition – Taxiway

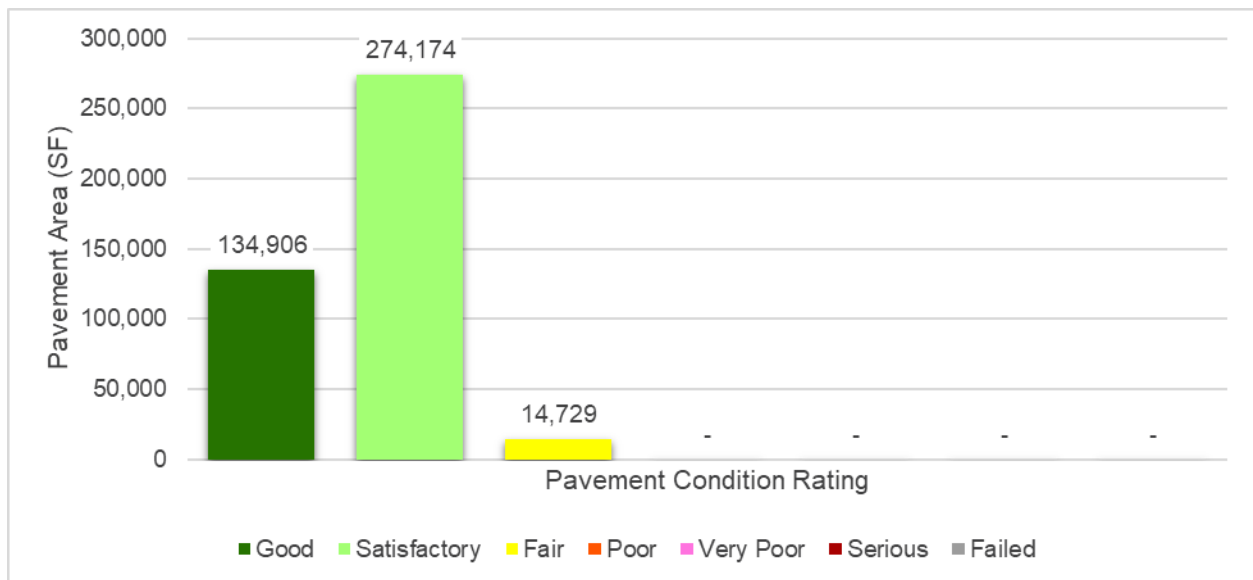


Figure 4.1.2 (d): Current Condition – Apron

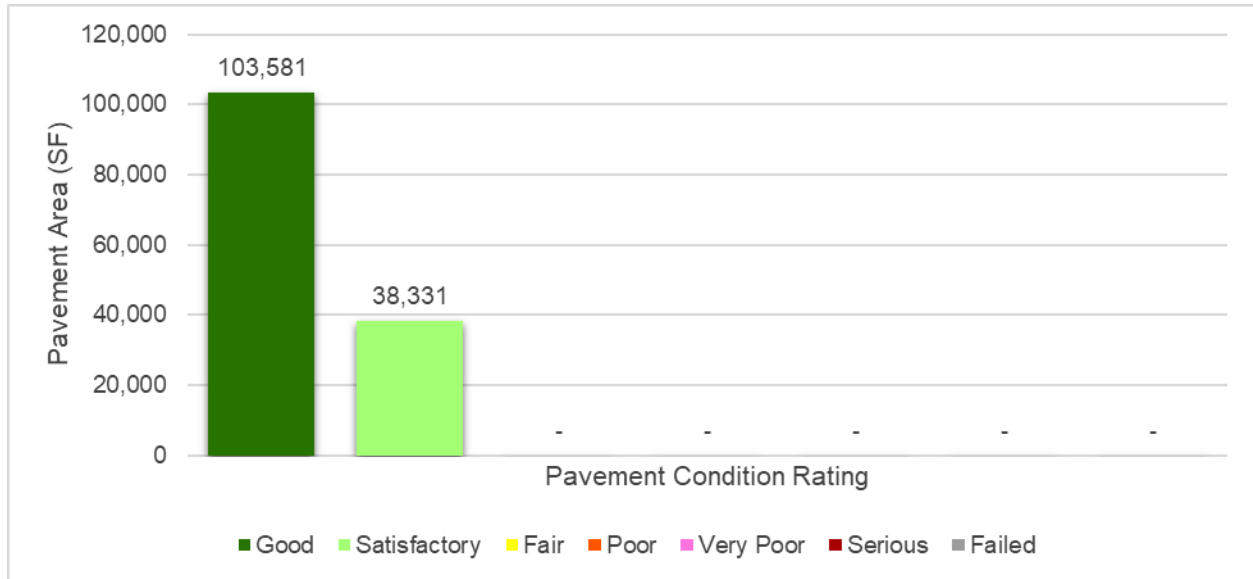


Table 4.1.2 details the branch-level condition for each airfield pavement branch.

Table 4.1.2: Current Condition Summary – Branch-Level

Branch ID	Branch Use	Number of Sections	Branch Area (SF)	Area-Weighted Avg PCI	Condition Rating
RW 5-23	Runway	6	941,550	88	Good
TW A	Taxiway	1	8,671	81	Satisfactory
TW B	Taxiway	1	14,426	100	Good
TW C	Taxiway	1	14,426	100	Good
TW E	Taxiway	1	14,729	70	Fair
TW F	Taxiway	1	14,426	100	Good
TW G	Taxiway	1	8,671	84	Satisfactory
TW H	Taxiway	1	7,946	78	Satisfactory
TW P	Taxiway	7	276,667	82	Satisfactory
TW P1	Taxiway	1	4,102	88	Good
TW P10	Taxiway	1	8,665	89	Good
TW P11	Taxiway	1	8,665	93	Good
TW P2	Taxiway	1	2,572	72	Satisfactory
TW P3	Taxiway	1	1,488	74	Satisfactory
TW P4	Taxiway	1	8,391	100	Good
TW P5	Taxiway	1	8,400	87	Good
TW P6	Taxiway	1	8,315	85	Satisfactory
TW P7	Taxiway	1	4,994	89	Good
TW P8	Taxiway	1	5,090	87	Good
TW P9	Taxiway	1	3,165	81	Satisfactory
AP CUSTOMS	Apron	1	82,167	91	Good
AP RU 23	Apron	1	21,414	88	Good
AP RU 5	Apron	2	38,331	78	Satisfactory

4.1.3 Section-Level Analysis

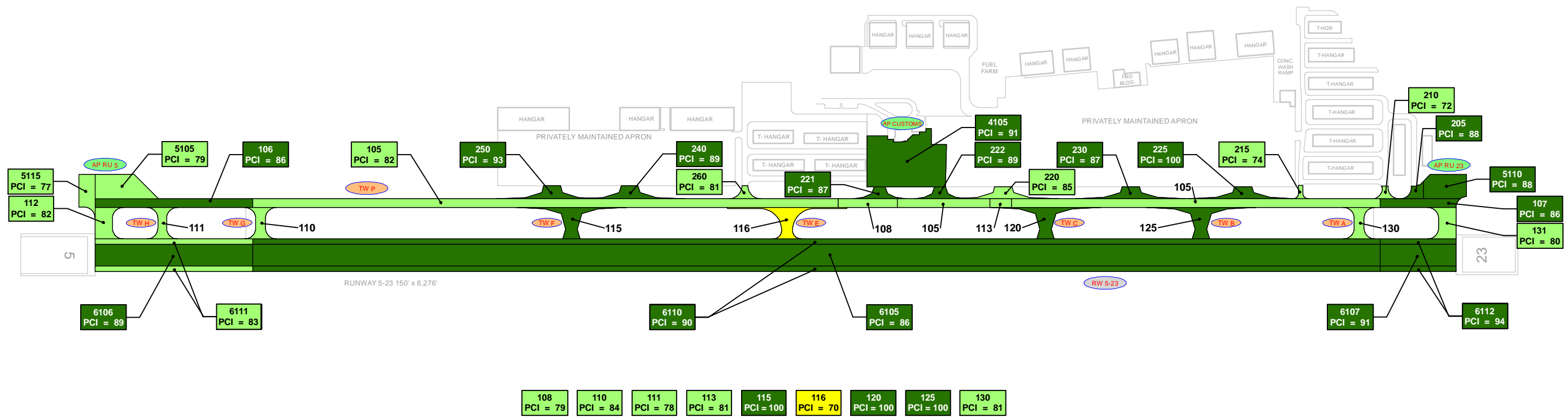
Table 4.1.3 provides each pavement section's area-weighted average PCI and the percent of distress related to load, climate, and other factors. The causes of condition deterioration help inform maintenance, repair, and rehabilitation decisions. For example, load-related distress can indicate that the pavement is reaching the end of its structural design life and the selected rehabilitation treatment should include either strengthening or reconstruction. **Figure 4.1.3** provides a technical exhibit that graphically depicts PCI values and ratings determined from this SAPMP System Update.

Pavement facilities that have been reconstructed within the past 24 months, or are anticipated for reconstruction within the next 24 months, may have been omitted from this assessment. Pavement that has received major rehabilitation will be set to a PCI of 100 for this analysis.

Table 4.1.3: Latest Pavement Condition Index Summary – Section-Level

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	Surface	PCI	Condition Rating	PCI % Climate	PCI % Load	PCI % Other	Sample Units Inspected	Total Sample Units in Section
BCT	RW 5-23	Runway	6105	520,000	AAC	86	Good	100	0	0	21	104
BCT	RW 5-23	Runway	6106	72,700	AAC	89	Good	100	0	0	3	14
BCT	RW 5-23	Runway	6107	35,000	AAC	91	Good	100	0	0	2	7
BCT	RW 5-23	Runway	6110	260,000	AAC	90	Good	92	0	8	11	52
BCT	RW 5-23	Runway	6111	36,350	AAC	83	Satisfactory	100	0	0	2	8
BCT	RW 5-23	Runway	6112	17,500	AAC	94	Good	100	0	0	1	4
BCT	TW A	Taxiway	130	8,671	AAC	81	Satisfactory	100	0	0	1	2
BCT	TW B	Taxiway	125	14,426	AC	100	Good	0	0	0	0	0
BCT	TW C	Taxiway	120	14,426	AC	100	Good	0	0	0	0	0
BCT	TW E	Taxiway	116	14,729	AC	70	Fair	46	0	54	1	4
BCT	TW F	Taxiway	115	14,426	AAC	100	Good	0	0	0	0	0
BCT	TW G	Taxiway	110	8,671	AAC	84	Satisfactory	100	0	0	1	2
BCT	TW H	Taxiway	111	7,946	AAC	78	Satisfactory	100	0	0	1	2
BCT	TW P	Taxiway	105	193,060	AAC	82	Satisfactory	100	0	0	6	48
BCT	TW P	Taxiway	106	29,080	AAC	86	Good	100	0	0	2	7
BCT	TW P	Taxiway	107	14,241	AAC	86	Good	100	0	0	1	4
BCT	TW P	Taxiway	108	10,940	AAC	79	Satisfactory	100	0	0	1	3
BCT	TW P	Taxiway	112	12,673	AAC	82	Satisfactory	100	0	0	1	2
BCT	TW P	Taxiway	113	4,000	AAC	81	Satisfactory	100	0	0	1	1
BCT	TW P	Taxiway	131	12,673	AAC	80	Satisfactory	100	0	0	1	2
BCT	TW P1	Taxiway	205	4,102	AAC	88	Good	100	0	0	1	1
BCT	TW P10	Taxiway	240	8,665	AAC	89	Good	100	0	0	1	2
BCT	TW P11	Taxiway	250	8,665	AAC	93	Good	100	0	0	1	2
BCT	TW P2	Taxiway	210	2,572	AAC	72	Satisfactory	100	0	0	1	1
BCT	TW P3	Taxiway	215	1,488	AAC	74	Satisfactory	100	0	0	1	1
BCT	TW P4	Taxiway	225	8,391	AAC	100	Good	0	0	0	0	0
BCT	TW P5	Taxiway	230	8,400	AAC	87	Good	100	0	0	1	2
BCT	TW P6	Taxiway	220	8,315	AAC	85	Satisfactory	100	0	0	1	2
BCT	TW P7	Taxiway	222	4,994	AC	89	Good	100	0	0	1	1
BCT	TW P8	Taxiway	221	5,090	AC	87	Good	100	0	0	1	1
BCT	TW P9	Taxiway	260	3,165	AAC	81	Satisfactory	100	0	0	1	1
BCT	AP CUSTOMS	Apron	4105	82,167	AC	91	Good	100	0	0	3	17
BCT	AP RU 23	Apron	5110	21,414	AAC	88	Good	100	0	0	1	4
BCT	AP RU 5	Apron	5105	26,544	AAC	79	Satisfactory	96	0	4	2	5
BCT	AP RU 5	Apron	5115	11,787	AC	77	Satisfactory	100	0	0	1	3

* Zero (0) Sample Units Inspected signifies that the pavement section was not inspected during this SAPMP System Update due to recent construction projects. These sections correlate with the gray sections on the Network Definition Exhibit.



LEGEND

- RW 13-31 — TYPICAL RUNWAY BRANCH ID
- TW A — TYPICAL TAXIWAY BRANCH ID
- AP S — TYPICAL APRON BRANCH ID

2022 PAVEMENT CONDITION INDEX

Color	PCI Range	Condition
Dark Green	86-100	Good
Light Green	71-85	Satisfactory
Yellow	56-70	Fair
Orange	41-55	Poor
Pink	26-40	Very Poor
Red	11-25	Serious
Grey	0-10	Failed

"SECTION ID" "PCI VALUE"

RUNWAY LENGTHS DEPICTED IN THIS DRAWING ARE FOR PAVEMENT MANAGEMENT PURPOSES ONLY AND MAY NOT MATCH PUBLISHED RUNWAY LENGTHS. DRAWING NOT TO SCALE.

4.2 Summary of Pavement Condition Evaluation Results

4.2.1 Network-Level Observations

The PCI assessment for Boca Raton Airport (BCT) was performed in August 2022. The overall area-weighted average PCI value of the network was 87, representing a condition rating of Good. A portion of the airfield pavement was not inspected due to recent construction in 2022. These areas include Taxiway B, Taxiway C, Taxiway F, and Taxiway P4.

Based on the FAA 5010 Report as of 10/27/2022, the Airport has reported 81,550 operations for 12 months ending 04/30/2018.

4.2.2 Branch-Level Observations

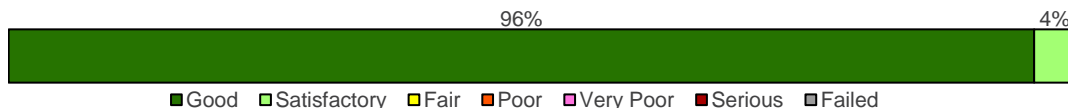
The following branch-level observations are a summary of select pavement facilities identified during the PCI assessment, including a discussion of general conditions and branch characteristics. The summary may not include all branches and/or sections within the Airport's airfield pavement network. Representative distress photographs of airfield pavements are presented in **Appendix D**. "Vicinity" photos refer to the approximate boundaries of an inspected sample unit within the section and provide an overview of the section condition but are not focused on a specific distress. The Re-inspection Report found in **Appendix E** provides listings of each sample unit and distress.

Runways

RW 5-23

Branch ID	Branch Use	Number of Sections	Branch Area (SF)	Branch Area-Weighted Avg PCI	Branch Condition Rating
RW 5-23	RUNWAY	6	941,550	88	Good

The following bar graph shows proportional distribution (as % of area within branch) of condition categories among sections within the branch. Given the individual section data shown in the subsequent table, the distribution is as follows: 96% Good (86-100 PCI), 4% Satisfactory (71-85 PCI).



Section ID	Surface Type	Section Area (SF)	PCI	Condition Rating
6105	AAC	520,000	86	Good
6106	AAC	72,700	89	Good
6107	AAC	35,000	91	Good
6110	AAC	260,000	90	Good
6111	AAC	36,350	83	Satisfactory
6112	AAC	17,500	94	Good

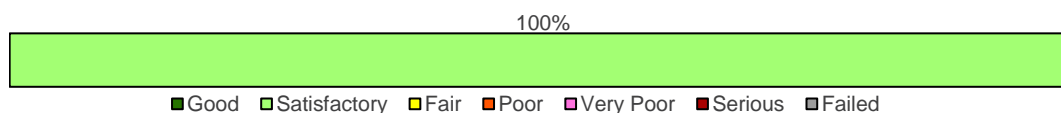
RW 5-23 consists of 6 flexible pavement sections, totaling 941,550 sf. The last major construction date for the branch was 2010, resulting in an area-weighted average age at inspection of 13 years old. Overall, RW 5-23 is in Good condition with an area-weighted average PCI of 88.

Taxiways

TW A

Branch ID	Branch Use	Number of Sections	Branch Area (SF)	Branch Area-Weighted Avg PCI	Branch Condition Rating
TW A	TAXIWAY	1	8,671	81	Satisfactory

The following bar graph shows proportional distribution (as % of area within branch) of condition categories among sections within the branch. Given the individual section data shown in the subsequent table, the distribution is as follows: 100% Satisfactory (71-85 PCI).



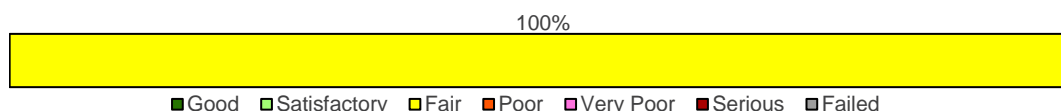
Section ID	Surface Type	Section Area (SF)	PCI	Condition Rating
130	AAC	8,671	81	Satisfactory

TW A consists of 1 flexible pavement section, totaling 8,671 sf. The last major construction date for the branch was 2010, resulting in an area-weighted average age at inspection of 13 years old. Overall, TW A is in Satisfactory condition with an area-weighted average PCI of 81.

TW E

Branch ID	Branch Use	Number of Sections	Branch Area (SF)	Branch Area-Weighted Avg PCI	Branch Condition Rating
TW E	TAXIWAY	1	14,729	70	Fair

The following bar graph shows proportional distribution (as % of area within branch) of condition categories among sections within the branch. Given the individual section data shown in the subsequent table, the distribution is as follows: 100% Fair (56-70 PCI).



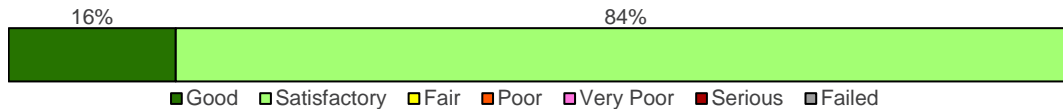
Section ID	Surface Type	Section Area (SF)	PCI	Condition Rating
116	AC	14,729	70	Fair

TW E consists of 1 flexible pavement section, totaling 14,729 sf. The last major construction date for the branch was 2010, resulting in an area-weighted average age at inspection of 13 years old. Overall, TW E is in Fair condition with an area-weighted average PCI of 70.

TW P

Branch ID	Branch Use	Number of Sections	Branch Area (SF)	Branch Area-Weighted Avg PCI	Branch Condition Rating
TW P	TAXIWAY	7	276,667	82	Satisfactory

The following bar graph shows proportional distribution (as % of area within branch) of condition categories among sections within the branch. Given the individual section data shown in the subsequent table, the distribution is as follows: 16% Good (86-100 PCI), 84% Satisfactory (71-85 PCI).



Section ID	Surface Type	Section Area (SF)	PCI	Condition Rating
105	AAC	193,060	82	Satisfactory
106	AAC	29,080	86	Good
107	AAC	14,241	86	Good
108	AAC	10,940	79	Satisfactory
112	AAC	12,673	82	Satisfactory
113	AAC	4,000	81	Satisfactory
131	AAC	12,673	80	Satisfactory

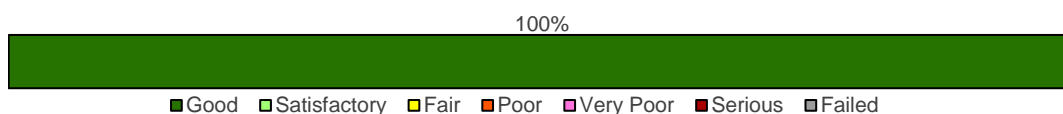
TW P consists of 7 flexible pavement sections, totaling 276,667 sf. The last major construction date for the branch was 2010, resulting in an area-weighted average age at inspection of 13 years old. Overall, TW P is in Satisfactory condition with an area-weighted average PCI of 82.

Aprons

AP CUSTOMS

Branch ID	Branch Use	Number of Sections	Branch Area (SF)	Branch Area-Weighted Avg PCI	Branch Condition Rating
AP CUSTOMS	APRON	1	82,167	91	Good

The following bar graph shows proportional distribution (as % of area within branch) of condition categories among sections within the branch. Given the individual section data shown in the subsequent table, the distribution is as follows: 100% Good (86-100 PCI).



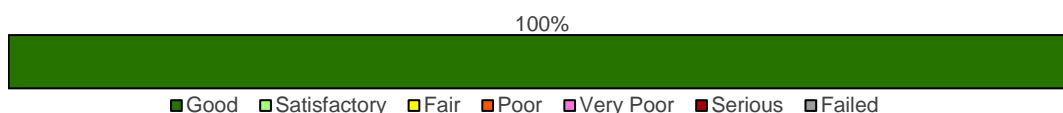
Section ID	Surface Type	Section Area (SF)	PCI	Condition Rating
4105	AC	82,167	91	Good

AP CUSTOMS consists of 1 flexible pavement section, totaling 82,167 sf. The last major construction date for the branch was 2018, resulting in an area-weighted average age at inspection of 4 years old. Overall, AP CUSTOMS is in Good condition with an area-weighted average PCI of 91.

AP RU 23

Branch ID	Branch Use	Number of Sections	Branch Area (SF)	Branch Area-Weighted Avg PCI	Branch Condition Rating
AP RU 23	APRON	1	21,414	88	Good

The following bar graph shows proportional distribution (as % of area within branch) of condition categories among sections within the branch. Given the individual section data shown in the subsequent table, the distribution is as follows: 100% Good (86-100 PCI).



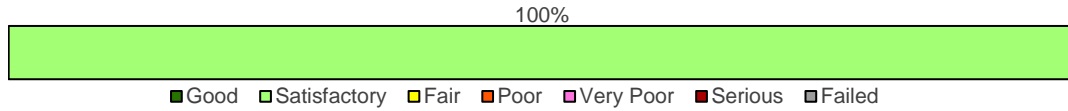
Section ID	Surface Type	Section Area (SF)	PCI	Condition Rating
5110	AAC	21,414	88	Good

AP RU 23 consists of 1 flexible pavement section, totaling 21,414 sf. The last major construction date for the branch was 2010, resulting in an area-weighted average age at inspection of 13 years old. Overall, AP RU 23 is in Good condition with an area-weighted average PCI of 88.

AP RU 5

Branch ID	Branch Use	Number of Sections	Branch Area (SF)	Branch Area-Weighted Avg PCI	Branch Condition Rating
AP RU 5	APRON	2	38,331	78	Satisfactory

The following bar graph shows proportional distribution (as % of area within branch) of condition categories among sections within the branch. Given the individual section data shown in the subsequent table, the distribution is as follows: 100% Satisfactory (71-85 PCI).



Section ID	Surface Type	Section Area (SF)	PCI	Condition Rating
5105	AAC	26,544	79	Satisfactory
5115	AC	11,787	77	Satisfactory

AP RU 5 consists of 2 flexible pavement sections, totaling 38,331 sf. The last major construction date for the branch was 2010, resulting in an area-weighted average age at inspection of 13 years old. Overall, AP RU 5 is in Satisfactory condition with an area-weighted average PCI of 78.



Chapter 5: SAPMP Customization



Chapter 5 – SAPMP Customization

Once the PAVER™ database is populated with inventory and condition data (including PCI and rank), it is further customized with key elements such as network-level attributes, performance models, critical PCI, maintenance policies, and unit costs that are specific to the FDOT SAPMP. Each of these factors play a role in the development of rehabilitation strategies as they help to identify maintenance and rehabilitation needs for long-term management.

The FDOT SAPMP is organized to provide airports with planning-level data and does not intend to preclude the responsible engineer from performing the appropriate level of investigation and analysis in determining the appropriate design details of a pavement rehabilitation. It would not be advisable to solely base design-level rehabilitation without the appropriate level of investigation and determination of pavement deterioration beyond that of a visual functional condition assessment.

5.1 Network-Level Customization

The network-level attribute fields used in the FDOT SAPMP PAVER™ database consist of the Network, Airport Classification, District, FAA ADO Area, Inspection Phase, and Continuing Florida Aviation System Planning Process (CFASPP) Center. Each of these elements are briefly defined below.

- » The “Network” field identifies the airport being analyzed;
- » The “Airport Classification” field classifies the Airport according to the type and volume of aircraft traffic;
 - “GA” for General Aviation, community airports
 - “RL” for Regional Relievers
 - “PR” for Primary/Commercial airports
- » The “District” field identifies the FDOT District to which the Airport belongs;
- » The “FAA ADO Area” is an area used by the Orlando ADO to assign airports within those areas to the responsible FAA ADO personnel (planners, engineers, and environmentalists);
- » The “Inspection Phase” denotes which phase of the SAPMP the Airport is surveyed (Phase 1 or Phase 2); and
- » The “CFASPP Center” identifies which Region or Metropolitan Area of the Continuing Florida Aviation Systems Planning Process an Airport falls within.

5.2 Pavement Condition Forecasts

Pavement performance models, alternatively known as forecast models, prediction curves, or family curves, are developed from past and current distress data, as well as age data. These prediction curves are used to develop forecasts of PCI values that then help determine optimum timing for pavement maintenance and rehabilitation.

5.2.1 Forecasting PCI Considerations

Performance models will continue to be refined as the FDOT updates the SAPMP with subsequent PCI surveys. With the refinement of additional PCI and age data points, the forecasting of pavement conditions will continue to better reflect the performance trends of airfield pavements in the FAS. As a reminder, forecasting of pavement condition for the Airport is intended for planning purposes only. **The estimation of forecasted PCI values gives no assurance of future pavement conditions as PCI values represent an engineering estimation to be used as a planning tool. Forecasted PCI data should not be the sole metric for determining the year in which a project should be planned. Design-level planning should be undertaken by the responsible engineer prior to the development of airfield design plans.** Design-level recommendations for pavement rehabilitation and/or reconstruction will require the appropriate application of the procedures defined in the FAA AC 150/5320-6F.

