FLORIDA DEPARTMENT OF TRANSPORTATION | **AVIATION OFFICE**

2022

Statewide Airfield Pavement Management Program

Airport Pavement Evaluation Report

PIE - St. Pete-Clearwater International Airport | *District 7*

FDOT AVIATION

Florida Department of Transportation

Statewide Airfield Pavement Management Program

Airport Pavement Evaluation Report

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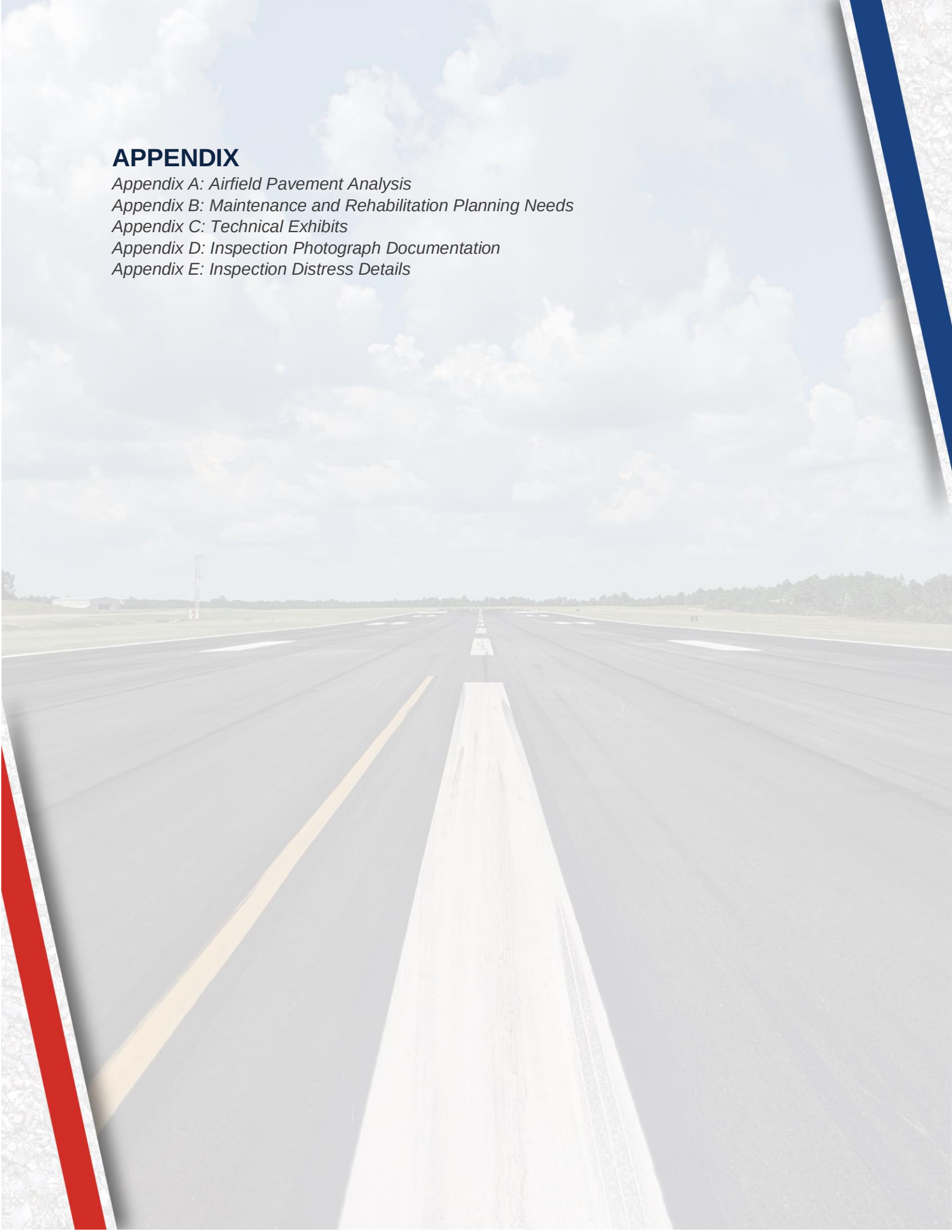

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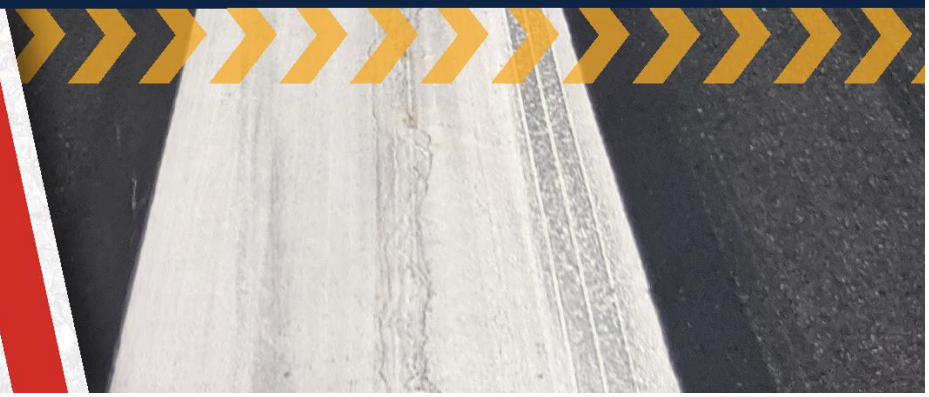


Executive Summary

Program Background

The FDOT Aviation Office (AO) has a mission to provide a safe and secure air transportation system that ensures the mobility of people and goods, enhances economic prosperity, and preserves the quality of our environment and communities. As part of ongoing efforts in fulfilling this mission, the Aviation Office is executing a System Update to the Statewide Airfield Pavement Management Program (SAPMP). The scope of the SAPMP encompasses 95 public-use airport facilities distributed throughout the seven (7) participating FDOT Districts. St. Pete-Clearwater International Airport's System Update results are presented in this report and can be utilized by FDOT and the Federal Aviation Administration (FAA) to identify, prioritize, and schedule pavement maintenance, repair, and major rehabilitation projects.

Pavement condition was assessed utilizing the pavement condition index (PCI) methodology as defined in FAA Advisory Circular 150/5380-7B "Airport Pavement Management Program (PMP)" using the procedures documented in ASTM D5340-20 **"**Standard Test Method for Airport Pavement Condition Index Surveys".

The PCI methodology provides a means for systematically assessing pavement condition and provides an indication of the degree of maintenance, repair, rehabilitation, or reconstruction efforts required to sustain functional pavement conditions. Pavement deterioration, in accordance with ASTM D5340-20, is characterized in terms of distinct distress types, distress severity levels, and quantity of distress. This information is utilized to calculate a PCI value ranging from 0 to 100, which provides an indication of the overall condition of the pavement, with "100" indicating a pavement in new condition and "0" indicating a failed pavement section. This is graphically depicted in **Figure E.1**.

Figure E.1: PCI Rating

Color	Range	Condition Rating
	86-100	Good
	71-85	Satisfactory
	56-70	Fair
	41-55	Poor
	26-40	Very Poor
	11-25	Serious
	0-10	Failed

Current Pavement Conditions

In April 2022, approximately 5.7 million square feet of pavement was assessed as part of the airside pavement network PCI survey at St. Pete-Clearwater International Airport (PIE). In general, airfield pavements at PIE are in Satisfactory condition with an area-weighted PCI of 80. The area-weighted average PCI values of the runways, taxiways, taxilanes, and aprons are 90, 73, 38, and 74, respectively. **Figure E.2** and **Table E.1** summarize the current PCI values for PIE.

Figure E.2: Current Condition Summary – Branch-Level

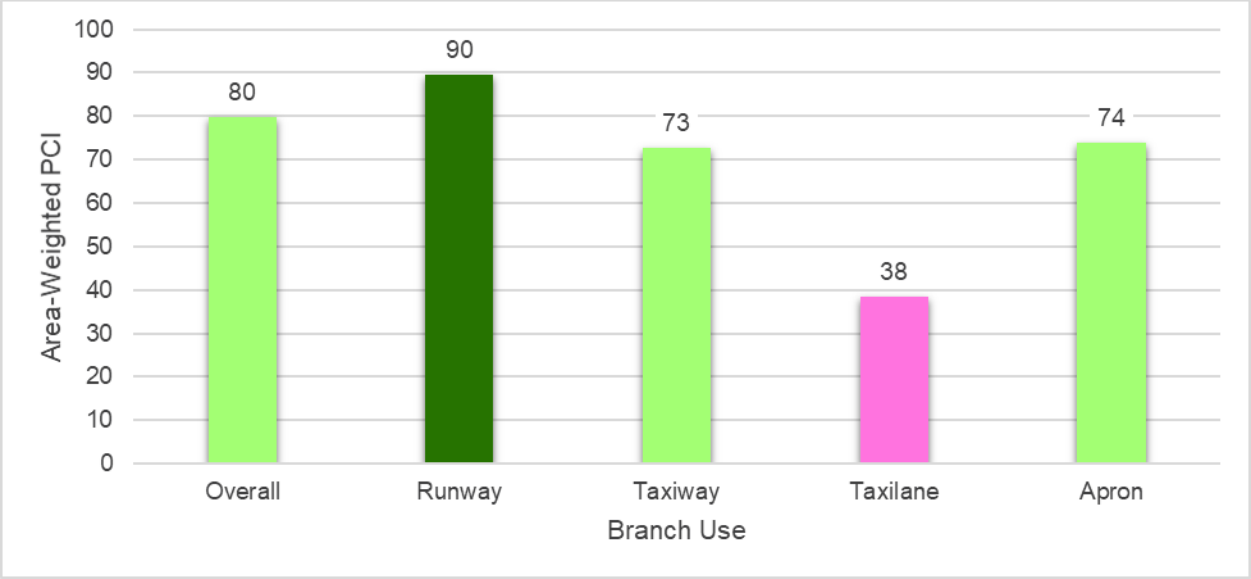


Table E.1: Pavement Condition Index Summary (Current PCI Survey) – Section Level

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	PCI	Condition Rating
PIE	RW 4-22	Runway	6205	474,873	73	Satisfactory
PIE	RW 4-22	Runway	6210	237,436	79	Satisfactory
PIE	RW 4-22	Runway	6215	50,072	63	Fair
PIE	RW 4-22	Runway	6220	25,036	70	Fair
PIE	RW 4-22	Runway	6225	45,300	54	Poor
PIE	RW 4-22	Runway	6230	22,650	24	Serious
PIE	RW 4-22	Runway	6235	9,700	100	Good
PIE	RW 4-22	Runway	6240	4,850	100	Good
PIE	RW 18-36	Runway	6115	135,960	100	Good
PIE	RW 18-36	Runway	6120	176,940	100	Good
PIE	RW 18-36	Runway	6155	99,000	100	Good
PIE	RW 18-36	Runway	6156	18,000	100	Good
PIE	RW 18-36	Runway	6157	12,000	100	Good
PIE	RW 18-36	Runway	6160	148,500	100	Good
PIE	RW 18-36	Runway	6165	40,500	100	Good
PIE	RW 18-36	Runway	6170	60,750	100	Good
PIE	RW 18-36	Runway	6175	170,280	100	Good

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	PCI	Condition Rating
PIE	RW 18-36	Runway	6180	255,420	100	Good
PIE	RW 18-36	Runway	6185	126,000	100	Good
PIE	RW 18-36	Runway	6190	189,000	100	Good
PIE	RW 18-36	Runway	6196	27,000	100	Good
PIE	FBO CONN	Taxiway	107	3,297	45	Poor
PIE	FBO CONN	Taxiway	108	3,361	32	Very Poor
PIE	FBO CONN	Taxiway	112	4,221	42	Poor
PIE	FBO CONN	Taxiway	114	2,361	59	Fair
PIE	FBO CONN	Taxiway	117	6,019	77	Satisfactory
PIE	FBO CONN	Taxiway	119	3,041	29	Very Poor
PIE	FBO CONN	Taxiway	125	4,598	58	Fair
PIE	FBO CONN	Taxiway	127	12,891	82	Satisfactory
PIE	TW A	Taxiway	115	224,709	65	Fair
PIE	TW A	Taxiway	130	358,395	71	Satisfactory
PIE	TW A	Taxiway	132	23,007	88	Good
PIE	TW A	Taxiway	155	6,259	89	Good
PIE	TW A	Taxiway	158	16,692	63	Fair
PIE	TW A	Taxiway	160	99,856	82	Satisfactory
PIE	TW A1	Taxiway	135	40,056	64	Fair
PIE	TW A1	Taxiway	140	14,541	67	Fair
PIE	TW A1	Taxiway	145	2,945	100	Good
PIE	TW A2	Taxiway	165	60,458	74	Satisfactory
PIE	TW A3	Taxiway	168	60,311	77	Satisfactory
PIE	TW A4	Taxiway	170	58,588	85	Satisfactory
PIE	TW A5	Taxiway	175	56,987	82	Satisfactory
PIE	TW A6	Taxiway	180	58,658	86	Good
PIE	TW A7	Taxiway	162	52,089	83	Satisfactory
PIE	TW B	Taxiway	205	6,200	91	Good
PIE	TW B	Taxiway	207	7,750	90	Good
PIE	TW B	Taxiway	210	6,353	88	Good
PIE	TW B	Taxiway	212	18,000	80	Satisfactory
PIE	TW B	Taxiway	215	15,387	85	Satisfactory
PIE	TW B	Taxiway	220	40,670	16	Serious
PIE	TW B	Taxiway	225	17,624	89	Good
PIE	TW F	Taxiway	610	43,041	76	Satisfactory
PIE	TW F	Taxiway	615	4,165	100	Good
PIE	TW G	Taxiway	1315	19,536	69	Fair
PIE	TW G	Taxiway	1320	15,822	62	Fair
PIE	TW G	Taxiway	1325	199,036	65	Fair
PIE	TW G	Taxiway	1340	14,004	82	Satisfactory
PIE	TW G1	Taxiway	1330	13,135	65	Fair
PIE	TW G1	Taxiway	1335	12,530	63	Fair
PIE	TW G2	Taxiway	1005	15,843	89	Good
PIE	TW G2	Taxiway	1010	8,964	89	Good
PIE	TW G3	Taxiway	605	10,930	24	Serious
PIE	TW G3	Taxiway	607	8,732	88	Good
PIE	TW H	Taxiway	810	59,729	5	Failed

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	PCI	Condition Rating
PIE	TW H	Taxiway	815	57,784	87	Good
PIE	TW H	Taxiway	820	4,760	88	Good
PIE	TW J	Taxiway	905	8,851	100	Good
PIE	TW L	Taxiway	1205	22,175	77	Satisfactory
PIE	TW L	Taxiway	1215	13,483	76	Satisfactory
PIE	TW L	Taxiway	1245	52,150	79	Satisfactory
PIE	TW P	Taxiway	1250	27,739	85	Satisfactory
PIE	TW P	Taxiway	1255	52,339	91	Good
PIE	TW Q	Taxiway	1705	4,449	94	Good
PIE	TW Q	Taxiway	1710	3,632	94	Good
PIE	TW T	Taxiway	2010	12,963	87	Good
PIE	TW T	Taxiway	2020	14,337	86	Good
PIE	TW T	Taxiway	2045	16,549	74	Satisfactory
PIE	TW T	Taxiway	2050	149,440	90	Good
PIE	TL T-HANG	Taxilane	1120	1,346	39	Very Poor
PIE	TL T-HANG	Taxilane	1125	1,472	38	Very Poor
PIE	AP MAIN	Apron	4103	122,390	100	Good
PIE	AP MAIN	Apron	4105	40,910	30	Very Poor
PIE	AP MAIN	Apron	4107	220,315	97	Good
PIE	AP MAIN	Apron	4110	56,000	74	Satisfactory
PIE	AP MAIN	Apron	4123	43,794	83	Satisfactory
PIE	AP MAIN	Apron	4150	14,083	79	Satisfactory
PIE	AP MAIN	Apron	4155	33,689	59	Fair
PIE	AP MAIN	Apron	4157	92,541	67	Fair
PIE	AP MAIN	Apron	4160	59,640	96	Good
PIE	AP MAIN	Apron	4165	66,649	95	Good
PIE	AP MAIN	Apron	4170	18,816	87	Good
PIE	AP MAIN	Apron	4175	14,910	5	Failed
PIE	AP MAIN	Apron	4176	3,573	28	Very Poor
PIE	AP MAIN	Apron	4177	20,899	80	Satisfactory
PIE	AP MAIN	Apron	4178	59,522	62	Fair
PIE	AP MAIN	Apron	4179	77,111	66	Fair
PIE	AP MAIN	Apron	4180	126,695	45	Poor
PIE	AP MAIN	Apron	4183	39,947	68	Fair
PIE	AP MAIN	Apron	4185	12,820	51	Poor
PIE	AP MAIN	Apron	4190	18,650	14	Serious
PIE	AP MAIN	Apron	4195	11,250	9	Failed
PIE	AP MAIN	Apron	4198	18,579	20	Serious
PIE	AP MAIN	Apron	4199	25,200	100	Good

Forecasted Pavement Conditions

Table E.2 provides section-level details for PCI forecasts. Pavement condition forecasts should be used for planning purposes only, as the actual condition of sections is subject to sensitivities in changes of traffic and maintenance frequency.

The estimation of forecasted PCI values gives no assurance of future pavement conditions as PCI values represent an engineering estimation to be used as a planning tool. Forecasted PCI data should not be the sole metric for determining the year in which a project should be planned. Design-level planning should be undertaken by the responsible engineer prior to the development of airfield design plans.

Table E.2: Forecasted PCI Values 2023-2032 – Section-Level

Network ID	Branch ID	Section ID	Current PCI	2023	2024	2025	2026	2027	2028	2029	2030	2031	2032
PIE	RW 4-22	6205	73	71	69	67	65	63	61	59	57	55	53
PIE	RW 4-22	6210	79	77	75	73	71	69	67	65	63	61	59
PIE	RW 4-22	6215	63	61	59	57	55	53	51	49	47	45	43
PIE	RW 4-22	6220	70	68	66	64	62	60	58	56	54	52	50
PIE	RW 4-22	6225	54	52	51	49	48	46	45	43	42	40	39
PIE	RW 4-22	6230	24	22	21	19	18	16	15	13	12	10	9
PIE	RW 4-22	6235	100	95	94	92	91	89	88	86	85	83	82
PIE	RW 4-22	6240	100	95	94	92	91	89	88	86	85	83	82
PIE	RW 18-36	6115	100	96	95	93	92	90	89	87	86	84	83
PIE	RW 18-36	6120	100	95	93	91	89	87	85	83	81	80	78
PIE	RW 18-36	6155	100	95	93	91	89	87	85	83	81	80	78
PIE	RW 18-36	6156	100	96	95	93	92	90	89	87	86	84	83
PIE	RW 18-36	6157	100	96	95	93	92	90	89	87	86	84	83
PIE	RW 18-36	6160	100	95	93	91	89	87	85	83	81	80	78
PIE	RW 18-36	6165	100	96	95	93	92	90	89	87	86	84	83
PIE	RW 18-36	6170	100	95	93	91	89	87	85	83	81	80	78
PIE	RW 18-36	6175	100	95	93	91	89	87	85	83	81	80	78
PIE	RW 18-36	6180	100	95	93	91	89	87	85	83	81	80	78
PIE	RW 18-36	6185	100	95	93	91	89	87	85	83	81	80	78
PIE	RW 18-36	6190	100	95	93	91	89	87	85	83	81	80	78
PIE	RW 18-36	6196	100	95	93	91	89	87	85	83	81	80	78
PIE	FBO CONN	107	45	44	43	41	40	38	36	34	32	30	27
PIE	FBO CONN	108	32	29	27	25	23	21	19	17	15	13	11
PIE	FBO CONN	112	42	40	39	37	35	33	30	28	25	21	18
PIE	FBO CONN	114	59	58	57	56	55	55	54	53	52	51	50
PIE	FBO CONN	117	77	75	73	71	69	67	66	64	63	62	60
PIE	FBO CONN	119	29	26	24	22	20	18	16	14	12	10	8
PIE	FBO CONN	125	58	57	56	55	54	54	53	52	52	51	50
PIE	FBO CONN	127	82	79	77	75	73	71	70	68	66	65	63
PIE	TW A	115	65	63	62	61	59	58	57	56	56	55	54
PIE	TW A	130	71	69	67	66	64	63	61	60	59	58	57
PIE	TW A	132	88	85	83	81	79	76	74	73	71	69	67