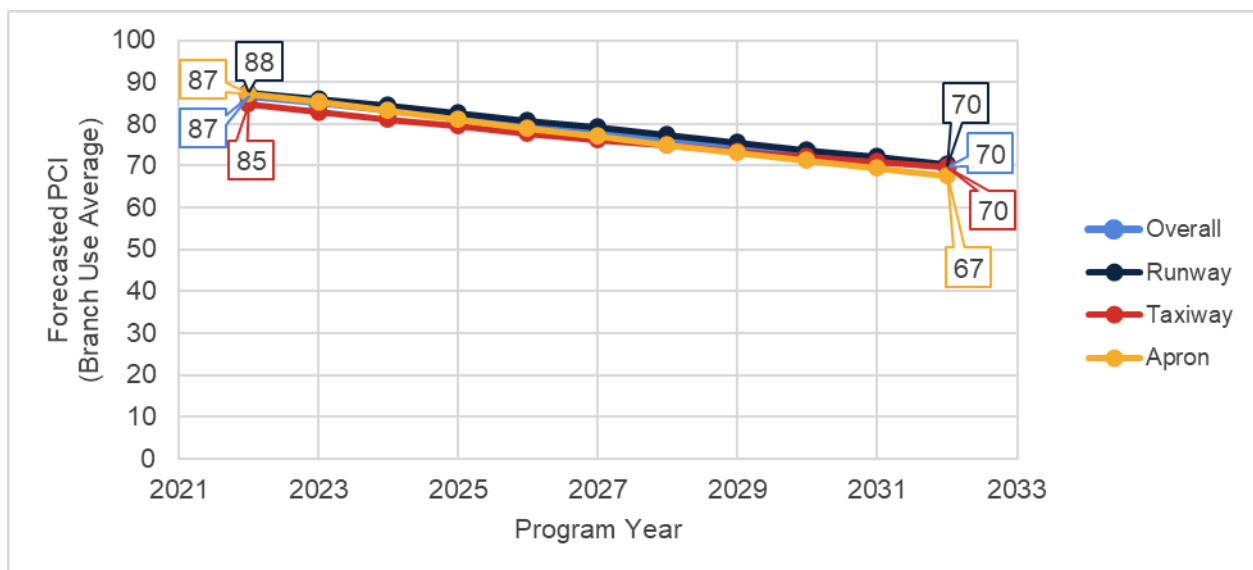
5.2.2 Performance Models

To develop pavement performance models, data for each section is combined into “groups” or “families” according to pavement type, traffic, and functional use. For the FDOT SAPMP, the models were defined for both PCC- and AC-surfaced pavements and further divided according to functional use. Based on average deterioration rates for different pavement types, each pavement section is assigned to a specific deterioration family to forecast the condition over a 10-year period.

5.2.3 Branch-Level Pavement Condition Forecast

Figure 5.2.3 depicts the branch-level pavement condition forecast for each branch use (Runway, Taxiway, Taxilane, and/or Apron) as well as the overall network. The condition forecasts are for a 10-year duration, starting in 2023 through 2032.

Figure 5.2.3: Forecasted Branch-Level Pavement Performance



5.2.4 Section-Level Pavement Condition Forecast

Table 5.2.4 provides section-level details for PCI forecasts. Pavement condition forecasts should be used for planning purposes only, as actual condition of sections is subject to the sensitivities in changes of traffic and maintenance frequency.

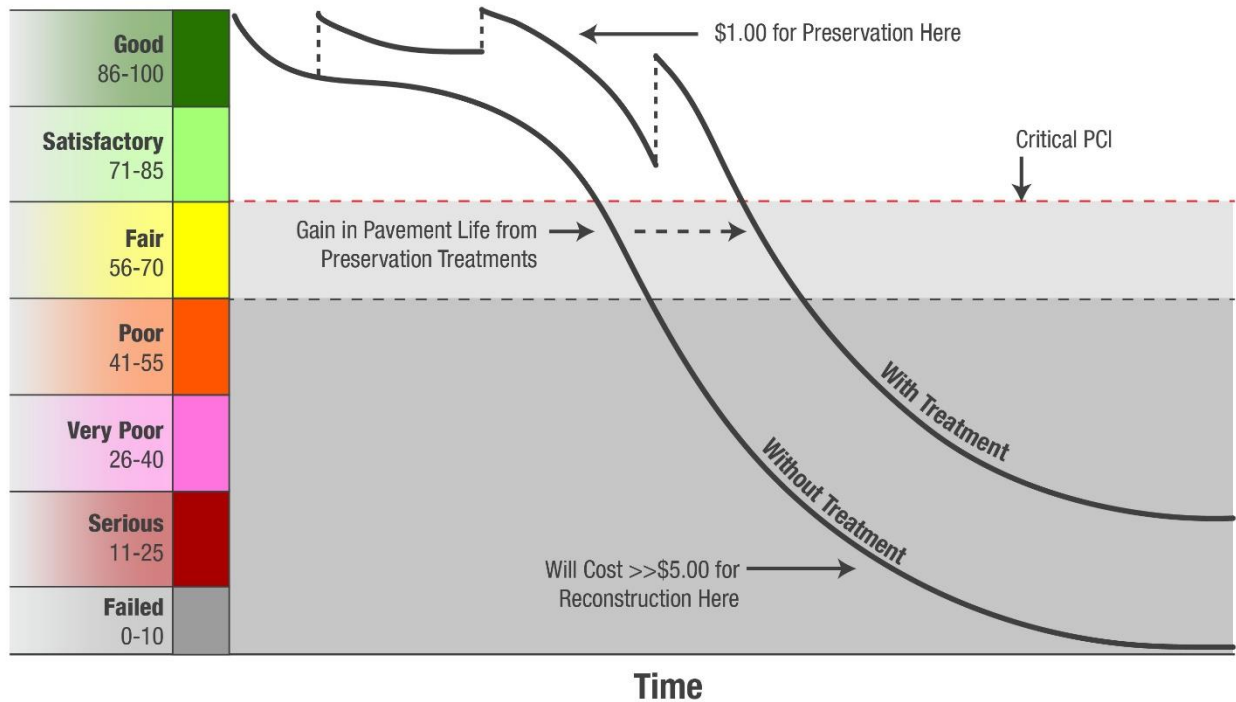
Table 5.2.4: Forecasted PCI Values 2023-2032 – Section-Level

Network ID	Branch ID	Section ID	Current PCI	2023	2024	2025	2026	2027	2028	2029	2030	2031	2032
BCT	RW 5-23	6105	86	84	83	81	79	78	76	74	72	71	69
BCT	RW 5-23	6106	89	87	86	84	82	81	79	77	75	74	72
BCT	RW 5-23	6107	91	89	88	86	84	83	81	79	77	76	74
BCT	RW 5-23	6110	90	88	87	85	83	82	80	78	76	75	73
BCT	RW 5-23	6111	83	81	80	78	76	75	73	71	69	68	66
BCT	RW 5-23	6112	94	92	91	89	87	86	84	82	80	79	77
BCT	TW A	130	81	79	78	76	75	73	72	71	69	68	67
BCT	TW B	125	100	98	95	93	91	89	87	85	83	82	80
BCT	TW C	120	100	98	95	93	91	89	87	85	83	82	80
BCT	TW E	116	70	69	68	67	66	66	65	64	63	63	62
BCT	TW F	115	100	98	95	93	91	88	86	84	83	81	79
BCT	TW G	110	84	82	81	79	77	76	74	73	71	70	69
BCT	TW H	111	78	77	75	74	72	71	70	69	67	66	65
BCT	TW P	105	82	80	79	77	76	74	73	71	70	69	68
BCT	TW P	106	86	84	82	81	79	77	76	74	73	71	70
BCT	TW P	107	86	84	82	81	79	77	76	74	73	71	70
BCT	TW P	108	79	78	76	74	73	72	70	69	68	67	66
BCT	TW P	112	82	80	79	77	76	74	73	71	70	69	68
BCT	TW P	113	81	79	78	76	75	73	72	71	69	68	67
BCT	TW P	131	80	79	77	75	74	73	71	70	69	68	67
BCT	TW P1	205	88	86	84	82	81	79	77	76	74	73	71
BCT	TW P10	240	89	87	85	83	81	80	78	76	75	73	72
BCT	TW P11	250	93	91	89	87	85	83	81	79	78	76	75
BCT	TW P2	210	72	71	70	68	67	66	65	64	63	63	62
BCT	TW P3	215	74	73	71	70	69	68	67	66	65	64	63
BCT	TW P4	225	100	98	95	93	91	88	86	84	83	81	79
BCT	TW P5	230	87	85	83	81	80	78	76	75	74	72	71
BCT	TW P6	220	85	83	81	80	78	76	75	74	72	71	70
BCT	TW P7	222	89	87	85	84	82	80	79	77	76	75	73
BCT	TW P8	221	87	85	84	82	80	79	77	76	75	73	72
BCT	TW P9	260	81	79	78	76	75	73	72	71	69	68	67
BCT	AP CUSTOMS	4105	91	89	87	85	83	81	79	77	75	74	72
BCT	AP RU 23	5110	88	86	84	82	79	77	75	73	71	69	66
BCT	AP RU 5	5105	79	77	75	73	70	68	66	64	62	60	57
BCT	AP RU 5	5115	77	75	74	72	70	69	67	66	65	63	62

5.3 Critical PCI Value

An important concept in pavement management is the critical PCI value, a value that prompts major rehabilitation activities. It serves as a condition threshold that helps determine a section's suitability to receive major work. As soon as a section's PCI reaches the critical PCI value, the rate of PCI loss (deterioration) is expected to increase. The critical PCI concept assumes that once a pavement section deteriorates to this critical level, it is more cost-effective to complete a major rehabilitation project rather than continuing to apply preventive maintenance or deferring major work until more costly reconstruction activities are required. **Figure 5.3 (a)** illustrates the benefit of applying lower cost preventive maintenance to extend the life of the pavement.

Figure 5.3 (a): Pavement Life and the Effect of Treatments



FAA Eligibility Thresholds: >70: Routine Maintenance 55-70: Rehabilitation Eligible <55: Reconstruction Eligible

**Figure is for conceptual purposes only – unit costs are not specific to airfield pavements.*

Critical PCI values vary and are typically based on a pavement's surface type, functional use, and importance, or priority, in daily operations. Pavement priority is generally assigned based on the branch use of a pavement section. In previous System Updates, the critical PCI value was set to 65 for all functional uses. Now, based on FAA Order 5100.38D Change 1 Airport Improvement Handbook, issued February 26, 2019, the FAA has established pavement construction based on thresholds that distinguish Rehabilitation and Reconstruction. Pavement sections between PCI Values 55 and 70 will be considered for Rehabilitation and sections less than 55 will be considered for Reconstruction at the planning-level, as shown in **Table 5.3 (a)**. The FDOT SAPMP will

integrate the PCI thresholds for airfield pavement projects to maintain alignment with the FAA AIP and/or PFC eligibility for project planning. Moving forward, the critical PCI value will be defined at 70 for the FDOT SAPMP. Critical PCI values for this SAPMP System Update are shown in **Table 5.3 (b)**.

Table 5.3 (a): AIP Handbook PCI Requirements for Airfield Pavement Projects

Airfield Pavement Project Type	PCI Requirement
Reconstruction	PCI < 55 (Poor)
Rehabilitation	PCI < 70 (Fair)
Maintenance	N/A

*Source: AIP Handbook, in reference to Runways, Taxiways, and Aprons as seen in table G-2, H-1, and I-1 respectively

Table 5.3 (b): Critical PCI Values by Branch Use

Runway	Taxiway	Apron
70	70	70

Figures 5.3 (b) and **5.3 (c)** depict the decision process for major rehabilitation project identification with the assumption of available funds (Shahin). Should funding be unavailable for pavement sections in need of major rehabilitation, the Airport may elect to apply appropriate localized stopgap repair strategies. As the figures show, once major rehabilitation has been applied, the PCI of the section is reset to 100.

Figure 5.3 (b): Major Rehabilitation Planning Decision Diagram, $PCI < \text{Critical } PCI$

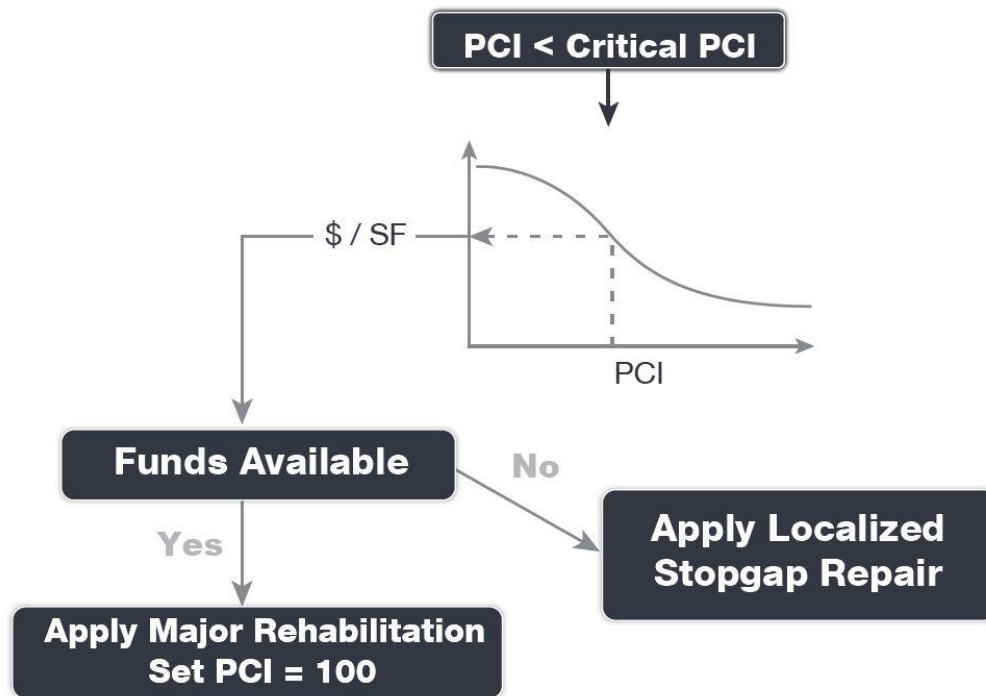
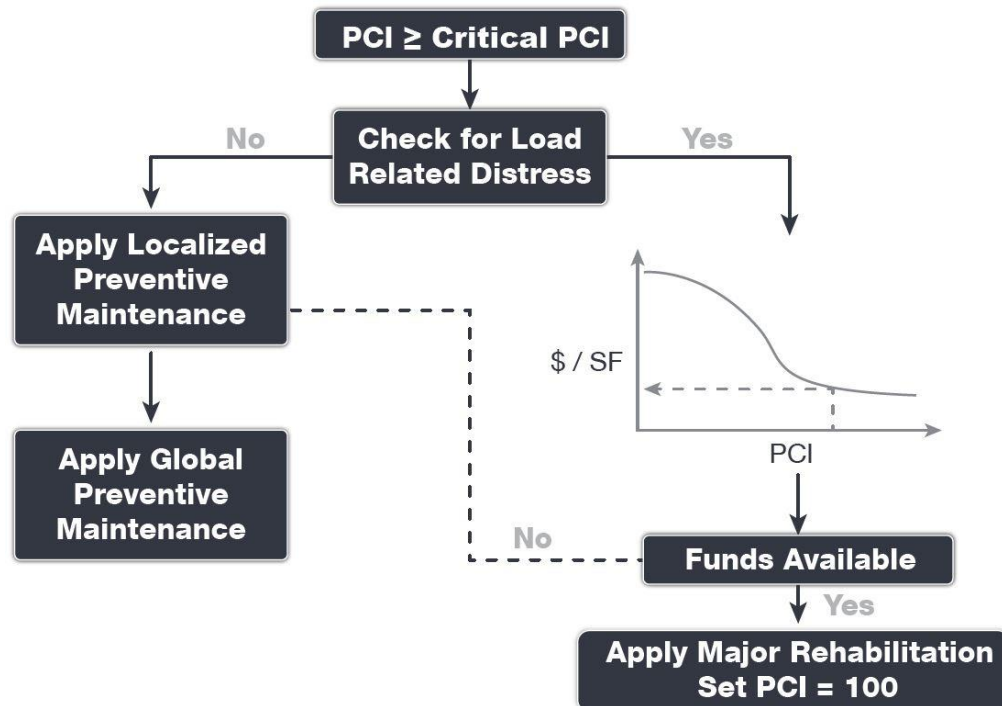


Figure 5.3 (c): Major Rehabilitation Planning Decision Diagram, $PCI \geq \text{Critical } PCI$



5.4 Localized Maintenance and Repair

This section discusses both localized maintenance and major rehabilitation methods and how they may be most effectively applied to extend the life of the pavement network. General maintenance and rehabilitation (M&R) methods are characterized under two (2) broad categories: localized maintenance and major rehabilitation.

Localized maintenance is best applied as a conservation measure and is applied to slow the rate of pavement deterioration. It may, however, be applied as a temporary corrective measure in isolated areas. Proactive localized maintenance, and specifically preservation, is highly recommended to the Airport. However, it is recognized that once pavements have deteriorated below a certain condition threshold (the critical PCI value), the pavement benefits from more substantial rehabilitation in lieu of localized repairs.

Major rehabilitation is recommended when a pavement section falls below the critical PCI value or if a pavement section has a significant presence of load-related distress. Major rehabilitation efforts can correct or improve structural deficiencies and/or functional deterioration for pavement sections within a network.

M&R planning combines methods of repair to address the cause of the problem rather than just treating the symptom. For example, a PCC corner break may require slab under-sealing, full-depth patching, and joint sealing. While these repair methods apply to specific distress and pavement types, they also consider the impact of Foreign Object Debris (FOD) on aircraft operations. Untidy or improperly constructed repair activities may disintegrate and potentially create FOD at or near the repair site. Therefore, maintenance activities must include quality control monitoring to ensure that repairs are conducted properly and clean-up activities are undertaken to address this potential. The current version of the FAA Advisory Circular 150/5210-24 “Airport Foreign Object Debris (FOD) Management” provides additional guidance for developing and managing an airport FOD program.

5.4.1 Localized Maintenance and Repair Approach

Localized maintenance differs from major rehabilitation in that localized maintenance is applied based on the distresses observed and not an averaged or forecasted PCI value. Treatments are selected based on the appropriate corrective measure for a given distress type and severity level. Localized maintenance can be applied either as a preventive measure or a safety (“stopgap”) measure. The two (2) types of localized maintenance are described below in further detail.

- » Localized Preventive Maintenance and Repair
 - Distress maintenance activities performed with the primary objective of slowing the rate of deterioration. These activities typically include crack sealing and patching.
- » Localized Stopgap/Safety Maintenance and Repair
 - Defined as the localized distress repair needed to keep a pavement in a safe and operational condition. These activities are typically applied to high-severity distresses or distresses impacting operations.

5.4.2 Localized Work Types

The following sections provide detailed descriptions of the maintenance policy work types identified in the Localized Maintenance Policy.

AC Crack Sealing

Crack sealing is the process of cleaning and sealing (or resealing) cracks in AC pavements. This repair is used to fill longitudinal and transverse cracks, including reflective cracks and block cracks that are wider than 1/8-inch. The purpose of this treatment is to prevent water and incompressible materials from entering cracks and causing further deterioration of the pavement structure. Accumulation of incompressible materials in cracks may lead to spalling and is a source of FOD. Crack sealing is cost-effective when used as a preventive measure. Depending on the size of the crack, routing and cleaning the crack may be necessary to remove the loose material within the crack for better adherence of the crack sealant to the crack face. Measurement of this work type is typically in linear feet.

AC Full-Depth Patching

This technique involves replacing the full thickness of the AC layer and may include replacement of the base and subbase layers. Full-depth patching is used to repair structural and material-related distresses, such as alligator cracking, corrugation, depressions, rutting, slippage cracking, and swelling in AC pavements. This repair may be limited to the top AC layer (partial-depth patch) if the base and subbase layers exhibit no signs of deterioration. Measurement of this work type is typically in square feet or square yards.

AC Partial-Depth AC Patching

This technique involves the removal of a given thickness of the surface layer using a milling machine and adding back a layer of AC pavement. This technique removes the deteriorated layer and provides a good bond for an overlay. It can correct or improve the structural capacity or functional requirement, such as skid resistance and ride quality. This repair is used for surface distresses that can occur over a large area, such as raveling, shoving, and bleeding. While mill and replace can be a major rehabilitation M&R method when applied at a large scale, its application in a localized capacity to treat specific distress types also classifies it under localized maintenance for the purpose of this study. After milling operations are completed, any cracks still present should be cleaned and sealed prior to the placement of a tack coat and AC overlay layer(s). Measurement of this work type is typically in square feet or square yards.

Grinding

Grinding is the process of removing a thin layer of the existing concrete by grinding it with a series of closely spaced, rotating saw blades. This method is used to re-profile jointed concrete pavements with poor ride quality due to faulting or warping. Grinding is also used to restore transverse drainage and to provide a textured pavement surface. The concern with this type of maintenance is that if too much material is removed, the overall structural composition of the pavement section may change, potentially reducing the overall life of the pavement. Measurement of this work type is typically in square feet or square yards.

Monitor Pavement

Monitor pavement is recommended when the distresses do not interfere with ride quality, do not have FOD potential, and do not pose an immediate safety concern.

PCC Crack Sealing

Crack sealing is the process of routing, cleaning, and sealing (or resealing) cracks in PCC pavement to prevent water from infiltrating into the pavement foundation and to stop the accumulation of incompressible materials in the cracks. Water entering cracks can weaken the subgrade, potentially leading to pumping, corner breaks, and/or shattered slabs. Accumulation of incompressible materials in cracks may lead to spalling and is a source of FOD. Routing and cleaning of the crack is often necessary to adhere the crack sealant to both sides of the crack. Measurement of this work type is typically in linear feet.

PCC Full-Depth Patching

This type of M&R activity involves full-depth replacement of a portion of a PCC slab. This repair is used for medium- and high-severity corner breaks, medium-severity durability cracking, medium-severity blowups and buckling, and high-severity large patches. This repair requires restoring load transfer if near a joint or crack. Measurement of this work type is typically in square feet or square yards.

PCC Joint Seal

Joint sealing is the process of cleaning and sealing (or resealing) joints in PCC pavement to prevent water from infiltrating into the pavement foundation and to stop the accumulation of incompressible materials in the joints. Water entering joints can weaken the subgrade, potentially leading to pumping, corner breaks, and/or shattered slabs. Accumulation of incompressible materials in joints leads to spalling of the concrete and is a source of FOD. In some cases, it may be necessary to re-saw the pavement joints to remove old material prior to resealing. Measurement of this work type is typically in linear feet.

PCC Partial-Depth Patching

Partial-depth patching involves removing shallow, localized areas of deteriorated or spalled PCC pavement and replacing them with a suitable patch-like cement concrete or epoxy concrete. This method is used to repair distresses that are confined to the top few inches of the slab, such as joint and corner spalling. This repair would require restoring the joint sealant if near a joint. Measurement of this work type is typically in square feet or square yards.

PCC Slab Replacement

This type of M&R activity involves full-depth replacement of an entire PCC slab. This repair is used to repair high-severity blowups and buckling, high-severity durability cracking, medium- and high-severity shattered slabs, and medium- and high-severity ASR. This repair requires restoring load transfer with adjacent slabs through dowels or similar means. Measurement of this work type is typically in square feet or square yards.

Surface Seal

Application of a surface treatment provides AC-surfaced pavements with an unoxidized layer of bituminous material that can help extend the life of a pavement that is experiencing climate-related distresses such as weathering and raveling. The surface treatment can also serve as a repair that re-establishes a bond between aggregates, slowing pavement deterioration and reducing FOD potential. Measurement of this work type is typically in square feet or square yards.

5.4.3 Localized Maintenance Planning-Level Unit Costs

The activities identified here are based on research of practical pavement treatments in consideration of the FAA AC 150/5380-6C. The Localized Maintenance Policies and associated planning-level unit costs are developed in consideration of a network-level analysis.

The Localized Maintenance and Repair Policies and associated planning-level unit costs are based on a statewide consideration of pavement treatments and construction costs from both airfield pavements and the FDOT Historical Cost Information archives. Furthermore, a consideration of limited repair quantities is factored into the determination of conservative planning-level unit costs. Neither the FDOT nor the Consultant team have control over the cost of labor, materials, equipment, the Contractor's methods of determining prices, or over competitive bidding or market conditions. Opinions of probable construction costs provided herein are based on the information known to the FDOT at this time and represent only the Consultant team's judgment as a design professional familiar with the construction industry. This Report cannot and does not guarantee that proposals, bids, or actual construction costs will not vary from its opinions of probable construction costs.

Tables 5.4.3 (a) and (b) display the cost by maintenance activity for AC and PCC pavement types, respectively. Because the localized maintenance activities identified for both preventive and stopgap work types are based on a statewide network approach, project-specific evaluations and maintenance quantities should be developed prior to construction.

Table 5.4.3 (a): Localized M&R Planning-Level Unit Costs – Asphalt Concrete

Localized Work Type	Reliever Costs	Work Type Unit
AC Crack Sealing	\$ 4.00	LF
AC Full-Depth Patching	\$ 11.50	SF
AC Partial-Depth Patching	\$ 4.75	SF
Surface Seal	\$ 0.75	SF

Table 5.4.3 (b): Localized M&R Planning-Level Unit Costs – Portland Cement Concrete

Localized Work Type	Reliever Costs	Work Type Unit
Grinding	\$ 2.00	SF
PCC Crack Sealing	\$ 7.00	LF
PCC Joint Seal	\$ 4.25	LF
PCC Full-Depth Patching	\$ 65.00	SF
PCC Partial-Depth Patching	\$ 169.00	SF
PCC Slab Replacement	\$ 51.50	SF

* PCC Partial-Depth Patching considers high-early-strength and high-performing repair material.

5.4.4 Localized Maintenance and Repair Policy

Table 5.4.4 and **Table 5.4.5** depicts the Localized Preventive Maintenance Policy and the Localized Stopgap Maintenance Policy for AC and PCC pavements. The resulting Localized Maintenance recommendations for this program are identified based on this policy.

Table 5.4.4: AC Pavement Localized Preventive & Stopgap Maintenance & Repair Policy

Distress	Severity	Description	AC Preventive Work Type	AC Stopgap Work Type
41	Low	Alligator Cracking	Monitor Pavement	Monitor Pavement
41	Medium	Alligator Cracking	AC Full Depth Patching	AC Full Depth Patching
41	High	Alligator Cracking	AC Full Depth Patching	AC Full Depth Patching
42	N/A	Bleeding	Monitor Pavement	Monitor Pavement
43	Low	Block Cracking	Monitor Pavement	Monitor Pavement
43	Medium	Block Cracking	AC Crack Sealing	Monitor Pavement
43	High	Block Cracking	AC Crack Sealing	AC Crack Sealing
44	Low	Corrugation	Monitor Pavement	Monitor Pavement
44	Medium	Corrugation	AC Full Depth Patching	Monitor Pavement
44	High	Corrugation	AC Full Depth Patching	AC Full Depth Patching
45	Low	Depression	Monitor Pavement	Monitor Pavement
45	Medium	Depression	AC Full Depth Patching	Monitor Pavement
45	High	Depression	AC Full Depth Patching	AC Full Depth Patching
46	N/A	Jet Blast	Monitor Pavement	Monitor Pavement
47	Low	Jt. Reflective Cracking	Monitor Pavement	Monitor Pavement
47	Medium	Jt. Reflective Cracking	AC Crack Sealing	Monitor Pavement
47	High	Jt. Reflective Cracking	AC Full Depth Patching	AC Full Depth Patching
48	Low	L&T Cracking	Monitor Pavement	Monitor Pavement
48	Medium	L&T Cracking	AC Crack Sealing	Monitor Pavement
48	High	L&T Cracking	AC Full Depth Patching	AC Full Depth Patching
49	N/A	Oil Spillage	Monitor Pavement	Monitor Pavement
50	Low	Patching	Monitor Pavement	Monitor Pavement
50	Medium	Patching	AC Full Depth Patching	Monitor Pavement
50	High	Patching	AC Full Depth Patching	AC Full Depth Patching
51	N/A	Polished Aggregate	Monitor Pavement	Monitor Pavement
52	Low	Raveling	Surface Seal	Monitor Pavement
52	Medium	Raveling	Surface Seal	Monitor Pavement
52	High	Raveling	AC Partial Depth Patching	AC Partial Depth Patching
53	Low	Rutting	Monitor Pavement	Monitor Pavement
53	Medium	Rutting	AC Full Depth Patching	Monitor Pavement
53	High	Rutting	AC Full Depth Patching	AC Full Depth Patching
54	Low	Shoving	Monitor Pavement	Monitor Pavement
54	Medium	Shoving	AC Partial Depth Patching	Monitor Pavement
54	High	Shoving	AC Full Depth Patching	AC Full Depth Patching
55	N/A	Slippage Cracking	AC Full Depth Patching	AC Full Depth Patching
56	Low	Swelling	Monitor Pavement	Monitor Pavement
56	Medium	Swelling	AC Full Depth Patching	Monitor Pavement
56	High	Swelling	AC Full Depth Patching	AC Full Depth Patching

Distress	Severity	Description	AC Preventive Work Type	AC Stopgap Work Type
57	Low	Weathering	Monitor Pavement	Monitor Pavement
57	Medium	Weathering	Surface Seal	Monitor Pavement
57	High	Weathering	AC Partial Depth Patching	Surface Seal

Table 5.4.5: PCC Pavement Localized Preventive & Stopgap Maintenance & Repair Policy

Distress	Severity	Description	PCC Preventive Work Type	PCC Stopgap Work Type
61	Low	Blow-up	PCC Full Depth Patching	Monitor Pavement
61	Medium	Blow-up	PCC Full Depth Patching	PCC Full Depth Patching
61	High	Blow-up	PCC Slab Replacement	PCC Slab Replacement
62	Low	Corner Break	Monitor Pavement	Monitor Pavement
62	Medium	Corner Break	PCC Full Depth Patching	PCC Full Depth Patching
62	High	Corner Break	PCC Full Depth Patching	PCC Full Depth Patching
63	Low	Linear Cracking	Monitor Pavement	Monitor Pavement
63	Medium	Linear Cracking	PCC Crack Sealing	PCC Crack Sealing
63	High	Linear Cracking	PCC Full Depth Patching	PCC Crack Sealing
64	Low	Durability Cracking	Monitor Pavement	Monitor Pavement
64	Medium	Durability Cracking	PCC Full Depth Patching	PCC Full Depth Patching
64	High	Durability Cracking	PCC Slab Replacement	PCC Slab Replacement
65	Low	Jt. Seal Damage	PCC Joint Seal	Monitor Pavement
65	Medium	Jt. Seal Damage	PCC Joint Seal	Monitor Pavement
65	High	Jt. Seal Damage	PCC Joint Seal	PCC Joint Seal
66	Low	Small Patch	Monitor Pavement	Monitor Pavement
66	Medium	Small Patch	PCC Partial Depth Patching	Monitor Pavement
66	High	Small Patch	PCC Partial Depth Patching	PCC Partial Depth Patching
67	Low	Large Patch	Monitor Pavement	Monitor Pavement
67	Medium	Large Patch	PCC Full Depth Patching	Monitor Pavement
67	High	Large Patch	PCC Full Depth Patching	PCC Full Depth Patching
68	N/A	Popouts	Monitor Pavement	Monitor Pavement
69	N/A	Pumping	Monitor Pavement	Monitor Pavement
70	Low	Scaling	Monitor Pavement	Monitor Pavement
70	Medium	Scaling	PCC Slab Replacement	Monitor Pavement
70	High	Scaling	PCC Slab Replacement	PCC Slab Replacement
71	Low	Faulting	Monitor Pavement	Monitor Pavement
71	Medium	Faulting	Grinding	Monitor Pavement
71	High	Faulting	PCC Slab Replacement	PCC Slab Replacement
72	Low	Shattered Slab	PCC Crack Sealing	Monitor Pavement
72	Medium	Shattered Slab	PCC Slab Replacement	PCC Crack Sealing
72	High	Shattered Slab	PCC Slab Replacement	PCC Slab Replacement
73	N/A	Shrinkage Cracking	Monitor Pavement	Monitor Pavement

Distress	Severity	Description	PCC Preventive Work Type	PCC Stopgap Work Type
74	Low	Joint Spall	Monitor Pavement	Monitor Pavement
74	Medium	Joint Spall	PCC Partial Depth Patching	PCC Partial Depth Patching
74	High	Joint Spall	PCC Partial Depth Patching	PCC Partial Depth Patching
75	Low	Corner Spall	Monitor Pavement	Monitor Pavement
75	Medium	Corner Spall	PCC Partial Depth Patching	PCC Partial Depth Patching
75	High	Corner Spall	PCC Partial Depth Patching	PCC Partial Depth Patching
76	Low	ASR	Monitor Pavement	Monitor Pavement
76	Medium	ASR	PCC Slab Replacement	PCC Slab Replacement
76	High	ASR	PCC Slab Replacement	PCC Slab Replacement

5.5 Major Rehabilitation

Major rehabilitation is recommended to correct or improve structural deficiencies and/or functional deterioration. Often, when pavements are subject to significant changes in the aircraft fleet mix (frequency and type), major rehabilitation is required to provide a pavement section that can meet the structural demands of traffic loading. Major rehabilitation is generally described as a pavement construction that removes and replaces the pavement surface, thus resetting the PCI value to 100 and the pavement age to zero. Typical policies include full- and partial-depth reconstruction and mill and overlay.

5.5.1 Major Rehabilitation Pavement Section Development

Once the timing of the major rehabilitation activity is determined based on the PCI value, existing as-built record documentation is used to determine typical rehabilitation processes and pavement sections. Refinement of the pavement section layers is performed in consideration of the FAA AC 150/5320-6F. It should be noted that no subsurface geotechnical investigation, American Land Title Association (ALTA)/American Congress on Surveying and Mapping (ACSM) Survey, topographic survey, utilities survey, environmental, or site-specific air traffic study(s) have been utilized in the development of the design criteria. No warranty or assurance is implied in this document for final design nor construction for any airfield pavements discussed within this Report.

Major rehabilitation is divided into two (2) policy categories as part of this System Update: Full-Depth Reconstruction (Reconstruction) and Intermediate Major Rehabilitation (Rehabilitation). Based on the pavement type, the general categories are defined as AC Reconstruction and AC Rehabilitation for AC, AAC, and APC pavement types, and PCC Reconstruction and PCC Rehabilitation for PCC pavement types. The pavement sections are based on the average Reliever Airport Type requirements; no pavement design has been performed in accordance with the FAA AC 150/5320-6F for the determined conceptual sections. **Table 5.5.1** provide details on the conceptual pavement sections developed for this study.

Table 5.5.1: Conceptual Pavement Sections for Major Rehabilitation

Rehabilitation Type	Reliever Pavement Section
AC Reconstruction	
<i>Full-depth asphalt pavement section reconstruction. Removal of existing pavement section and construction of a new section.</i> PCI <55	Pavement Removal
	Unclassified Excavation
	Subgrade Stabilization (12")
	Limerock Base Course (8")
	Prime Coat
	Tack Coat
	P-401 Surface Course (4")
	<i>Excludes any paved shoulder features</i>
AC Rehabilitation	
<i>Combination of asphalt pavement milling and replacement overlay with 15% of the areas subject to full-depth reconstruction.</i> PCI = 55 to 70	15% AC Reconstruction
	Mill and Overlay
	AC Milling (3")
	Tack Coat
	P-401 Surface Course (3")
	<i>Excludes any paved shoulder features</i>
PCC Reconstruction	
<i>Full-depth rigid pavement section reconstruction.</i> PCI < 55	Pavement Removal
	Unclassified Excavation
	Subgrade Stabilization (12")
	Limerock Base Course (6")
	P-501 PCC Pavement (14")
	PCC Joint Seal
PCC Rehabilitation	
<i>Rehabilitation of PCC pavement with a combination of crack sealing, joint seal replacement, limited patching, and replacement of 15% of slab panels.</i> PCI = 55 to 70	15% Slab Replacement
	Joint and Crack Seal
	Limited Patching

The identification of rehabilitation needs and conceptual pavement sections have been determined at the planning level. Design-level investigation is recommended prior to developing construction-level design documents and budgets. This type of construction typically warrants consideration for non-pavement efforts that may include drainage, turfing, electrical lighting, pavement marking, construction contingency, mobilization costs, and project soft costs.

Reconstruction (AC or PCC)

Reconstruction is the removal and replacement of the existing AC or PCC pavement and base layer and includes preparation of the existing subgrade material. This technique is utilized when the pavement is badly deteriorated or a structural improvement is required. Reconstruction is used when the pavements are structurally deficient and an overlay is not possible due to adjacent pavement grades.

AC Rehabilitation

AC Rehabilitation, for the purposes of this SAPMP, is a removal of all or a portion of the asphalt surface through milling and replacing the milled depth with an overlay of asphalt. This rehabilitation activity is typically applied to pavement that does not require a structural improvement and does not display an extensive amount of load-related distresses. However, this work type conservatively accounts for 15% of the planned area to receive a full-depth replacement of the pavement structure. This is meant to capture any deficiencies that may not be apparent from a visual evaluation of the surface of the pavement. This work type occurs on pavement sections with a PCI value between 55 and 70. As a general rule of thumb, intermediate rehabilitation activities have a shorter pavement life compared to a full-depth reconstruction, but AC Rehabilitation will still reset the pavement to a PCI of 100.

PCC Rehabilitation

PCC Rehabilitation, for the purposes of this SAPMP, is a planning-level estimate of several concurrent PCC maintenance activities intended to raise the PCI above Critical without reconstructing the entire area. This work type accounts for the replacement of 15% of the slabs as well as a PCC patching, crack sealing, and joint sealing for areas outside of the panel replacement. This work type occurs on pavement sections with a PCI value between 55 and 70.


5.5.2 Major Rehabilitation Planning-Level Unit Costs

Planning-level opinions of probable construction cost developed for this System Update are based on archived bid tabulations and records from airfield pavement projects provided by participating airports. A review of cost trends and cost factors have been incorporated to assist airports in planning for project budgets.


Neither the FDOT nor the Consultant team have control over the cost of labor, materials, equipment, Contractor's methods of determining prices, or over competitive bidding or market conditions. Opinions of probable construction costs provided herein are based on the information known to the FDOT at this time and represent only the Consultant team's judgment as a design professional familiar with the construction industry. This Report cannot and does not guarantee that proposals, bids, or actual construction costs will not vary from its opinions of probable construction costs. **Table 5.5.2** depicts the associated work type planning-level unit costs for Major Rehabilitation for each pavement type.

Table 5.5.2: RL Major Rehabilitation Planning-Level Unit Cost by Pavement Type

Rehabilitation Type	PCI Range	Asphalt Concrete Cost per SF	Portland Cement Concrete Cost Per SF
Rehabilitation	55 to 70	\$10.50	\$22.50
Reconstruction	0 to 55	\$18.50	\$45.00



Chapter 6: M&R Planning and Budget Scenario Analysis



Chapter 6 – M&R Planning and Budget Scenario Analysis

6.1 Localized Maintenance and Repair Analysis and Recommendations

This FDOT SAPMP System Update provides a planning-level estimation of Localized Maintenance and Repair costs based on the results of the latest PCI assessment performed at the Airport. Due to the limited sample units inspected in certain pavement sections, a statistical extrapolation of distresses is used to estimate the quantities of recommended repair activities at the section level, based the policies defined in **5.4.4 Localized Maintenance and Repair Policy**. These work quantities are limited to a near-term application since they were determined directly from the PCI assessment efforts. As pavements continue to deteriorate year-to-year, quantities and/or distress severities may increase, which will affect the amount and type of localized maintenance required. This analysis can be utilized as a planning tool to assist Airport staff in determining an annual budget allocation for maintenance activities that will help maintain Airport pavements above the critical PCI value and extend the life of the pavement.

Table 6.1 (a) provides a summary of the anticipated planning-level costs for Year 1 Localized Preventive Maintenance and Localized Stopgap Maintenance. The following table depicts planning-level costs rounded up to the next 10-dollar increment.

Table 6.1 (a): Year 1 Summary of Localized Maintenance

Work Category	Cost
Preventive	\$ 63,520
Stopgap	\$ -
Planning-Level Localized M&R Needs =	\$ 63,520

Localized Preventive Maintenance is typically applied to pavements that are in a condition above the critical PCI value of the pavement section. Localized Stopgap Maintenance is typically applied to pavement sections that are at or below the critical PCI value. Application of localized maintenance and repair should be coordinated with the planning of major rehabilitation efforts identified through the Major Rehabilitation analysis. Pavements with stopgap recommendations that are subject to near-term major rehabilitation efforts may remove the need to perform localized (stopgap) maintenance efforts in subsequent years.

Table 6.1 (b) summarizes the anticipated Year 1 Localized Maintenance recommendations by work type, based on the PCI assessment efforts performed as part of this SAPMP System Update. The following table depicts planning-level costs rounded up to the next 10-dollar increment.

Table 6.1 (b): Year 1 Localized Maintenance by Work Type Summary

Localized Maintenance Category	Localized Work Type	Rough Estimate of Work Quantity	Work Units	Planning Material Cost
Localized Preventive Maintenance	AC Crack Sealing	178	LF	\$ 720
	Surface Seal	83,168	SF	\$ 62,530
	AC Full-Depth Patching	24	SF	\$ 270

Table 6.1 (c) provides a breakdown of the anticipated planning-level costs by section for those areas exhibiting distresses that would benefit from Year 1 Localized M&R. The table shows the approximate improved “End Condition” PCI value of the section after the application of Localized M&R. This approximation is intended to depict a planning-level estimate of the effect of the localized M&R on the section-level PCI; the performance of the work does not guarantee the pavement will not deteriorate in other ways outside of the described treatment. The following table depicts planning-level costs rounded up to the next 10-dollar increment.

Table 6.1 (c): Section-Level Year 1 Localized M&R Planning Cost Summary

Network ID	Branch ID	Section ID	Area (SF)	Start PCI	End PCI	Cost
BCT	RW 5-23	6105	520,000	86	92	\$ 36,140
BCT	RW 5-23	6106	72,700	89	92	\$ 1,820
BCT	RW 5-23	6107	35,000	91	94	\$ 1,320
BCT	RW 5-23	6110	260,000	90	91	\$ 3,510
BCT	RW 5-23	6111	36,350	83	90	\$ 1,450
BCT	RW 5-23	6112	17,500	94	94	\$ -
BCT	TW A	130	8,671	81	86	\$ 650
BCT	TW B	125	14,426	100	100	\$ -
BCT	TW C	120	14,426	100	100	\$ -
BCT	TW E	116	14,729	70	70	\$ -
BCT	TW F	115	14,426	100	100	\$ -
BCT	TW G	110	8,671	84	87	\$ 330
BCT	TW H	111	7,946	78	85	\$ 410
BCT	TW P	105	193,060	82	88	\$ 10,240
BCT	TW P	106	29,080	86	90	\$ 1,110
BCT	TW P	107	14,241	86	89	\$ 540
BCT	TW P	108	10,940	79	82	\$ 420
BCT	TW P	112	12,673	82	89	\$ 540
BCT	TW P	113	4,000	81	90	\$ 290
BCT	TW P	131	12,673	80	85	\$ 270
BCT	TW P1	205	4,102	88	91	\$ 160
BCT	TW P10	240	8,665	89	92	\$ 180
BCT	TW P11	250	8,665	93	95	\$ 180
BCT	TW P2	210	2,572	72	87	\$ 180
BCT	TW P3	215	1,488	74	80	\$ 70
BCT	TW P4	225	8,391	100	100	\$ -
BCT	TW P5	230	8,400	87	89	\$ 180
BCT	TW P6	220	8,315	85	91	\$ 260
BCT	TW P7	222	4,994	89	89	\$ -

Network ID	Branch ID	Section ID	Area (SF)	Start PCI	End PCI	Cost
BCT	TW P8	221	5,090	87	87	\$ -
BCT	TW P9	260	3,165	81	88	\$ 130
BCT	AP CUSTOMS	4105	82,167	91	92	\$ 1,380
BCT	AP RU 23	5110	21,414	88	92	\$ 810
BCT	AP RU 5	5105	26,544	79	81	\$ 490
BCT	AP RU 5	5115	11,787	77	80	\$ 420

6.2 Major Rehabilitation Needs

Major rehabilitation is identified within the FDOT SAPMP as a major construction activity that results in a substantial improvement to the pavement condition and resets the pavement section's PCI value to 100. Major rehabilitation recommendations (AC Rehabilitation, AC Reconstruction, PCC Rehabilitation, and PCC Reconstruction) should be considered as planning-level only. Additional design-level investigation in accordance with FAA Advisory Circulars is required. Recommendations identified within this planning document do not imply final design.

The objective of the Major Pavement Rehabilitation Needs analysis is to develop planning-level projects within an Airport's airfield pavement network. As depicted in **Figures 5.3 (b) and (c)** in **Chapter 5**, major rehabilitation activities are recommended when a pavement section has deteriorated below the critical PCI value, a point at which localized maintenance and repair activities may not be a cost-effective solution. In addition, major rehabilitation is also recommended when the section's PCI value is above the critical PCI value with the section exhibiting a significant amount of load-related distresses. Identification of rehabilitation needs is done at the section-level. This, however, does not limit the Airport from further refining limits of project planning areas.

6.2.1 10-Year Unconstrained Budget Major Rehabilitation Needs

Major rehabilitation needs are identified by analyzing the Airport's pavement condition in relationship to critical PCI values, major rehabilitation policies, and unit costs, assuming there are no budget constraints. This is done over a 10-year analysis period. While this is financially impractical, it does yield the unbiased pavement needs over a 10-year time frame at the Airport given current and forecasted pavement conditions. The FDOT recognizes that airports are constrained by budgets and does not intend to convey an unrealistic approach of addressing pavement rehabilitation. Each airport has a unique set of challenges and FDOT's goals are to provide it with the data needed to formulate a practical Capital Improvement Program and identify needs in the Joint Automated Capital Improvement Program (JACIP). This includes:

- » An estimation of current pavement condition;
- » Major pavement rehabilitation needs based on condition and policies; and
- » Planning-level cost estimates for the major rehabilitation needs.

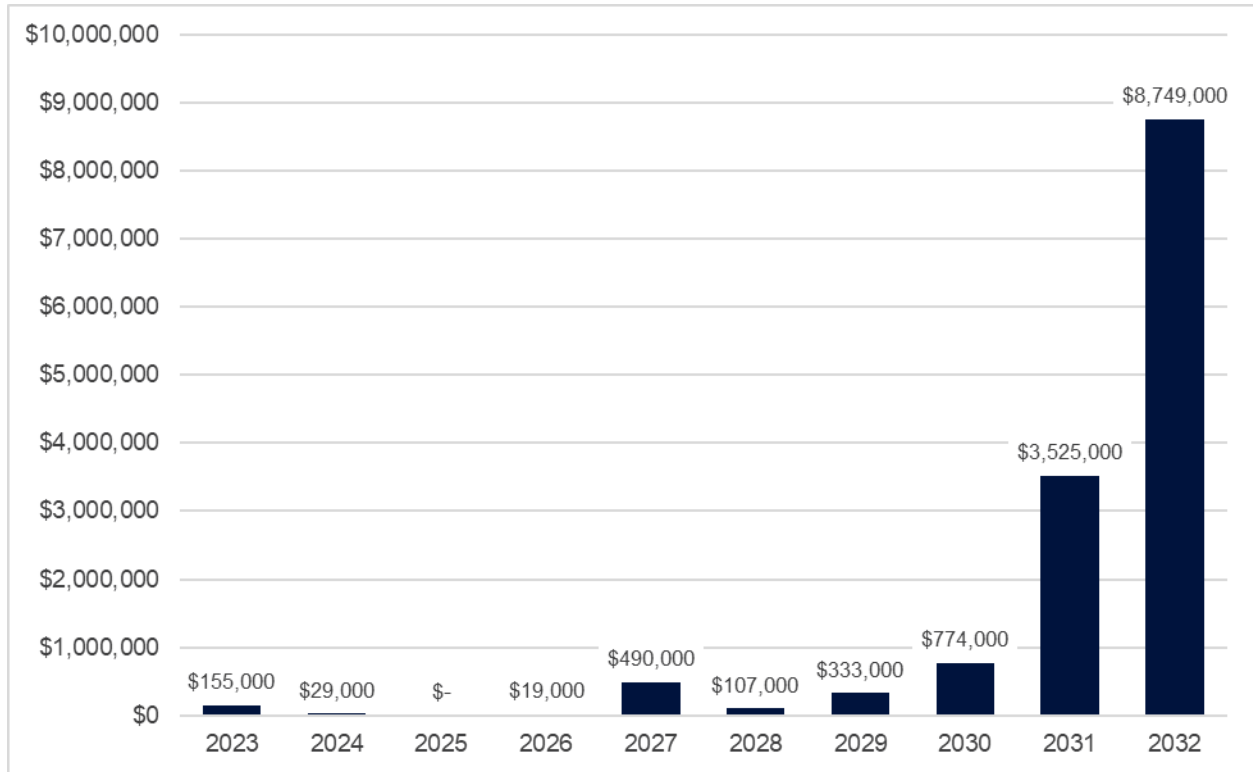
Table 6.2.1 (a) summarizes section-level major rehabilitation needs forecasted for a 10-year period. It should be noted that the following table depicts planning-level costs and has been rounded up to the nearest \$1,000 for planning purposes.

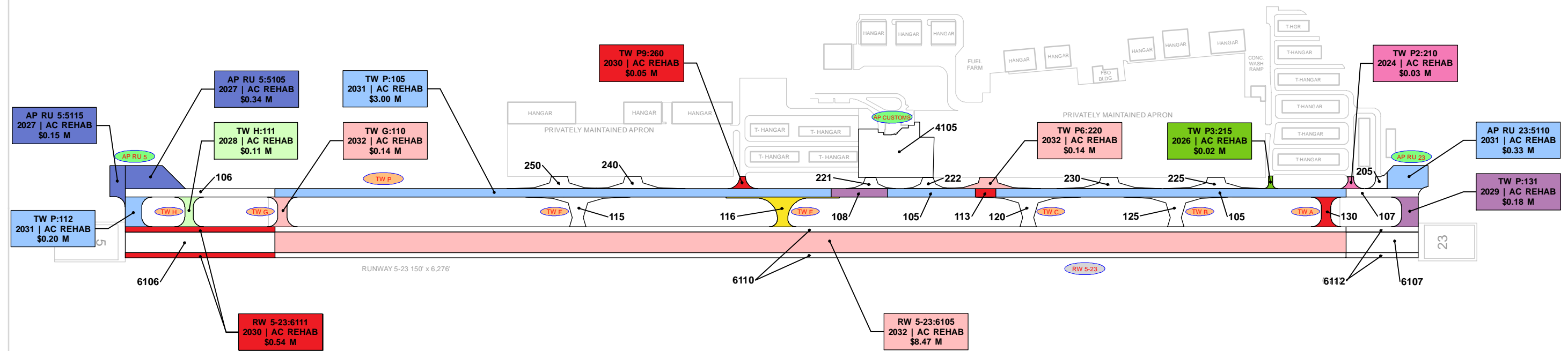
Table 6.2.1 (a): Section-Level 10-Year Major Rehabilitation Needs

Program Year	Network ID	Branch ID	Section ID	Surface	Area (SF)	PCI Before	Rehabilitation Type	Planning Cost Estimate
2023	BCT	TW E	116	AC	14,729	69	AC Rehabilitation	\$ 155,000
2024	BCT	TW P2	210	AAC	2,572	70	AC Rehabilitation	\$ 29,000
2026	BCT	TW P3	215	AAC	1,488	69	AC Rehabilitation	\$ 19,000
2027	BCT	AP RU 5	5105	AAC	26,544	68	AC Rehabilitation	\$ 339,000
2027	BCT	AP RU 5	5115	AC	11,787	69	AC Rehabilitation	\$ 151,000
2028	BCT	TW H	111	AAC	7,946	70	AC Rehabilitation	\$ 107,000
2029	BCT	TW P	108	AAC	10,940	69	AC Rehabilitation	\$ 154,000
2029	BCT	TW P	131	AAC	12,673	70	AC Rehabilitation	\$ 179,000
2030	BCT	RW 5-23	6111	AAC	36,350	69	AC Rehabilitation	\$ 538,000
2030	BCT	TW A	130	AAC	8,671	69	AC Rehabilitation	\$ 129,000
2030	BCT	TW P	113	AAC	4,000	69	AC Rehabilitation	\$ 60,000
2030	BCT	TW P9	260	AAC	3,165	69	AC Rehabilitation	\$ 47,000
2031	BCT	TW P	105	AAC	193,060	69	AC Rehabilitation	\$ 2,995,000
2031	BCT	TW P	112	AAC	12,673	69	AC Rehabilitation	\$ 197,000
2031	BCT	AP RU 23	5110	AAC	21,414	69	AC Rehabilitation	\$ 333,000
2032	BCT	RW 5-23	6105	AAC	520,000	69	AC Rehabilitation	\$ 8,471,000
2032	BCT	TW G	110	AAC	8,671	69	AC Rehabilitation	\$ 142,000
2032	BCT	TW P6	220	AAC	8,315	70	AC Rehabilitation	\$ 136,000

Figure 6.2.1 (a) summarizes the section-level major rehabilitation needs for a 10-year period between 2023 and 2032. **Figure 6.2.1 (b)**, the Airfield Pavement Major Rehabilitation Exhibit, graphically depicts the major rehabilitation needs with rounded costs. As suggested previously, this is planning-level data that can be used by the Airport to support developing a practical CIP.