Network ID	Branch ID	Section ID	Current PCI	2023	2024	2025	2026	2027	2028	2029	2030	2031	2032
PIE	TW A	155	89	86	84	82	79	77	75	73	71	70	68
PIE	TW A	158	63	61	60	59	58	57	56	55	54	54	53
PIE	TW A	160	82	79	77	75	73	71	70	68	66	65	63
PIE	TW A1	135	64	62	61	60	59	58	57	56	55	54	53
PIE	TW A1	140	67	65	64	62	61	60	59	58	57	56	55
PIE	TW A1	145	100	94	92	90	88	86	84	83	81	80	78
PIE	TW A2	165	74	72	71	70	69	68	67	66	65	64	63
PIE	TW A3	168	77	75	74	73	72	70	69	68	67	66	65
PIE	TW A4	170	85	83	81	80	78	77	75	74	73	72	71
PIE	TW A5	175	82	80	79	77	76	74	73	72	71	70	69
PIE	TW A6	180	86	84	82	81	79	78	76	75	74	72	71
PIE	TW A7	162	83	80	78	76	74	72	70	69	67	65	64
PIE	TW B	205	91	88	86	83	81	79	77	75	73	71	69
PIE	TW B	207	90	87	85	82	80	78	76	74	72	70	69
PIE	TW B	210	88	85	83	81	79	76	74	73	71	69	67
PIE	TW B	212	80	77	75	73	72	70	68	66	65	63	62
PIE	TW B	215	85	83	81	80	78	77	75	74	73	72	71
PIE	TW B	220	16	13	11	9	7	5	3	1	0	0	0
PIE	TW B	225	89	87	85	83	82	80	78	77	76	74	73
PIE	TW F	610	76	74	73	72	71	70	69	68	67	66	65
PIE	TW F	615	100	94	91	89	86	84	82	80	78	76	74
PIE	TW G	1315	69	68	67	66	65	64	63	62	61	61	60
PIE	TW G	1320	62	60	59	58	57	56	55	55	54	53	52
PIE	TW G	1325	65	63	62	61	59	58	57	56	56	55	54
PIE	TW G	1340	82	79	77	75	73	71	70	68	66	65	63
PIE	TW G1	1330	65	63	62	61	59	58	57	56	56	55	54
PIE	TW G1	1335	63	61	60	59	58	57	56	55	54	54	53
PIE	TW G2	1005	89	86	84	82	79	77	75	73	71	70	68
PIE	TW G2	1010	89	86	84	82	79	77	75	73	71	70	68
PIE	TW G3	605	24	20	16	12	7	2	0	0	0	0	0
PIE	TW G3	607	88	85	83	81	79	76	74	73	71	69	67
PIE	TW H	810	5	0	0	0	0	0	0	0	0	0	0
PIE	TW H	815	87	85	83	81	80	78	77	76	74	73	72
PIE	TW H	820	88	86	84	82	81	79	78	76	75	74	72
PIE	TW J	905	100	93	91	89	87	85	83	82	80	79	77
PIE	TW L	1205	77	75	74	73	72	70	69	68	67	66	65
PIE	TW L	1215	76	74	73	72	71	70	69	68	67	66	65
PIE	TW L	1245	79	77	76	75	73	72	71	70	69	68	67
PIE	TW P	1250	85	83	81	80	78	77	75	74	73	72	71
PIE	TW P	1255	91	89	87	85	83	82	80	78	77	76	74
PIE	TW Q	1705	94	91	89	86	84	82	80	77	75	73	72
PIE	TW Q	1710	94	91	89	88	86	84	82	81	79	78	76
PIE	TW T	2010	87	84	82	80	78	76	74	72	70	68	67
PIE	TW T	2020	86	83	81	79	77	75	73	71	69	68	66
PIE	TW T	2045	74	72	70	68	67	65	64	62	61	60	59
PIE	TW T	2050	90	87	85	82	80	78	76	74	72	70	69

Network ID	Branch ID	Section ID	Current PCI	2023	2024	2025	2026	2027	2028	2029	2030	2031	2032
PIE	TL T-HANG	1120	39	37	35	33	31	29	27	25	23	21	19
PIE	TL T-HANG	1125	38	36	34	32	30	28	26	24	22	20	18
PIE	AP MAIN	4103	100	99	98	97	96	94	93	92	91	90	89
PIE	AP MAIN	4105	30	27	25	22	20	17	14	11	9	6	3
PIE	AP MAIN	4107	97	96	94	93	92	91	90	89	89	88	87
PIE	AP MAIN	4110	74	72	70	68	66	65	63	62	60	59	57
PIE	AP MAIN	4123	83	80	78	76	74	72	70	68	66	65	63
PIE	AP MAIN	4150	79	76	74	72	70	69	67	65	64	62	61
PIE	AP MAIN	4155	59	57	56	54	53	52	50	49	47	46	44
PIE	AP MAIN	4157	67	65	63	62	60	59	57	56	55	53	52
PIE	AP MAIN	4160	96	95	93	92	91	90	90	89	88	87	86
PIE	AP MAIN	4165	95	94	93	92	91	90	89	88	87	86	86
PIE	AP MAIN	4170	87	84	81	79	77	75	73	71	69	67	66
PIE	AP MAIN	4175	5	1	0	0	0	0	0	0	0	0	0
PIE	AP MAIN	4176	28	26	24	23	21	19	18	16	14	13	11
PIE	AP MAIN	4177	80	77	75	73	71	69	68	66	64	63	61
PIE	AP MAIN	4178	62	60	59	57	56	54	53	52	50	49	47
PIE	AP MAIN	4179	66	64	62	61	59	58	57	55	54	52	51
PIE	AP MAIN	4180	45	43	41	40	38	36	34	32	30	28	26
PIE	AP MAIN	4183	68	66	64	63	61	60	58	57	55	54	53
PIE	AP MAIN	4185	51	49	48	46	45	43	42	40	38	36	34
PIE	AP MAIN	4190	14	10	7	3	0	0	0	0	0	0	0
PIE	AP MAIN	4195	9	5	1	0	0	0	0	0	0	0	0
PIE	AP MAIN	4198	20	16	13	10	7	3	0	0	0	0	0
PIE	AP MAIN	4199	100	99	98	97	96	94	93	92	91	90	89

Major Rehabilitation Planning 2023-2032

Localized maintenance and repair policies identified within this report are categorized as preventive or stopgap based on FDOT SAPMP and FAA maintenance policies and recommendations. Major rehabilitation is identified within the FDOT SAPMP as a major construction activity that results in a reset of a pavement section's PCI to a value of 100. Major rehabilitation activities can include mill and Asphalt Concrete (AC) overlay, Portland cement concrete (PCC) pavement repair and slab replacement, and full-depth reconstruction. It is recommended that the Airport use this report as a planning tool for future project development and prioritization. Localized maintenance, repair, and major rehabilitation recommendations should be considered as planning-level only. Final localized maintenance, repair, and major rehabilitation recommendations are subject to change based on Airport prioritization and further design-level evaluations.

Due to FAA Order 5100.38D Change 1 Airport Improvement Program (AIP) Handbook (February 26, 2019), a substantial update to the FDOT SAPMP policy on identifying major rehabilitation work has been incorporated in this System Update. In previous System Updates, major rehabilitation had been identified for pavement sections below a PCI Value of 65; however, based on the thresholds identified by the FAA in the AIP Handbook, major rehabilitation will now be identified for pavement sections below a PCI value of 70.

The results of the maintenance, repair, and major rehabilitation analysis identified approximately $62.19M in major rehabilitation needs for the 10-year forecast period. Year 1 major needs are $33.60M and localized maintenance needs for Year 1 are $0.35M.

Table E.3: Major Rehabilitation Planning 2023-2032

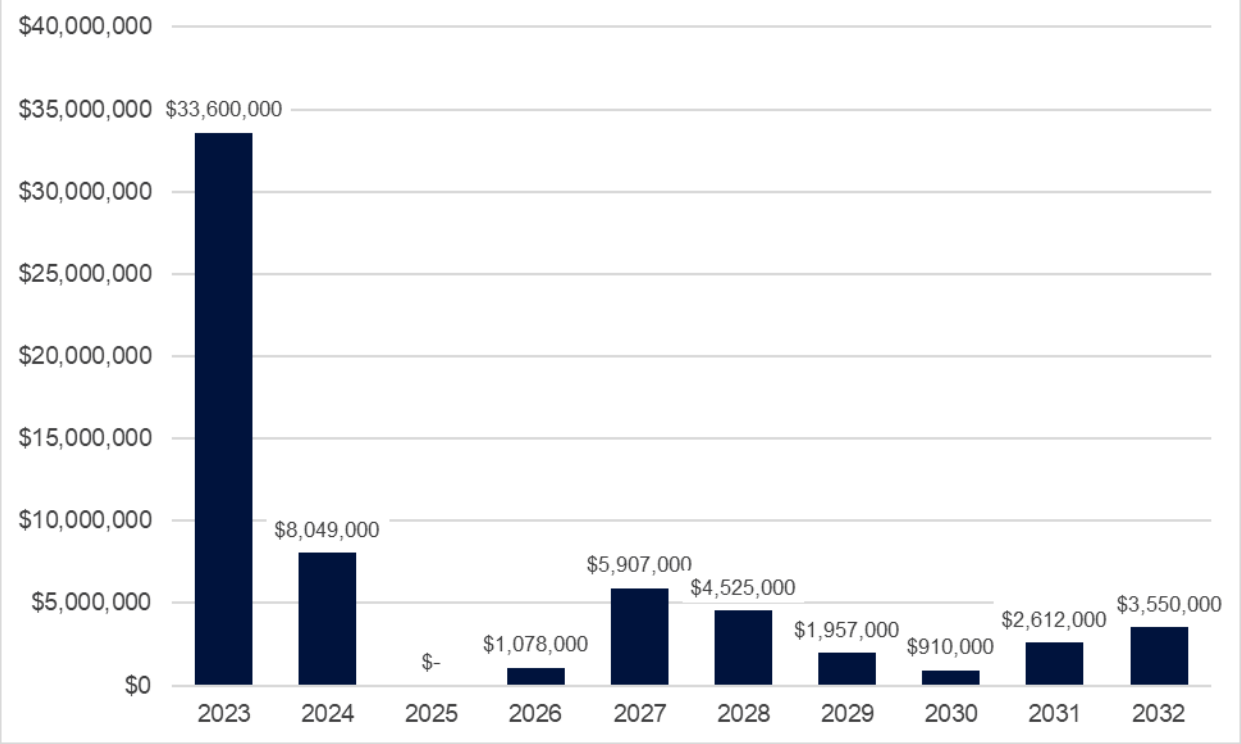
Program Year	Network ID	Branch ID	Section ID	Surface	Area (SF)	PCI Before	Rehabilitation Type	Planning Cost Estimate
2023	**PIE**	RW 4-22	6215	AAC	50,072	61	AC Rehabilitation	$ 701,000
2023	**PIE**	RW 4-22	6220	AAC	25,036	68	AC Rehabilitation	$ 351,000
2023	**PIE**	RW 4-22	6225	AC	45,300	52	AC Reconstruction	$ 1,382,000
2023	**PIE**	RW 4-22	6230	AC	22,650	22	AC Reconstruction	$ 691,000
2023	**PIE**	FBO CONN	107	AAC	3,297	44	AC Reconstruction	$ 101,000
2023	**PIE**	FBO CONN	108	AC	3,361	29	AC Reconstruction	$ 103,000
2023	**PIE**	FBO CONN	112	AAC	4,221	40	AC Reconstruction	$ 129,000
2023	**PIE**	FBO CONN	114	AC	2,361	58	AC Rehabilitation	$ 34,000
2023	**PIE**	FBO CONN	119	AC	3,041	26	AC Reconstruction	$ 93,000
2023	**PIE**	FBO CONN	125	APC	4,598	57	AC Rehabilitation	$ 65,000
2023	**PIE**	TW A	115	AAC	224,709	63	AC Rehabilitation	$ 3,146,000
2023	**PIE**	TW A	130	AAC	358,395	69	AC Rehabilitation	$ 5,018,000
2023	**PIE**	TW A	158	AAC	16,692	61	AC Rehabilitation	$ 234,000
2023	**PIE**	TW A1	135	AAC	40,056	62	AC Rehabilitation	$ 561,000
2023	**PIE**	TW A1	140	AAC	14,541	65	AC Rehabilitation	$ 204,000
2023	**PIE**	TW B	220	AC	40,670	13	AC Reconstruction	$ 1,241,000
2023	**PIE**	TW G	1315	AC	19,536	68	AC Rehabilitation	$ 274,000
2023	**PIE**	TW G	1320	AAC	15,822	60	AC Rehabilitation	$ 222,000
2023	**PIE**	TW G	1325	AAC	199,036	63	AC Rehabilitation	$ 2,787,000

Program Year	Network ID	Branch ID	Section ID	Surface	Area (SF)	PCI Before	Rehabilitation Type	Planning Cost Estimate
2023	PIE	TW G1	1330	AAC	13,135	63	AC Rehabilitation	$ 184,000
2023	PIE	TW G1	1335	AAC	12,530	61	AC Rehabilitation	$ 176,000
2023	PIE	TW G3	605	AAC	10,930	20	AC Reconstruction	$ 334,000
2023	PIE	TW H	810	AAC	59,729	0	AC Reconstruction	$ 1,822,000
2023	PIE	TL T-HANG	1120	AC	1,346	37	AC Reconstruction	$ 42,000
2023	PIE	TL T-HANG	1125	AC	1,472	36	AC Reconstruction	$ 45,000
2023	PIE	AP MAIN	4105	APC	40,910	27	AC Reconstruction	$ 1,248,000
2023	PIE	AP MAIN	4155	AAC	33,689	57	AC Rehabilitation	$ 472,000
2023	PIE	AP MAIN	4157	AAC	92,541	65	AC Rehabilitation	$ 1,296,000
2023	PIE	AP MAIN	4175	PCC	14,910	1	PCC Reconstruction	$ 895,000
2023	PIE	AP MAIN	4176	AC	3,573	26	AC Reconstruction	$ 109,000
2023	PIE	AP MAIN	4178	APC	59,522	60	AC Rehabilitation	$ 834,000
2023	PIE	AP MAIN	4179	APC	77,111	64	AC Rehabilitation	$ 1,080,000
2023	PIE	AP MAIN	4180	AAC	126,695	43	AC Reconstruction	$ 3,865,000
2023	PIE	AP MAIN	4183	AAC	39,947	66	AC Rehabilitation	$ 560,000
2023	PIE	AP MAIN	4185	APC	12,820	49	AC Reconstruction	$ 392,000
2023	PIE	AP MAIN	4190	PCC	18,650	10	PCC Reconstruction	$ 1,119,000
2023	PIE	AP MAIN	4195	PCC	11,250	5	PCC Reconstruction	$ 675,000
2023	PIE	AP MAIN	4198	PCC	18,579	16	PCC Reconstruction	$ 1,115,000
2024	PIE	RW 4-22	6205	AAC	474,873	69	AC Rehabilitation	$ 6,981,000
2024	PIE	TW T	2045	AAC	16,549	70	AC Rehabilitation	$ 244,000
2024	PIE	AP MAIN	4110	APC	56,000	70	AC Rehabilitation	$ 824,000
2026	PIE	FBO CONN	117	AAC	6,019	69	AC Rehabilitation	$ 98,000
2026	PIE	TW A2	165	AC	60,458	69	AC Rehabilitation	$ 980,000
2027	PIE	RW 4-22	6210	AAC	237,436	69	AC Rehabilitation	$ 4,041,000
2027	PIE	TW B	212	AAC	18,000	70	AC Rehabilitation	$ 307,000
2027	PIE	TW F	610	AC	43,041	70	AC Rehabilitation	$ 733,000
2027	PIE	TW L	1215	AC	13,483	70	AC Rehabilitation	$ 230,000
2027	PIE	AP MAIN	4150	AAC	14,083	69	AC Rehabilitation	$ 240,000
2027	PIE	AP MAIN	4177	APC	20,899	69	AC Rehabilitation	$ 356,000
2028	PIE	FBO CONN	127	APC	12,891	70	AC Rehabilitation	$ 231,000
2028	PIE	TW A	160	AAC	99,856	70	AC Rehabilitation	$ 1,785,000
2028	PIE	TW A3	168	AC	60,311	69	AC Rehabilitation	$ 1,078,000
2028	PIE	TW G	1340	AAC	14,004	70	AC Rehabilitation	$ 251,000
2028	PIE	TW L	1205	AC	22,175	69	AC Rehabilitation	$ 397,000
2028	PIE	AP MAIN	4123	APC	43,794	70	AC Rehabilitation	$ 783,000
2029	PIE	TW A7	162	AAC	52,089	69	AC Rehabilitation	$ 978,000
2029	PIE	TW L	1245	AC	52,150	70	AC Rehabilitation	$ 979,000
2030	PIE	TW T	2010	AAC	12,963	70	AC Rehabilitation	$ 256,000
2030	PIE	TW T	2020	AAC	14,337	69	AC Rehabilitation	$ 283,000
2030	PIE	AP MAIN	4170	AAC	18,816	69	AC Rehabilitation	$ 371,000
2031	PIE	TW A	132	AAC	23,007	69	AC Rehabilitation	$ 476,000
2031	PIE	TW A	155	AAC	6,259	70	AC Rehabilitation	$ 130,000
2031	PIE	TW A5	175	AC	56,987	70	AC Rehabilitation	$ 1,179,000
2031	PIE	TW B	210	AAC	6,353	69	AC Rehabilitation	$ 132,000
2031	PIE	TW G2	1005	AAC	15,843	70	AC Rehabilitation	$ 328,000

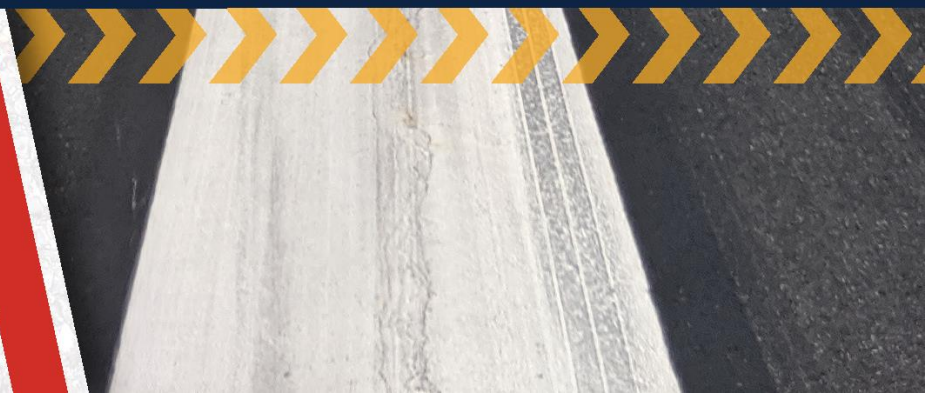
Program Year	Network ID	Branch ID	Section ID	Surface	Area (SF)	PCI Before	Rehabilitation Type	Planning Cost Estimate
2031	**PIE**	TW G2	1010	AAC	8,964	70	AC Rehabilitation	$ 186,000
2031	**PIE**	TW G3	607	AAC	8,732	69	AC Rehabilitation	$ 181,000
2032	**PIE**	TW B	205	AAC	6,200	69	AC Rehabilitation	$ 135,000
2032	**PIE**	TW B	207	AAC	7,750	69	AC Rehabilitation	$ 169,000
2032	**PIE**	TW T	2050	AAC	149,440	69	AC Rehabilitation	$ 3,246,000

**All planning cost values have been rounded up to the nearest thousand dollars.*

Figure E.3: 10-Year Major Rehabilitation Needs by Program Year



Chapter 1: Introduction



Chapter 1 – Introduction

The State of Florida has 128 public airports, 100 of which are recognized as part of the Federal Aviation Administration's (FAA) National Plan of Integrated Airport Systems (NPIAS). These public-use airports are vital to Florida's economy as well as the economy of the United States. The Florida Airport System (FAS) provides opportunities for the State to capitalize on an increasingly global marketplace. Florida's system of commercial service and general aviation airports are important to businesses throughout the State as air travel is essential to tourism, Florida's most prominent industry.

1.1 Background

In 1992, the Florida Department of Transportation (FDOT) established the Statewide Airfield Pavement Management Program (SAPMP) to provide program managers, District Aviation Offices, and Airport operators with a system to proactively manage airfield pavement infrastructure within the FAS. The SAPMP includes network-level Pavement Condition Index (PCI) surveys for Airport facilities that are categorized as General Aviation (GA), Reliever (RL), and Primary/Commercial (PR). Currently, the SAPMP includes 95 participating public-use airports with pavement facilities and provides its users with comprehensive data to better manage their pavement assets.

There are millions of square feet of pavement infrastructure at airports across a network of runways, taxiways, aprons, and other areas. This pavement infrastructure is vital to the support and safety of aircraft operations. Timely maintenance, repair, and major rehabilitation of pavement infrastructure allows the Airport to operate safely, efficiently, and economically without excessive down time.

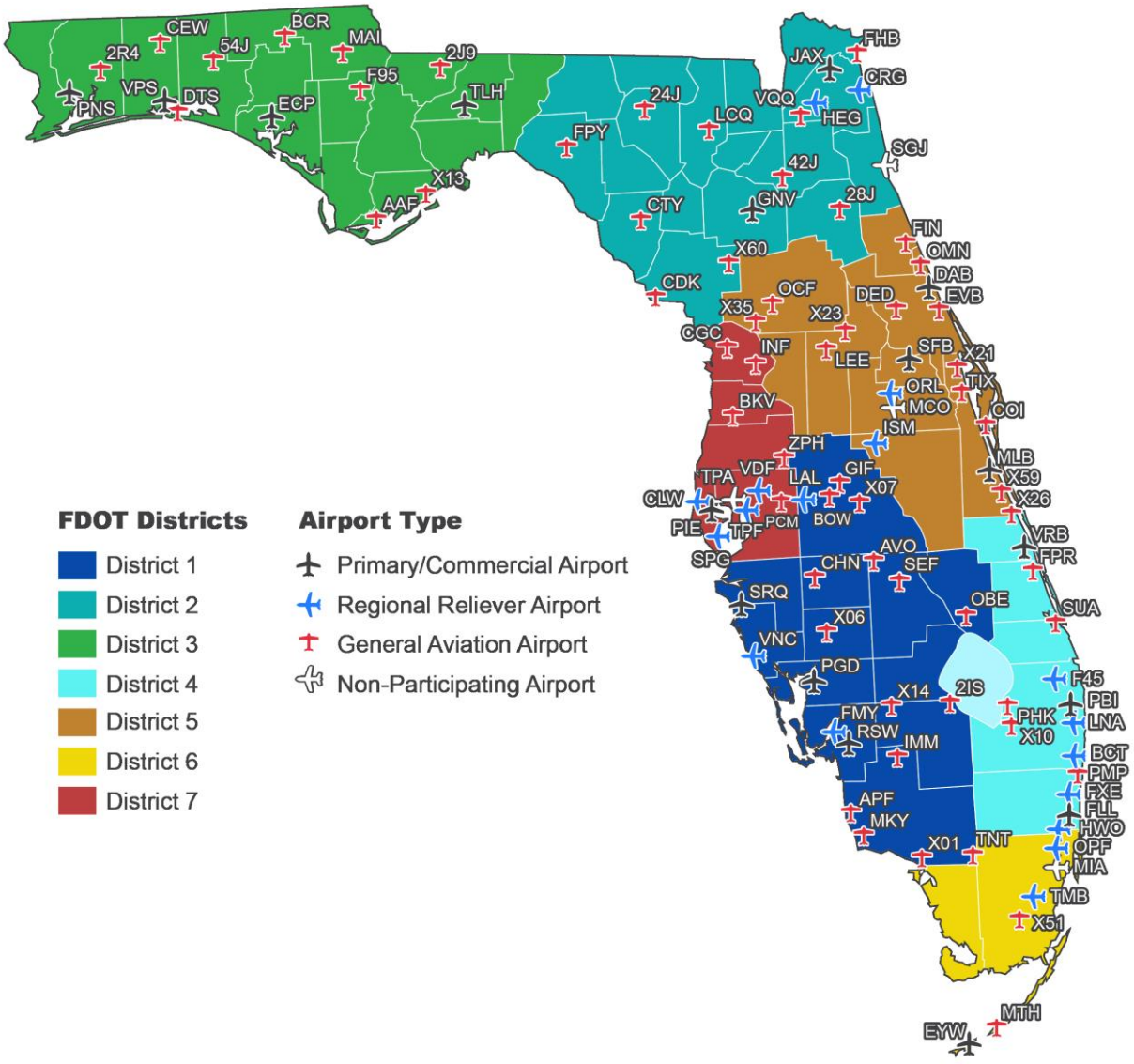
Airports participating in the Airport Improvement Program (AIP) Grant Program are required by the FAA to develop and implement a pavement maintenance program in order to be eligible for funding, per FAA Advisory Circulars 150/5380-6C "Guidelines and Procedures for Maintenance of Airport Pavements" and 150/5380-7B "Airport Pavement Management Program (PMP)". The AIP program requires detailed assessments of airfield pavements at least once a year for a pavement management program. The frequency of the detailed inspections may be extended to every three years if the pavement is assessed according to the PCI survey procedure described in ASTM D5340-20 "Standard Test Method for Airport Pavement Condition Index Surveys".

In general, adherence to the FAA Advisory Circulars is mandatory for projects funded with federal grant monies through the AIP program and with revenue from the Passenger Facilities Charges (PFC) Program. Further information is detailed in FAA Grant Assurance No. 11 "Pavement Maintenance," No. 34 "Policies, Standards, and Specifications," and PFC Assurance No. 9 "Standards and Specifications." The FDOT performs the SAPMP System Updates for the benefit of participating public-use and publicly-owned airports through the Aviation Office (AO).

The SAPMP addresses the requirements of maintaining an effective pavement management program for participating airports at the network level. Network-level management of pavement assets provides insight for short-term and long-term budget needs, understanding of the overall condition of the network (current and future), and knowledge of the pavement facilities that are

under consideration for projects. A network-level evaluation can support the identification of maintenance, repair, and major rehabilitation needs and budgetary planning-level opinions of probable construction costs.

Figure 1.1: Florida Aviation System (Facilities with Pavement) and FDOT Districts



1.2 Stakeholders

The SAPMP is performed for the benefit of the stakeholders. The table below outlines the primary stakeholders of the FDOT SAPMP and their role in the program.

Table 1.2: FDOT SAPMP Stakeholders

Role	Description
FAA Orlando Airports District Office (Orlando ADO)	Key Stakeholder: local ADO Program Manager personnel that oversees the grant administration of AIP grant with Planning Agency Sponsor (Florida Department of Transportation).
Florida Department of Transportation (FDOT)	Key Stakeholder: the FDOT is the "Sponsor" for the AIP grant agreement. Specifically, the Aviation Office (AO) provides development and operations support for the Florida Airport System.
FDOT District Offices	The seven (7) FDOT District Offices, specifically the Aviation representatives, provide essential support to the SAPMP System Update and the AO Program Manager (AO-PM). Each District supports the SAPMP's ongoing efforts by providing local construction cost information throughout the State, which is used as the basis of development for maintenance, repair, and major rehabilitation opinions of probable construction costs for planning purposes.
Participating Public-Use and Publicly-Owned Airports	The airports are the end-user and primary beneficiary of the SAPMP. The SAPMP provides a specific Airport Pavement Evaluation Report that meets the requirements of the FAA AC 150/5380-7B. Individual participating airports are provided a final Airport Pavement Evaluation Report by the Consultant that is specific to each airport's airfield PCI assessment.
Aviation Office Program Manager (AO-PM)	FDOT AO Airport Engineering Manager: oversees and manages the overall Program System Update.

1.3 General Scope of Work

The SAPMP is limited to performing tasks in adherence to the key elements of an effective pavement management program on a statewide level. The primary tasks undertaken to update the FDOT SAPMP include, but are not limited to:

- Research and evaluation of existing record documentation;
- Establishment of a pavement system inventory;
- Development of a pavement network definition map and supplemental GIS model;
- Functional pavement evaluations via the PCI assessment method;
- Customization of PAVER™ software including prioritization, policies, and performance models;
- Analysis of condition data; and
- Maintenance, repair, and rehabilitation planning.

1.4 FDOT SAPMP Objectives

The SAPMP enables the FDOT AO and FAA to monitor pavement conditions at airports in the Florida Airport System. The SAPMP provides objective condition information needed to make informed decisions regarding the significant capital investment that the public-use airport pavement infrastructure represents.

Airport staff are responsible for making decisions regarding the timing and type of maintenance and rehabilitation activities that should be completed in order to maintain an acceptable operational condition and adequate load-carrying capacity. Utilizing the SAPMP will help Airport staff better understand the relative condition of their pavement facilities and when those facilities should be rehabilitated. The data collected from the SAPMP can be used for project programming for the next 10 years. This report summarizes the data collection, analysis, program update, and implementation of the FDOT SAPMP.

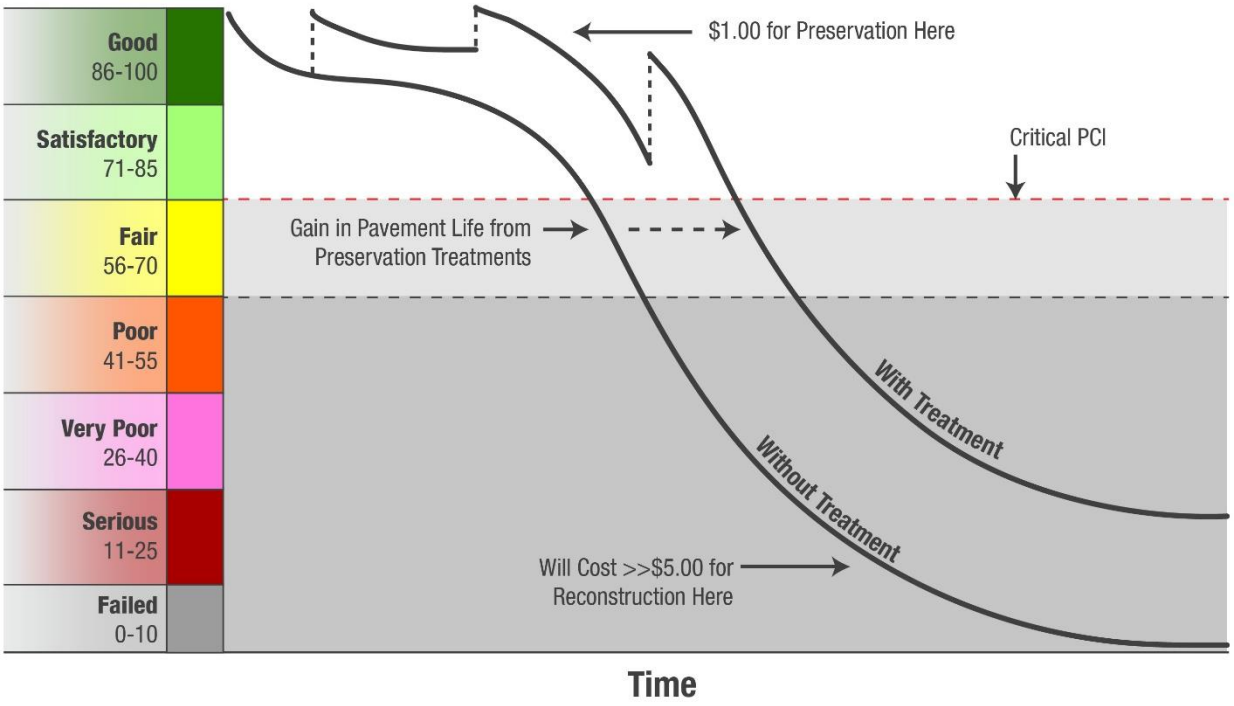
A comprehensive SAPMP provides information that assists with the project programming process. The primary objectives of the FDOT SAPMP consist of the following:

- Assist airports in meeting the requirements of Public Law 103-305;
- Assist airports in complying with FAA Grant Assurances 11 and 19;
- Provide airports with functional pavement condition in accordance with ASTM D5340-20 (current) and with the FAA AC 150/5380-7B (current) based on visual assessment efforts;
- Provide airports with planning-level guidance on maintenance, repair, and rehabilitation in accordance with the FAA AC 150/5380-6C (current) based on pavement conditions and distress data in terms of type, severity, and extent; and
- Provide airports, FDOT Districts, FDOT AO, and the FAA Airports District Office with long-term, planning-level forecasts of pavement performance and rehabilitation budgetary needs (e.g., maintenance, repair, and major reconstruction) through reports.

From a pavement management perspective, one of the most valuable aspects of the PCI methodology is the ability to save money by effectively prioritizing the rehabilitation of pavement assets before they reach critical condition. Critical PCI values are assigned to deterioration models for pavement assets based on their respective use and rank. The concept of critical PCI will be further discussed in **Chapter 5**, but it is used as a benchmark to help identify pavement assets that should receive rehabilitation. In doing so, the PCI methodology can help create a proactive maintenance and rehabilitation (M&R) strategy to effectively address pavement projects before the cost of these projects increases significantly.

With M&R costs escalating over time, the consequences of inadequate maintenance practices can result in an inefficient allocation of funding. If maintenance is conducted before a significant decline in pavement condition occurs, substantial repair and/or rehabilitation costs may be avoided or delayed. **Figure 1.4** illustrates how the cost of pavement repairs can significantly increase if M&R activities are delayed.

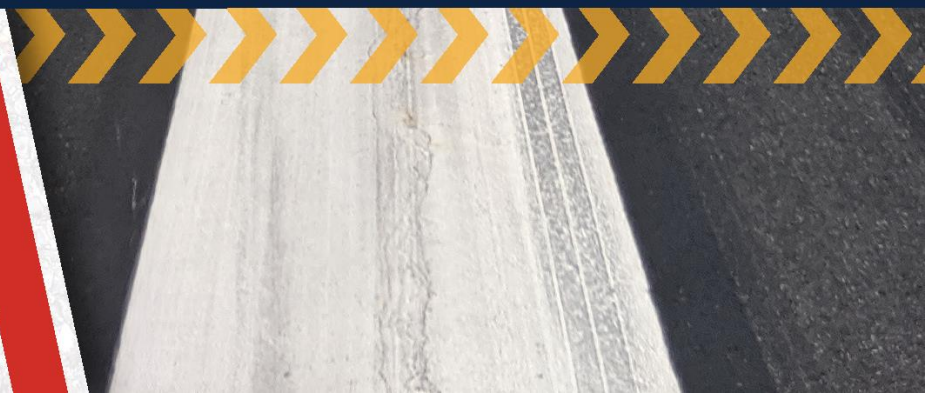
Figure 1.4: Pavement Life and the Effect of Treatments



FAA Eligibilty Thresholds: ☐ **>70:** Routine Maintenance ☐ **55-70:** Rehabilitation Eligible ☐ **<55:** Reconstruction Eligible

**Figure is for conceptual purposes only – unit costs are not specific to airfield pavements*

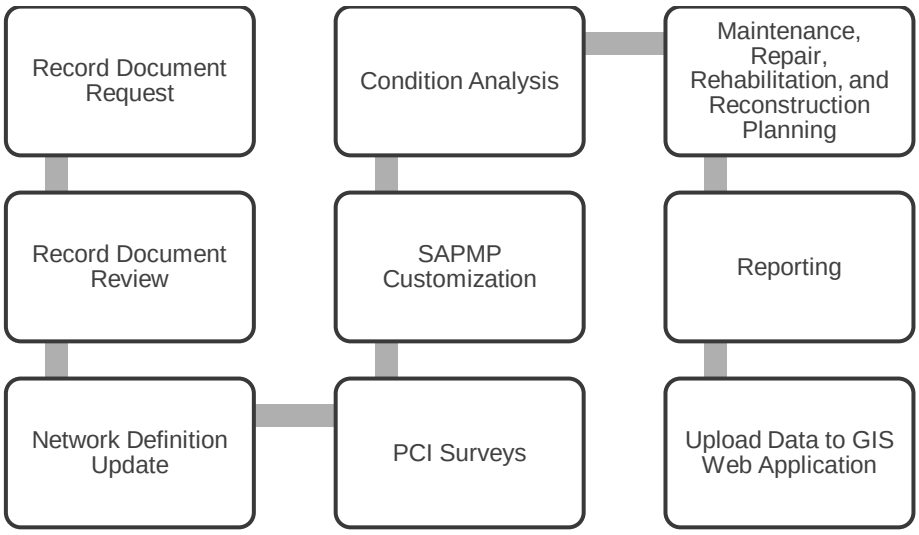
Chapter 2: Methodology



Chapter 2 – Methodology

An effective pavement management program incorporates both the regular collection of pavement condition information and communication of information to appropriate sponsors. This chapter of the report defines the specific methods utilized as part of the SAPMP System Update to meet the requirements of an effective pavement management system as defined by the FAA AC 150/5380-7B. **Figure 2** summarizes the overall process for the FDOT SAPMP.

Figure 2: FDOT SAPMP General Process



2.1 Airfield Pavement Database

This SAPMP utilizes PAVER™ 7.0 software as its airfield pavement database. The PAVER™ software application was developed by the U.S. Army Construction Engineering Research Laboratory and sponsored by the FAA, Federal Highway Administration, U.S. Army, U.S. Air Force, and U.S. Navy to meet the objectives of an effective pavement management system. The PAVER™ database includes a network-level inventory of the participating airport's eligible airfield pavement facilities. PAVER™ can achieve the following pavement management objectives:

- Create a manageable inventory system;
- Analyze the current condition of pavements in accordance with ASTM D5340-20;
- Develop pavement performance models to forecast conditions; and
- Generate maintenance, repair, and major rehabilitation recommendations based on budgetary scenarios.

PAVER™ inventory management is based on a tiered organizational structure consisting of networks, branches, sections, and samples, with the sample being the smallest unit of management. Critical elements of an effective pavement management program are maintained within the network-level PAVER™ database and typically consist of pavement inventory

characteristics, pavement structure, work history, historic condition records, and analytical customization.

2.2 Airfield Pavement Record Keeping (Historical Records Research)

In accordance with the FAA AC 150/5380-7B, it is a best practice that airports maintain records of all airfield construction and maintenance (routine, emergency, and proactive) related to the pavement facilities. These records should consist of:

- Location and limits of work;
- Types and severities of repaired distresses;
- Work type and cost; and
- Supporting documents (e.g., contract documents, construction drawings, specifications, bid tabulations, repair products, and photograph records).

As part of the SAPMP, participating airport's staff was asked to provide documentation regarding the historical work performed at the Airport, including construction drawings and bid tabulations. This information is used to identify location, limits, type of work, pavement cross-sections, and representative material costs.

Updated historical data collected during this task was entered into the PAVER™ database. This database includes the following fields for historical information:

- Date of last construction/rehabilitation
- Work type performed
- Comments for documenting pavement cross-section
- Pavement surface type
- Section area (limits of work)

The SAPMP PAVER™ database accuracy is limited to the record documentation provided by the participating airports. Airport Sponsors should rely on this information as a planning tool and defer to final as-built plans, record drawings, and/or engineer's construction report for pavement construction records.

2.3 Airfield Pavement Structure

A pavement is a prepared surface designed to provide a continuous, smooth ride at a certain speed and to support an estimated amount of traffic for a certain number of years. A pavement structure is composed of constructed layers consisting of subgrade, subbase, base, structural, and surface courses. For the FDOT SAPMP, two (2) predominant pavement types are classified for evaluation and analysis: Asphalt Concrete (AC) and Portland cement concrete (PCC). Composite Structures, known as Whitetopping Pavements consisting of PCC on AC, are also present at limited airports in Florida and are evaluated separately.

2.3.1 Asphalt Concrete

Asphalt concrete is a pavement comprised of aggregate mixture with an asphalt cement binder. The FDOT SAPMP categorizes three (3) Asphalt Concrete surface types: Asphalt Concrete (AC), Asphalt Concrete overlaid on Asphalt Concrete (AAC), and Asphalt Concrete overlaid on Portland cement concrete (APC).

Asphalt Concrete (AC)

A flexible pavement section consisting of aggregate mixture with asphalt cement binder layered on engineered base course material that is layered on subbase and subgrade soil material.

Asphalt Concrete Overlaid on Asphalt Concrete (AAC)

A flexible pavement section consisting of aggregate mixture with asphalt cement binder layered on an existing flexible AC pavement section. Airfield pavement sections are considered to be AAC when a pavement rehabilitation includes a pavement milling and resurfacing operation or a direct overlay of Asphalt Concrete without surface preparation.

Asphalt Concrete Overlaid on Portland Cement Concrete (APC)

A flexible pavement section consisting of aggregate mixture with asphalt cement binder layered on an existing PCC pavement section. This unique pavement composition may result in distinct pavement distress manifestations known as reflective joint cracking.

2.3.2 Portland Cement Concrete

Portland cement concrete is a pavement comprised of aggregate mixture with a Portland cement binder. The FDOT SAPMP categorizes Portland cement concrete (PCC) as the primary rigid pavement section.

Portland Cement Concrete (PCC)

A rigid pavement section composed of Portland cement concrete placed on a granular or treated base course that is supported on a compacted subgrade. The concrete surface provides a texture of nonskid qualities, prevents the infiltration of surface water into the subgrade, and provides structural support for airplane loading. Rigid pavement construction requires the layout of appropriately designed joints. Concrete overlays built in accordance with the FAA Advisory Circular 150/5320-6F “Airport Pavement Design and Evaluation” are recognized as PCC pavement.