Figure 6.2.1 (a): 10-Year Major Rehabilitation Needs by Program Year





LEGEND

— TYPICAL RUNWAY BRANCH ID
— TYPICAL TAXIWAY BRANCH ID
— TYPICAL APRON BRANCH ID

PROGRAM YEAR

2023	2028
2024	2029
2025	2030
2026	2031
2027	2032

"BRANCH"; "SECTION"
"YEAR"; "REHAB ACTIVITY"
"EST. COST"

RUNWAY LENGTHS DEPICTED IN THIS DRAWING ARE FOR PAVEMENT MANAGEMENT PURPOSES ONLY AND MAY NOT MATCH PUBLISHED RUNWAY LENGTHS. DRAWING NOT TO SCALE.



Chapter 7: Conclusion



Chapter 7 – Conclusion

7.1 Recommendations

7.1.1 Continued PCI Surveys

It is recommended that the Airport continue to perform regularly scheduled PCI surveys in accordance with the ASTM D5340-20 (or latest edition) to monitor the condition of airfield pavement facilities.

A high priority should be placed on maintaining good record keeping and re-inspecting the Airport's maintained pavement facilities to ensure continued safe aircraft operations. Per the FAA AC 150/5380-7B, a series of scheduled periodic inspections must be carried out for an effective maintenance program. Re-inspection of pavements should be scheduled in a timely manner to ensure that all areas, particularly those that may not come under day-to-day observation, are thoroughly evaluated and reported.

7.1.2 Localized Maintenance and Repair

While deterioration of the pavements due to usage and exposure to the environment cannot be prevented, applying timely and effective maintenance efforts can slow the anticipated rate of deterioration. Lack of adequate and timely maintenance is a significant factor in pavement deterioration. **Chapter 6** identified localized maintenance and repair needs. It is recommended that Airport sponsors coordinate with their respective Airport maintenance staff and Airport engineer when developing project-level maintenance and repair efforts.

7.1.3 Major Rehabilitation

Chapter 6 also identified major pavement rehabilitation project needs from 2023-2032. Identification of these rehabilitation needs are performed at the section level for manageable project areas and assume an unconstrained budget scenario. Given the uncertainty in Airport-specific budget information and prioritization goals, the unconstrained budget scenario represents a conservative scenario and identifies pavement needs over a 10-year period. Certainly, it is understood that most airports are faced with constrained budgets, thus further evaluation of projects based on prioritization, operational criticality, funding availability, and practicality is recommended.

7.1.4 Pavement Management System

The following recommendations are made to fully implement an effective pavement management program for the Airport:

- » Develop a detailed preventive maintenance program for the Airport based on the recommendations provided in **Section 6.1**;
- » Further refine and implement the identified 10-year major rehabilitation needs provided in **Section 6.2**;
- » Maintain detailed records on pavement maintenance, construction, and inspection; and
- » Maintain records on major pavement construction projects (year, scope, cost, and construction documents).

7.2 Supporting Documents

Airfield Pavement Network Definition Exhibit

The Airfield Pavement Network Definition Exhibit is located in **Chapter 3** and **Appendix C**. The Exhibit depicts the airfield layout in a manner that defines the airfield pavement infrastructure as branches, sections, and sample units in accordance with the ASTM D5340-20. The Exhibit is intended for planning purposes only. Further details can be found on the Airport's adopted Airport Layout Plan. Detailed characteristics are tabulated in **Appendix A**.

Airfield Pavement System Inventory Exhibit

The Airfield Pavement System Inventory Exhibit is located in **Chapter 3** and **Appendix C**. The Exhibit depicts recent and/or anticipated construction activity within the airfield pavement facilities reported by Airport staff. The Exhibit is intended to schematically identify the pavement limits of work and general work description. The information reported on the Airport Response Form provided by each participating airport was used as the basis of the changes. Furthermore, changes are confirmed at the Airport with Airport staff during the in-brief and debrief meeting.

Airfield Pavement Estimated Age Exhibit

The Airfield Pavement Estimated Age Exhibit is located in **Chapter 3** and **Appendix C**. Based on the review of historic airfield pavement construction activities, the Exhibit provides the approximate limits of the age of the pavement sections since the last major construction activity has occurred. This is intended to be a rough estimate based on interpretation of the limited data available at the time of report.

Airfield Pavement Condition Index Exhibit

The Airfield Pavement Condition Index Exhibit is located in **Chapter 4** and **Appendix C**. The Exhibit is a visual summary of the latest conditions reported from the PCI assessment performed at the Airport. Distress analysis occurred in accordance with ASTM D5340-20 (referenced in **Appendix E**), with results being analyzed using PAVER™ software to determine PCI values. The PCI values are identified in the Exhibit and graphically represented using the standard ASTM D5340-20 condition rating categories.

Airfield Pavement Major Rehabilitation Exhibit

The Airfield Pavement Major Rehabilitation Exhibit is located in **Chapter 6** and **Appendix C**. The Exhibit has been prepared based on the section condition analysis, pavement condition forecasts, and major rehabilitation needs analysis. The Exhibit graphically depicts the inventory with the associated rehabilitation type activity, program year, and the planning-level costs. Area limits, rehabilitation type, and planning-level costs should not be considered a design-level recommendation. A tabulation of the 10-Year Major Rehabilitation is located in **Appendix B**.

Inspection Photograph Documentation

Representative field conditions from the PCI assessment are documented with digital photographs located in **Appendix D**. Select photographs are provided with a limited caption on the distress(es) observed. "Vicinity" photos refer to the approximate boundaries of an inspected sample unit within the section and provide an overview of the section condition but are not focused on a specific distress. The Appendix does not contain photographs for every section and sample unit.

7.3 Conclusion

The FDOT SAPMP System Update Phase 2 2021-2023 was completed for the Airport on behalf of the FDOT AO in accordance with the FAA AC 150/5380-7B and 150/5380-6C. FDOT's implementation of the SAPMP has assisted public airports with this requirement in performing PCI survey inspections and analysis in accordance with the ASTM D5340-20.

7.4 References

The following documents are referenced as specific guidelines and procedures for maintaining Airport pavements, establishing an effective pavement maintenance program, and identifying specific pavement distresses, probable causes of distresses, survey guidelines, and recommended methods of repair.

- » ASTM D5340-20, Standard Test Method for Airport Pavement Condition Index Surveys, American Society for Testing and Materials, West Conshohocken, PA, 2018.
- » AC 150/5210-24 Airport Foreign Object Debris (FOD) Management, Federal Aviation Administration, Washington, D.C., 2010.
- » AC 150/5320-6F, Airport Pavement Design and Evaluation, Federal Aviation Administration, Washington, D.C., 2016.
- » AC 150/5380-7B, Airport Pavement Management Program (PMP), Federal Aviation Administration, Washington, D.C., 2014.
- » AC 150/5380-6C, Guidelines and Procedures for Maintenance of Airport Pavements, Federal Aviation Administration, Washington, D.C., 2014.
- » AC 150/5370-10H, Standard Specifications for Construction of Airports, Federal Aviation Administration, Washington, D.C., 2018.
- » Airport Improvement Program Handbook, Order 5100.38D, Change 1, Federal Aviation Administration, Washington, D.C., 2019.
- » Tri-Service Pavements Working Group (TSPWG) Manual 3-270-08. 14-03, Preventive Maintenance Plan (PMP) for Airfield Pavements, Department of Defense, Washington, D.C., 2019.
- » Unified Facilities Criteria (UFC) 3-260-16, O&M Manual: Standard Practice for Airfield Pavement Condition Surveys, Department of Defense, Washington, D.C., 2019.
- » Unified Facilities Criteria (UFC) 3-260-03, Airfield Pavement Evaluation, Department of Defense, Washington, D.C., 2001.
- » Shahin, Mohamed Y., Pavement Management for Airports, Roads, and Parking Lots, Springer, 2005.

A wide-angle photograph of an airfield runway stretching into the distance under a bright blue sky with scattered white clouds. The runway is dark asphalt with a central white dashed line and yellow edge lines. The image is framed by a red diagonal bar on the left and a blue diagonal bar on the right.

Appendix A: Airfield Pavement Analysis

A close-up, low-angle view of the runway pavement, showing a concrete slab with a yellow dashed line and a yellow chevron marking. The image is framed by a red diagonal bar on the left and a blue diagonal bar on the right.

Table A.1: Pavement System Inventory Details

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	Surface Type	Estimate of Last Construction Date
BCT	RW 5-23	Runway	6105	520,000	AAC	1/1/2010
BCT	RW 5-23	Runway	6106	72,700	AAC	1/1/2010
BCT	RW 5-23	Runway	6107	35,000	AAC	1/1/2010
BCT	RW 5-23	Runway	6110	260,000	AAC	1/1/2010
BCT	RW 5-23	Runway	6111	36,350	AAC	1/1/2010
BCT	RW 5-23	Runway	6112	17,500	AAC	1/1/2010
BCT	TW A	Taxiway	130	8,671	AAC	1/1/2010
BCT	TW B	Taxiway	125	14,426	AC	7/1/2022
BCT	TW C	Taxiway	120	14,426	AC	7/1/2022
BCT	TW E	Taxiway	116	14,729	AC	1/1/2010
BCT	TW F	Taxiway	115	14,426	AAC	7/1/2022
BCT	TW G	Taxiway	110	8,671	AAC	1/1/2010
BCT	TW H	Taxiway	111	7,946	AAC	1/1/2010
BCT	TW P	Taxiway	105	193,060	AAC	1/1/2010
BCT	TW P	Taxiway	106	29,080	AAC	1/1/2010
BCT	TW P	Taxiway	107	14,241	AAC	1/1/2010
BCT	TW P	Taxiway	108	10,940	AAC	1/1/2010
BCT	TW P	Taxiway	112	12,673	AAC	1/1/2010
BCT	TW P	Taxiway	113	4,000	AAC	1/1/2010
BCT	TW P	Taxiway	131	12,673	AAC	1/1/2010
BCT	TW P1	Taxiway	205	4,102	AAC	1/1/2010
BCT	TW P10	Taxiway	240	8,665	AAC	1/1/2010
BCT	TW P11	Taxiway	250	8,665	AAC	1/1/2010
BCT	TW P2	Taxiway	210	2,572	AAC	1/1/2010
BCT	TW P3	Taxiway	215	1,488	AAC	1/1/2010
BCT	TW P4	Taxiway	225	8,391	AAC	7/1/2022
BCT	TW P5	Taxiway	230	8,400	AAC	1/1/2010
BCT	TW P6	Taxiway	220	8,315	AAC	1/1/2010
BCT	TW P7	Taxiway	222	4,994	AC	2/24/2018
BCT	TW P8	Taxiway	221	5,090	AC	2/24/2018
BCT	TW P9	Taxiway	260	3,165	AAC	1/1/2010
BCT	AP CUSTOMS	Apron	4105	82,167	AC	2/24/2018
BCT	AP RU 23	Apron	5110	21,414	AAC	1/1/2010
BCT	AP RU 5	Apron	5105	26,544	AAC	1/1/2010
BCT	AP RU 5	Apron	5115	11,787	AC	1/1/2010

Table A.2: Pavement Condition Index Summary (Current PCI Survey) – Section Level

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	PCI	Condition Rating
BCT	RW 5-23	Runway	6105	520,000	86	Good
BCT	RW 5-23	Runway	6106	72,700	89	Good
BCT	RW 5-23	Runway	6107	35,000	91	Good
BCT	RW 5-23	Runway	6110	260,000	90	Good
BCT	RW 5-23	Runway	6111	36,350	83	Satisfactory
BCT	RW 5-23	Runway	6112	17,500	94	Good
BCT	TW A	Taxiway	130	8,671	81	Satisfactory
BCT	TW B	Taxiway	125	14,426	100	Good
BCT	TW C	Taxiway	120	14,426	100	Good
BCT	TW E	Taxiway	116	14,729	70	Fair
BCT	TW F	Taxiway	115	14,426	100	Good
BCT	TW G	Taxiway	110	8,671	84	Satisfactory
BCT	TW H	Taxiway	111	7,946	78	Satisfactory
BCT	TW P	Taxiway	105	193,060	82	Satisfactory
BCT	TW P	Taxiway	106	29,080	86	Good
BCT	TW P	Taxiway	107	14,241	86	Good
BCT	TW P	Taxiway	108	10,940	79	Satisfactory
BCT	TW P	Taxiway	112	12,673	82	Satisfactory
BCT	TW P	Taxiway	113	4,000	81	Satisfactory
BCT	TW P	Taxiway	131	12,673	80	Satisfactory
BCT	TW P1	Taxiway	205	4,102	88	Good
BCT	TW P10	Taxiway	240	8,665	89	Good
BCT	TW P11	Taxiway	250	8,665	93	Good
BCT	TW P2	Taxiway	210	2,572	72	Satisfactory
BCT	TW P3	Taxiway	215	1,488	74	Satisfactory
BCT	TW P4	Taxiway	225	8,391	100	Good
BCT	TW P5	Taxiway	230	8,400	87	Good
BCT	TW P6	Taxiway	220	8,315	85	Satisfactory
BCT	TW P7	Taxiway	222	4,994	89	Good
BCT	TW P8	Taxiway	221	5,090	87	Good
BCT	TW P9	Taxiway	260	3,165	81	Satisfactory
BCT	AP CUSTOMS	Apron	4105	82,167	91	Good
BCT	AP RU 23	Apron	5110	21,414	88	Good
BCT	AP RU 5	Apron	5105	26,544	79	Satisfactory
BCT	AP RU 5	Apron	5115	11,787	77	Satisfactory

Table A.3: Forecasted PCI Values 2023-2032 – Section-Level

Network ID	Branch ID	Section ID	Current PCI	2023	2024	2025	2026	2027	2028	2029	2030	2031	2032
BCT	RW 5-23	6105	86	84	83	81	79	78	76	74	72	71	69
BCT	RW 5-23	6106	89	87	86	84	82	81	79	77	75	74	72
BCT	RW 5-23	6107	91	89	88	86	84	83	81	79	77	76	74
BCT	RW 5-23	6110	90	88	87	85	83	82	80	78	76	75	73
BCT	RW 5-23	6111	83	81	80	78	76	75	73	71	69	68	66
BCT	RW 5-23	6112	94	92	91	89	87	86	84	82	80	79	77
BCT	TW A	130	81	79	78	76	75	73	72	71	69	68	67
BCT	TW B	125	100	98	95	93	91	89	87	85	83	82	80
BCT	TW C	120	100	98	95	93	91	89	87	85	83	82	80
BCT	TW E	116	70	69	68	67	66	66	65	64	63	63	62
BCT	TW F	115	100	98	95	93	91	88	86	84	83	81	79
BCT	TW G	110	84	82	81	79	77	76	74	73	71	70	69
BCT	TW H	111	78	77	75	74	72	71	70	69	67	66	65
BCT	TW P	105	82	80	79	77	76	74	73	71	70	69	68
BCT	TW P	106	86	84	82	81	79	77	76	74	73	71	70
BCT	TW P	107	86	84	82	81	79	77	76	74	73	71	70
BCT	TW P	108	79	78	76	74	73	72	70	69	68	67	66
BCT	TW P	112	82	80	79	77	76	74	73	71	70	69	68
BCT	TW P	113	81	79	78	76	75	73	72	71	69	68	67
BCT	TW P	131	80	79	77	75	74	73	71	70	69	68	67
BCT	TW P1	205	88	86	84	82	81	79	77	76	74	73	71
BCT	TW P10	240	89	87	85	83	81	80	78	76	75	73	72
BCT	TW P11	250	93	91	89	87	85	83	81	79	78	76	75
BCT	TW P2	210	72	71	70	68	67	66	65	64	63	63	62
BCT	TW P3	215	74	73	71	70	69	68	67	66	65	64	63
BCT	TW P4	225	100	98	95	93	91	88	86	84	83	81	79
BCT	TW P5	230	87	85	83	81	80	78	76	75	74	72	71
BCT	TW P6	220	85	83	81	80	78	76	75	74	72	71	70
BCT	TW P7	222	89	87	85	84	82	80	79	77	76	75	73
BCT	TW P8	221	87	85	84	82	80	79	77	76	75	73	72
BCT	TW P9	260	81	79	78	76	75	73	72	71	69	68	67
BCT	AP CUSTOMS	4105	91	89	87	85	83	81	79	77	75	74	72
BCT	AP RU 23	5110	88	86	84	82	79	77	75	73	71	69	66
BCT	AP RU 5	5105	79	77	75	73	70	68	66	64	62	60	57
BCT	AP RU 5	5115	77	75	74	72	70	69	67	66	65	63	62

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Pavement Database: FDOT

Network: BOCA RATON AIRP Branch: AP CUSTOM CUSTOMS APRON Section: 4105 Surface: AC
 L.C.D. 2/24/2018 Use: APRON Rank: P Length: 360.00 (Ft) Width: 235.00 (Ft) True Area: 82167.00002 (SqFt)

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
2/24/2018	CR-AC	Complete Reconstruction - AC	0.00	0.00	<input checked="" type="checkbox"/>	
1/1/1995	NC-AC	New Construction - AC	0.00	0.00	<input checked="" type="checkbox"/>	
1/1/1995	ST-SC	Surface Treatment - Seal Coat	16,433.20	0.00	<input type="checkbox"/>	

Network: BOCA RATON AIRP Branch: AP RU 23 RUN-UP APRON Section: 5110 Surface: AAC
 L.C.D. 1/1/2010 Use: APRON Rank: P Length: 199.00 (Ft) Width: 111.00 (Ft) True Area: 21414.00000 (SqFt)

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2010	ML-OVL	Mill and Overlay	0.00	0.00	<input checked="" type="checkbox"/>	MILL AND OVERLAY: 1.5" P-401,
1/1/1996	IMPORT ED	BUILT	0.00	2.00	<input checked="" type="checkbox"/>	1996: 2" AC ON 6" LIMEROCK

Network: BOCA RATON AIRP Branch: AP RU 5 RUN-UP APRON Section: 5105 Surface: AAC
 L.C.D. 1/1/2010 Use: APRON Rank: P Length: 240.00 (Ft) Width: 120.00 (Ft) True Area: 26544.00000 (SqFt)

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2010	ML-OVL	Mill and Overlay	0.00	0.00	<input checked="" type="checkbox"/>	MILL AND OVERLAY: 1.5" P-401,
1/1/1996	IMPORT ED	BUILT	0.00	2.00	<input checked="" type="checkbox"/>	1996: 2" AC ON 6" LIMEROCK

Network: BOCA RATON AIRP Branch: AP RU 5 RUN-UP APRON Section: 5115 Surface: AC
 L.C.D. 1/1/2010 Use: APRON Rank: P Length: 150.00 (Ft) Width: 75.00 (Ft) True Area: 11787.00000 (SqFt)

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2010	NU-IN	New Construction - Initial	0.00	0.00	<input checked="" type="checkbox"/>	4" P-401, 12" P-211, 6" P-152

Network: BOCA RATON AIRP Branch: RW 5-23 RUNWAY 5-23 Section: 6105 Surface: AAC
 L.C.D. 1/1/2010 Use: RUNWAY Rank: P Length: 5,200.00 (Ft) Width: 100.00 (Ft) True Area: 520000.0001 (SqFt)

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
11/1/2018	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	<input type="checkbox"/>	
1/1/2010	ML-OVL	Mill and Overlay	0.00	0.00	<input checked="" type="checkbox"/>	MILL AND OVERLAY: 1.5" P-401,
1/1/1984	IMPORT ED	OVERLAY	0.00	2.50	<input checked="" type="checkbox"/>	1984: 2.5" - 6" P-401 OVERLAY
1/1/1984	IMPORT ED	OVERLAY	0.00	0.00	<input checked="" type="checkbox"/>	SOIL: SP
1/1/1974	IMPORT ED	BUILT	0.00	2.50	<input checked="" type="checkbox"/>	1974: 2.5" AC ON 6" LIME ROCK BASE

Network: BOCA RATON AIRP Branch: RW 5-23 RUNWAY 5-23 Section: 6106 Surface: AAC
 L.C.D. 1/1/2010 Use: RUNWAY Rank: P Length: 725.00 (Ft) Width: 100.00 (Ft) True Area: 72700.00002 (SqFt)

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
11/1/2018	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	<input type="checkbox"/>	
1/1/2010	ML-OVL	Mill and Overlay	0.00	0.00	<input checked="" type="checkbox"/>	MILL AND OVERLAY: 1.5" P-401,
1/1/1996	IMPORT ED	BUILT	0.00	2.00	<input checked="" type="checkbox"/>	1996: 2" AC ON 6" LIMEROCK

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Pavement Database: FDOT

Network: BOCA RATON AIRP		Branch: RW 5-23		RUNWAY 5-23		Section: 6107	Surface: AAC
L.C.D. 1/1/2010	Use: RUNWAY	Rank: P	Length: 350.00 (Ft)	Width: 100.00 (Ft)	True Area: 35000.00001 (SqFt)		
Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments	
11/1/2018	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	<input type="checkbox"/>	MILL AND OVERLAY: 1.5" P-401, 1996: 2" AC ON 6" LIMEROCK	
1/1/2010	ML-OVL	Mill and Overlay	0.00	0.00	<input checked="" type="checkbox"/>		
1/1/1996	IMPORT ED	BUILT	0.00	2.00	<input checked="" type="checkbox"/>		

Network: BOCA RATON AIRP		Branch: RW 5-23		RUNWAY 5-23		Section: 6110	Surface: AAC
L.C.D. 1/1/2010	Use: RUNWAY	Rank: P	Length: 10,400.00 (Ft)	Width: 25.00 (Ft)	True Area: 260000.00000 (SqFt)		
Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments	
1/1/2021	PA-AC	Patching - AC	0.00	0.00	<input type="checkbox"/>	MILL AND OVERLAY: 1.5" P-401, 1984: 2.5" P-401 OVERLAY	
11/1/2018	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	<input type="checkbox"/>		
1/1/2010	ML-OVL	Mill and Overlay	0.00	0.00	<input checked="" type="checkbox"/>		
1/1/1984	IMPORT ED	OVERLAY	0.00	2.50	<input checked="" type="checkbox"/>		
1/1/1984	IMPORT ED	OVERLAY	0.00	0.00	<input checked="" type="checkbox"/>	SOIL: SP	
1/1/1974	IMPORT ED	BUILT	0.00	2.50	<input checked="" type="checkbox"/>	1974: 2.5" AC ON 6" LIME ROCK BASE	

Network: BOCA RATON AIRP		Branch: RW 5-23		RUNWAY 5-23		Section: 6111	Surface: AAC
L.C.D. 1/1/2010	Use: RUNWAY	Rank: P	Length: 1,450.00 (Ft)	Width: 25.00 (Ft)	True Area: 36350.00001 (SqFt)		
Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments	
11/1/2018	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	<input type="checkbox"/>	MILL AND OVERLAY: 1.5" P-401, 1996: 2" AC ON 6" LIMEROCK	
1/1/2010	ML-OVL	Mill and Overlay	0.00	0.00	<input checked="" type="checkbox"/>		
1/1/1996	IMPORT ED	BUILT	0.00	2.00	<input checked="" type="checkbox"/>		

Network: BOCA RATON AIRP		Branch: RW 5-23		RUNWAY 5-23		Section: 6112	Surface: AAC
L.C.D. 1/1/2010	Use: RUNWAY	Rank: P	Length: 700.00 (Ft)	Width: 25.00 (Ft)	True Area: 17500.00000 (SqFt)		
Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments	
11/1/2018	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	<input type="checkbox"/>	MILL AND OVERLAY: 1.5" P-401, 1996: 2" AC ON 6" LIMEROCK	
1/1/2010	ML-OVL	Mill and Overlay	0.00	0.00	<input checked="" type="checkbox"/>		
1/1/1996	IMPORT ED	BUILT	0.00	2.00	<input checked="" type="checkbox"/>		

Network: BOCA RATON AIRP		Branch: TW A		TAXIWAY A		Section: 130	Surface: AAC
L.C.D. 1/1/2010	Use: TAXIWAY	Rank: P	Length: 145.00 (Ft)	Width: 45.00 (Ft)	True Area: 8671.000002 (SqFt)		
Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments	
1/1/2010	ML-OVL	Mill and Overlay	0.00	0.00	<input checked="" type="checkbox"/>	MILL AND OVERLAY: 1.5" P-401, 1984: 2.5" AC OVERLAY	
1/1/1984	IMPORT ED	OVERLAY	0.00	2.50	<input checked="" type="checkbox"/>		
1/1/1974	IMPORT ED	BUILT	0.00	2.00	<input checked="" type="checkbox"/>	1974: 2" AC ON 6" LIME ROCK ON 4" WORK PLATFORM (100% DENS	

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Network: BOCA RATON AIRP		Branch: TW B	TAXIWAY B		Section: 125	Surface: AC
L.C.D. 7/1/2022	Use: TAXIWAY	Rank: P	Length: 145.00 (Ft)	Width: 96.00 (Ft)	True Area: 14426.00000 (SqFt)	
Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
7/1/2022	NC-AC	New Construction - AC	0.00	0.00	<input checked="" type="checkbox"/>	4" P-401, 12" P-211

Network: BOCA RATON AIRP		Branch: TW C	TAXIWAY C		Section: 120	Surface: AC
L.C.D. 7/1/2022	Use: TAXIWAY	Rank: P	Length: 145.00 (Ft)	Width: 96.00 (Ft)	True Area: 14426.00000 (SqFt)	
Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
7/1/2022	NC-AC	New Construction - AC	0.00	0.00	<input checked="" type="checkbox"/>	4" P-401, 12" P-211

Network: BOCA RATON AIRP		Branch: TW E	TAXIWAY E		Section: 116	Surface: AC
L.C.D. 1/1/2010	Use: TAXIWAY	Rank: P	Length: 145.00 (Ft)	Width: 60.00 (Ft)	True Area: 14729.00000 (SqFt)	
Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2010	NU-IN	New Construction - Initial	0.00	0.00	<input checked="" type="checkbox"/>	3" P-401, 18" P-211, 6" P-152

Network: BOCA RATON AIRP		Branch: TW F	TAXIWAY F		Section: 115	Surface: AAC
L.C.D. 7/1/2022	Use: TAXIWAY	Rank: P	Length: 145.00 (Ft)	Width: 96.00 (Ft)	True Area: 14426.00000 (SqFt)	
Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
7/1/2022	ML-OVL	Mill and Overlay	0.00	0.00	<input checked="" type="checkbox"/>	Variable 2" Mill and P-401 Overlay, F
1/1/2010	ML-OVL	Mill and Overlay	0.00	0.00	<input checked="" type="checkbox"/>	
1/1/1984	IMPORT ED	OVERLAY	0.00	2.50	<input checked="" type="checkbox"/>	1984: 2.5" AC OVERLAY
1/1/1984	IMPORT ED	OVERLAY	0.00	0.00	<input checked="" type="checkbox"/>	SOIL: SP
1/1/1974	IMPORT ED	BUILT	0.00	2.00	<input checked="" type="checkbox"/>	1974: 2" AC ON 6" LIME ROCK BASE ON 4" WORK PLATFORM (1