2.3.3 Composite Structure – Whitetopping Pavement

Whitetopping pavement is a composite pavement comprised of relatively thin PCC overlaid on an existing AC pavement structure. There are three (3) types of Whitetopping Pavements: Conventional (WT), Thin (TWT), and Ultra-Thin (UWT).

Conventional Whitetopping (WT)

A composite pavement structure consisting of a modified PCC overlaid on an existing AC pavement section. The modified PCC layer is typically greater than 6 inches in thickness.

Thin Whitetopping (TWT)
A composite pavement structure consisting of modified PCC overlaid on an existing AC pavement section. The modified PCC layer is typically between 4 and 6 inches in thickness.

Ultra-Thin Whitetopping (UWT)
A composite pavement structure consisting of a modified PCC overlaid on an existing AC pavement section. The modified PCC layer is typically between 2 and 4 inches in thickness.

2.4 Airfield Pavement Traffic

A pavement section is typically designed to meet the needs of the user (airlines, air cargo, general aviation, and/or military) in providing a safe, smooth, operational surface. Pavement deterioration generally occurs gradually from aircraft loading and environmental conditions.

This System Update does not involve a study or analysis of PIE's aircraft fleet mix or traffic operations. However, it is strongly recommended that the Airport incorporate the requirements of the FAA AC 150/5320-6F when developing design-level rehabilitation activities; this AC provides guidance on incorporation of aircraft traffic fleet mix data.

2.5 Pavement Management Program Network Definition Terminology

To facilitate an effective pavement management program, a pavement network must be established and subdivided into smaller, manageable working units. Sectioning of the pavement network was established in a prior System Update and was revised during this SAPMP to account for work that has been performed on the airfield since the previous Update. Information from historic records is used to help define the limits of the smaller working units. A critical input for a pavement inventory and network definition is the date of last major construction or rehabilitation, as this type of work will reset the section PCI to a value of 100.

The following sections define the common terms used in pavement management systems and cover their application for this SAPMP System Update.

2.5.1 Pavement Network Identification

Establishing the pavement network is the first step in organizing pavements into a structure for pavement management. The network is the starting point of the hierarchy of pavement management organization. A network typically consists of one or more pavement *branches*, which have one or more pavement *sections*. For example, a network can be all the pavements within an Airport's airfield or all the pavements in a statewide program. For the FDOT SAPMP, a network represents an individual Airport's airfield pavement facilities maintained by the Airport.

2.5.2 Pavement Branch Identification

A pavement branch, also known as a facility, is a logical unit of generally identifiable pavement within a network that has a distinct functional classification. For example, within an airfield, each runway, taxiway, or apron is considered a branch. Each branch contains at least one section but may contain more if pavement feature characteristics are distinct throughout the branch.

2.5.3 Pavement Section Identification

A pavement section, or feature, is a subdivision of a branch and has consistent characteristics throughout its length or area. These characteristics include structural composition (pavement layer material type and thickness), construction history, age, traffic type, traffic frequency, and pavement condition. A section is the basic management unit of a pavement network and is the level at which maintenance, repair, or major rehabilitation treatments are considered.

2.5.4 Pavement Sample Unit Identification

A pavement sample unit is an arbitrarily defined subdivision of a pavement section that has a standard size range of 20 contiguous slabs (±8 slabs) for PCC pavement and 5,000 contiguous square feet (±2,000 SF) for AC. A sample unit is the smallest subdivision of a pavement network and is analyzed during field assessments to establish condition ratings.

2.5.5 Terminology Summary

Below is a summary table, **Table 2.5.5**, with definitions and examples of common SAPMP terminology.

Table 2.5.5: SAPMP Terminology

SAPMP Terminology	Common Definition	Airport Example
Network	Totality of pavement assets maintained by the Airport.	"Tallahassee International Airport – Airfield Pavements"
Branch Name	Commonly defined asset name as established by Airport and by use.	"Runway 18-36"
Branch ID	Codified shorthand name for commonly defined asset established for database identification.	"RW 18-36" RW, Branch Use, "Runway" "Runway 18-36", Runway Facility
Section ID	Codified identification for pavement asset that is distinct by pavement composition, work history, aircraft loading, or condition.	"6105"
Sample Unit	A numeric identification of an area of pavement (5,000 ± 2,000 SF of AC or 20 ± 8 slabs of PCC) that has been inspected in accordance with ASTM D5340-20.	"300"

2.6 Airfield PCI Survey Methodology

In adherence to the FAA AC 150/5380-7B, the FDOT SAPMP utilizes the PCI survey method to collect pavement distress data and analyze the condition. The PCI survey procedure is a visual statistical sampling of pavements for recording primary distress types (e.g., cracking and deformation), associated severities, and quantities as defined by the ASTM D5340-20. This effort is the primary means of obtaining and recording pavement distress data. The PCI survey consists primarily of visual assessments of pavement surfaces for signs of distress and deterioration resulting from loading (aircraft) and environmental influences.

Overall, a visual pavement condition survey provides an indication of the cause and rate of deterioration of a pavement section from a functional point of view and can help identify if any underlying structural deficiencies are present. Although a visual PCI survey does not predict the remaining structural life of a pavement section or its ability to support loads, it does assess the rating of the operational surface. Functional condition, determined by the PCI method, can provide a cost-effective means to plan for pavement rehabilitation projects. Timely application of pavement rehabilitation may lead to the extension of functional life of individual pavement sections. This method varies from structural evaluation; functional condition is limited to visually observed distresses and indicative modes of pavement deterioration. A formal structural evaluation analyzes subsurface conditions, material characteristics, and qualitative pavement structure attributes. A structural evaluation may consist of subsurface geotechnical exploration, falling weight deflectometer testing, petrographic testing, material coring, and/or flexural testing.

2.6.1 Pavement Distress Types

For each sample, the severity and quantity of defined distresses are recorded and then analyzed in accordance with the ASTM D5340-20 standard, which identifies 17 AC distress types and 16 PCC distress types. **Tables 2.6.1 (a)** and **2.6.1 (b)** identify these distresses and their common causes or mechanisms.

Table 2.6.1 (a): Pavement Distress Types – Asphalt Concrete

Distress Mechanism	Distress Type
Load	Alligator Cracking Rutting
Climate/Durability	Block Cracking Joint Reflection Cracking Longitudinal and Transverse Cracking (LT) Raveling Shoving Weathering
Construction/Material	Bleeding Corrugation Depression Polished Aggregate Slippage Cracking Swelling
Other	Jet Blast Erosion Oil Spillage Patching and Utility Cut Patching

Table 2.6.1 (b): Pavement Distress Types – Portland Cement Concrete

Distress Mechanism	Distress Type
Load	Corner Break Longitudinal, Transverse, and Diagonal Cracking (LTD) Pumping Shattered Slab/Intersecting Cracks
Climate/Durability	Blowup Durability "D" Cracking Joint Seal Damage Popouts
Construction/Material	Alkali Silica Reaction (ASR) Scaling Shrinkage Cracking
Other	Corner Spalling Joint Spalling Large Patching and Utility Cut Settlement or Faulting Small Patching

2.6.2 PCI Survey Procedures

PCI surveys are conducted on sample units defined in previous System Updates. Sample units are subject to change at the discretion of field personnel and/or to major pavement rehabilitation treatments. Furthermore, access to sample units based on accessibility or operational impacts may affect the overall sampling rate effort at each airport. **Tables 2.6.2 (a)** and **(b)** define the sampling criteria used by the FDOT SAPMP. A higher sampling rate may be utilized to achieve greater statistical confidence, should the Airport have the available resources to perform PCI survey independent of the FDOT SAPMP.

Table 2.6.2 (a): Recommended Sampling Rates for Asphalt Concrete

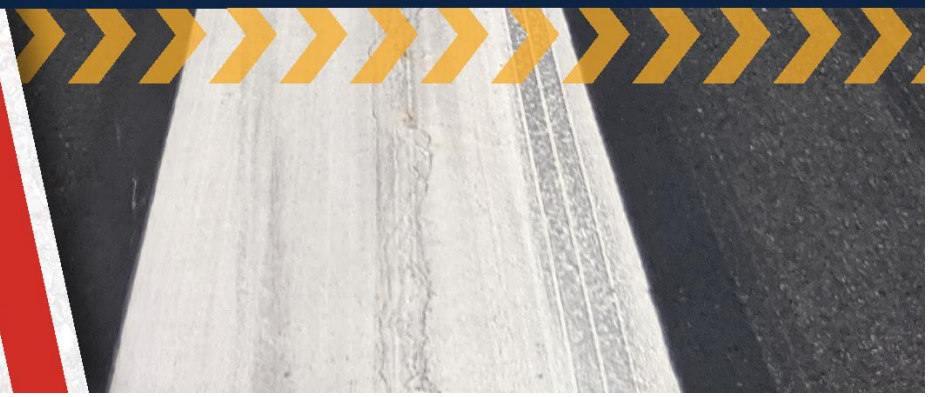
Number of Total Sample Units in Section	Runway Sampling Rate	Taxiways, Aprons, and Others Sampling Rate
1 - 4	1	1
5 - 10	2	1
11 - 15	3	2
16 - 30	5	3
31 - 40	7	4
41 - 50	8	5
51 or more	20% but ≤ 20	10% but ≤ 10

Table 2.6.2 (b): Recommended Sampling Rates for Portland Cement Concrete

Number of Total Sample Units in Section	Runway Sampling Rate	Taxiways, Aprons, and Others Sampling Rate
1 - 3	1	1
4 - 6	2	1
7 - 10	3	2
11 - 15	4	2
16 - 20	5	3
21 - 30	7	3
31 - 40	8	4
41 - 50	10	5
51 or more	20% but ≤ 20	10% but ≤ 10

The FDOT SAPMP is limited to select sample units for each section identified in each airport's Airfield Pavement Network Definition. The intent is to perform a limited amount of sample unit PCI surveys to reasonably reflect the functional condition. Due to the limited sampling criteria, there may be instances of pavement distress and deterioration outside of the inspected sample units that were not observed.

Chapter 3: Airfield Pavement System Inventory



Chapter 3 – Airfield Pavement System Inventory

This chapter discusses the inventory data collected from the Airport and summarizes network-level characteristics of the Airport's airfield pavements. At the start of each FDOT SAPMP System Update, all airports are asked to review the existing Airfield Pavement Network Definition Exhibit for accuracy. Furthermore, participating airports are asked to provide documentation of any recent or anticipated construction related to their airfield pavements.

3.1 Airfield Pavement Network Information

3.1.1 Previous and/or Anticipated Airfield Pavement Construction

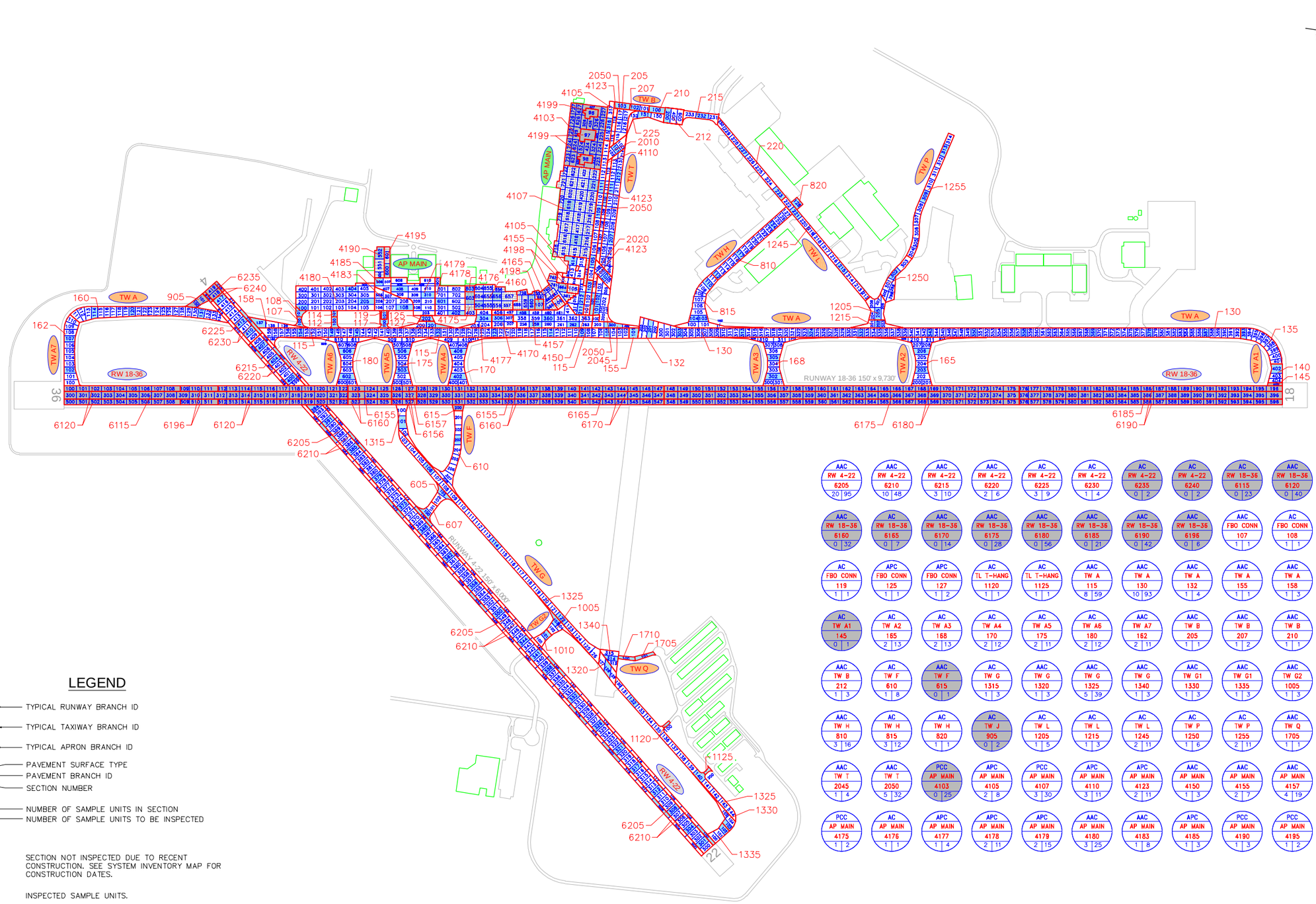
Based on information provided by the Airport, **Table 3.1.1** summarizes recent or anticipated airfield pavement construction projects since 2017.

Table 3.1.1: Summary of Previous and/or Anticipated Airfield Pavement Construction

Construction Year	Location	Work Type / Pavement Section
2017	TW A, TW A7	Mill and Overlay
	TW H	Complete Reconstruction - AC
2018	TW B, TW G, TW G1, TW G2, TW Q, TW T, AP MAIN	Mill and Overlay
	TW B, TW G, TW Q	New Construction - AC
	TW F	Complete Reconstruction - AC
2019	TW A	Mill and Overlay
2020	RW 4-22, TW J	New Construction - AC
	RW 18-36, TW A1	Complete Reconstruction - AC | 4" P-401, 5" P-401 Base, 6" P-211
	RW 18-36, TW F	Mill and Overlay | Variable depth mill, 4" P-401 overlay; Isolated areas of AC reconstruction (4" P-401, 5" P-401 Base, 6" P-211)
	RW 18-36	Mill and Overlay | Variable depth mill, 2" P-401 overlay
2023	AP MAIN	Complete Reconstruction - PCC
	TW C, TW D, TW G3	New Construction

The Airport provided a combination of record drawings, reports, and staff input, which aided in developing the construction history of the Airport's pavements since inception. Major rehabilitation and construction activities performed in the last 24 months, or anticipated in the next 24 months, are assumed to restore the PCI to 100. These activities include pavement overlay, mill and overlay, new construction, and/or complete reconstruction. These pavements were not formally subject to a PCI assessment and actual conditions may vary. Furthermore, any localized maintenance or repair performed in the assessment areas that would improve the PCI are considered in the condition analysis.

Figure 3.1.1 (a), the Airfield Pavement Network Definition Exhibit, provides details of the PCI assessment efforts. The Exhibit identifies pavement facilities, surface types, section definitions, and sample unit delineations. **Figure 3.1.1 (b)**, the Airfield Pavement System Inventory Exhibit, provides details of the work history updates communicated by the Airport. The Exhibit provides the approximate limits of recent and/or anticipated construction on the airfield pavement facilities. The limits are based on documentation provided by the Airport and, if constructed, are confirmed during field surveys.



LEGEND

- RW 13-31 — TYPICAL RUNWAY BRANCH ID
- TW A — TYPICAL TAXIWAY BRANCH ID
- AP S — TYPICAL APRON BRANCH ID
- AAC / AP MAIN / 4105 / 10 | 100 — PAVEMENT SURFACE TYPE; PAVEMENT BRANCH ID; SECTION NUMBER; NUMBER OF SAMPLE UNITS IN SECTION; NUMBER OF SAMPLE UNITS TO BE INSPECTED
- AAC / AP MAIN / 4105 / 0 | 100 — SECTION NOT INSPECTED DUE TO RECENT CONSTRUCTION. SEE SYSTEM INVENTORY MAP FOR CONSTRUCTION DATES.
- 100 — INSPECTED SAMPLE UNITS.

TOTAL SAMPLES INSPECTED = 171
AC: 160 PCC: 11

RUNWAYS LENGTHS DEPICTED IN THIS DRAWING ARE FOR PAVEMENT MANAGEMENT PURPOSES ONLY AND MAY NOT MATCH PUBLISHED RUNWAY LENGTHS. DRAWING NOT TO SCALE.

AAC RW 4-22 6205 20 | 95	AAC RW 4-22 6210 10 | 48	AAC RW 4-22 6215 3 | 10	AAC RW 4-22 6220 2 | 6	AC RW 4-22 6225 3 | 9	AC RW 4-22 6230 1 | 4	AC RW 4-22 6235 0 | 2	AC RW 4-22 6240 0 | 2	AC RW 18-36 6115 0 | 23	AAC RW 18-36 6120 0 | 40	AAC RW 18-36 6155 0 | 16	AC RW 18-36 6156 0 | 4	AC RW 18-36 6157 0 | 2
AAC RW 18-36 6160 0 | 32	AC RW 18-36 6165 0 | 7	AAC RW 18-36 6170 0 | 14	AAC RW 18-36 6175 0 | 28	AAC RW 18-36 6180 0 | 56	AAC RW 18-36 6185 0 | 21	AAC RW 18-36 6190 0 | 42	AAC RW 18-36 6195 0 | 8	AAC FBO CONN 107 1 | 1	AC FBO CONN 108 1 | 1	AAC FBO CONN 112 1 | 1	AC FBO CONN 114 1 | 1	AAC FBO CONN 117 1 | 1
AC FBO CONN 119 1 | 1	APC FBO CONN 125 1 | 1	APC FBO CONN 127 1 | 2	AC TL T-HANG 1120 1 | 1	AC TL T-HANG 1125 1 | 1	AAC TW A 115 8 | 59	AAC TW A 130 10 | 93	AAC TW A 132 1 | 4	AAC TW A 155 1 | 1	AAC TW A 158 1 | 3	AAC TW A 160 3 | 24	AC TW A1 135 1 | 9	AAC TW A1 140 1 | 3
AC TW A1 145 0 | 1	AC TW A2 165 2 | 13	AC TW A3 168 2 | 13	AC TW A4 170 2 | 12	AC TW A5 175 2 | 11	AC TW A6 180 2 | 12	AAC TW A7 162 2 | 11	AAC TW B 205 1 | 1	AAC TW B 207 1 | 2	AAC TW B 210 1 | 1	AAC TW B 215 1 | 3	AC TW B 220 2 | 8	AC TW B 225 1 | 3
AAC TW B 212 1 | 3	AC TW F 610 1 | 8	AAC TW F 615 0 | 1	AC TW G 1315 1 | 3	AAC TW G 1320 1 | 3	AAC TW G 1325 5 | 39	AAC TW G 1340 1 | 3	AAC TW G1 1330 1 | 3	AAC TW G1 1335 1 | 3	AAC TW G2 1005 1 | 3	AAC TW G2 1010 1 | 2	AAC TW G3 605 1 | 2	AAC TW G3 607 1 | 2
AAC TW H 810 3 | 16	AC TW H 815 3 | 12	AC TW H 820 1 | 1	AC TW J 905 0 | 2	AC TW L 1205 1 | 5	AC TW L 1215 1 | 3	AC TW L 1245 2 | 11	AC TW P 1250 1 | 6	AC TW P 1255 2 | 11	AAC TW Q 1705 1 | 1	AC TW Q 1710 1 | 1	AAC TW T 2010 1 | 3	AAC TW T 2020 1 | 4
AAC TW T 2045 1 | 4	AAC TW T 2050 5 | 32	PCC AP MAIN 4103 0 | 25	APC AP MAIN 4105 2 | 8	PCC AP MAIN 4107 3 | 30	APC AP MAIN 4110 3 | 11	APC AP MAIN 4123 2 | 11	AAC AP MAIN 4150 1 | 3	AAC AP MAIN 4155 2 | 7	AAC AP MAIN 4157 4 | 19	PCC AP MAIN 4160 2 | 11	PCC AP MAIN 4165 2 | 14	AAC AP MAIN 4170 1 | 4
PCC AP MAIN 4175 1 | 2	AC AP MAIN 4176 1 | 1	APC AP MAIN 4177 1 | 4	APC AP MAIN 4178 2 | 11	APC AP MAIN 4179 2 | 15	AAC AP MAIN 4180 3 | 25	AAC AP MAIN 4183 1 | 8	APC AP MAIN 4185 1 | 3	PCC AP MAIN 4190 1 | 3	PCC AP MAIN 4195 1 | 2	PCC AP MAIN 4198 1 | 3	PCC AP MAIN 4199 0 | 3	

PIE

AIRFIELD PAVEMENT NETWORK DEFINITION EXHIBIT

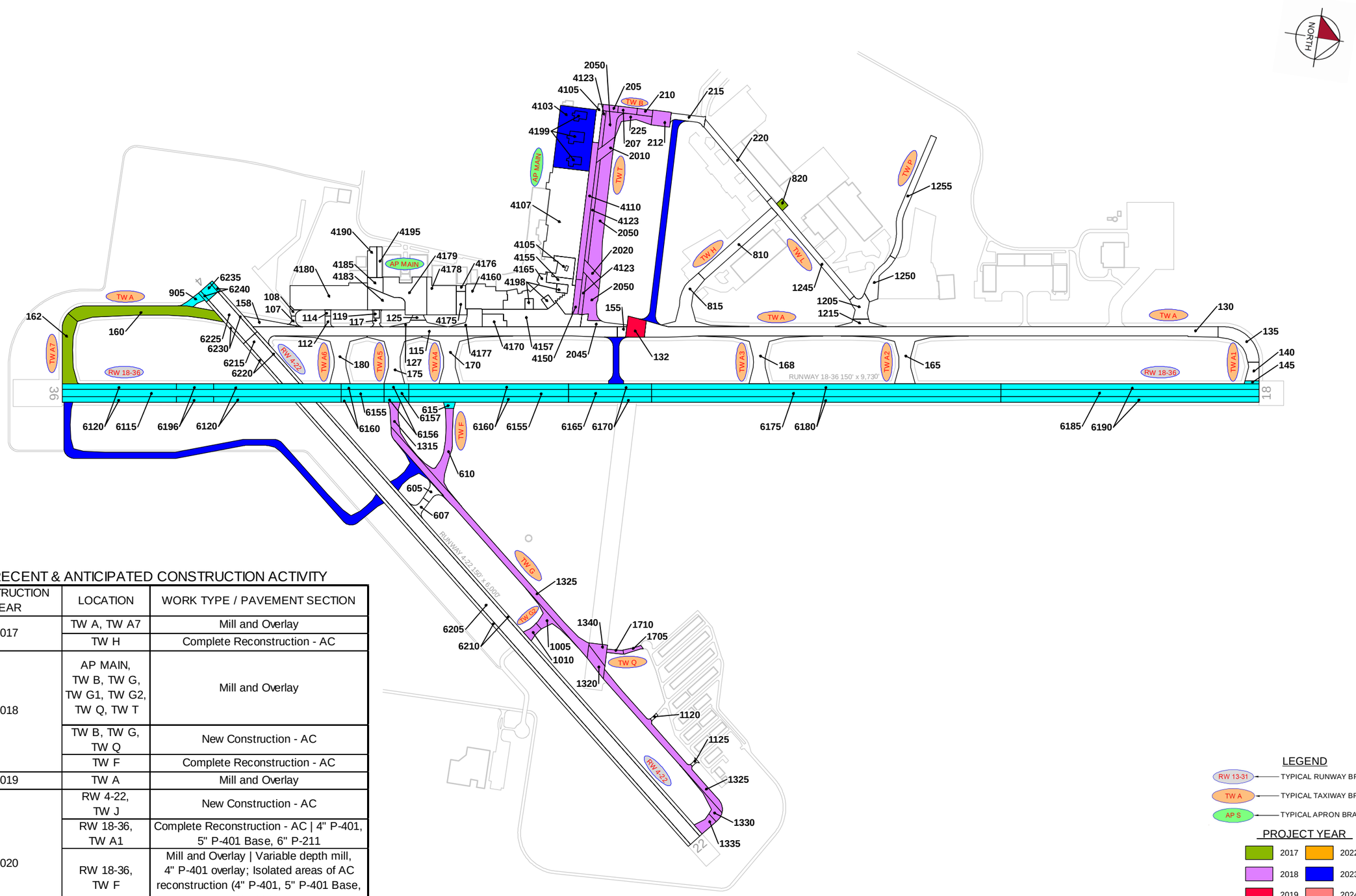
Statewide Airfield Pavement Management Program

ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

FDOT

2022

PIE



RECENT & ANTICIPATED CONSTRUCTION ACTIVITY

CONSTRUCTION YEAR	LOCATION	WORK TYPE / PAVEMENT SECTION
2017	TW A, TW A7	Mill and Overlay
	TW H	Complete Reconstruction - AC
2018	AP MAIN, TW B, TW G, TW G1, TW G2, TW Q, TW T	Mill and Overlay
	TW B, TW G, TW Q	New Construction - AC
	TW F	Complete Reconstruction - AC
2019	TW A	Mill and Overlay
2020	RW 4-22, TW J	New Construction - AC
	RW 18-36, TW A1	Complete Reconstruction - AC | 4" P-401, 5" P-401 Base, 6" P-211
	RW 18-36, TW F	Mill and Overlay | Variable depth mill, 4" P-401 overlay; Isolated areas of AC reconstruction (4" P-401, 5" P-401 Base, 6" P-211)
	RW 18-36	Mill and Overlay | Variable depth mill, 2" P-401 overlay
2023	AP MAIN	Complete Reconstruction - PCC
	TW C, TW D, TW G3	New Construction

LEGEND

RW 13-31 — TYPICAL RUNWAY BRANCH ID

TW A — TYPICAL TAXIWAY BRANCH ID

AP S — TYPICAL APRON BRANCH ID

PROJECT YEAR

2017		2022	
2018		2023	
2019		2024	
2020		2025	
2021		2026	

RUNWAY LENGTHS DEPICTED IN THIS DRAWING ARE FOR PAVEMENT MANAGEMENT PURPOSES ONLY AND MAY NOT MATCH PUBLISHED RUNWAY LENGTHS. DRAWING NOT TO SCALE.

AIRFIELD PAVEMENT SYSTEM INVENTORY EXHIBIT

Statewide Airfield Pavement Management Program

ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

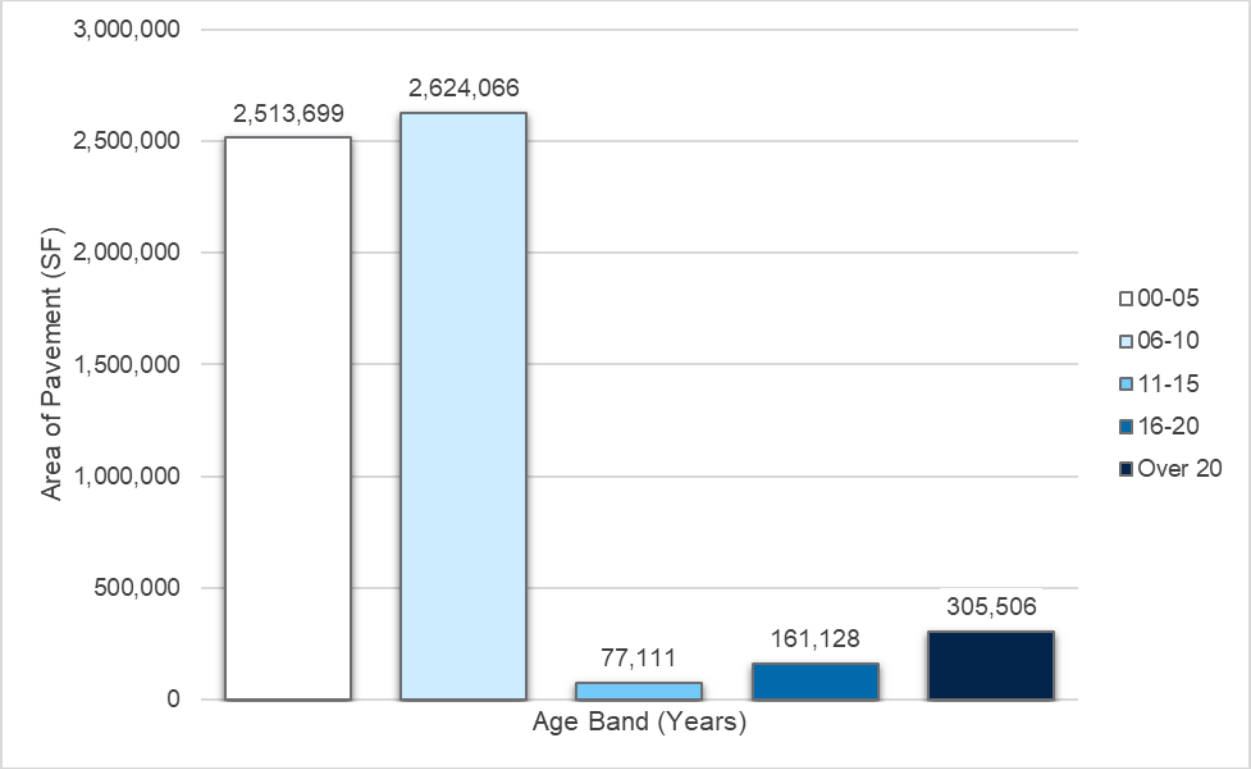
FDOT

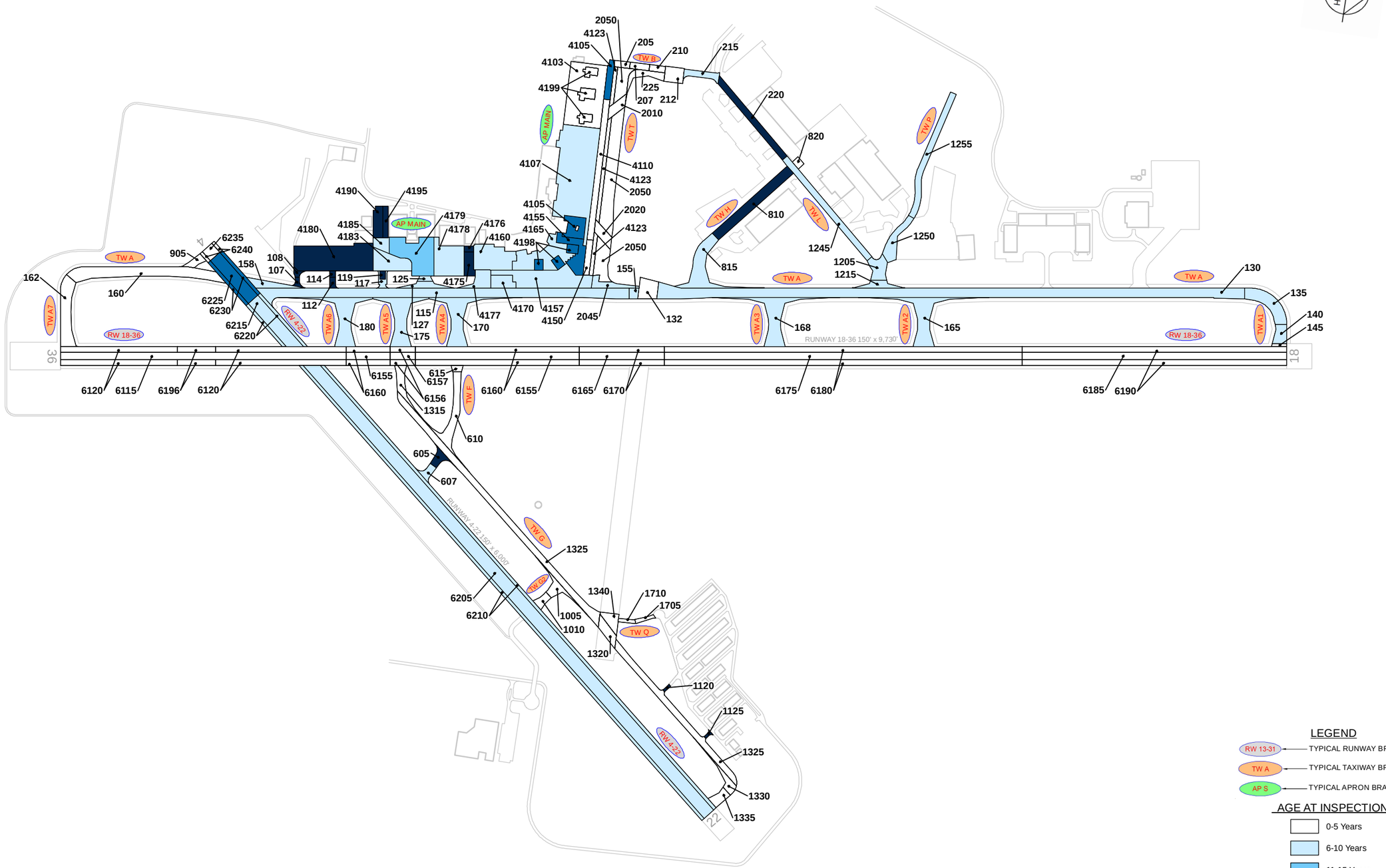
2022

3.1.2 Estimated Pavement Age

Standard pavement design practice considers a design life of 20 years. Design inputs typically require subgrade soil conditions, pavement layer material characteristics, and anticipated loading (aircraft fleet mix) for the design-life period. Based on the review of historic airfield pavement construction activities, **Figure 3.1.2 (a)** summarizes the age of the pavement sections since the last major construction activity has occurred. **Figure 3.1.2 (b)** provides the approximate limits of those age ranges on the airfield pavement facilities. This is intended to be a rough estimate based on interpretation of the limited data available at the time of report. The estimation of pavement age is based on information requested from the Airport.

Figure 3.1.2 (a): Age of Pavements at PCI Survey





LEGEND

- RW 13-31 — TYPICAL RUNWAY BRANCH ID
- TW A — TYPICAL TAXIWAY BRANCH ID
- AP S — TYPICAL APRON BRANCH ID

AGE AT INSPECTION

- 0-5 Years
- 6-10 Years
- 11-15 Years
- 16-20 Years
- > 20 Years

RUNWAY LENGTHS DEPICTED IN THIS DRAWING ARE FOR PAVEMENT MANAGEMENT PURPOSES ONLY AND MAY NOT MATCH PUBLISHED RUNWAY LENGTHS. DRAWING NOT TO SCALE.

PIE

AIRFIELD PAVEMENT
ESTIMATED AGE EXHIBIT

Statewide Airfield Pavement Management Program

ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

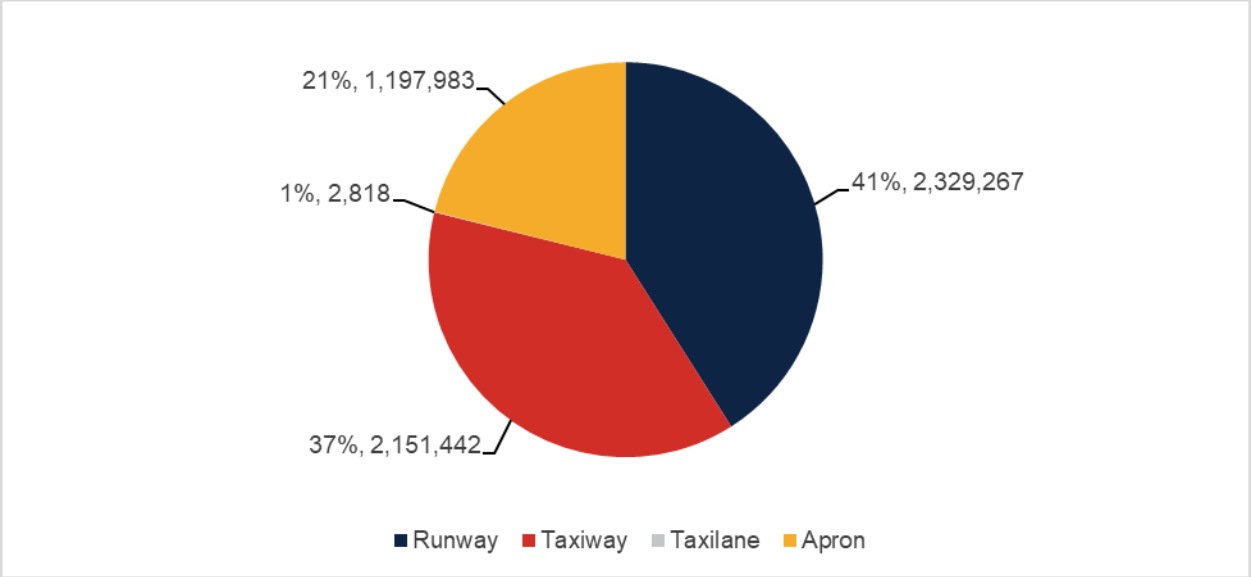
FDOT

2022

3.1.3 Functional Use

Pavements are subject to variations in aircraft loading patterns based on use and overall operations. This is termed "functional use" or "branch use." For this SAPMP System Update, the following categories of pavement functional use are identified: runway, taxiway, taxilane, and apron. **Figure 3.1.3** summarizes pavement functional use by area and excludes paved shoulders.

Figure 3.1.3: Airfield Pavement Branch Use by Area (SF)

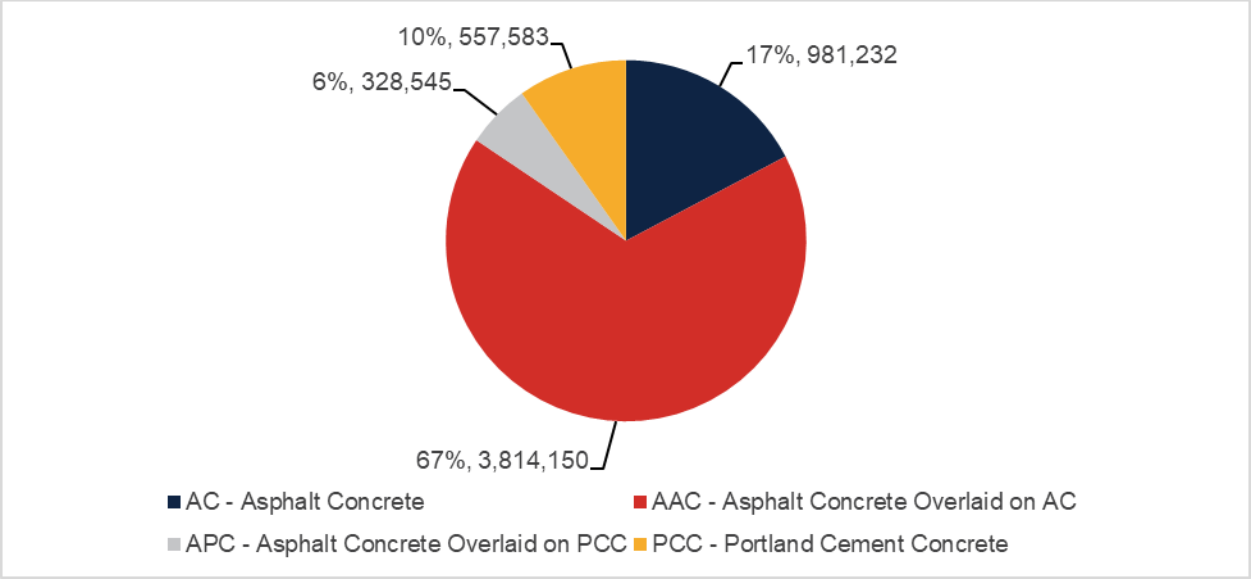


3.1.4 Pavement Surface Type

The airfield pavement facility surface types within the SAPMP include four (4) common types of pavement: Asphalt Concrete (AC), Asphalt Concrete overlaid on Asphalt Concrete (AAC), Asphalt Concrete overlaid on Portland cement concrete (APC), and Portland cement concrete (PCC).

Based on the record documentation incorporated within the SAPMP database and as observed during airfield pavement field assessments, pavement surface types have been assigned to the various pavement sections. **Figure 3.1.4** summarizes the applicable pavement types observed at PIE.

Figure 3.1.4: Airfield Pavement Surface Type by Area (SF)



3.1.5 Pavement System Inventory Details

The pavement inventory scope includes updates to existing pavement geometry and the development of an AutoCAD model with spatial projection for use within GIS. **Appendix C** includes the Airfield Pavement Network Definition Exhibit and the Airfield Pavement System Inventory Exhibit, which visually summarize the results of the airfield pavement system inventory analysis.