Network: BOCA RATON AIRP		Branch: TW G	TAXIWAY G		Section: 110	Surface: AAC
L.C.D. 1/1/2010	Use: TAXIWAY	Rank: P	Length: 145.00 (Ft)	Width: 45.00 (Ft)	True Area: 8671.000002 (SqFt)	
Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2010	ML-OVL	Mill and Overlay	0.00	0.00	<input checked="" type="checkbox"/>	MILL AND OVERLAY: 1.5" P-401, 1984: 2.5" AC OVERLAY
1/1/1984	IMPORT ED	OVERLAY	0.00	2.50	<input checked="" type="checkbox"/>	
1/1/1984	IMPORT ED	OVERLAY	0.00	0.00	<input checked="" type="checkbox"/>	SOIL: SP
1/1/1974	IMPORT ED	BUILT	0.00	2.00	<input checked="" type="checkbox"/>	1974: 2" AC ON 6" LIME ROCK BASE ON 4" WORK PLATFORM (1

Network: BOCA RATON AIRP		Branch: TW H	TAXIWAY H		Section: 111	Surface: AAC
L.C.D. 1/1/2010	Use: TAXIWAY	Rank: P	Length: 145.00 (Ft)	Width: 40.00 (Ft)	True Area: 7946.000002 (SqFt)	
Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2010	ML-OVL	Mill and Overlay	0.00	0.00	<input checked="" type="checkbox"/>	MILL AND OVERLAY: 1.5" P-401, 1996: 2" AC ON 6" LIMEROCK
1/1/1996	IMPORT ED	BUILT	0.00	2.00	<input checked="" type="checkbox"/>	

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Network: BOCA RATON AIRP		Branch: TW P10		TAXIWAY P10		Section: 240	Surface: AAC
L.C.D. 1/1/2010		Use: TAXIWAY	Rank: P	Length: 60.00 (Ft)	Width: 77.00 (Ft)	True Area: 8665.000002 (SqFt)	
Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments	
1/1/2021	PA-AC	Patching - AC	0.00	0.00	<input type="checkbox"/>	Fillet widening, 4" P-401, 12" P-211 o	
1/1/2010	ML-OVL	Mill and Overlay	0.00	0.00	<input checked="" type="checkbox"/>	MILL AND OVERLAY: 1.5" P-401,	
1/1/2002	NC-AC	New Construction - AC	0.00	0.00	<input checked="" type="checkbox"/>		

Network: BOCA RATON AIRP		Branch: TW P		TAXIWAY P		Section: 105	Surface: AAC
L.C.D. 1/1/2010		Use: TAXIWAY	Rank: P	Length: 4,930.00 (Ft)	Width: 40.00 (Ft)	True Area: 193060.0000 (SqFt)	
Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments	
1/1/2010	ML-OVL	Mill and Overlay	0.00	0.00	<input checked="" type="checkbox"/>	MILL AND OVERLAY: 1.5" P-401,	
1/1/1984	IMPORT ED	OVERLAY	0.00	2.50	<input checked="" type="checkbox"/>	1984: 2.5" AC OVERLAY	
1/1/1984	IMPORT ED	OVERLAY	0.00	0.00	<input checked="" type="checkbox"/>	SOIL: SP	
1/1/1974	IMPORT ED	BUILT	0.00	2.00	<input checked="" type="checkbox"/>	1974: 2" AC ON 6" LIME ROCK BASE ON 4" WORK PLATFORM (C	

Network: BOCA RATON AIRP		Branch: TW P		TAXIWAY P		Section: 106	Surface: AAC
L.C.D. 1/1/2010		Use: TAXIWAY	Rank: P	Length: 725.00 (Ft)	Width: 40.00 (Ft)	True Area: 29080.00000 (SqFt)	
Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments	
1/1/2010	ML-OVL	Mill and Overlay	0.00	0.00	<input checked="" type="checkbox"/>	MILL AND OVERLAY: 1.5" P-401,	
1/1/1996	IMPORT ED	BUILT	0.00	2.00	<input checked="" type="checkbox"/>	1996: 2" AC ON 6" LIMEROCK	

Network: BOCA RATON AIRP		Branch: TW P		TAXIWAY P		Section: 107	Surface: AAC
L.C.D. 1/1/2010		Use: TAXIWAY	Rank: P	Length: 400.00 (Ft)	Width: 40.00 (Ft)	True Area: 14241.00000 (SqFt)	
Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments	
1/1/2010	ML-OVL	Mill and Overlay	0.00	0.00	<input checked="" type="checkbox"/>	MILL AND OVERLAY: 1.5" P-401,	
1/1/1996	IMPORT ED	BUILT	0.00	2.00	<input checked="" type="checkbox"/>	1996: 2" AC ON 6" LIMEROCK	

Network: BOCA RATON AIRP		Branch: TW P		TAXIWAY P		Section: 108	Surface: AAC
L.C.D. 1/1/2010		Use: TAXIWAY	Rank: P	Length: 225.00 (Ft)	Width: 40.00 (Ft)	True Area: 10940.00000 (SqFt)	
Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments	
1/1/2010	ML-OVL	Mill and Overlay	0.00	0.00	<input checked="" type="checkbox"/>	MILL AND OVERLAY: 1.5" P-401,	
1/1/1996	IMPORT ED	BUILT	0.00	0.00	<input checked="" type="checkbox"/>	1996: AC OVERLAY	

Network: BOCA RATON AIRP		Branch: TW P		TAXIWAY P		Section: 112	Surface: AAC
L.C.D. 1/1/2010		Use: TAXIWAY	Rank: P	Length: 145.00 (Ft)	Width: 80.00 (Ft)	True Area: 12673.00000 (SqFt)	
Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments	
1/1/2010	ML-OVL	Mill and Overlay	0.00	0.00	<input checked="" type="checkbox"/>	MILL AND OVERLAY: 1.5" P-401,	
1/1/1996	IMPORT ED	BUILT	0.00	2.00	<input checked="" type="checkbox"/>	1996: 2" AC ON 6" LIMEROCK	

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Network: BOCA RATON AIRP		Branch: TW P11		TAXIWAY P11		Section: 250	Surface: AAC
L.C.D. 1/1/2010	Use: TAXIWAY	Rank: P	Length: 60.00 (Ft)	Width: 77.00 (Ft)	True Area: 8665.000002 (SqFt)		
Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments	
1/1/2021	PA-AC	Patching - AC	0.00	0.00	<input type="checkbox"/>	Fillet widening, 4" P-401, 12" P-211 o	
1/1/2010	ML-OVL	Mill and Overlay	0.00	0.00	<input checked="" type="checkbox"/>	MILL AND OVERLAY: 1.5" P-401,	
1/1/2002	NC-AC	New Construction - AC	0.00	0.00	<input checked="" type="checkbox"/>		

Network: BOCA RATON AIRP		Branch: TW P		TAXIWAY P		Section: 113	Surface: AAC
L.C.D. 1/1/2010	Use: TAXIWAY	Rank: P	Length: 50.00 (Ft)	Width: 40.00 (Ft)	True Area: 4000.000001 (SqFt)		
Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments	
1/1/2010	ML-OVL	Mill and Overlay	0.00	0.00	<input checked="" type="checkbox"/>	MILL AND OVERLAY: 1.5" P-401, EST 1996: PATCH	
1/1/1996	IMPORT ED	BUILT	0.00	0.00	<input checked="" type="checkbox"/>		

Network: BOCA RATON AIRP		Branch: TW P1		TAXIWAY P1		Section: 205	Surface: AAC
L.C.D. 1/1/2010	Use: TAXIWAY	Rank: P	Length: 56.00 (Ft)	Width: 59.00 (Ft)	True Area: 4102.000001 (SqFt)		
Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments	
1/1/2010	ML-OVL	Mill and Overlay	0.00	0.00	<input checked="" type="checkbox"/>	MILL AND OVERLAY: 1.5" P-401, 1996: 2" AC ON 6" LIMEROCK	
1/1/1996	IMPORT ED	BUILT	0.00	2.00	<input checked="" type="checkbox"/>		

Network: BOCA RATON AIRP		Branch: TW P		TAXIWAY P		Section: 131	Surface: AAC
L.C.D. 1/1/2010	Use: TAXIWAY	Rank: P	Length: 145.00 (Ft)	Width: 12673.0 (Ft)	True Area: 12673.000000 (SqFt)		
Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments	
1/1/2010	ML-OVL	Mill and Overlay	0.00	0.00	<input checked="" type="checkbox"/>	1996: 2" AC ON 6" LIMEROCK	
1/1/1996	IMPORT ED	BUILT	0.00	2.00	<input checked="" type="checkbox"/>		

Network: BOCA RATON AIRP		Branch: TW P2		TAXIWAY P2		Section: 210	Surface: AAC
L.C.D. 1/1/2010	Use: TAXIWAY	Rank: P	Length: 59.00 (Ft)	Width: 30.00 (Ft)	True Area: 2572.000000 (SqFt)		
Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments	
1/1/2010	ML-OVL	Mill and Overlay	0.00	0.00	<input checked="" type="checkbox"/>		
1/1/1996	NC-AC	New Construction - AC	0.00	0.00	<input checked="" type="checkbox"/>		

Network: BOCA RATON AIRP		Branch: TW P3		TAXIWAY P3		Section: 215	Surface: AAC
L.C.D. 1/1/2010	Use: TAXIWAY	Rank: P	Length: 61.00 (Ft)	Width: 20.00 (Ft)	True Area: 1488.000000 (SqFt)		
Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments	
1/1/2010	ML-OVL	Mill and Overlay	0.00	0.00	<input checked="" type="checkbox"/>		
1/1/1996	NC-AC	New Construction - AC	0.00	0.00	<input checked="" type="checkbox"/>		

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Network: BOCA RATON AIRP		Branch: TW P4	TAXIWAY P4		Section: 225	Surface: AAC
L.C.D. 7/1/2022	Use: TAXIWAY	Rank: P	Length: 55.00 (Ft)	Width: 80.00 (Ft)	True Area: 8391.000002 (SqFt)	
Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
7/1/2022	ML-OVL	Mill and Overlay	0.00	0.00	<input checked="" type="checkbox"/>	Variable 2" Mill and P-401 Overlay, F
1/1/2010	ML-OVL	Mill and Overlay	0.00	0.00	<input checked="" type="checkbox"/>	MILL AND OVERLAY: 1.5" P-401,
1/1/1984	IMPORT ED	BUILT	0.00	0.00	<input checked="" type="checkbox"/>	ESTIMATE 1984 AC OVERLAY ON EXISTING AC

Network: BOCA RATON AIRP		Branch: TW P5	TAXIWAY P5		Section: 230	Surface: AAC
L.C.D. 1/1/2010	Use: TAXIWAY	Rank: P	Length: 55.00 (Ft)	Width: 80.00 (Ft)	True Area: 8400.000002 (SqFt)	
Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2021	PA-AC	Patching - AC	0.00	0.00	<input type="checkbox"/>	Fillet widening, 4" P-401, 12" P-211 o
1/1/2010	ML-OVL	Mill and Overlay	0.00	0.00	<input checked="" type="checkbox"/>	MILL AND OVERLAY: 1.5" P-401,
1/1/1984	IMPORT ED	BUILT	0.00	0.00	<input checked="" type="checkbox"/>	ESTIMATE 1984 AC OVERLAY ON EXISTING AC

Network: BOCA RATON AIRP		Branch: TW P6	TAXIWAY P6		Section: 220	Surface: AAC
L.C.D. 1/1/2010	Use: TAXIWAY	Rank: P	Length: 55.00 (Ft)	Width: 77.00 (Ft)	True Area: 8315.000002 (SqFt)	
Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2021	PA-AC	Patching - AC	0.00	0.00	<input type="checkbox"/>	Fillet widening, 4" P-401, 12" P-211 o
1/1/2010	ML-OVL	Mill and Overlay	0.00	0.00	<input checked="" type="checkbox"/>	MILL AND OVERLAY: 1.5" P-401,
1/1/1984	IMPORT ED	BUILT	0.00	0.00	<input checked="" type="checkbox"/>	ESTIMATE 1984 AC PAVEMENT

Network: BOCA RATON AIRP		Branch: TW P7	TAXIWAY P7		Section: 222	Surface: AC
L.C.D. 2/24/2018	Use: TAXIWAY	Rank: P	Length: 55.00 (Ft)	Width: 51.00 (Ft)	True Area: 4994.000001 (SqFt)	
Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
2/24/2018	NC-AC	New Construction - AC	0.00	0.00	<input checked="" type="checkbox"/>	

Network: BOCA RATON AIRP		Branch: TW P8	TAXIWAY P8		Section: 221	Surface: AC
L.C.D. 2/24/2018	Use: TAXIWAY	Rank: P	Length: 55.00 (Ft)	Width: 50.00 (Ft)	True Area: 5090.000001 (SqFt)	
Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
2/24/2018	CR-AC	Complete Reconstruction - AC	0.00	0.00	<input checked="" type="checkbox"/>	
1/1/2010	ML-OVL	Mill and Overlay	0.00	0.00	<input checked="" type="checkbox"/>	MILL AND OVERLAY: 1.5" P-401,
1/1/1996	IMPORT ED	BUILT	0.00	2.00	<input checked="" type="checkbox"/>	1996: 2" AC ON 6" LIMEROCK

Network: BOCA RATON AIRP		Branch: TW P9	TAXIWAY P9		Section: 260	Surface: AAC
L.C.D. 1/1/2010	Use: TAXIWAY	Rank: P	Length: 60.00 (Ft)	Width: 40.00 (Ft)	True Area: 3165.000000 (SqFt)	
Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2010	ML-OVL	Mill and Overlay	0.00	0.00	<input checked="" type="checkbox"/>	MILL AND OVERLAY: 1.5" P-401,
1/1/1998	NU-IN	New Construction - Initial	0.00	0.00	<input checked="" type="checkbox"/>	

Summary:

Work Description	Section Count	Area Total (SqFt)	Thickness Avg (in)	Thickness STD (in)
BUILT	24	1,340,187.00	1.62	0.84
Complete Reconstruction - AC	2	87,257.00	0.00	0.00
Mill and Overlay	31	1,387,559.00	0.00	0.00
New Construction - AC	8	137,403.00	0.00	0.00
New Construction - Initial	3	29,681.00	0.00	0.00
OVERLAY	11	2,000,985.00	1.36	1.24
Patching - AC	5	294,045.00	0.00	0.00
Surface Treatment - Seal Coat	7	1,023,717.00	0.00	0.00

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Branch ID	Number of Sections	Sum Section Length (Ft)	Avg Section Width (Ft)	True Area (SqFt)	Use	Average PCI	Standard Deviation PCI	Weighted Average PCI
AP CUSTO	1	360.00	235.00	82,167.00	APRON	91.00	0.00	91.00
AP RU 23	1	199.00	111.00	21,414.00	APRON	88.00	0.00	88.00
AP RU 5	2	390.00	97.50	38,331.00	APRON	78.00	1.00	78.38
RW 5-23	6	18,825.00	62.50	941,550.00	RUNWAY	88.83	3.53	87.55
TW A	1	145.00	45.00	8,671.00	TAXIWAY	81.00	0.00	81.00
TW B	1	145.00	96.00	14,426.00	TAXIWAY	100.00	0.00	100.00
TW C	1	145.00	96.00	14,426.00	TAXIWAY	100.00	0.00	100.00
TW E	1	145.00	60.00	14,729.00	TAXIWAY	70.00	0.00	70.00
TW F	1	145.00	96.00	14,426.00	TAXIWAY	100.00	0.00	100.00
TW G	1	145.00	45.00	8,671.00	TAXIWAY	84.00	0.00	84.00
TW H	1	145.00	40.00	7,946.00	TAXIWAY	78.00	0.00	78.00
TW P	7	6,620.00	1,850.43	276,667.00	TAXIWAY	82.29	2.55	82.40
TW P1	1	56.00	59.00	4,102.00	TAXIWAY	88.00	0.00	88.00
TW P10	1	60.00	77.00	8,665.00	TAXIWAY	89.00	0.00	89.00
TW P11	1	60.00	77.00	8,665.00	TAXIWAY	93.00	0.00	93.00
TW P2	1	59.00	30.00	2,572.00	TAXIWAY	72.00	0.00	72.00
TW P3	1	61.00	20.00	1,488.00	TAXIWAY	74.00	0.00	74.00
TW P4	1	55.00	80.00	8,391.00	TAXIWAY	100.00	0.00	100.00
TW P5	1	55.00	80.00	8,400.00	TAXIWAY	87.00	0.00	87.00
TW P6	1	55.00	77.00	8,315.00	TAXIWAY	85.00	0.00	85.00
TW P7	1	55.00	51.00	4,994.00	TAXIWAY	89.00	0.00	89.00
TW P8	1	55.00	50.00	5,090.00	TAXIWAY	87.00	0.00	87.00
TW P9	1	60.00	40.00	3,165.00	TAXIWAY	81.00	0.00	81.00

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Pavement Database: FDOT

Use Category	Number of Sections	Total Area (SqFt)	Arithmetic Average PCI	Average STD PCI	Weighted Average PCI
APRON	4	141,912.00	83.75	5.89	87.14
RUNWAY	6	941,550.00	88.83	3.53	87.55
TAXIWAY	25	423,809.00	85.36	8.26	84.62
ALL	35	1,507,271.00	85.77	7.55	86.69

Pavement Database: FDOT

NetworkId: BCT

Branch ID	Section ID	Last Const. Date	Surface	Use	Rank	Lanes	True Area (SqFt)	Last Inspection Date	Age At Inspection	PCI
AP CUSTOMS	4105	2/24/2018	AC	APRON	P	0	82,167.00	8/11/2022	4	91
AP RU 23	5110	1/1/2010	AAC	APRON	P	0	21,414.00	8/11/2022	12	88
AP RU 5	5105	1/1/2010	AAC	APRON	P	0	26,544.00	8/11/2022	12	79
AP RU 5	5115	1/1/2010	AC	APRON	P	0	11,787.00	8/11/2022	12	77
RW 5-23	6105	1/1/2010	AAC	RUNWAY	P	0	520,000.00	8/11/2022	12	86
RW 5-23	6106	1/1/2010	AAC	RUNWAY	P	0	72,700.00	8/11/2022	12	89
RW 5-23	6107	1/1/2010	AAC	RUNWAY	P	0	35,000.00	8/11/2022	12	91
RW 5-23	6110	1/1/2010	AAC	RUNWAY	P	0	260,000.00	8/11/2022	12	90
RW 5-23	6111	1/1/2010	AAC	RUNWAY	P	0	36,350.00	8/11/2022	12	83
RW 5-23	6112	1/1/2010	AAC	RUNWAY	P	0	17,500.00	8/11/2022	12	94
TW A	130	1/1/2010	AAC	TAXIWAY	P	0	8,671.00	8/11/2022	12	81
TW B	125	7/1/2022	AC	TAXIWAY	P	0	14,426.00	7/1/2022	0	100
TW C	120	7/1/2022	AC	TAXIWAY	P	0	14,426.00	7/1/2022	0	100
TW E	116	1/1/2010	AC	TAXIWAY	P	0	14,729.00	8/11/2022	12	70
TW F	115	7/1/2022	AAC	TAXIWAY	P	0	14,426.00	7/1/2022	0	100
TW G	110	1/1/2010	AAC	TAXIWAY	P	0	8,671.00	8/11/2022	12	84
TW H	111	1/1/2010	AAC	TAXIWAY	P	0	7,946.00	8/11/2022	12	78
TW P	105	1/1/2010	AAC	TAXIWAY	P	0	193,060.00	8/11/2022	12	82
TW P	106	1/1/2010	AAC	TAXIWAY	P	0	29,080.00	8/11/2022	12	86
TW P	107	1/1/2010	AAC	TAXIWAY	P	0	14,241.00	8/11/2022	12	86
TW P	108	1/1/2010	AAC	TAXIWAY	P	0	10,940.00	8/11/2022	12	79
TW P	112	1/1/2010	AAC	TAXIWAY	P	0	12,673.00	8/11/2022	12	82
TW P	113	1/1/2010	AAC	TAXIWAY	P	0	4,000.00	8/11/2022	12	81
TW P	131	1/1/2010	AAC	TAXIWAY	P	0	12,673.00	8/11/2022	12	80
TW P1	205	1/1/2010	AAC	TAXIWAY	P	0	4,102.00	8/11/2022	12	88
TW P10	240	1/1/2010	AAC	TAXIWAY	P	0	8,665.00	8/11/2022	12	89
TW P11	250	1/1/2010	AAC	TAXIWAY	P	0	8,665.00	8/11/2022	12	93
TW P2	210	1/1/2010	AAC	TAXIWAY	P	0	2,572.00	8/11/2022	12	72
TW P3	215	1/1/2010	AAC	TAXIWAY	P	0	1,488.00	8/11/2022	12	74
TW P4	225	7/1/2022	AAC	TAXIWAY	P	0	8,391.00	7/1/2022	0	100
TW P5	230	1/1/2010	AAC	TAXIWAY	P	0	8,400.00	8/11/2022	12	87
TW P6	220	1/1/2010	AAC	TAXIWAY	P	0	8,315.00	8/11/2022	12	85
TW P7	222	2/24/2018	AC	TAXIWAY	P	0	4,994.00	8/11/2022	4	89
TW P8	221	2/24/2018	AC	TAXIWAY	P	0	5,090.00	8/11/2022	4	87
TW P9	260	1/1/2010	AAC	TAXIWAY	P	0	3,165.00	8/11/2022	12	81

Pavement Database: FDOT

Age Category	Average Age at Inspection	Total Area (SqFt)	Number of Sections	Arithmetic Average PCI	Standard Deviation PCI	Weighted Average PCI
00-02		51,669.00	4	100.00	0.00	100.00
03-05	4	92,251.00	3	89.00	1.63	90.67
11-15	12	1,363,351.00	28	83.39	5.94	85.92
ALL	10	1,507,271.00	35	85.77	7.55	86.69



Appendix B: Maintenance and Rehabilitation Planning Needs



Table B.1: Localized Maintenance and Repair Needs Based on Current Distresses

Network ID	Branch ID	Section ID	Description	Severity	Distress Qty	Distress Unit	Distress Density	Policy Type	Localized Work Type	Work Qty	Work Unit	Unit Cost	Work Cost
BCT	RW 5-23	6105	RAVELING	Low	9,746	SF	1.9%	Preventive	Surface Seal	9,747	SF	\$ 0.75	\$ 7,310
BCT	RW 5-23	6105	WEATHERING	Medium	38,435	SF	7.4%	Preventive	Surface Seal	38,436	SF	\$ 0.75	\$ 28,830
BCT	RW 5-23	6106	RAVELING	Low	1,212	SF	1.7%	Preventive	Surface Seal	1,212	SF	\$ 0.75	\$ 910
BCT	RW 5-23	6106	WEATHERING	Medium	1,212	SF	1.7%	Preventive	Surface Seal	1,212	SF	\$ 0.75	\$ 910
BCT	RW 5-23	6107	WEATHERING	Medium	1,750	SF	5.0%	Preventive	Surface Seal	1,750	SF	\$ 0.75	\$ 1,320
BCT	RW 5-23	6110	WEATHERING	Medium	4,680	SF	1.8%	Preventive	Surface Seal	4,680	SF	\$ 0.75	\$ 3,510
BCT	RW 5-23	6111	L & T CR	Medium	19	LF	0.1%	Preventive	AC Crack Sealing	19	LF	\$ 4.00	\$ 80
BCT	RW 5-23	6111	RAVELING	Medium	8	SF	0.0%	Preventive	Surface Seal	8	SF	\$ 0.75	\$ 10
BCT	RW 5-23	6111	WEATHERING	Medium	1,817	SF	5.0%	Preventive	Surface Seal	1,817	SF	\$ 0.75	\$ 1,370
BCT	TW A	130	WEATHERING	Medium	866	SF	10.0%	Preventive	Surface Seal	865	SF	\$ 0.75	\$ 650
BCT	TW G	110	WEATHERING	Medium	434	SF	5.0%	Preventive	Surface Seal	434	SF	\$ 0.75	\$ 330
BCT	TW H	111	RAVELING	Low	156	SF	2.0%	Preventive	Surface Seal	156	SF	\$ 0.75	\$ 120
BCT	TW H	111	WEATHERING	Medium	390	SF	4.9%	Preventive	Surface Seal	390	SF	\$ 0.75	\$ 300
BCT	TW P	105	L & T CR	Medium	145	LF	0.1%	Preventive	AC Crack Sealing	145	LF	\$ 4.00	\$ 580
BCT	TW P	105	RAVELING	Low	2,075	SF	1.1%	Preventive	Surface Seal	2,075	SF	\$ 0.75	\$ 1,560
BCT	TW P	105	WEATHERING	Medium	10,795	SF	5.6%	Preventive	Surface Seal	10,795	SF	\$ 0.75	\$ 8,100
BCT	TW P	106	RAVELING	Low	16	SF	0.1%	Preventive	Surface Seal	16	SF	\$ 0.75	\$ 20
BCT	TW P	106	WEATHERING	Medium	1,454	SF	5.0%	Preventive	Surface Seal	1,454	SF	\$ 0.75	\$ 1,100
BCT	TW P	107	WEATHERING	Medium	712	SF	5.0%	Preventive	Surface Seal	713	SF	\$ 0.75	\$ 540
BCT	TW P	108	WEATHERING	Medium	547	SF	5.0%	Preventive	Surface Seal	547	SF	\$ 0.75	\$ 420
BCT	TW P	112	L & T CR	Medium	14	LF	0.1%	Preventive	AC Crack Sealing	14	LF	\$ 4.00	\$ 60
BCT	TW P	112	WEATHERING	Medium	634	SF	5.0%	Preventive	Surface Seal	634	SF	\$ 0.75	\$ 480
BCT	TW P	113	RAVELING	Low	184	SF	4.6%	Preventive	Surface Seal	184	SF	\$ 0.75	\$ 140
BCT	TW P	113	WEATHERING	Medium	191	SF	4.8%	Preventive	Surface Seal	191	SF	\$ 0.75	\$ 150
BCT	TW P	131	PATCHING	Medium	8	SF	0.1%	Preventive	AC Full-Depth Patching	24	SF	\$ 11.50	\$ 270
BCT	TW P1	205	WEATHERING	Medium	205	SF	5.0%	Preventive	Surface Seal	205	SF	\$ 0.75	\$ 160
BCT	TW P10	240	WEATHERING	Medium	234	SF	2.7%	Preventive	Surface Seal	234	SF	\$ 0.75	\$ 180
BCT	TW P11	250	WEATHERING	Medium	234	SF	2.7%	Preventive	Surface Seal	234	SF	\$ 0.75	\$ 180
BCT	TW P2	210	RAVELING	Medium	120	SF	4.7%	Preventive	Surface Seal	120	SF	\$ 0.75	\$ 90
BCT	TW P2	210	WEATHERING	Medium	117	SF	4.6%	Preventive	Surface Seal	117	SF	\$ 0.75	\$ 90
BCT	TW P3	215	RAVELING	Low	15	SF	1.0%	Preventive	Surface Seal	15	SF	\$ 0.75	\$ 20
BCT	TW P3	215	WEATHERING	Medium	74	SF	5.0%	Preventive	Surface Seal	74	SF	\$ 0.75	\$ 60
BCT	TW P5	230	WEATHERING	Medium	240	SF	2.9%	Preventive	Surface Seal	240	SF	\$ 0.75	\$ 180
BCT	TW P6	220	RAVELING	Low	158	SF	1.9%	Preventive	Surface Seal	158	SF	\$ 0.75	\$ 120
BCT	TW P6	220	WEATHERING	Medium	178	SF	2.1%	Preventive	Surface Seal	178	SF	\$ 0.75	\$ 140
BCT	TW P9	260	RAVELING	Medium	5	SF	0.2%	Preventive	Surface Seal	5	SF	\$ 0.75	\$ 10
BCT	TW P9	260	WEATHERING	Medium	158	SF	5.0%	Preventive	Surface Seal	158	SF	\$ 0.75	\$ 120
BCT	AP CUSTOMS	4105	WEATHERING	Medium	1,836	SF	2.2%	Preventive	Surface Seal	1,836	SF	\$ 0.75	\$ 1,380
BCT	AP RU 23	5110	WEATHERING	Medium	1,073	SF	5.0%	Preventive	Surface Seal	1,073	SF	\$ 0.75	\$ 810
BCT	AP RU 5	5105	WEATHERING	Medium	653	SF	2.5%	Preventive	Surface Seal	652	SF	\$ 0.75	\$ 490
BCT	AP RU 5	5115	WEATHERING	Medium	553	SF	4.7%	Preventive	Surface Seal	553	SF	\$ 0.75	\$ 420