Table 3.1.5 displays the section-level pavement inventory data, which is based on record documentation provided by the airports and from previous System Updates. The information presented relies on the accuracy and the adequacy of data provided. In some cases, characteristics such as pavement area may be estimated based on aerial interpretation of spatially-projected imagery. Additionally, if the last construction date is unknown, a date of January 1 of the estimated year was assigned to the section. The accuracy of data is appropriate for this network-level planning document. Should the Airport perform rehabilitation work, it is recommended that project-level investigations be performed to support the data accuracy needed for design and construction.

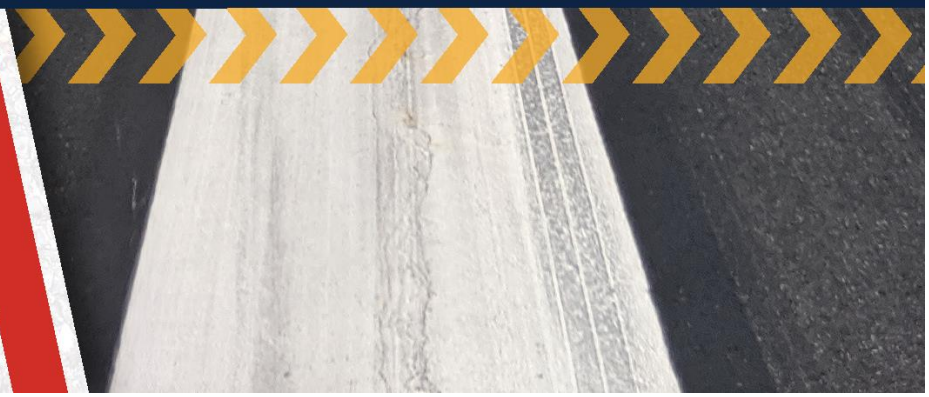
Table 3.1.5: Pavement System Inventory Details

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	Surface Type	Estimate of Last Construction Date
PIE	RW 4-22	Runway	6205	474,873	AAC	1/1/2012
PIE	RW 4-22	Runway	6210	237,436	AAC	1/1/2012
PIE	RW 4-22	Runway	6215	50,072	AAC	1/1/2012
PIE	RW 4-22	Runway	6220	25,036	AAC	1/1/2012
PIE	RW 4-22	Runway	6225	45,300	AC	1/1/2006
PIE	RW 4-22	Runway	6230	22,650	AC	1/1/2006
PIE	RW 4-22	Runway	6235	9,700	AC	4/1/2020
PIE	RW 4-22	Runway	6240	4,850	AC	4/1/2020
PIE	RW 18-36	Runway	6115	135,960	AC	12/1/2020

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	Surface Type	Estimate of Last Construction Date
PIE	RW 18-36	Runway	6120	176,940	AAC	12/1/2020
PIE	RW 18-36	Runway	6155	99,000	AAC	12/1/2020
PIE	RW 18-36	Runway	6156	18,000	AC	12/1/2020
PIE	RW 18-36	Runway	6157	12,000	AC	12/1/2020
PIE	RW 18-36	Runway	6160	148,500	AAC	12/1/2020
PIE	RW 18-36	Runway	6165	40,500	AC	12/1/2020
PIE	RW 18-36	Runway	6170	60,750	AAC	12/1/2020
PIE	RW 18-36	Runway	6175	170,280	AAC	12/1/2020
PIE	RW 18-36	Runway	6180	255,420	AAC	12/1/2020
PIE	RW 18-36	Runway	6185	126,000	AAC	12/1/2020
PIE	RW 18-36	Runway	6190	189,000	AAC	12/1/2020
PIE	RW 18-36	Runway	6196	27,000	AAC	12/1/2020
PIE	FBO CONN	Taxiway	107	3,297	AAC	1/1/1990
PIE	FBO CONN	Taxiway	108	3,361	AC	1/1/1994
PIE	FBO CONN	Taxiway	112	4,221	AAC	1/1/1990
PIE	FBO CONN	Taxiway	114	2,361	AC	1/1/1968
PIE	FBO CONN	Taxiway	117	6,019	AAC	8/1/2016
PIE	FBO CONN	Taxiway	119	3,041	AC	1/1/1968
PIE	FBO CONN	Taxiway	125	4,598	APC	8/1/2016
PIE	FBO CONN	Taxiway	127	12,891	APC	8/1/2016
PIE	TW A	Taxiway	115	224,709	AAC	8/1/2016
PIE	TW A	Taxiway	130	358,395	AAC	8/1/2016
PIE	TW A	Taxiway	132	23,007	AAC	5/1/2019
PIE	TW A	Taxiway	155	6,259	AAC	8/1/2016
PIE	TW A	Taxiway	158	16,692	AAC	8/1/2016
PIE	TW A	Taxiway	160	99,856	AAC	1/1/2017
PIE	TW A1	Taxiway	135	40,056	AAC	8/1/2016
PIE	TW A1	Taxiway	140	14,541	AAC	8/1/2016
PIE	TW A1	Taxiway	145	2,945	AC	12/1/2020
PIE	TW A2	Taxiway	165	60,458	AC	8/1/2016
PIE	TW A3	Taxiway	168	60,311	AC	8/1/2016
PIE	TW A4	Taxiway	170	58,588	AC	8/1/2016
PIE	TW A5	Taxiway	175	56,987	AC	8/1/2016
PIE	TW A6	Taxiway	180	58,658	AC	8/1/2016
PIE	TW A7	Taxiway	162	52,089	AAC	1/1/2017
PIE	TW B	Taxiway	205	6,200	AAC	6/1/2018
PIE	TW B	Taxiway	207	7,750	AAC	6/1/2018
PIE	TW B	Taxiway	210	6,353	AAC	6/1/2018
PIE	TW B	Taxiway	212	18,000	AAC	6/1/2018
PIE	TW B	Taxiway	215	15,387	AC	1/1/2012
PIE	TW B	Taxiway	220	40,670	AC	1/1/1965
PIE	TW B	Taxiway	225	17,624	AC	6/1/2018
PIE	TW F	Taxiway	610	43,041	AC	6/1/2018
PIE	TW F	Taxiway	615	4,165	AAC	12/1/2020
PIE	TW G	Taxiway	1315	19,536	AC	6/1/2018
PIE	TW G	Taxiway	1320	15,822	AAC	6/1/2018
PIE	TW G	Taxiway	1325	199,036	AAC	6/1/2018
PIE	TW G	Taxiway	1340	14,004	AAC	6/1/2018
PIE	TW G1	Taxiway	1330	13,135	AAC	6/1/2018
PIE	TW G1	Taxiway	1335	12,530	AAC	6/1/2018

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	Surface Type	Estimate of Last Construction Date
PIE	TW G2	Taxiway	1005	15,843	AAC	6/1/2018
PIE	TW G2	Taxiway	1010	8,964	AAC	6/1/2018
PIE	TW G3	Taxiway	605	10,930	AAC	1/1/1984
PIE	TW G3	Taxiway	607	8,732	AAC	1/1/2012
PIE	TW H	Taxiway	810	59,729	AAC	1/2/1965
PIE	TW H	Taxiway	815	57,784	AC	8/1/2016
PIE	TW H	Taxiway	820	4,760	AC	1/1/2017
PIE	TW J	Taxiway	905	8,851	AC	4/1/2020
PIE	TW L	Taxiway	1205	22,175	AC	8/1/2016
PIE	TW L	Taxiway	1215	13,483	AC	8/1/2016
PIE	TW L	Taxiway	1245	52,150	AC	8/1/2016
PIE	TW P	Taxiway	1250	27,739	AC	1/1/2016
PIE	TW P	Taxiway	1255	52,339	AC	1/1/2016
PIE	TW Q	Taxiway	1705	4,449	AAC	6/1/2018
PIE	TW Q	Taxiway	1710	3,632	AC	6/1/2018
PIE	TW T	Taxiway	2010	12,963	AAC	6/1/2018
PIE	TW T	Taxiway	2020	14,337	AAC	6/1/2018
PIE	TW T	Taxiway	2045	16,549	AAC	8/1/2016
PIE	TW T	Taxiway	2050	149,440	AAC	6/1/2018
PIE	TL T-HANG	Taxilane	1120	1,346	AC	1/1/1984
PIE	TL T-HANG	Taxilane	1125	1,472	AC	1/1/1984
PIE	AP MAIN	Apron	4103	122,390	PCC	1/1/2023
PIE	AP MAIN	Apron	4105	40,910	APC	1/2/2003
PIE	AP MAIN	Apron	4107	220,315	PCC	1/1/2016
PIE	AP MAIN	Apron	4110	56,000	APC	6/1/2018
PIE	AP MAIN	Apron	4123	43,794	APC	6/1/2018
PIE	AP MAIN	Apron	4150	14,083	AAC	6/1/2018
PIE	AP MAIN	Apron	4155	33,689	AAC	1/1/2003
PIE	AP MAIN	Apron	4157	92,541	AAC	8/1/2016
PIE	AP MAIN	Apron	4160	59,640	PCC	1/1/2016
PIE	AP MAIN	Apron	4165	66,649	PCC	1/1/2012
PIE	AP MAIN	Apron	4170	18,816	AAC	8/1/2016
PIE	AP MAIN	Apron	4175	14,910	PCC	1/1/1942
PIE	AP MAIN	Apron	4176	3,573	AC	12/25/1955
PIE	AP MAIN	Apron	4177	20,899	APC	8/1/2016
PIE	AP MAIN	Apron	4178	59,522	APC	1/1/2013
PIE	AP MAIN	Apron	4179	77,111	APC	10/1/2011
PIE	AP MAIN	Apron	4180	126,695	AAC	1/2/1968
PIE	AP MAIN	Apron	4183	39,947	AAC	1/1/2013
PIE	AP MAIN	Apron	4185	12,820	APC	1/1/2013
PIE	AP MAIN	Apron	4190	18,650	PCC	1/1/1942
PIE	AP MAIN	Apron	4195	11,250	PCC	1/1/1942
PIE	AP MAIN	Apron	4198	18,579	PCC	1/1/2003
PIE	AP MAIN	Apron	4199	25,200	PCC	1/1/2023

Chapter 4: Airfield Pavement Condition Analysis



Chapter 4 – Airfield Pavement Condition Analysis

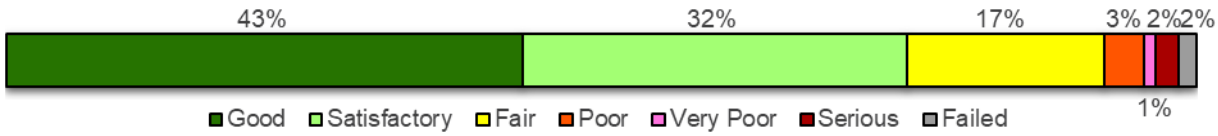
The Pavement Condition Index (PCI) provides insight to possible causes of deterioration to help support pavement maintenance and rehabilitation planning. Distress type, severity, and extent are required in the computation of a PCI value. The PCI method of pavement condition evaluation is strictly a visual review of surface condition, also referred to as a functional evaluation. Further evaluation of pavement conditions may be necessary, such as structural evaluation, for design- and/or project-level determination of pavement rehabilitation needs.

4.1 Airfield Pavement Condition Index

4.1.1 Network-Level Analysis

The following figure, **Figure 4.1.1**, summarizes the network-level pavement condition analysis based on the most recent survey results. On a network level, approximately 75% of inspected pavements are in Good or Satisfactory condition. Presently, roughly 17% of inspected pavements are in Fair condition and the remaining 8% of inspected pavements are in Poor or worse condition.

Figure 4.1.1: Current Condition – Overall Network



4.1.2 Branch-Level Analysis

The following **Figures 4.1.2 (a)-(e)** summarize branch-level pavement conditions according to the most recent PCI assessment results.

Figure 4.1.2 (a): Current Condition Summary – Branch-Level

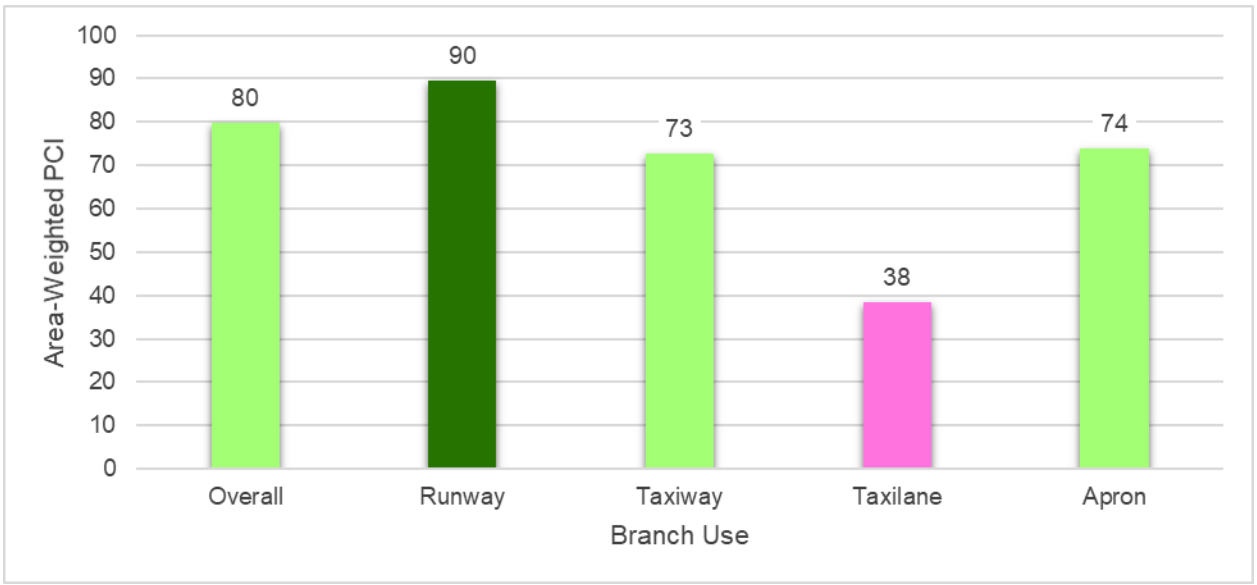


Figure 4.1.2 (b): Current Condition – Runway

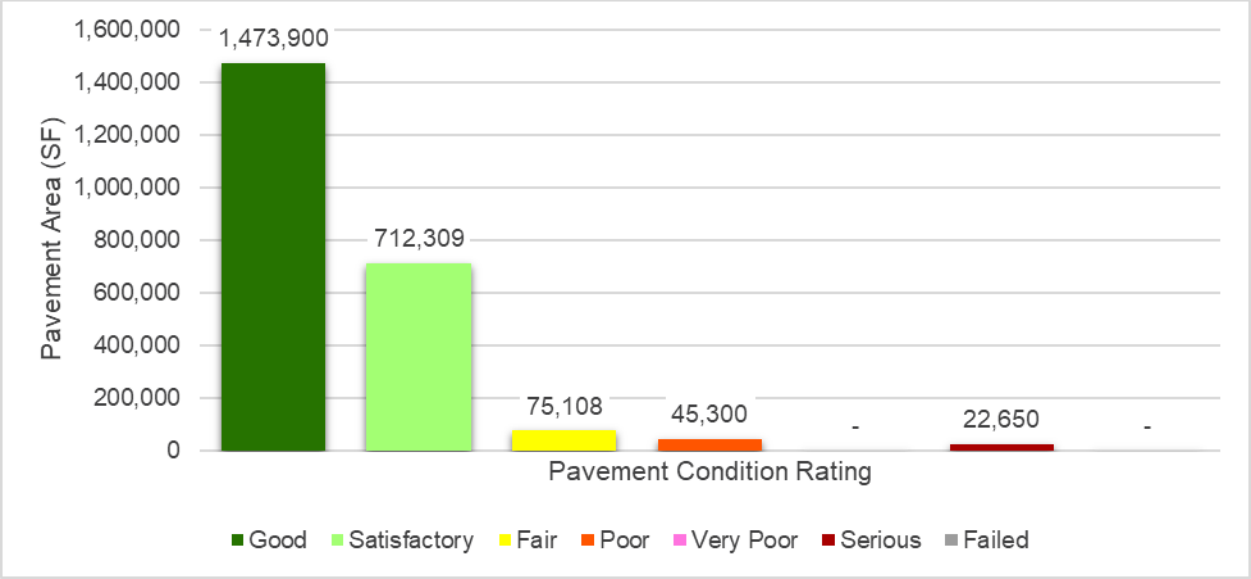


Figure 4.1.2 (c): Current Condition – Taxiway

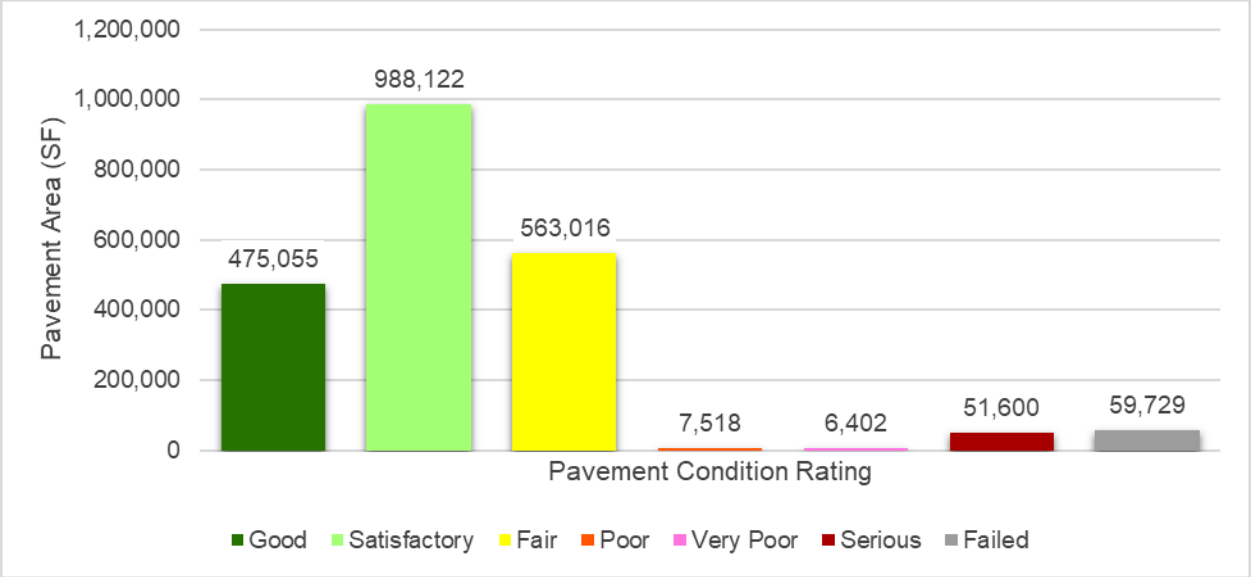


Figure 4.1.2 (d): Current Condition – Taxilane

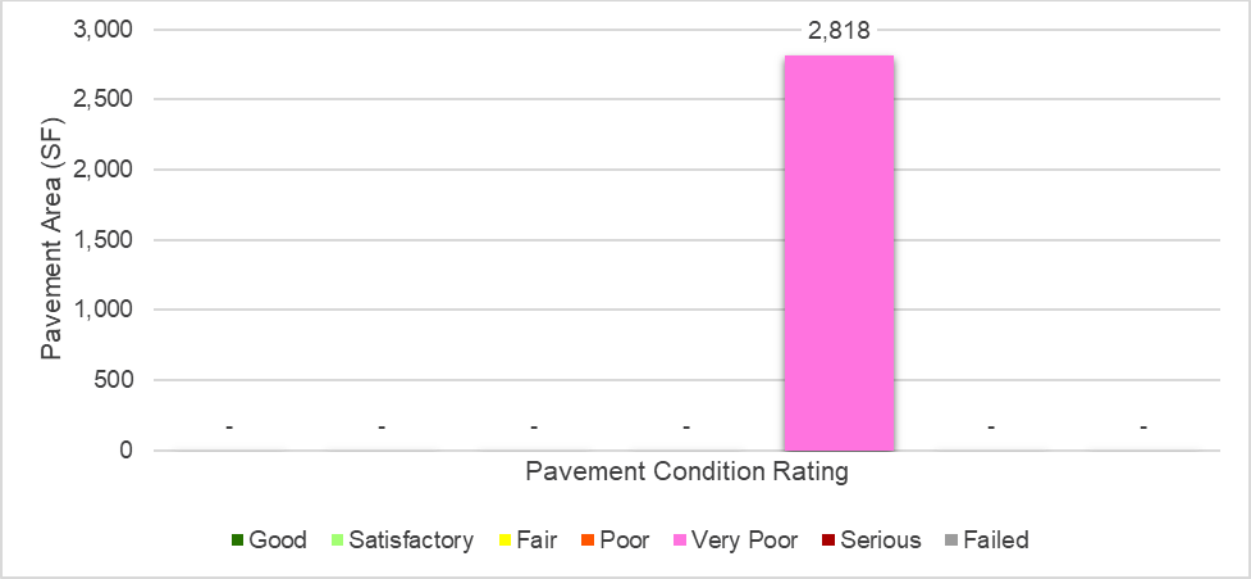


Figure 4.1.2 (e): Current Condition – Apron

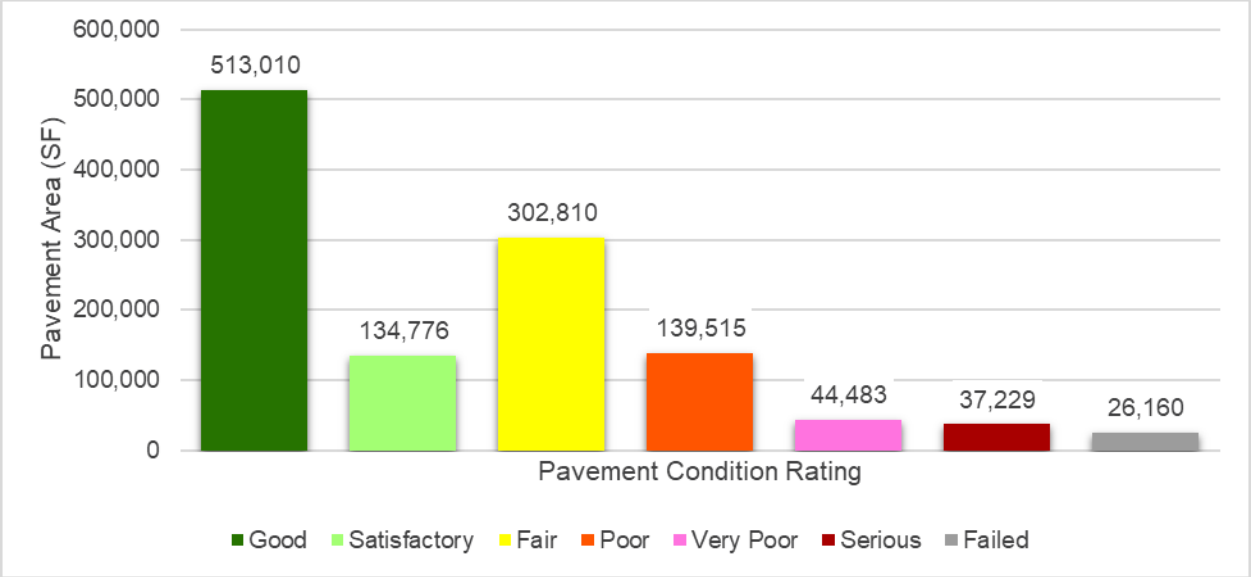


Table 4.1.2 details the branch-level condition for each airfield pavement branch.

Table 4.1.2: Current Condition Summary – Branch-Level

Branch ID	Branch Use	Number of Sections	Branch Area (SF)	Area-Weighted Avg PCI	Condition Rating
RW 4-22	Runway	8	869,917	72	Satisfactory
RW 18-36	Runway	13	1,459,350	100	Good
FBO CONN	Taxiway	8	39,789	62	Fair
TW A	Taxiway	6	728,918	71	Satisfactory
TW A1	Taxiway	3	57,542	67	Fair
TW A2	Taxiway	1	60,458	74	Satisfactory
TW A3	Taxiway	1	60,311	77	Satisfactory
TW A4	Taxiway	1	58,588	85	Satisfactory
TW A5	Taxiway	1	56,987	82	Satisfactory
TW A6	Taxiway	1	58,658	86	Good
TW A7	Taxiway	1	52,089	83	Satisfactory
TW B	Taxiway	7	111,984	61	Fair
TW F	Taxiway	2	47,206	78	Satisfactory
TW G	Taxiway	4	248,398	66	Fair
TW G1	Taxiway	2	25,665	64	Fair
TW G2	Taxiway	2	24,807	89	Good
TW G3	Taxiway	2	19,662	52	Poor
TW H	Taxiway	3	122,273	47	Poor
TW J	Taxiway	1	8,851	100	Good
TW L	Taxiway	3	87,808	78	Satisfactory
TW P	Taxiway	2	80,078	89	Good
TW Q	Taxiway	2	8,081	94	Good
TW T	Taxiway	4	193,289	88	Good
TL T-HANG	Taxilane	2	2,818	38	Very Poor
AP MAIN	Apron	23	1,197,983	74	Satisfactory

4.1.3 Section-Level Analysis

Table 4.1.3 provides each pavement section's area-weighted average PCI and the percent of distress related to load, climate, and other factors. The causes of condition deterioration help inform maintenance, repair, and rehabilitation decisions. For example, load-related distress can indicate that the pavement is reaching the end of its structural design life and the selected rehabilitation treatment should include either strengthening or reconstruction. **Figure 4.1.3** provides a technical exhibit that graphically depicts PCI values and ratings determined from this SAPMP System Update.

Pavement facilities that have been reconstructed within the past 24 months, or are anticipated for reconstruction within the next 24 months, may have been omitted from this assessment. Pavement that has received major rehabilitation will be set to a PCI of 100 for this analysis.

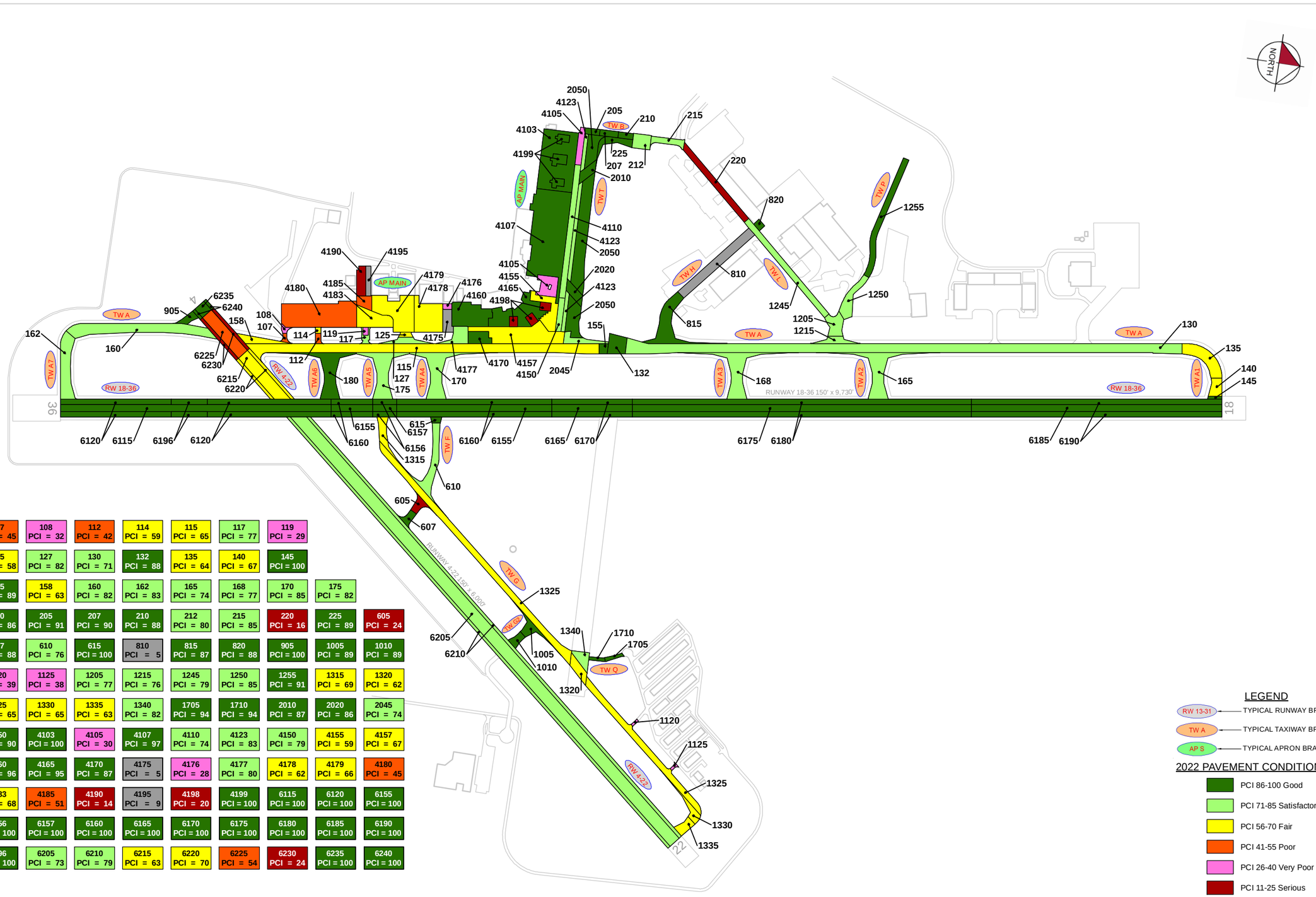
Table 4.1.3: Latest Pavement Condition Index Summary – Section-Level

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	Surface	PCI	Condition Rating	PCI % Climate	PCI % Load	PCI % Other	Sample Units Inspected	Total Sample Units in Section
PIE	RW 4-22	Runway	6205	474,873	AAC	73	Satisfactory	81	17	2	20	95
PIE	RW 4-22	Runway	6210	237,436	AAC	79	Satisfactory	100	0	0	10	48
PIE	RW 4-22	Runway	6215	50,072	AAC	63	Fair	82	14	4	3	10
PIE	RW 4-22	Runway	6220	25,036	AAC	70	Fair	100	0	0	2	6
PIE	RW 4-22	Runway	6225	45,300	AC	54	Poor	17	47	36	3	9
PIE	RW 4-22	Runway	6230	22,650	AC	24	Serious	20	34	46	1	4
PIE	RW 4-22	Runway	6235	9,700	AC	100	Good	0	0	0	0	0
PIE	RW 4-22	Runway	6240	4,850	AC	100	Good	0	0	0	0	0
PIE	RW 18-36	Runway	6115	135,960	AC	100	Good	0	0	0	0	0
PIE	RW 18-36	Runway	6120	176,940	AAC	100	Good	0	0	0	0	0
PIE	RW 18-36	Runway	6155	99,000	AAC	100	Good	0	0	0	0	0
PIE	RW 18-36	Runway	6156	18,000	AC	100	Good	0	0	0	0	0
PIE	RW 18-36	Runway	6157	12,000	AC	100	Good	0	0	0	0	0
PIE	RW 18-36	Runway	6160	148,500	AAC	100	Good	0	0	0	0	0
PIE	RW 18-36	Runway	6165	40,500	AC	100	Good	0	0	0	0	0
PIE	RW 18-36	Runway	6170	60,750	AAC	100	Good	0	0	0	0	0
PIE	RW 18-36	Runway	6175	170,280	AAC	100	Good	0	0	0	0	0
PIE	RW 18-36	Runway	6180	255,420	AAC	100	Good	0	0	0	0	0
PIE	RW 18-36	Runway	6185	126,000	AAC	100	Good	0	0	0	0	0
PIE	RW 18-36	Runway	6190	189,000	AAC	100	Good	0	0	0	0	0
PIE	RW 18-36	Runway	6196	27,000	AAC	100	Good	0	0	0	0	0
PIE	FBO CONN	Taxiway	107	3,297	AAC	45	Poor	74	18	8	1	1
PIE	FBO CONN	Taxiway	108	3,361	AC	32	Very Poor	85	15	0	1	1
PIE	FBO CONN	Taxiway	112	4,221	AAC	42	Poor	56	44	0	1	1
PIE	FBO CONN	Taxiway	114	2,361	AC	59	Fair	77	0	23	1	1
PIE	FBO CONN	Taxiway	117	6,019	AAC	77	Satisfactory	100	0	0	1	1
PIE	FBO CONN	Taxiway	119	3,041	AC	29	Very Poor	36	57	7	1	1
PIE	FBO CONN	Taxiway	125	4,598	APC	58	Fair	72	0	28	1	1
PIE	FBO CONN	Taxiway	127	12,891	APC	82	Satisfactory	88	0	12	1	2
PIE	TW A	Taxiway	115	224,709	AAC	65	Fair	49	44	7	8	59
PIE	TW A	Taxiway	130	358,395	AAC	71	Satisfactory	93	0	7	10	93
PIE	TW A	Taxiway	132	23,007	AAC	88	Good	82	0	18	1	4
PIE	TW A	Taxiway	155	6,259	AAC	89	Good	100	0	0	1	1
PIE	TW A	Taxiway	158	16,692	AAC	63	Fair	29	71	0	1	3
PIE	TW A	Taxiway	160	99,856	AAC	82	Satisfactory	100	0	0	3	24
PIE	TW A1	Taxiway	135	40,056	AAC	64	Fair	88	0	12	1	9
PIE	TW A1	Taxiway	140	14,541	AAC	67	Fair	95	0	5	1	3
PIE	TW A1	Taxiway	145	2,945	AC	100	Good	0	0	0	0	0
PIE	TW A2	Taxiway	165	60,458	AC	74	Satisfactory	73	0	27	2	13
PIE	TW A3	Taxiway	168	60,311	AC	77	Satisfactory	96	0	4	2	13
PIE	TW A4	Taxiway	170	58,588	AC	85	Satisfactory	100	0	0	2	12
PIE	TW A5	Taxiway	175	56,987	AC	82	Satisfactory	47	53	0	2	11
PIE	TW A6	Taxiway	180	58,658	AC	86	Good	100	0	0	2	12

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	Surface	PCI	Condition Rating	PCI % Climate	PCI % Load	PCI % Other	Sample Units Inspected	Total Sample Units in Section
PIE	TW A7	Taxiway	162	52,089	AAC	83	Satisfactory	57	0	43	2	11
PIE	TW B	Taxiway	205	6,200	AAC	91	Good	89	0	11	1	1
PIE	TW B	Taxiway	207	7,750	AAC	90	Good	100	0	0	1	2
PIE	TW B	Taxiway	210	6,353	AAC	88	Good	88	0	12	1	1
PIE	TW B	Taxiway	212	18,000	AAC	80	Satisfactory	87	0	13	1	3
PIE	TW B	Taxiway	215	15,387	AC	85	Satisfactory	98	0	2	1	3
PIE	TW B	Taxiway	220	40,670	AC	16	Serious	39	61	0	2	8
PIE	TW B	Taxiway	225	17,624	AC	89	Good	100	0	0	1	3
PIE	TW F	Taxiway	610	43,041	AC	76	Satisfactory	100	0	0	1	8
PIE	TW F	Taxiway	615	4,165	AAC	100	Good	0	0	0	0	0
PIE	TW G	Taxiway	1315	19,536	AC	69	Fair	18	82	0	1	3
PIE	TW G	Taxiway	1320	15,822	AAC	62	Fair	95	0	5	1	3
PIE	TW G	Taxiway	1325	199,036	AAC	65	Fair	100	0	0	5	39
PIE	TW G	Taxiway	1340	14,004	AAC	82	Satisfactory	100	0	0	1	3
PIE	TW G1	Taxiway	1330	13,135	AAC	65	Fair	60	40	0	1	3
PIE	TW G1	Taxiway	1335	12,530	AAC	63	Fair	29	71	0	1	3
PIE	TW G2	Taxiway	1005	15,843	AAC	89	Good	100	0	0	1	3
PIE	TW G2	Taxiway	1010	8,964	AAC	89	Good	100	0	0	1	2
PIE	TW G3	Taxiway	605	10,930	AAC	24	Serious	67	18	15	1	2
PIE	TW G3	Taxiway	607	8,732	AAC	88	Good	100	0	0	1	2
PIE	TW H	Taxiway	810	59,729	AAC	5	Failed	45	55	0	3	16
PIE	TW H	Taxiway	815	57,784	AC	87	Good	100	0	0	3	12
PIE	TW H	Taxiway	820	4,760	AC	88	Good	100	0	0	1	1
PIE	TW J	Taxiway	905	8,851	AC	100	Good	0	0	0	0	0
PIE	TW L	Taxiway	1205	22,175	AC	77	Satisfactory	95	0	5	1	5
PIE	TW L	Taxiway	1215	13,483	AC	76	Satisfactory	100	0	0	1	3
PIE	TW L	Taxiway	1245	52,150	AC	79	Satisfactory	93	0	7	2	11
PIE	TW P	Taxiway	1250	27,739	AC	85	Satisfactory	100	0	0	1	6
PIE	TW P	Taxiway	1255	52,339	AC	91	Good	100	0	0	2	11
PIE	TW Q	Taxiway	1705	4,449	AAC	94	Good	100	0	0	1	1
PIE	TW Q	Taxiway	1710	3,632	AC	94	Good	100	0	0	1	1
PIE	TW T	Taxiway	2010	12,963	AAC	87	Good	100	0	0	1	3
PIE	TW T	Taxiway	2020	14,337	AAC	86	Good	100	0	0	1	4
PIE	TW T	Taxiway	2045	16,549	AAC	74	Satisfactory	91	0	9	1	4
PIE	TW T	Taxiway	2050	149,440	AAC	90	Good	100	0	0	5	32
PIE	TL T-HANG	Taxilane	1120	1,346	AC	39	Very Poor	100	0	0	1	1
PIE	TL T-HANG	Taxilane	1125	1,472	AC	38	Very Poor	100	0	0	1	1
PIE	AP MAIN	Apron	4103	122,390	PCC	100	Good	0	0	0	0	0
PIE	AP MAIN	Apron	4105	40,910	APC	30	Very Poor	93	0	7	2	8
PIE	AP MAIN	Apron	4107	220,315	PCC	97	Good	0	0	100	3	30
PIE	AP MAIN	Apron	4110	56,000	APC	74	Satisfactory	75	0	25	3	11
PIE	AP MAIN	Apron	4123	43,794	APC	83	Satisfactory	91	0	9	2	11
PIE	AP MAIN	Apron	4150	14,083	AAC	79	Satisfactory	100	0	0	1	3
PIE	AP MAIN	Apron	4155	33,689	AAC	59	Fair	87	0	13	2	7
PIE	AP MAIN	Apron	4157	92,541	AAC	67	Fair	87	0	13	4	19
PIE	AP MAIN	Apron	4160	59,640	PCC	96	Good	0	0	100	2	11

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	Surface	PCI	Condition Rating	PCI % Climate	PCI % Load	PCI % Other	Sample Units Inspected	Total Sample Units in Section
PIE	AP MAIN	Apron	4165	66,649	PCC	95	Good	0	57	43	2	14
PIE	AP MAIN	Apron	4170	18,816	AAC	87	Good	100	0	0	1	4
PIE	AP MAIN	Apron	4175	14,910	PCC	5	Failed	7	59	34	1	2
PIE	AP MAIN	Apron	4176	3,573	AC	28	Very Poor	67	17	16	1	1
PIE	AP MAIN	Apron	4177	20,899	APC	80	Satisfactory	75	0	25	1	4
PIE	AP MAIN	Apron	4178	59,522	APC	62	Fair	97	0	3	2	11
PIE	AP MAIN	Apron	4179	77,111	APC	66	Fair	94	0	6	2	15
PIE	AP MAIN	Apron	4180	126,695	AAC	45	Poor	82	17	1	3	25
PIE	AP MAIN	Apron	4183	39,947	AAC	68	Fair	97	0	3	1	8
PIE	AP MAIN	Apron	4185	12,820	APC	51	Poor	100	0	0	1	3
PIE	AP MAIN	Apron	4190	18,650	PCC	14	Serious	9	70	21	1	3
PIE	AP MAIN	Apron	4195	11,250	PCC	9	Failed	8	83	9	1	2
PIE	AP MAIN	Apron	4198	18,579	PCC	20	Serious	0	86	14	1	3
PIE	AP MAIN	Apron	4199	25,200	PCC	100	Good	0	0	0	0	0

** Zero (0) Sample Units Inspected signifies that the pavement section was not inspected during this SAPMP System Update due to recent construction projects. These sections correlate with the gray sections on the Network Definition Exhibit.*



107 PCI = 45	108 PCI = 32	112 PCI = 42	114 PCI = 59	115 PCI = 65	117 PCI = 77	119 PCI = 29
125 PCI = 58	127 PCI = 82	130 PCI = 71	132 PCI = 88	135 PCI = 64	140 PCI = 67	145 PCI = 100
155 PCI = 89	158 PCI = 63	160 PCI = 82	162 PCI = 83	165 PCI = 74	168 PCI = 77	170 PCI = 85	175 PCI = 82
180 PCI = 86	205 PCI = 91	207 PCI = 90	210 PCI = 88	212 PCI = 80	215 PCI = 85	220 PCI = 16	225 PCI = 89	605 PCI = 24
607 PCI = 88	610 PCI = 76	615 PCI = 100	810 PCI = 5	815 PCI = 87	820 PCI = 88	905 PCI = 100	1005 PCI = 89	1010 PCI = 89
1120 PCI = 39	1125 PCI = 38	1205 PCI = 77	1215 PCI = 76	1245 PCI = 79	1250 PCI = 85	1255 PCI = 91	1315 PCI = 69	1320 PCI = 62
1325 PCI = 65	1330 PCI = 65	1335 PCI = 63	1340 PCI = 82	1705 PCI = 94	1710 PCI = 94	2010 PCI = 87	2020 PCI = 86	2045 PCI = 74
2050 PCI = 90	4103 PCI = 100	4105 PCI = 30	4107 PCI = 97	4110 PCI = 74	4123 PCI = 83	4150 PCI = 79	4155 PCI = 59	4157 PCI = 67
4160 PCI = 96	4165 PCI = 95	4170 PCI = 87	4175 PCI = 5	4176 PCI = 28	4177 PCI = 80	4178 PCI = 62	4179 PCI = 66	4180 PCI = 45
4183 PCI = 68	4185 PCI = 51	4190 PCI = 14	4195 PCI = 9	4198 PCI = 20	4199 PCI = 100	6115 PCI = 100	6120 PCI = 100	6155 PCI = 100
6156 PCI = 100	6157 PCI = 100	6160 PCI = 100	6165 PCI = 100	6170 PCI = 100	6175 PCI = 100	6180 PCI = 100	6185 PCI = 100	6190 PCI = 100
6196 PCI = 100	6205 PCI = 73	6210 PCI = 79	6215 PCI = 63	6220 PCI = 70	6225 PCI = 54	6230 PCI = 24	6235 PCI = 100	6240 PCI = 100

LEGEND

- RW 13-31 — TYPICAL RUNWAY BRANCH ID
- TW A — TYPICAL TAXIWAY BRANCH ID
- AP S — TYPICAL APRON BRANCH ID

2022 PAVEMENT CONDITION INDEX

- PCI 86-100 Good
- PCI 71-85 Satisfactory
- PCI 56-70 Fair
- PCI 41-55 Poor
- PCI 26-40 Very Poor
- PCI 11-25 Serious
- PCI 0-10 Failed

"SECTION ID"
"PCI VALUE"

RUNWAY LENGTHS DEPICTED IN THIS DRAWING ARE FOR PAVEMENT MANAGEMENT PURPOSES ONLY AND MAY NOT MATCH PUBLISHED RUNWAY LENGTHS. DRAWING NOT TO SCALE.