Airport Pavement Evaluation Report

Statewide Airfield Pavement Management Program

Table B.2: Section-Level 10-Year Major Rehabilitation Needs

Program Year	Network ID	Branch ID	Section ID	Surface	Area (SF)	PCI Before	Rehabilitation Type	Planning Cost Estimate
2023	BCT	TW E	116	AC	14,729	69	AC Rehabilitation	\$ 155,000
2024	BCT	TW P2	210	AAC	2,572	70	AC Rehabilitation	\$ 29,000
2026	BCT	TW P3	215	AAC	1,488	69	AC Rehabilitation	\$ 19,000
2027	BCT	AP RU 5	5105	AAC	26,544	68	AC Rehabilitation	\$ 339,000
2027	BCT	AP RU 5	5115	AC	11,787	69	AC Rehabilitation	\$ 151,000
2028	BCT	TW H	111	AAC	7,946	70	AC Rehabilitation	\$ 107,000
2029	BCT	TW P	108	AAC	10,940	69	AC Rehabilitation	\$ 154,000
2029	BCT	TW P	131	AAC	12,673	70	AC Rehabilitation	\$ 179,000
2030	BCT	RW 5-23	6111	AAC	36,350	69	AC Rehabilitation	\$ 538,000
2030	BCT	TW A	130	AAC	8,671	69	AC Rehabilitation	\$ 129,000
2030	BCT	TW P	113	AAC	4,000	69	AC Rehabilitation	\$ 60,000
2030	BCT	TW P9	260	AAC	3,165	69	AC Rehabilitation	\$ 47,000
2031	BCT	TW P	105	AAC	193,060	69	AC Rehabilitation	\$ 2,995,000
2031	BCT	TW P	112	AAC	12,673	69	AC Rehabilitation	\$ 197,000
2031	BCT	AP RU 23	5110	AAC	21,414	69	AC Rehabilitation	\$ 333,000
2032	BCT	RW 5-23	6105	AAC	520,000	69	AC Rehabilitation	\$ 8,471,000
2032	BCT	TW G	110	AAC	8,671	69	AC Rehabilitation	\$ 142,000
2032	BCT	TW P6	220	AAC	8,315	70	AC Rehabilitation	\$ 136,000

**All planning cost values have been rounded up to the nearest thousand dollars.*



Appendix C: Technical Exhibits



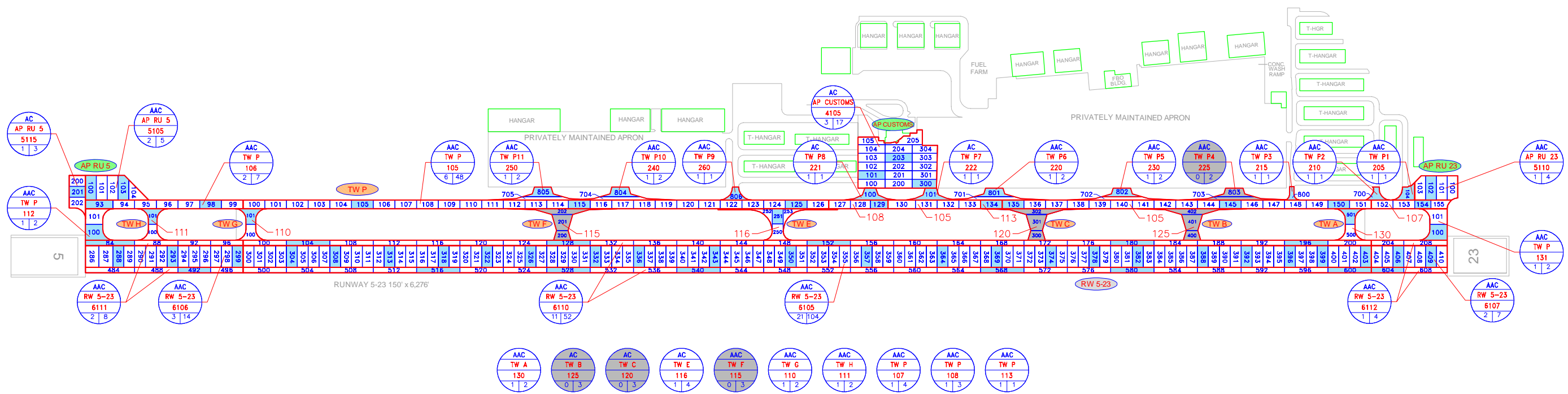


Diagram illustrating the components of a pavement sample unit and section:

- RW 13-31** → TYPICAL RUNWAY BRANCH ID
- TWA** → TYPICAL TAXIWAY BRANCH ID
- APS** → TYPICAL APRON BRANCH ID
- AAC** → PAVEMENT SURFACE TYPE
- AP MAIN** → PAVEMENT BRANCH ID
- 4105** → SECTION NUMBER
- 10/100** → NUMBER OF SAMPLE UNITS IN SECTION
- 10/100** → NUMBER OF SAMPLE UNITS TO BE INSPECTED

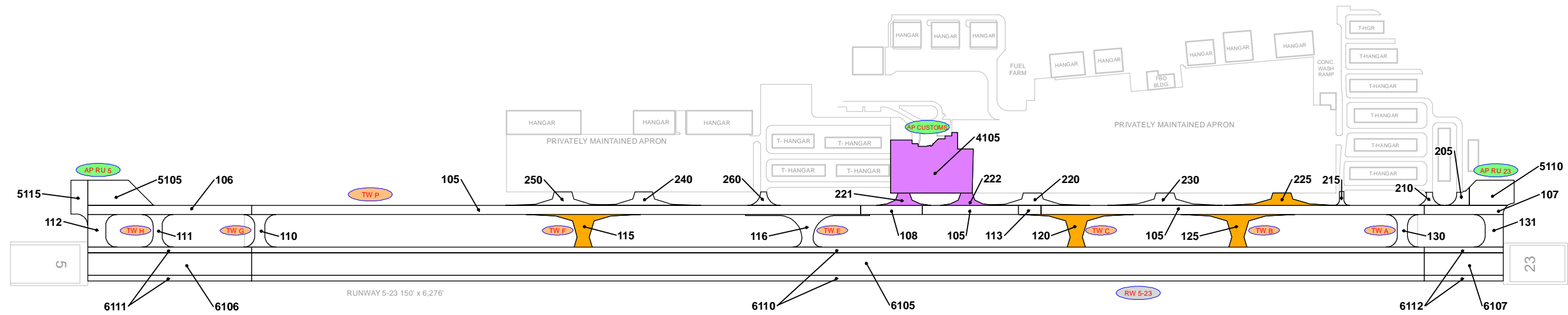
Diagram illustrating a section not inspected due to recent construction:

- AAC** → SECTION NOT INSPECTED DUE TO RECENT CONSTRUCTION. SEE SYSTEM INVENTORY MAP FOR CONSTRUCTION DATES.
- AP MAIN** → SECTION NOT INSPECTED DUE TO RECENT CONSTRUCTION. SEE SYSTEM INVENTORY MAP FOR CONSTRUCTION DATES.
- 4105** → SECTION NOT INSPECTED DUE TO RECENT CONSTRUCTION. SEE SYSTEM INVENTORY MAP FOR CONSTRUCTION DATES.
- 0/100** → SECTION NOT INSPECTED DUE TO RECENT CONSTRUCTION. SEE SYSTEM INVENTORY MAP FOR CONSTRUCTION DATES.

Diagram illustrating inspected sample units:

- 100** → INSPECTED SAMPLE UNITS.

RUNWAY LENGTHS DEPICTED IN THIS DRAWING ARE FOR PAVEMENT MANAGEMENT PURPOSES ONLY AND MAY NOT MATCH PUBLISHED RUNWAY LENGTHS. DRAWING NOT TO SCALE.



RECENT & ANTICIPATED CONSTRUCTION ACTIVITY		
CONSTRUCTION YEAR	LOCATION	WORK TYPE / PAVEMENT SECTION
2018	TW P8, AP CUSTOMS	Complete Reconstruction - AC
	TW P7	New Construction - AC
2022	TW B, TW C	New Construction - AC 4" P-401, 12" P-211
	TW F, TW P4	Mill and Overlay Variable 2" Mill and P-401 Overlay, Fillet Widening 4" P-401, 12" P-211

LEGEND

TYPICAL RUNWAY BRANCH ID

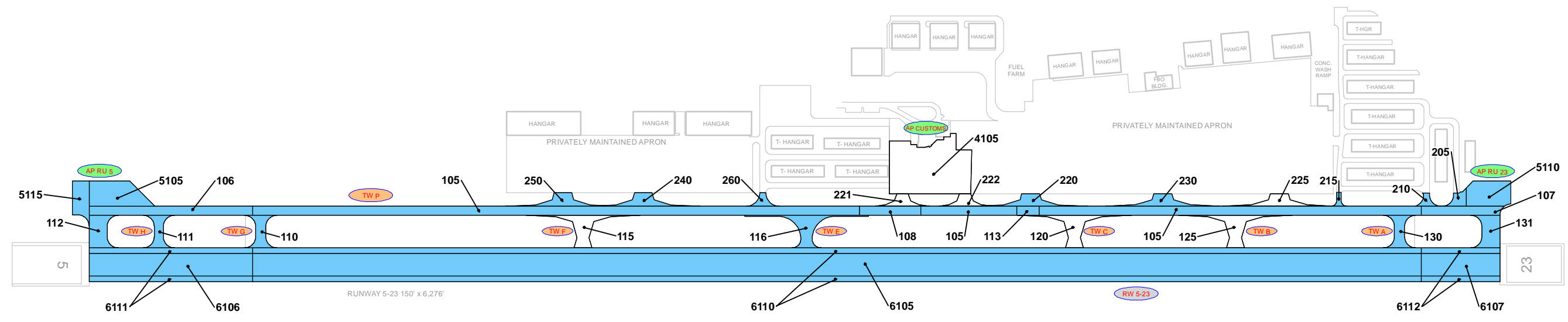
TYPICAL TAXIWAY BRANCH ID

TYPICAL APRON BRANCH ID

PROJECT YEAR

	2017		2022
	2018		2023
	2019		2024
	2020		2025
	2021		2026

RUNWAY LENGTHS DEPICTED IN THIS DRAWING ARE FOR PAVEMENT MANAGEMENT PURPOSES ONLY AND MAY NOT MATCH PUBLISHED RUNWAY LENGTHS. DRAWING NOT TO SCALE.



LEGEND

TYPICAL RUNWAY BRANCH ID

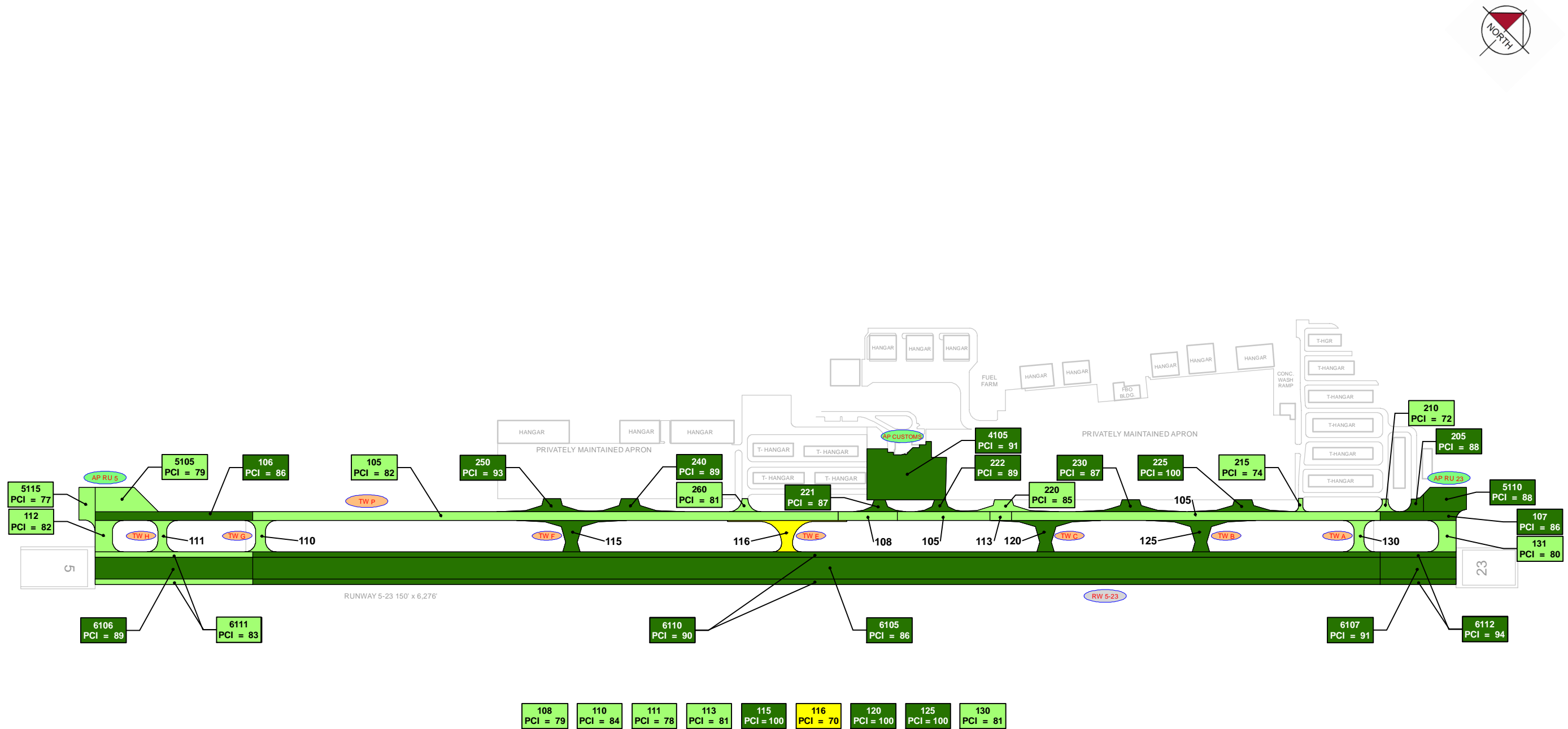
TYPICAL TAXIWAY BRANCH ID

TYPICAL APRON BRANCH ID

AGE AT INSPECTION








	0-5 Years
	6-10 Years
	11-15 Years
	16-20 Years
	> 20 Years

RUNWAY LENGTHS DEPICTED IN THIS DRAWING ARE FOR PAVEMENT MANAGEMENT PURPOSES ONLY AND MAY NOT MATCH PUBLISHED RUNWAY LENGTHS. DRAWING NOT TO SCALE.



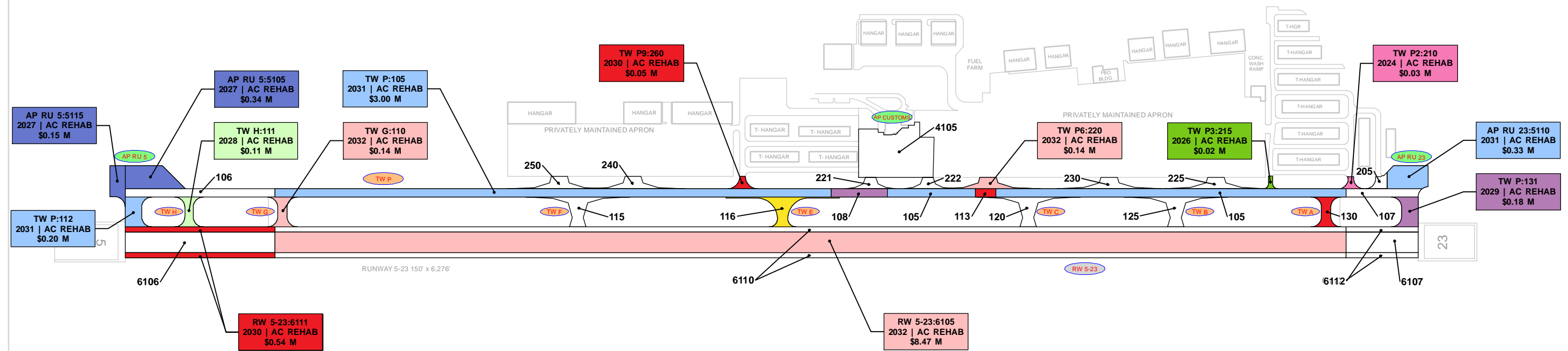
 ← TYPICAL RUNWAY BRANCH ID
 ← TYPICAL TAXIWAY BRANCH ID
 ← TYPICAL APRON BRANCH ID

2022 PAVEMENT CONDITION INDEX

	PCI 86-100 Good
	PCI 71-85 Satisfactory
	PCI 56-70 Fair
	PCI 41-55 Poor
	PCI 26-40 Very Poor
	PCI 11-25 Serious
	PCI 0-10 Failed

"SECTION ID"	"PCI VALUE"
--------------	-------------

RUNWAY LENGTHS DEPICTED IN THIS DRAWING ARE FOR PAVEMENT MANAGEMENT PURPOSES ONLY AND MAY NOT MATCH PUBLISHED RUNWAY LENGTHS. DRAWING NOT TO SCALE.



AP RU 5:5115
2027 | AC REHAB
\$0.15 M

AP RU 5:5105
2027 | AC REHAB
\$0.34 M

TW H:111
2028 | AC REHAB
\$0.11 M

TW P:112
2031 | AC REHAB
\$0.20 M

TW P:105
2031 | AC REHAB
\$3.00 M

TW G:110
2032 | AC REHAB
\$0.14 M

TW P9:260
2030 | AC REHAB
\$0.05 M

RW 5-23:6105
2032 | AC REHAB
\$8.47 M

TW P2:210
2024 | AC REHAB
\$0.03 M

AP RU 23:5110
2031 | AC REHAB
\$0.33 M

TW P:131
2029 | AC REHAB
\$0.18 M

TW E:116
2023 | AC REHAB
\$0.16 M

TW P:108
2029 | AC REHAB
\$0.15 M

TW P:113
2030 | AC REHAB
\$0.06 M

TW A:130
2030 | AC REHAB
\$0.13 M

LEGEND

RW 13-31 — TYPICAL RUNWAY BRANCH ID

TW A — TYPICAL TAXIWAY BRANCH ID

AP S — TYPICAL APRON BRANCH ID

PROGRAM YEAR

2023	2028
2024	2029
2025	2030
2026	2031
2027	2032

"BRANCH","SECTION"
"YEAR","REHAB ACTIVITY"
"EST. COST"

RUNWAY LENGTHS DEPICTED IN THIS DRAWING ARE FOR PAVEMENT MANAGEMENT PURPOSES ONLY AND MAY NOT MATCH PUBLISHED RUNWAY LENGTHS. DRAWING NOT TO SCALE.



Appendix D: Inspection Photograph Documentation





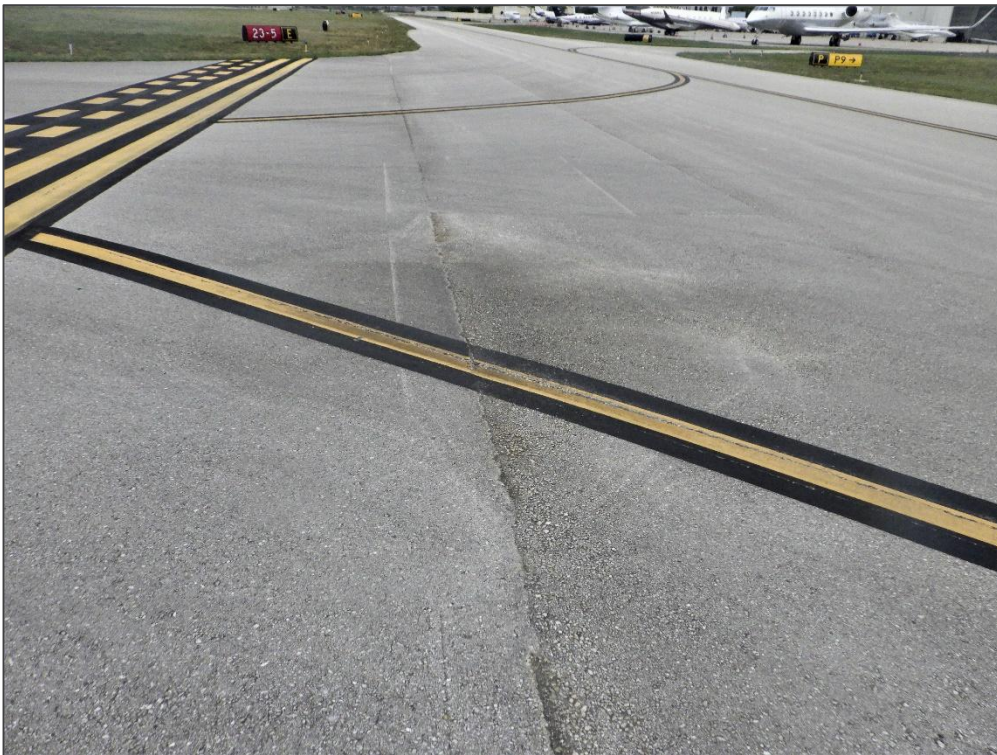
RW 5-23, Section 6105, Sample Unit 304 –Raveling



RW 5-23, Section 6105, Sample Unit 382 – Longitudinal & Transverse Cracking and Raveling



RW 5-23, Section 6111, Sample Unit 84 – Vicinity



TW E, Section 116, Sample Unit 251 – Depression and Longitudinal & Transverse Cracking



TW P, Section 105, Sample Unit 105 – Bleeding



TW P, Section 105, Sample Unit 145 – Raveling



AP RU 5, Section 5115, Sample Unit 201 – Patching



AP CUSTOMS, Section 4105, Sample Unit 101 – Vicinity



Appendix E: Inspection Distress Details



Re-Inspection Report

FDOT

Generated Date 11/17/2022

Page 1 of 36

Network:	BCT		Name:	BOCA RATON AIRPORT		
Branch:	AP CUSTOMS	Name:	CUSTOMS APRON	Use:	APRON	Area: 82,167 SqFt
Section:	4105	of 1	From: -	To: -	Last Const.: 2/24/2018	
Surface:	AC	Family:	CA653-RL-AP-AC	Zone:	Category:	Rank: P
Area:	82,167 SqFt	Length:	360 Ft	Width:	235 Ft	
Slabs:		Slab Length:	Ft	Slab Width:	Ft	Joint Length: Ft
Shoulder:		Street Type:		Grade:	0	Lanes: 0
Section Comments:						
Work Date:	1/1/1995	Work Type:	Surface Treatment - Seal Coat	Code:	ST-SC	Is Major M&R: False
Work Date:	1/1/1995	Work Type:	New Construction - AC	Code:	NC-AC	Is Major M&R: True
Work Date:	2/24/2018	Work Type:	Complete Reconstruction - AC	Code:	CR-AC	Is Major M&R: True
Last Insp. Date:	8/11/2022	TotalSamples:	17	Surveyed:	3	
Conditions:	PCI:	91				
Inspection Comments:						
Sample Number:	101	Type:	R	Area:	5000.00 SqFt	PCI: 92
Sample Comments:						
48	L & T CR	L	6.00	Ft		
57	WEATHERING	L	5000.00	SqFt		
Sample Number:	203	Type:	R	Area:	5000.00 SqFt	PCI: 94
Sample Comments:						
57	WEATHERING	L	5000.00	SqFt		
Sample Number:	300	Type:	R	Area:	4500.00 SqFt	PCI: 86
Sample Comments:						
48	L & T CR	L	25.00	Ft		
57	WEATHERING	L	4176.00	SqFt		
57	WEATHERING	M	324.00	SqFt		

Network:	BCT			Name:	BOCA RATON AIRPORT							
Branch:	AP RU 23		Name:	RUN-UP APRON 23		Use:	APRON	Area:	21,414 SqFt			
Section:	5110		of	1		From:	-		To:	-	Last Const.:	1/1/2010
Surface:	AAC		Family:	CA653-RL-AP-AAC-APC		Zone:			Category:	Rank: P		
Area:	21,414 SqFt		Length:	199 Ft		Width:	111 Ft					
Slabs:			Slab Length:	Ft		Slab Width:	Ft		Joint Length:	Ft		
Shoulder:			Street Type:			Grade:	0		Lanes:	0		
Section Comments:												
Work Date:	1/1/1996		Work Type:	BUILT				Code:	IMPORTED		Is Major M&R:	True
Work Date:	1/1/2010		Work Type:	Mill and Overlay				Code:	ML-OVL		Is Major M&R:	True
Last Insp. Date:	8/11/2022		TotalSamples:	4		Surveyed:	1					
Conditions:	PCI: 88											
Inspection Comments:												
Sample Number:	102		Type:	R		Area:	5550.00 SqFt		PCI:	88		
Sample Comments:												
48	L & T CR		L	2.00 Ft								
57	WEATHERING		L	5272.00 SqFt								
57	WEATHERING		M	278.00 SqFt								

Network:	BCT		Name:	BOCA RATON AIRPORT								
Branch:	AP RU 5		Name:	RUN-UP APRON 5		Use:	APRON	Area:	38,331 SqFt			
Section:	5105		of	2		From:	-		To:	-	Last Const.:	1/1/2010
Surface:	AAC		Family:	CA653-RL-AP-AAC-APC		Zone:			Category:	Rank: P		
Area:	26,544 SqFt		Length:	240 Ft		Width:	120 Ft					
Slabs:			Slab Length:	Ft		Slab Width:	Ft		Joint Length:	Ft		
Shoulder:			Street Type:			Grade:	0		Lanes:	0		
Section Comments:												
Work Date:	1/1/1996		Work Type:	BUILT				Code:	IMPORTED		Is Major M&R:	True
Work Date:	1/1/2010		Work Type:	Mill and Overlay				Code:	ML-OVL		Is Major M&R:	True
Last Insp. Date:	8/11/2022		TotalSamples:	5		Surveyed:	2					
Conditions:	PCI: 79											
Inspection Comments:												
Sample Number:	100		Type:	R		Area:	5600.00 SqFt		PCI:	71		
Sample Comments:												
48	L & T CR		L	2.00 Ft								
50	PATCHING		L	1196.00 SqFt								
56	SWELLING		L	2.00 SqFt								
57	WEATHERING		L	4404.00 SqFt								
Sample Number:	103		Type:	R		Area:	5420.00 SqFt		PCI:	88		
Sample Comments:												
48	L & T CR		L	10.00 Ft								
57	WEATHERING		L	5149.00 SqFt								
57	WEATHERING		M	271.00 SqFt								

Network:	BCT			Name:	BOCA RATON AIRPORT						
Branch:	AP RU 5		Name:	RUN-UP APRON 5		Use:	APRON	Area:	38,331 SqFt		
Section:	5115 of 2		From:	-			To:	-			
Surface:	AC		Family:	CA653-RL-AP-AC		Zone:	Category:		Rank: P		
Area:	11,787 SqFt		Length:	150 Ft		Width:	75 Ft				
Slabs:	Slab Length:		Ft		Slab Width:		Ft		Joint Length:	Ft	
Shoulder:	Street Type:		Grade:		0		Lanes:		0		
Section Comments:											
Work Date:	1/1/2010		Work Type: New Construction - Initial				Code:	NU-IN		Is Major M&R:	True
Last Insp. Date:	8/11/2022		TotalSamples:	3		Surveyed:		1			
Conditions:	PCI: 77										
Inspection Comments:											
Sample Number:	201		Type:	R		Area:	3750.00 SqFt		PCI:	77	
Sample Comments:											
48	L & T CR		L	13.00 Ft							
50	PATCHING		L	231.00 SqFt							
57	WEATHERING		L	3343.00 SqFt							
57	WEATHERING		M	176.00 SqFt							

Network:	BCT		Name:	BOCA RATON AIRPORT										
Branch:	RW 5-23		Name:	RUNWAY 5-23		Use:	RUNWAY	Area:	941,550 SqFt					
Section:	6105		of	6		From:	-		To:	-		Last Const.:	1/1/2010	
Surface:	AAC		Family:	CA653-RL-RW-AAC-APC		Zone:			Category:			Rank:	P	
Area:	520,000 SqFt		Length:	5,200 Ft		Width:			100 Ft					
Slabs:			Slab Length:	Ft		Slab Width:			Ft	Joint Length:			Ft	
Shoulder:			Street Type:			Grade:	0		Lanes:	0				
Section Comments:														
Work Date:	1/1/1974		Work Type:	BUILT				Code:	IMPORTED		Is Major M&R:	True		
Work Date:	1/1/1984		Work Type:	OVERLAY				Code:	IMPORTED		Is Major M&R:	True		
Work Date:	1/1/1984		Work Type:	OVERLAY				Code:	IMPORTED		Is Major M&R:	True		
Work Date:	1/1/2010		Work Type:	Mill and Overlay				Code:	ML-OVL		Is Major M&R:	True		
Work Date:	11/1/2018		Work Type:	Surface Treatment - Seal Coat				Code:	ST-SC		Is Major M&R:	False		
Last Insp. Date:	8/11/2022		TotalSamples:	104		Surveyed:	21							
Conditions:	PCI: 86													
Inspection Comments:														
Sample Number:	304		Type:	R		Area:	5000.00 SqFt		PCI:	78				
Sample Comments:														
52	RAVELING		L	1572.00 SqFt										
57	WEATHERING		L	3428.00 SqFt										
Sample Number:	308		Type:	R		Area:	5000.00 SqFt		PCI:	85				
Sample Comments:														
48	L & T CR		L	7.00 Ft										
52	RAVELING		L	27.00 SqFt										
57	WEATHERING		L	4476.00 SqFt										
57	WEATHERING		M	497.00 SqFt										
Sample Number:	313		Type:	R		Area:	5000.00 SqFt		PCI:	86				
Sample Comments:														
48	L & T CR		L	8.00 Ft										
57	WEATHERING		L	4500.00 SqFt										
57	WEATHERING		M	500.00 SqFt										
Sample Number:	318		Type:	R		Area:	5000.00 SqFt		PCI:	84				
Sample Comments:														
48	L & T CR		L	50.00 Ft										
57	WEATHERING		L	4500.00 SqFt										
57	WEATHERING		M	500.00 SqFt										
Sample Number:	322		Type:	R		Area:	5000.00 SqFt		PCI:	83				
Sample Comments:														
48	L & T CR		L	91.00 Ft										
57	WEATHERING		L	4500.00 SqFt										
57	WEATHERING		M	500.00 SqFt										
Sample Number:	326		Type:	R		Area:	5000.00 SqFt		PCI:	89				
Sample Comments:														
57	WEATHERING		L	4500.00 SqFt										
57	WEATHERING		M	500.00 SqFt										
Sample Number:	332		Type:	R		Area:	5000.00 SqFt		PCI:	89				
Sample Comments:														
57	WEATHERING		L	4500.00 SqFt										
57	WEATHERING		M	500.00 SqFt										

Sample Number: 336		Type:	R	Area:		5000.00 SqFt	PCI: 91
Sample Comments:							
57	WEATHERING		L	4750.00	SqFt		
57	WEATHERING		M	250.00	SqFt		
Sample Number: 343		Type:	R	Area:		5000.00 SqFt	PCI: 85
Sample Comments:							
48	L & T CR		L	46.00	Ft		
52	RAVELING		L	10.00	SqFt		
57	WEATHERING		L	4740.00	SqFt		
57	WEATHERING		M	250.00	SqFt		
Sample Number: 350		Type:	R	Area:		5000.00 SqFt	PCI: 86
Sample Comments:							
48	L & T CR		L	13.00	Ft		
52	RAVELING		L	1.00	SqFt		
57	WEATHERING		L	4749.00	SqFt		
57	WEATHERING		M	250.00	SqFt		
Sample Number: 357		Type:	R	Area:		5000.00 SqFt	PCI: 91
Sample Comments:							
57	WEATHERING		L	4750.00	SqFt		
57	WEATHERING		M	250.00	SqFt		
Sample Number: 364		Type:	R	Area:		5000.00 SqFt	PCI: 81
Sample Comments:							
48	L & T CR		L	17.00	Ft		
52	RAVELING		L	100.00	SqFt		
57	WEATHERING		L	4410.00	SqFt		
57	WEATHERING		M	490.00	SqFt		
Sample Number: 369		Type:	R	Area:		5000.00 SqFt	PCI: 85
Sample Comments:							
52	RAVELING		L	116.00	SqFt		
57	WEATHERING		L	4396.00	SqFt		
57	WEATHERING		M	488.00	SqFt		
Sample Number: 374		Type:	R	Area:		5000.00 SqFt	PCI: 83
Sample Comments:							
48	L & T CR		L	36.00	Ft		
52	RAVELING		L	32.00	SqFt		
57	WEATHERING		L	4471.00	SqFt		
57	WEATHERING		M	497.00	SqFt		
Sample Number: 378		Type:	R	Area:		5000.00 SqFt	PCI: 83
Sample Comments:							
48	L & T CR		L	23.00	Ft		
52	RAVELING		L	42.00	SqFt		
57	WEATHERING		L	4462.00	SqFt		
57	WEATHERING		M	496.00	SqFt		
Sample Number: 382		Type:	R	Area:		5000.00 SqFt	PCI: 82
Sample Comments:							
48	L & T CR		L	49.00	Ft		
52	RAVELING		L	48.00	SqFt		
57	WEATHERING		L	4457.00	SqFt		
57	WEATHERING		M	495.00	SqFt		
Sample Number: 388		Type:	R	Area:		5000.00 SqFt	PCI: 84
Sample Comments:							
48	L & T CR		L	15.00	Ft		
52	RAVELING		L	20.00	SqFt		
57	WEATHERING		L	4482.00	SqFt		
57	WEATHERING		M	498.00	SqFt		

Sample Number: 392		Type:	R	Area:		5000.00 SqFt	PCI:	85
Sample Comments:								
48	L & T CR		L	18.00	Ft			
57	WEATHERING		L	4500.00	SqFt			
57	WEATHERING		M	500.00	SqFt			
Sample Number: 396		Type:	R	Area:		5000.00 SqFt	PCI:	94
Sample Comments:								
57	WEATHERING		L	5000.00	SqFt			
Sample Number: 399		Type:	R	Area:		5000.00 SqFt	PCI:	91
Sample Comments:								
57	WEATHERING		L	4750.00	SqFt			
57	WEATHERING		M	250.00	SqFt			
Sample Number: 403		Type:	R	Area:		5000.00 SqFt	PCI:	92
Sample Comments:								
57	WEATHERING		L	4950.00	SqFt			
57	WEATHERING		M	50.00	SqFt			

Network:	BCT		Name:	BOCA RATON AIRPORT							
Branch:	RW 5-23		Name:	RUNWAY 5-23		Use:	RUNWAY	Area:	941,550 SqFt		
Section:	6106 of 6		From:	-		To:	-		Last Const.:	1/1/2010	
Surface:	AAC		Family:	CA653-RL-RW-AAC-APC		Zone:			Category:	Rank: P	
Area:	72,700 SqFt		Length:	725 Ft		Width:	100 Ft				
Slabs:			Slab Length:	Ft		Slab Width:	Ft		Joint Length:	Ft	
Shoulder:			Street Type:			Grade:	0		Lanes:	0	
Section Comments:											
Work Date:	1/1/1996		Work Type: BUILT				Code:	IMPORTED		Is Major M&R:	True
Work Date:	1/1/2010		Work Type: Mill and Overlay				Code:	ML-OVL		Is Major M&R:	True
Work Date:	11/1/2018		Work Type: Surface Treatment - Seal Coat				Code:	ST-SC		Is Major M&R:	False
Last Insp. Date:	8/11/2022		TotalSamples:	14		Surveyed:	3				
Conditions:	PCI: 89										
Inspection Comments:											
Sample Number:	288		Type:	R		Area:	5000.00 SqFt		PCI:	92	
Sample Comments:											
48	L & T CR		L	4.00 Ft							
57	WEATHERING		L	5000.00 SqFt							
Sample Number:	293		Type:	R		Area:	5000.00 SqFt		PCI:	88	
Sample Comments:											
52	RAVELING		L	250.00 SqFt							
57	WEATHERING		L	4750.00 SqFt							
Sample Number:	299		Type:	R		Area:	5000.00 SqFt		PCI:	88	
Sample Comments:											
48	L & T CR		L	3.00 Ft							
57	WEATHERING		L	4750.00 SqFt							
57	WEATHERING		M	250.00 SqFt							

Network:	BCT		Name:	BOCA RATON AIRPORT								
Branch:	RW 5-23		Name:	RUNWAY 5-23		Use:	RUNWAY		Area:	941,550 SqFt		
Section:	6107 of 6		From:	-		To:	-		Last Const.:	1/1/2010		
Surface:	AAC		Family:	CA653-RL-RW-AAC-APC		Zone:			Category:	Rank: P		
Area:	35,000 SqFt		Length:	350 Ft		Width:	100 Ft					
Slabs:			Slab Length:	Ft		Slab Width:	Ft		Joint Length:	Ft		
Shoulder:			Street Type:			Grade:	0		Lanes:	0		
Section Comments:												
Work Date:	1/1/1996		Work Type:	BUILT				Code:	IMPORTED		Is Major M&R:	True
Work Date:	1/1/2010		Work Type:	Mill and Overlay				Code:	ML-OVL		Is Major M&R:	True
Work Date:	11/1/2018		Work Type:	Surface Treatment - Seal Coat				Code:	ST-SC		Is Major M&R:	False
Last Insp. Date: 8/11/2022												
Conditions: PCI: 91			TotalSamples:	7		Surveyed: 2						
Inspection Comments:												
Sample Number:	406		Type:	R		Area:	5000.00 SqFt		PCI:	91		
Sample Comments:												
57	WEATHERING		L	4750.00 SqFt								
57	WEATHERING		M	250.00 SqFt								
Sample Number:	409		Type:	R		Area:	5000.00 SqFt		PCI:	91		
Sample Comments:												
57	WEATHERING		L	4750.00 SqFt								
57	WEATHERING		M	250.00 SqFt								

Network:	BCT		Name:	BOCA RATON AIRPORT								
Branch:	RW 5-23		Name:	RUNWAY 5-23		Use:	RUNWAY		Area:	941,550 SqFt		
Section:	6110 of 6		From:	-		To:	-		Last Const.:	1/1/2010		
Surface:	AAC		Family:	CA653-RL-RW-AAC-APC		Zone:			Category:	Rank: P		
Area:	260,000 SqFt		Length:	10,400 Ft		Width:	25 Ft					
Slabs:			Slab Length:	Ft		Slab Width:	Ft		Joint Length:	Ft		
Shoulder:			Street Type:			Grade:	0		Lanes:	0		
Section Comments:												
Work Date:	1/1/1974		Work Type:	BUILT				Code:	IMPORTED		Is Major M&R:	True
Work Date:	1/1/1984		Work Type:	OVERLAY				Code:	IMPORTED		Is Major M&R:	True
Work Date:	1/1/1984		Work Type:	OVERLAY				Code:	IMPORTED		Is Major M&R:	True
Work Date:	1/1/2010		Work Type:	Mill and Overlay				Code:	ML-OVL		Is Major M&R:	True
Work Date:	11/1/2018		Work Type:	Surface Treatment - Seal Coat				Code:	ST-SC		Is Major M&R:	False
Work Date:	1/1/2021		Work Type:	Patching - AC				Code:	PA-AC		Is Major M&R:	False
Last Insp. Date: 8/11/2022												
			TotalSamples:	52		Surveyed:		11				
Conditions:	PCI: 90											
Inspection Comments:												
Sample Number:	104		Type:	R		Area:	5000.00 SqFt		PCI:	90		
Sample Comments:												
48	L & T CR		L	19.00 Ft								
57	WEATHERING		L	5000.00 SqFt								
Sample Number:	128		Type:	R		Area:	5000.00 SqFt		PCI:	90		
Sample Comments:												
48	L & T CR		L	36.00 Ft								
57	WEATHERING		L	5000.00 SqFt								
Sample Number:	152		Type:	R		Area:	5000.00 SqFt		PCI:	92		
Sample Comments:												
48	L & T CR		L	5.00 Ft								
57	WEATHERING		L	5000.00 SqFt								
Sample Number:	180		Type:	R		Area:	5000.00 SqFt		PCI:	90		
Sample Comments:												
48	L & T CR		L	17.00 Ft								
57	WEATHERING		L	5000.00 SqFt								
Sample Number:	196		Type:	R		Area:	5000.00 SqFt		PCI:	91		
Sample Comments:												
57	WEATHERING		L	4760.00 SqFt								
57	WEATHERING		M	240.00 SqFt								
Sample Number:	516		Type:	R		Area:	5000.00 SqFt		PCI:	86		
Sample Comments:												
48	L & T CR		L	59.00 Ft								
57	WEATHERING		L	4750.00 SqFt								
57	WEATHERING		M	250.00 SqFt								
Sample Number:	528		Type:	R		Area:	5000.00 SqFt		PCI:	91		
Sample Comments:												
57	WEATHERING		L	4750.00 SqFt								
57	WEATHERING		M	250.00 SqFt								
Sample Number:	540		Type:	R		Area:	5000.00 SqFt		PCI:	94		
Sample Comments:												

57	WEATHERING	L	5000.00	SqFt		
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Sample Number:		568	Type:	R	Area:	5000.00 SqFt
Sample Comments:					PCI:	89
48	L & T CR	L	73.00	Ft		
57	WEATHERING	L	5000.00	SqFt		
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Sample Number:		580	Type:	R	Area:	5000.00 SqFt
Sample Comments:					PCI:	89
48	L & T CR	L	56.00	Ft		
57	WEATHERING	L	5000.00	SqFt		
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Sample Number:		600	Type:	R	Area:	5000.00 SqFt
Sample Comments:					PCI:	87
48	L & T CR	L	3.00	Ft		
56	SWELLING	L	3.00	SqFt		
57	WEATHERING	L	4750.00	SqFt		
57	WEATHERING	M	250.00	SqFt		

Network:		BCT		Name:		BOCA RATON AIRPORT						
Branch:	RW 5-23		Name:	RUNWAY 5-23		Use:	RUNWAY	Area:	941,550 SqFt			
Section:	6111		of	6	From:	-		To:	-		Last Const.:	1/1/2010
Surface:	AAC		Family:	CA653-RL-RW-AAC-APC		Zone:			Category:	Rank: P		
Area:	36,350 SqFt		Length:	1,450 Ft		Width:	25 Ft					
Slabs:			Slab Length:	Ft		Slab Width:	Ft		Joint Length:	Ft		
Shoulder:			Street Type:			Grade:	0		Lanes:	0		
Section Comments:												
Work Date:	1/1/1996		Work Type:	BUILT				Code:	IMPORTED		Is Major M&R:	True
Work Date:	1/1/2010		Work Type:	Mill and Overlay				Code:	ML-OVL		Is Major M&R:	True
Work Date:	11/1/2018		Work Type:	Surface Treatment - Seal Coat				Code:	ST-SC		Is Major M&R:	False
Last Insp. Date:	8/11/2022		TotalSamples:	8		Surveyed:	2					
Conditions:	PCI: 83											
Inspection Comments:												
Sample Number:	492		Type:	R		Area:	3750.00 SqFt		PCI:	84		
Sample Comments:												
48	L & T CR		L	5.00 Ft								
52	RAVELING		M	2.00 SqFt								
57	WEATHERING		L	3561.00 SqFt								
57	WEATHERING		M	187.00 SqFt								
Sample Number:	84		Type:	R		Area:	5675.00 SqFt		PCI:	82		
Sample Comments:												
48	L & T CR		L	59.00 Ft								
48	L & T CR		M	5.00 Ft								
57	WEATHERING		L	5391.00 SqFt								
57	WEATHERING		M	284.00 SqFt								

Network:	BCT			Name:	BOCA RATON AIRPORT						
Branch:	RW 5-23		Name:	RUNWAY 5-23		Use:	RUNWAY		Area:	941,550 SqFt	
Section:	6112 of 6		From:	-			To:	-		Last Const.:	1/1/2010
Surface:	AAC		Family:	CA653-RL-RW-AAC-APC		Zone:			Category:	Rank: P	
Area:	17,500 SqFt		Length:	700 Ft		Width:	25 Ft				
Slabs:	Slab Length:		Ft		Slab Width:	Ft		Joint Length:	Ft		
Shoulder:	Street Type:				Grade:	0		Lanes:	0		
Section Comments:											
Work Date:	1/1/1996		Work Type: BUILT				Code:	IMPORTED		Is Major M&R:	True
Work Date:	1/1/2010		Work Type: Mill and Overlay				Code:	ML-OVL		Is Major M&R:	True
Work Date:	11/1/2018		Work Type: Surface Treatment - Seal Coat				Code:	ST-SC		Is Major M&R:	False
Last Insp. Date:	8/11/2022		TotalSamples:	4		Surveyed:	1				
Conditions:	PCI: 94										
Inspection Comments:											
Sample Number:	604		Type:	R		Area:	3750.00 SqFt		PCI:	94	
Sample Comments:											
57	WEATHERING		L	3750.00 SqFt							

Network:		BCT		Name:		BOCA RATON AIRPORT						
Branch:	TW A		Name:	TAXIWAY A		Use:	TAXIWAY	Area:	8,671 SqFt			
Section:	130		of	1	From:	-		To:	-		Last Const.:	1/1/2010
Surface:	AAC		Family:	CA653-RL-TW-AAC-APC		Zone:			Category:	Rank: P		
Area:	8,671 SqFt		Length:	145 Ft		Width:	45 Ft					
Slabs:			Slab Length:	Ft		Slab Width:	Ft		Joint Length:	Ft		
Shoulder:			Street Type:			Grade:	0		Lanes:	0		
Section Comments:												
Work Date:	1/1/1974		Work Type: BUILT				Code:	IMPORTED		Is Major M&R: True		
Work Date:	1/1/1984		Work Type: OVERLAY				Code:	IMPORTED		Is Major M&R: True		
Work Date:	1/1/2010		Work Type: Mill and Overlay				Code:	ML-OVL		Is Major M&R: True		
Last Insp. Date:	8/11/2022		TotalSamples:	2		Surveyed:	1					
Conditions:	PCI: 81											
Inspection Comments:												
Sample Number:	501		Type:	R		Area:	4336.00 SqFt		PCI:	81		
Sample Comments:												
48	L & T CR		L	115.00 Ft								
57	WEATHERING		L	3903.00 SqFt								
57	WEATHERING		M	433.00 SqFt								

Network:	BCT		Name:	BOCA RATON AIRPORT							
Branch:	TW E		Name:	TAXIWAY E		Use:	TAXIWAY	Area:	14,729 SqFt		
Section:	116	of 1	From:	-			To:	-		Last Const.:	1/1/2010
Surface:	AC	Family:	CA653-RL-TW-AC		Zone:		Category:		Rank:	P	
Area:	14,729 SqFt	Length:	145 Ft		Width:	60 Ft					
Slabs:		Slab Length:	Ft		Slab Width:	Ft		Joint Length:	Ft		
Shoulder:		Street Type:			Grade:	0		Lanes:	0		
Section Comments:											
Work Date:	1/1/2010		Work Type:	New Construction - Initial			Code:	NU-IN		Is Major M&R:	True
Last Insp. Date:	8/11/2022		TotalSamples:	4		Surveyed:	1				
Conditions:	PCI:	70									
Inspection Comments:											
Sample Number:	251	Type:	R	Area:	3500.00 SqFt		PCI:	70			
Sample Comments:											
45	DEPRESSION	L	180.00 SqFt								
48	L & T CR	L	113.00 Ft								
57	WEATHERING	L	3500.00 SqFt								

Network:	BCT		Name:	BOCA RATON AIRPORT					
Branch:	TW F		Name:	TAXIWAY F		Use:	TAXIWAY	Area:	14,426 SqFt
Section:	115	of	1	From:	-	To:	-	Last Const.:	7/1/2022
Surface:	AAC	Family:	CA653-RL-TW-AAC-APC	Zone:		Category:		Rank:	P
Area:	14,426 SqFt	Length:	145 Ft	Width:	96 Ft				
Slabs:		Slab Length:	Ft	Slab Width:	Ft	Joint Length:	Ft		
Shoulder:		Street Type:		Grade:	0	Lanes:	0		
Section Comments:									
Work Date:	1/1/1974	Work Type: BUILT				Code:	IMPORTED	Is Major M&R:	True
Work Date:	1/1/1984	Work Type: OVERLAY				Code:	IMPORTED	Is Major M&R:	True
Work Date:	1/1/1984	Work Type: OVERLAY				Code:	IMPORTED	Is Major M&R:	True
Work Date:	1/1/2010	Work Type: Mill and Overlay				Code:	ML-OVL	Is Major M&R:	True
Work Date:	7/1/2022	Work Type: Mill and Overlay				Code:	ML-OVL	Is Major M&R:	True
Last Insp. Date:	5/16/2019	TotalSamples:	2	Surveyed:	1				
Conditions:	PCI: 86	NOTE: *** Pre-Construction PCI ***							
Inspection Comments:									
Sample Number:	200	Type:	R	Area:	4073.00 SqFt	PCI:	86		
Sample Comments:									
48	L & T CR	L	24.00	Ft					
52	RAVELING	L	81.00	SqFt					
57	WEATHERING	L	3992.00	SqFt					

Network:	BCT	Name:		BOCA RATON AIRPORT					
Branch:	TW G	Name:		TAXIWAY G		Use:	TAXIWAY	Area:	8,671 SqFt
Section:	110	of	1	From:	-	To:	-	Last Const.:	1/1/2010
Surface:	AAC	Family:	CA653-RL-TW-AAC-APC	Zone:		Category:		Rank:	P
Area:	8,671 SqFt	Length:	145 Ft	Width:	45 Ft				
Slabs:	Slab Length:	Ft	Slab Width:	Ft	Joint Length:	Ft			
Shoulder:	Street Type:	Grade:	0	Lanes:	0				
Section Comments:									
Work Date:	1/1/1974	Work Type: BUILT				Code:	IMPORTED	Is Major M&R:	True
Work Date:	1/1/1984	Work Type: OVERLAY				Code:	IMPORTED	Is Major M&R:	True
Work Date:	1/1/1984	Work Type: OVERLAY				Code:	IMPORTED	Is Major M&R:	True
Work Date:	1/1/2010	Work Type: Mill and Overlay				Code:	ML-OVL	Is Major M&R:	True
Last Insp. Date:	8/11/2022	TotalSamples:	2	Surveyed:	1				
Conditions:	PCI: 84								
Inspection Comments:									
Sample Number:	101	Type:	R	Area:	4335.00 SqFt	PCI:	84		
Sample Comments:									
48	L & T CR	L	90.00	Ft					
57	WEATHERING	L	4118.00	SqFt					
57	WEATHERING	M	217.00	SqFt					

Network:		BCT		Name:		BOCA RATON AIRPORT								
Branch:	TW H		Name:	TAXIWAY H		Use:	TAXIWAY	Area:	7,946 SqFt					
Section:	111		of	1		From:	-		To:	-		Last Const.:	1/1/2010	
Surface:	AAC		Family:	CA653-RL-TW-AAC-APC		Zone:			Category:			Rank:	P	
Area:	7,946 SqFt		Length:	145 Ft		Width:	40 Ft							
Slabs:			Slab Length:	Ft		Slab Width:	Ft		Joint Length:			Ft		
Shoulder:			Street Type:			Grade:	0		Lanes:	0				
Section Comments:														
Work Date:	1/1/1996		Work Type:	BUILT				Code:	IMPORTED		Is Major M&R:	True		
Work Date:	1/1/2010		Work Type:	Mill and Overlay				Code:	ML-OVL		Is Major M&R:	True		
Last Insp. Date:	8/11/2022		TotalSamples:	2		Surveyed:	1							
Conditions:	PCI: 78													
Inspection Comments:														
Sample Number:	101		Type:	R		Area:	3973.00 SqFt		PCI:	78				
Sample Comments:														
48	L & T CR		L	113.00		Ft								
52	RAVELING		L	78.00		SqFt								
57	WEATHERING		L	3700.00		SqFt								
57	WEATHERING		M	195.00		SqFt								

Network:	BCT		Name:		BOCA RATON AIRPORT					
Branch:	TW P		Name:		TAXIWAY P	Use:	TAXIWAY	Area:	276,667 SqFt	
Section:	105	of 7		From:	-	To:		-	Last Const.: 1/1/2010	
Surface:	AAC	Family:	CA653-RL-TW-AAC-APC		Zone:	Category:		Rank: P		
Area:	193,060 SqFt		Length:	4,930 Ft		Width:	40 Ft			
Slabs:	Slab Length:		Ft		Slab Width:	Ft		Joint Length:	Ft	
Shoulder:	Street Type:		Grade:		0		Lanes:	0		
Section Comments:										
Work Date:	1/1/1974		Work Type: BUILT				Code:	IMPORTED		Is Major M&R: True
Work Date:	1/1/1984		Work Type: OVERLAY				Code:	IMPORTED		Is Major M&R: True
Work Date:	1/1/1984		Work Type: OVERLAY				Code:	IMPORTED		Is Major M&R: True
Work Date:	1/1/2010		Work Type: Mill and Overlay				Code:	ML-OVL		Is Major M&R: True
Last Insp. Date:	8/11/2022		TotalSamples:	48		Surveyed: 6				
Conditions:	PCI: 82									
Inspection Comments:										
Sample Number:	105	Type:	R	Area:	4000.00 SqFt		PCI:	77		
Sample Comments:										
42	BLEEDING	N	8.00 SqFt							
48	L & T CR	L	99.00 Ft							
48	L & T CR	M	18.00 Ft							
57	WEATHERING	L	3800.00 SqFt							
57	WEATHERING	M	200.00 SqFt							
Sample Number:	115	Type:	R	Area:	4000.00 SqFt		PCI:	83		
Sample Comments:										
48	L & T CR	L	31.00 Ft							
52	RAVELING	L	45.00 SqFt							
57	WEATHERING	L	3767.00 SqFt							
57	WEATHERING	M	188.00 SqFt							
Sample Number:	125	Type:	R	Area:	4000.00 SqFt		PCI:	88		
Sample Comments:										
52	RAVELING	L	3.00 SqFt							
57	WEATHERING	L	3597.00 SqFt							
57	WEATHERING	M	400.00 SqFt							
Sample Number:	135	Type:	R	Area:	4000.00 SqFt		PCI:	78		
Sample Comments:										
48	L & T CR	L	111.00 Ft							
48	L & T CR	L	21.00 Ft							
52	RAVELING	L	50.00 SqFt							
57	WEATHERING	L	3068.00 SqFt							
57	WEATHERING	M	162.00 SqFt							
Sample Number:	145	Type:	R	Area:	4000.00 SqFt		PCI:	81		
Sample Comments:										
48	L & T CR	L	47.00 Ft							
52	RAVELING	L	160.00 SqFt							
57	WEATHERING	L	3648.00 SqFt							
57	WEATHERING	M	192.00 SqFt							
Sample Number:	150	Type:	R	Area:	4000.00 SqFt		PCI:	86		
Sample Comments:										
48	L & T CR	L	42.00 Ft							
57	WEATHERING	L	3800.00 SqFt							
57	WEATHERING	M	200.00 SqFt							

Network:	BCT		Name:	BOCA RATON AIRPORT								
Branch:	TW P		Name:	TAXIWAY P		Use:	TAXIWAY		Area:	276,667 SqFt		
Section:	106 of 7		From:	-		To:	-		Last Const.:	1/1/2010		
Surface:	AAC		Family:	CA653-RL-TW-AAC-APC		Zone:			Category:	Rank: P		
Area:	29,080 SqFt		Length:	725 Ft		Width:	40 Ft					
Slabs:	Slab Length:		Ft		Slab Width:	Ft		Joint Length:	Ft			
Shoulder:	Street Type:		Grade:		0		Lanes:	0				
Section Comments:												
Work Date:	1/1/1996		Work Type:	BUILT				Code:	IMPORTED		Is Major M&R:	True
Work Date:	1/1/2010		Work Type:	Mill and Overlay				Code:	ML-OVL		Is Major M&R:	True
Last Insp. Date:	8/11/2022		TotalSamples:	7		Surveyed:	2					
Conditions:	PCI: 86											
Inspection Comments:												
Sample Number:	93		Type:	R		Area:	5080.00 SqFt		PCI:	87		
Sample Comments:												
48	L & T CR		L	23.00 Ft								
57	WEATHERING		L	4826.00 SqFt								
57	WEATHERING		M	254.00 SqFt								
Sample Number:	98		Type:	R		Area:	4000.00 SqFt		PCI:	85		
Sample Comments:												
48	L & T CR		L	41.00 Ft								
52	RAVELING		L	5.00 SqFt								
57	WEATHERING		L	3795.00 SqFt								
57	WEATHERING		M	200.00 SqFt								

Network:		BCT		Name:		BOCA RATON AIRPORT								
Branch:	TW P		Name:	TAXIWAY P		Use:	TAXIWAY	Area:	276,667 SqFt					
Section:	107		of	7		From:	-		To:	-		Last Const.:	1/1/2010	
Surface:	AAC		Family:	CA653-RL-TW-AAC-APC		Zone:			Category:			Rank:	P	
Area:	14,241 SqFt		Length:	400 Ft		Width:	40 Ft							
Slabs:			Slab Length:	Ft		Slab Width:	Ft		Joint Length:			Ft		
Shoulder:			Street Type:			Grade:	0		Lanes:	0				
Section Comments:														
Work Date:	1/1/1996		Work Type:	BUILT		Code:	IMPORTED		Is Major M&R:	True				
Work Date:	1/1/2010		Work Type:	Mill and Overlay		Code:	ML-OVL		Is Major M&R:	True				
Last Insp. Date:	8/11/2022		TotalSamples:	4		Surveyed:	1							
Conditions:	PCI: 86													
Inspection Comments:														
Sample Number:	154		Type:	R		Area:	3000.00 SqFt		PCI:	86				
Sample Comments:														
48	L & T CR		L	31.00 Ft										
57	WEATHERING		L	2850.00 SqFt										
57	WEATHERING		M	150.00 SqFt										

Network:	BCT			Name:	BOCA RATON AIRPORT						
Branch:	TW P		Name:	TAXIWAY P		Use:	TAXIWAY	Area:	276,667 SqFt		
Section:	108	of	7	From:	-		To:	-		Last Const.:	1/1/2010
Surface:	AAC	Family:	CA653-RL-TW-AAC-APC		Zone:			Category:	Rank: P		
Area:	10,940 SqFt		Length:	225 Ft		Width:	40 Ft				
Slabs:	Slab Length:		Ft		Slab Width:	Ft		Joint Length:	Ft		
Shoulder:	Street Type:				Grade:	0		Lanes:	0		
Section Comments:											
Work Date:	1/1/1996		Work Type: BUILT				Code:	IMPORTED		Is Major M&R:	True
Work Date:	1/1/2010		Work Type: Mill and Overlay				Code:	ML-OVL		Is Major M&R:	True
Last Insp. Date:	8/11/2022		TotalSamples:	3		Surveyed:	1				
Conditions:	PCI:	79									
Inspection Comments:											
Sample Number:	129	Type:	R	Area:	3280.00 SqFt		PCI:	79			
Sample Comments:											
48	L & T CR		L	131.00 Ft							
57	WEATHERING		L	2904.00 SqFt							
57	WEATHERING		M	164.00 SqFt							

Network:		BCT		Name:		BOCA RATON AIRPORT								
Branch:	TW P		Name:	TAXIWAY P		Use:	TAXIWAY	Area:	276,667 SqFt					
Section:	112		of	7		From:	-		To:	-		Last Const.:	1/1/2010	
Surface:	AAC		Family:	CA653-RL-TW-AAC-APC		Zone:			Category:			Rank:	P	
Area:	12,673 SqFt		Length:	145 Ft		Width:	80 Ft							
Slabs:			Slab Length:	Ft		Slab Width:	Ft		Joint Length:			Ft		
Shoulder:			Street Type:			Grade:	0		Lanes:	0				
Section Comments:														
Work Date:	1/1/1996		Work Type:	BUILT				Code:	IMPORTED		Is Major M&R:	True		
Work Date:	1/1/2010		Work Type:	Mill and Overlay				Code:	ML-OVL		Is Major M&R:	True		
Last Insp. Date:	8/11/2022		TotalSamples:	2		Surveyed:	1							
Conditions:	PCI: 82													
Inspection Comments:														
Sample Number:	100		Type:	R		Area:	6337.00 SqFt		PCI:	82				
Sample Comments:														
48	L & T CR		L	77.00 Ft										
48	L & T CR		M	7.00 Ft										
57	WEATHERING		L	6020.00 SqFt										
57	WEATHERING		M	317.00 SqFt										

Network:	BCT			Name:	BOCA RATON AIRPORT						
Branch:	TW P		Name:	TAXIWAY P		Use:	TAXIWAY	Area:	276,667 SqFt		
Section:	113 of 7		From:	-			To:	-		Last Const.:	1/1/2010
Surface:	AAC		Family:	CA653-RL-TW-AAC-APC		Zone:			Category:	Rank: P	
Area:	4,000 SqFt		Length:	50 Ft		Width:	40 Ft				
Slabs:	Slab Length:		Ft		Slab Width:		Ft		Joint Length:	Ft	
Shoulder:	Street Type:				Grade:	0		Lanes:	0		
Section Comments:											
Work Date:	1/1/1996		Work Type: BUILT				Code:	IMPORTED		Is Major M&R:	True
Work Date:	1/1/2010		Work Type: Mill and Overlay				Code:	ML-OVL		Is Major M&R:	True
Last Insp. Date: 8/11/2022											
TotalSamples:			1		Surveyed: 1						
Conditions:	PCI: 81										
Inspection Comments:											
Sample Number:	134		Type:	R		Area:	4000.00 SqFt		PCI:	81	
Sample Comments:											
48	L & T CR		L		24.00 Ft						
52	RAVELING		L		184.00 SqFt						
57	WEATHERING		L		3625.00 SqFt						
57	WEATHERING		M		191.00 SqFt						

Network:		BCT		Name:		BOCA RATON AIRPORT									
Branch:		TW P		Name:		TAXIWAY P		Use:		TAXIWAY		Area:		276,667 SqFt	
Section:		131		of 7		From:		-		To:		-		Last Const.: 1/1/2010	
Surface:		AAC		Family:		CA653-RL-TW-AAC-APC		Zone:		Category:		Rank:		P	
Area:		12,673 SqFt		Length:		145 Ft		Width:		12,673 Ft					
Slabs:		Slab Length:		Ft		Slab Width:		Ft		Joint Length:		Ft			
Shoulder:		Street Type:		Grade:		0		Lanes:		0					
Section Comments:															
Work Date:		1/1/1996		Work Type:		BUILT		Code:		IMPORTED		Is Major M&R:		True	
Work Date:		1/1/2010		Work Type:		Mill and Overlay		Code:		ML-OVL		Is Major M&R:		True	
Last Insp. Date:		8/11/2022		TotalSamples:		2		Surveyed:		1					
Conditions:		PCI: 80													
Inspection Comments:															
Sample Number:		100		Type:		R		Area:		6337.00 SqFt		PCI:		80	
Sample Comments:															
48	L & T CR			L		33.00 Ft									
50	PATCHING			L		152.00 SqFt									
50	PATCHING			M		4.00 SqFt									
57	WEATHERING			L		6181.00 SqFt									

Network:		BCT		Name:		BOCA RATON AIRPORT						
Branch:	TW P1		Name:	TAXIWAY P1		Use:	TAXIWAY	Area:	4,102 SqFt			
Section:	205		of	1	From:	-		To:	-		Last Const.:	1/1/2010
Surface:	AAC		Family:	CA653-RL-TW-AAC-APC		Zone:			Category:	Rank: P		
Area:	4,102 SqFt		Length:	56 Ft		Width:	59 Ft					
Slabs:			Slab Length:	Ft		Slab Width:	Ft		Joint Length:	Ft		
Shoulder:			Street Type:			Grade:	0		Lanes:	0		
Section Comments:												
Work Date:	1/1/1996		Work Type:	BUILT				Code:	IMPORTED		Is Major M&R:	True
Work Date:	1/1/2010		Work Type:	Mill and Overlay				Code:	ML-OVL		Is Major M&R:	True
Last Insp. Date:	8/11/2022		TotalSamples:	1		Surveyed:	1					
Conditions:	PCI: 88											
Inspection Comments:												
Sample Number:	104		Type:	R		Area:	4102.00 SqFt		PCI:	88		
Sample Comments:												
48	L & T CR		L	6.00 Ft								
57	WEATHERING		L	3897.00 SqFt								
57	WEATHERING		M	205.00 SqFt								

Network:		BCT		Name:		BOCA RATON AIRPORT											
Branch:		TW P10		Name:		TAXIWAY P10		Use:		TAXIWAY		Area:		8,665 SqFt			
Section:		240		of 1		From:		-		To:		-		Last Const.:		1/1/2010	
Surface:		AAC		Family:		CA653-RL-TW-AAC-APC		Zone:		Category:		Rank:		P			
Area:		8,665 SqFt		Length:		60 Ft		Width:		77 Ft							
Slabs:		Slab Length:		Ft		Slab Width:		Ft		Joint Length:		Ft					
Shoulder:		Street Type:		Grade:		0		Lanes:		0							
Section Comments:																	
Work Date:		1/1/2002		Work Type:		New Construction - AC		Code:		NC-AC		Is Major M&R:		True			
Work Date:		1/1/2010		Work Type:		Mill and Overlay		Code:		ML-OVL		Is Major M&R:		True			
Work Date:		1/1/2021		Work Type:		Patching - AC		Code:		PA-AC		Is Major M&R:		False			
Last Insp. Date:		8/11/2022		TotalSamples:		2		Surveyed:		1							
Conditions:		PCI: 89															
Inspection Comments:																	
Sample Number:		804		Type:		R		Area:		4596.00 SqFt		PCI:		89			
Sample Comments:																	
48		L & T CR		L		13.00 Ft											
57		WEATHERING		L		2351.00 SqFt											
57		WEATHERING		M		124.00 SqFt											

Network:	BCT			Name:	BOCA RATON AIRPORT									
Branch:	TW P11		Name:	TAXIWAY P11		Use:	TAXIWAY		Area:	8,665 SqFt				
Section:	250		of	1		From:	-		To:	-		Last Const.:	1/1/2010	
Surface:	AAC		Family:	CA653-RL-TW-AAC-APC		Zone:			Category:			Rank:	P	
Area:	8,665 SqFt		Length:	60 Ft		Width:	77 Ft							
Slabs:			Slab Length:	Ft		Slab Width:	Ft		Joint Length:	Ft				
Shoulder:			Street Type:			Grade:	0		Lanes:	0				
Section Comments:														
Work Date:	1/1/2002		Work Type:	New Construction - AC				Code:	NC-AC		Is Major M&R:	True		
Work Date:	1/1/2010		Work Type:	Mill and Overlay				Code:	ML-OVL		Is Major M&R:	True		
Work Date:	1/1/2021		Work Type:	Patching - AC				Code:	PA-AC		Is Major M&R:	False		
Last Insp. Date:	8/11/2022		TotalSamples:	2		Surveyed:	1							
Conditions:	PCI: 93													
Inspection Comments:														
Sample Number:	805		Type:	R		Area:	4596.00 SqFt		PCI:	93				
Sample Comments:														
57	WEATHERING		L	2351.00 SqFt										
57	WEATHERING		M	124.00 SqFt										

Network:		BCT		Name:		BOCA RATON AIRPORT								
Branch:	TW P2		Name:	TAXIWAY P2		Use:	TAXIWAY	Area:	2,572 SqFt					
Section:	210		of	1		From:	-		To:	-		Last Const.:	1/1/2010	
Surface:	AAC		Family:	CA653-RL-TW-AAC-APC		Zone:			Category:			Rank:	P	
Area:	2,572 SqFt		Length:	59 Ft		Width:	30 Ft							
Slabs:			Slab Length:	Ft		Slab Width:	Ft		Joint Length:			Ft		
Shoulder:			Street Type:			Grade:	0		Lanes:	0				
Section Comments:														
Work Date:	1/1/1996		Work Type:	New Construction - AC				Code:	NC-AC		Is Major M&R:	True		
Work Date:	1/1/2010		Work Type:	Mill and Overlay				Code:	ML-OVL		Is Major M&R:	True		
Last Insp. Date:	8/11/2022		TotalSamples:	1		Surveyed:	1							
Conditions:	PCI: 72													
Inspection Comments:														
Sample Number:	700		Type:	R		Area:	2572.00 SqFt		PCI:	72				
Sample Comments:														
48	L & T CR		L	54.00		Ft								
52	RAVELING		M	120.00		SqFt								
57	WEATHERING		L	2335.00		SqFt								
57	WEATHERING		M	117.00		SqFt								

Network:	BCT			Name:	BOCA RATON AIRPORT				
Branch:	TW P3		Name:	TAXIWAY P3		Use:	TAXIWAY	Area:	1,488 SqFt
Section:	215	of 1	From:	-			To:	-	Last Const.: 1/1/2010
Surface:	AAC	Family:	CA653-RL-TW-AAC-APC		Zone:	Category:		Rank:	P
Area:	1,488 SqFt		Length:	61 Ft		Width:	20 Ft		
Slabs:	Slab Length:		Ft		Slab Width:	Ft		Joint Length:	Ft
Shoulder:	Street Type:		Grade:		0		Lanes:	0	
Section Comments:									
Work Date:	1/1/1996		Work Type: New Construction - AC				Code:	NC-AC	Is Major M&R: True
Work Date:	1/1/2010		Work Type: Mill and Overlay				Code:	ML-OVL	Is Major M&R: True
Last Insp. Date:	8/11/2022		TotalSamples:	1		Surveyed: 1			
Conditions:	PCI: 74								
Inspection Comments:									
Sample Number:	800	Type:	R	Area:	1489.00 SqFt		PCI:	74	
Sample Comments:									
48	L & T CR		L	75.00 Ft					
52	RAVELING		L	15.00 SqFt					
57	WEATHERING		L	1400.00 SqFt					
57	WEATHERING		M	74.00 SqFt					

Network:		BCT		Name:		BOCA RATON AIRPORT								
Branch:	TW P4		Name:	TAXIWAY P4		Use:	TAXIWAY	Area:	8,391 SqFt					
Section:	225		of	1		From:	-		To:	-		Last Const.:	7/1/2022	
Surface:	AAC		Family:	CA653-RL-TW-AAC-APC		Zone:			Category:			Rank:	P	
Area:	8,391 SqFt		Length:	55 Ft		Width:	80 Ft							
Slabs:			Slab Length:	Ft		Slab Width:	Ft		Joint Length:			Ft		
Shoulder:			Street Type:			Grade:	0		Lanes:	0				
Section Comments:														
Work Date:	1/1/1984		Work Type:				BUILT		Code:	IMPORTED		Is Major M&R:	True	
Work Date:	1/1/2010		Work Type:				Mill and Overlay		Code:	ML-OVL		Is Major M&R:	True	
Work Date:	7/1/2022		Work Type:				Mill and Overlay		Code:	ML-OVL		Is Major M&R:	True	
Last Insp. Date:	5/16/2019		TotalSamples:	1		Surveyed:	1							
Conditions:	PCI: 85		NOTE: *** Pre-Construction PCI ***											
Inspection Comments:														
Sample Number:	803		Type:	R		Area:	3670.00 SqFt		PCI:	85				
Sample Comments:														
48	L & T CR		L	55.00 Ft										
52	RAVELING		L	73.00 SqFt										
57	WEATHERING		L	3597.00 SqFt										

Network:		BCT		Name:		BOCA RATON AIRPORT						
Branch:	TW P5		Name:	TAXIWAY P5		Use:	TAXIWAY	Area:	8,400 SqFt			
Section:	230		of	1	From:	-		To:	-		Last Const.:	1/1/2010
Surface:	AAC		Family:	CA653-RL-TW-AAC-APC		Zone:			Category:	Rank: P		
Area:	8,400 SqFt		Length:	55 Ft		Width:	80 Ft					
Slabs:			Slab Length:	Ft		Slab Width:	Ft		Joint Length:	Ft		
Shoulder:			Street Type:			Grade:	0		Lanes:	0		
Section Comments:												
Work Date:	1/1/1984		Work Type: BUILT				Code:	IMPORTED		Is Major M&R:	True	
Work Date:	1/1/2010		Work Type: Mill and Overlay				Code:	ML-OVL		Is Major M&R:	True	
Work Date:	1/1/2021		Work Type: Patching - AC				Code:	PA-AC		Is Major M&R:	False	
Last Insp. Date:	8/11/2022		TotalSamples:	2		Surveyed:	1					
Conditions:	PCI: 87											
Inspection Comments:												
Sample Number:	802		Type:	R		Area:	4202.00 SqFt		PCI:	87		
Sample Comments:												
48	L & T CR		L	59.00 Ft								
57	WEATHERING		L	2280.00 SqFt								
57	WEATHERING		M	120.00 SqFt								

Network:	BCT			Name:	BOCA RATON AIRPORT				
Branch:	TW P6		Name:	TAXIWAY P6		Use:	TAXIWAY	Area:	8,315 SqFt
Section:	220	of 1	From:	-			To:	-	Last Const.: 1/1/2010
Surface:	AAC	Family:	CA653-RL-TW-AAC-APC	Zone:				Category:	Rank: P
Area:	8,315 SqFt	Length:	55 Ft	Width:	77 Ft				
Slabs:	Slab Length:		Ft	Slab Width:		Ft	Joint Length:		Ft
Shoulder:	Street Type:		Grade: 0			Lanes: 0			
Section Comments:									
Work Date:	1/1/1984	Work Type: BUILT				Code:	IMPORTED	Is Major M&R:	True
Work Date:	1/1/2010	Work Type: Mill and Overlay				Code:	ML-OVL	Is Major M&R:	True
Work Date:	1/1/2021	Work Type: Patching - AC				Code:	PA-AC	Is Major M&R:	False
Last Insp. Date:	8/11/2022	TotalSamples:	2	Surveyed:		1			
Conditions:	PCI: 85								
Inspection Comments:									
Sample Number:	801	Type:	R	Area:	4202.00 SqFt	PCI:	85		
Sample Comments:									
48	L & T CR	L	36.00 Ft						
52	RAVELING	L	80.00 SqFt						
57	WEATHERING	L	1630.00 SqFt						
57	WEATHERING	M	90.00 SqFt						

Network:	BCT			Name:	BOCA RATON AIRPORT						
Branch:	TW P7		Name:	TAXIWAY P7		Use:	TAXIWAY	Area:	4,994 SqFt		
Section:	222	of	1	From:	-	To:	-	Last Const.:	2/24/2018		
Surface:	AC	Family:	CA653-RL-TW-AC		Zone:		Category:		Rank:	P	
Area:	4,994 SqFt		Length:	55 Ft		Width:	51 Ft				
Slabs:		Slab Length:	Ft		Slab Width:	Ft		Joint Length:	Ft		
Shoulder:		Street Type:			Grade:	0		Lanes:	0		
Section Comments:											
Work Date:	2/24/2018		Work Type:	New Construction - AC			Code:	NC-AC		Is Major M&R:	True
Last Insp. Date:	8/11/2022		TotalSamples:	1		Surveyed:	1				
Conditions:	PCI:	89									
Inspection Comments:											
Sample Number:	101	Type:	R	Area:	4998.00 SqFt		PCI:	89			
Sample Comments:											
48	L & T CR		L	77.00 Ft							
57	WEATHERING		L	4998.00 SqFt							

Network:		BCT		Name:		BOCA RATON AIRPORT								
Branch:	TW P8		Name:	TAXIWAY P8		Use:	TAXIWAY	Area:	5,090 SqFt					
Section:	221		of	1		From:	-		To:	-		Last Const.:	2/24/2018	
Surface:	AC		Family:	CA653-RL-TW-AC		Zone:			Category:			Rank:	P	
Area:	5,090 SqFt		Length:	55 Ft		Width:	50 Ft							
Slabs:			Slab Length:	Ft		Slab Width:	Ft		Joint Length:			Ft		
Shoulder:			Street Type:			Grade:	0		Lanes:	0				
Section Comments:														
Work Date:	1/1/1996		Work Type:				BUILT		Code:	IMPORTED		Is Major M&R:	True	
Work Date:	1/1/2010		Work Type:				Mill and Overlay		Code:	ML-OVL		Is Major M&R:	True	
Work Date:	2/24/2018		Work Type:				Complete Reconstruction - AC		Code:	CR-AC		Is Major M&R:	True	
Last Insp. Date:	8/11/2022		TotalSamples:	1		Surveyed:	1							
Conditions:	PCI:		87											
Inspection Comments:														
Sample Number:	100		Type:	R		Area:	5090.00 SqFt		PCI:	87				
Sample Comments:														
48	L & T CR		L	109.00 Ft										
57	WEATHERING		L	5090.00 SqFt										

Network:		BCT		Name:		BOCA RATON AIRPORT						
Branch:	TW P9		Name:	TAXIWAY P9		Use:	TAXIWAY	Area:	3,165 SqFt			
Section:	260		of	1	From:	-		To:	-		Last Const.:	1/1/2010
Surface:	AAC		Family:	CA653-RL-TW-AAC-APC		Zone:			Category:	Rank: P		
Area:	3,165 SqFt		Length:	60 Ft		Width:	40 Ft					
Slabs:			Slab Length:	Ft		Slab Width:	Ft		Joint Length:	Ft		
Shoulder:			Street Type:			Grade:	0		Lanes:	0		
Section Comments:												
Work Date:	1/1/1998		Work Type: New Construction - Initial					Code:	NU-IN		Is Major M&R:	True
Work Date:	1/1/2010		Work Type: Mill and Overlay					Code:	ML-OVL		Is Major M&R:	True
Last Insp. Date:	8/11/2022		TotalSamples:	1		Surveyed:		1				
Conditions:	PCI: 81											
Inspection Comments:												
Sample Number:	806		Type:	R		Area:	3165.00 SqFt		PCI:	81		
Sample Comments:												
48	L & T CR		L	53.00 Ft								
52	RAVELING		M	5.00 SqFt								
57	WEATHERING		L	3002.00 SqFt								
57	WEATHERING		M	158.00 SqFt								



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