PIE

AIRFIELD PAVEMENT CONDITION INDEX EXHIBIT

Statewide Airfield Pavement Management Program

ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

FDOT

2022

4.2 Summary of Pavement Condition Evaluation Results

4.2.1 Network-Level Observations

The PCI assessment for St. Pete-Clearwater International Airport (PIE) was performed in April 2022. The overall area-weighted average PCI value of the network was 80, representing a condition rating of Satisfactory. A large portion of the airfield pavement was not inspected due to recent construction in 2020. These areas include the entirety of Runway 18-36 and a portion of Runway 4-22, Taxiway A1, Taxiway F, and Taxiway J. Additionally, the western most portion of the Main Apron was not inspected due to the upcoming PCC reconstruction project in 2023.

Based on the FAA 5010 Report as of 04/22/2021, the Airport has reported 131,763 operations for 12 months ending 12/31/2021.

4.2.2 Branch-Level Observations

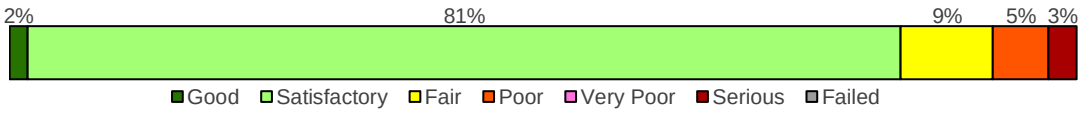
The following branch-level observations are a summary of select pavement facilities identified during the PCI assessment, including a discussion of general conditions and branch characteristics. The summary may not include all branches and/or sections within the Airport's airfield pavement network. Representative distress photographs of airfield pavements are presented in **Appendix D**. "Vicinity" photos refer to the approximate boundaries of an inspected sample unit within the section and provide an overview of the section condition but are not focused on a specific distress. The Re-inspection Report found in **Appendix E** provides listings of each sample unit and distress.

Runways

RW 4-22

Branch ID	Branch Use	Number of Sections	Branch Area (SF)	Branch Area-Weighted Avg PCI	Branch Condition Rating
RW 4-22	RUNWAY	8	869,917	72	Satisfactory

The following bar graph shows proportional distribution (as % of area within branch) of condition categories among sections within the branch. Given the individual section data shown in the subsequent table, the distribution is as follows: 2% Good (86-100 PCI), 81% Satisfactory (71-85 PCI), 9% Fair (56-70 PCI), 5% Poor (41-55 PCI), 3% Serious (11-25 PCI).



Section ID	Surface Type	Section Area (SF)	PCI	Condition Rating
6205	AAC	474,873	73	Satisfactory
6210	AAC	237,436	79	Satisfactory
6215	AAC	50,072	63	Fair
6220	AAC	25,036	70	Fair

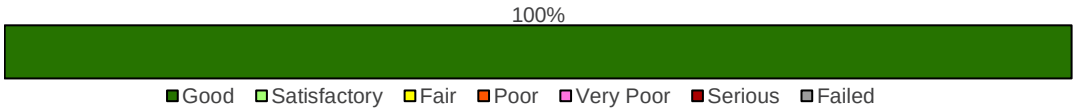
Section ID	Surface Type	Section Area (SF)	PCI	Condition Rating
6225	AC	45,300	54	Poor
6230	AC	22,650	24	Serious
6235	AC	9,700	100	Good
6240	AC	4,850	100	Good

RW 4-22 consists of 8 flexible pavement sections, totaling 869,917 sf. The last major construction dates range from 2006 to 2020, resulting in an area-weighted average age at inspection of 11 years old. Overall, RW 4-22 is in Satisfactory condition with an area-weighted average PCI of 72.

RW 18-36

Branch ID	Branch Use	Number of Sections	Branch Area (SF)	Branch Area-Weighted Avg PCI	Branch Condition Rating
RW 18-36	RUNWAY	13	1,459,350	100	Good

The following bar graph shows proportional distribution (as % of area within branch) of condition categories among sections within the branch. Given the individual section data shown in the subsequent table, the distribution is as follows: 100% Good (86-100 PCI).



Section ID	Surface Type	Section Area (SF)	PCI	Condition Rating
6115	AC	135,960	100	Good
6120	AAC	176,940	100	Good
6155	AAC	99,000	100	Good
6156	AC	18,000	100	Good
6157	AC	12,000	100	Good
6160	AAC	148,500	100	Good
6165	AC	40,500	100	Good
6170	AAC	60,750	100	Good
6175	AAC	170,280	100	Good
6180	AAC	255,420	100	Good
6185	AAC	126,000	100	Good
6190	AAC	189,000	100	Good
6196	AAC	27,000	100	Good

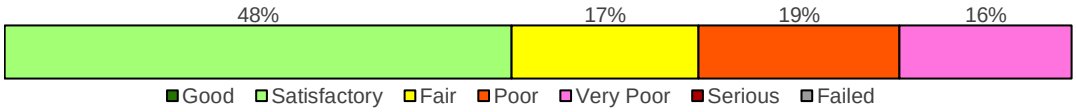
RW 18-36 consists of 13 flexible pavement sections, totaling 1,459,350 sf. The last major construction date for the branch was 2020. Overall, RW 18-36 is in Good condition with an area-weighted average PCI of 100.

Taxiways

FBO CONN

Branch ID	Branch Use	Number of Sections	Branch Area (SF)	Branch Area-Weighted Avg PCI	Branch Condition Rating
FBO CONN	TAXIWAY	8	39,789	62	Fair

The following bar graph shows proportional distribution (as % of area within branch) of condition categories among sections within the branch. Given the individual section data shown in the subsequent table, the distribution is as follows: 48% Satisfactory (71-85 PCI), 17% Fair (56-70 PCI), 19% Poor (41-55 PCI), 16% Very Poor (26-40 PCI).



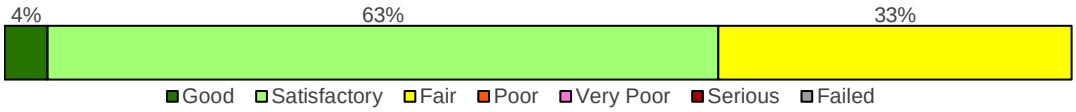
Section ID	Surface Type	Section Area (SF)	PCI	Condition Rating
107	AAC	3,297	45	Poor
108	AC	3,361	32	Very Poor
112	AAC	4,221	42	Poor
114	AC	2,361	59	Fair
117	AAC	6,019	77	Satisfactory
119	AC	3,041	29	Very Poor
125	APC	4,598	58	Fair
127	APC	12,891	82	Satisfactory

FBO CONN consists of 8 flexible pavement sections, totaling 39,789 sf. The last major construction dates range from 1968 to 2016, resulting in an area-weighted average age at inspection of 19 years old. Overall, FBO CONN is in Fair condition with an area-weighted average PCI of 62.

TW A

Branch ID	Branch Use	Number of Sections	Branch Area (SF)	Branch Area-Weighted Avg PCI	Branch Condition Rating
TW A	TAXIWAY	6	728,918	71	Satisfactory

The following bar graph shows proportional distribution (as % of area within branch) of condition categories among sections within the branch. Given the individual section data shown in the subsequent table, the distribution is as follows: 4% Good (86-100 PCI), 63% Satisfactory (71-85 PCI), 33% Fair (56-70 PCI).



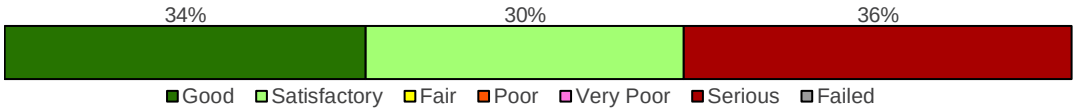
Section ID	Surface Type	Section Area (SF)	PCI	Condition Rating
115	AAC	224,709	65	Fair
130	AAC	358,395	71	Satisfactory
132	AAC	23,007	88	Good
155	AAC	6,259	89	Good
158	AAC	16,692	63	Fair
160	AAC	99,856	82	Satisfactory

TW A consists of 6 flexible pavement sections, totaling 728,918 sf. The last major construction dates range from 2016 to 2019, resulting in an area-weighted average age at inspection of 6 years old. Overall, TW A is in Satisfactory condition with an area-weighted average PCI of 71.

TW B

Branch ID	Branch Use	Number of Sections	Branch Area (SF)	Branch Area-Weighted Avg PCI	Branch Condition Rating
TW B	TAXIWAY	7	111,984	61	Fair

The following bar graph shows proportional distribution (as % of area within branch) of condition categories among sections within the branch. Given the individual section data shown in the subsequent table, the distribution is as follows: 34% Good (86-100 PCI), 30% Satisfactory (71-85 PCI), 36% Serious (11-25 PCI).



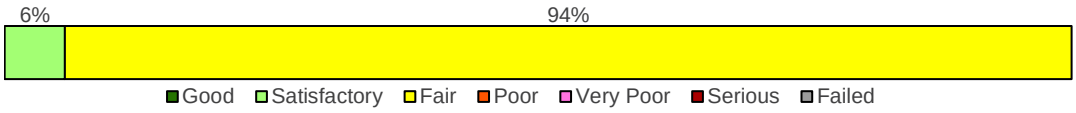
Section ID	Surface Type	Section Area (SF)	PCI	Condition Rating
205	AAC	6,200	91	Good
207	AAC	7,750	90	Good
210	AAC	6,353	88	Good
212	AAC	18,000	80	Satisfactory
215	AC	15,387	85	Satisfactory
220	AC	40,670	16	Serious
225	AC	17,624	89	Good

TW B consists of 7 flexible pavement sections, totaling 111,984 sf. The last major construction dates range from 1965 to 2018, resulting in an area-weighted average age at inspection of 24 years old. Overall, TW B is in Fair condition with an area-weighted average PCI of 61.

TW G

Branch ID	Branch Use	Number of Sections	Branch Area (SF)	Branch Area-Weighted Avg PCI	Branch Condition Rating
TW G	TAXIWAY	4	248,398	66	Fair

The following bar graph shows proportional distribution (as % of area within branch) of condition categories among sections within the branch. Given the individual section data shown in the subsequent table, the distribution is as follows: 6% Satisfactory (71-85 PCI), 94% Fair (56-70 PCI).



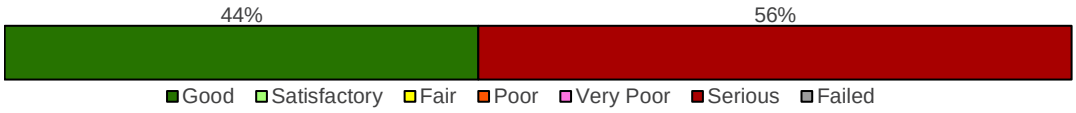
Section ID	Surface Type	Section Area (SF)	PCI	Condition Rating
1315	AC	19,536	69	Fair
1320	AAC	15,822	62	Fair
1325	AAC	199,036	65	Fair
1340	AAC	14,004	82	Satisfactory

TW G consists of 4 flexible pavement sections, totaling 248,398 sf. The last major construction date for the branch was 2018, resulting in an area-weighted average age at inspection of 4 years old. Overall, TW G is in Fair condition with an area-weighted average PCI of 66.

TW G3

Branch ID	Branch Use	Number of Sections	Branch Area (SF)	Branch Area-Weighted Avg PCI	Branch Condition Rating
TW G3	TAXIWAY	2	19,662	52	Poor

The following bar graph shows proportional distribution (as % of area within branch) of condition categories among sections within the branch. Given the individual section data shown in the subsequent table, the distribution is as follows: 44% Good (86-100 PCI), 56% Serious (11-25 PCI).



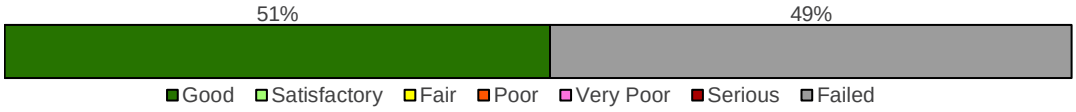
Section ID	Surface Type	Section Area (SF)	PCI	Condition Rating
605	AAC	10,930	24	Serious
607	AAC	8,732	88	Good

TW G3 consists of 2 flexible pavement sections, totaling 19,662 sf. The last major construction dates range from 1984 to 2012, resulting in an area-weighted average age at inspection of 26 years old. Overall, TW G3 is in Poor condition with an area-weighted average PCI of 52.

TW H

Branch ID	Branch Use	Number of Sections	Branch Area (SF)	Branch Area-Weighted Avg PCI	Branch Condition Rating
TW H	TAXIWAY	3	122,273	47	Poor

The following bar graph shows proportional distribution (as % of area within branch) of condition categories among sections within the branch. Given the individual section data shown in the subsequent table, the distribution is as follows: 51% Good (86-100 PCI), 49% Failed (0-10 PCI).



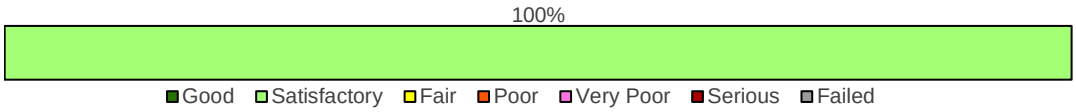
Section ID	Surface Type	Section Area (SF)	PCI	Condition Rating
810	AAC	59,729	5	Failed
815	AC	57,784	87	Good
820	AC	4,760	88	Good

TW H consists of 3 flexible pavement sections, totaling 122,273 sf. The last major construction dates range from 1965 to 2017, resulting in an area-weighted average age at inspection of 31 years old. Overall, TW H is in Poor condition with an area-weighted average PCI of 47.

TW L

Branch ID	Branch Use	Number of Sections	Branch Area (SF)	Branch Area-Weighted Avg PCI	Branch Condition Rating
TW L	TAXIWAY	3	87,808	78	Satisfactory

The following bar graph shows proportional distribution (as % of area within branch) of condition categories among sections within the branch. Given the individual section data shown in the subsequent table, the distribution is as follows: 100% Satisfactory (71-85 PCI).



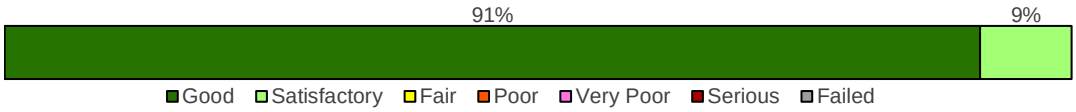
Section ID	Surface Type	Section Area (SF)	PCI	Condition Rating
1205	AC	22,175	77	Satisfactory
1215	AC	13,483	76	Satisfactory
1245	AC	52,150	79	Satisfactory

TW L consists of 3 flexible pavement sections, totaling 87,808 sf. The last major construction date for the branch was 2016, resulting in an area-weighted average age at inspection of 6 years old. Overall, TW L is in Satisfactory condition with an area-weighted average PCI of 78.

TW T

Branch ID	Branch Use	Number of Sections	Branch Area (SF)	Branch Area-Weighted Avg PCI	Branch Condition Rating
TW T	TAXIWAY	4	193,289	88	Good

The following bar graph shows proportional distribution (as % of area within branch) of condition categories among sections within the branch. Given the individual section data shown in the subsequent table, the distribution is as follows: 91% Good (86-100 PCI), 9% Satisfactory (71-85 PCI).



Section ID	Surface Type	Section Area (SF)	PCI	Condition Rating
2010	AAC	12,963	87	Good
2020	AAC	14,337	86	Good
2045	AAC	16,549	74	Satisfactory
2050	AAC	149,440	90	Good

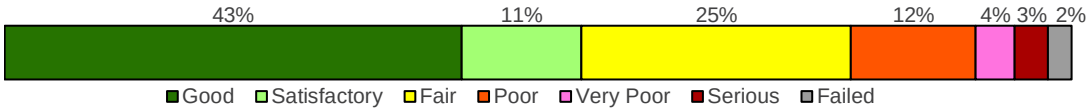
TW T consists of 4 flexible pavement sections, totaling 193,289 sf. The last major construction dates range from 2016 to 2018, resulting in an area-weighted average age at inspection of 4 years old. Overall, TW T is in Good condition with an area-weighted average PCI of 88.

Aprons

AP MAIN

Branch ID	Branch Use	Number of Sections	Branch Area (SF)	Branch Area-Weighted Avg PCI	Branch Condition Rating
AP MAIN	APRON	23	1,197,983	74	Satisfactory

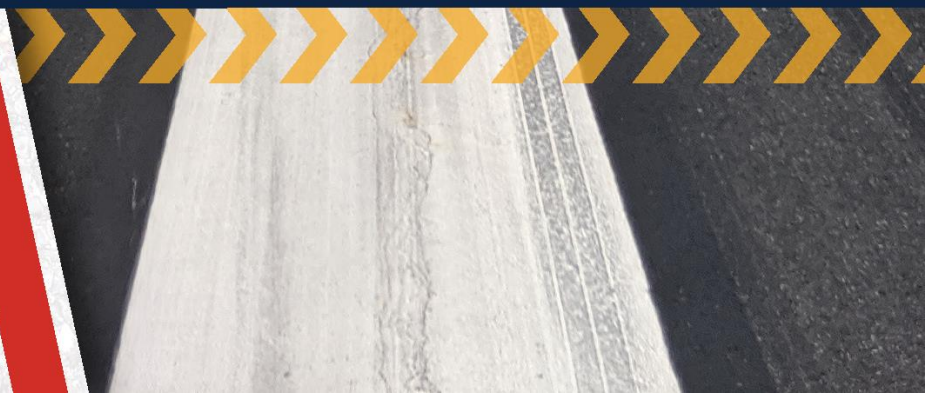
The following bar graph shows proportional distribution (as % of area within branch) of condition categories among sections within the branch. Given the individual section data shown in the subsequent table, the distribution is as follows: 43% Good (86-100 PCI), 11% Satisfactory (71-85 PCI), 25% Fair (56-70 PCI), 12% Poor (41-55 PCI), 4% Very Poor (26-40 PCI), 3% Serious (11-25 PCI), 2% Failed (0-10 PCI).



Section ID	Surface Type	Section Area (SF)	PCI	Condition Rating
4103	PCC	122,390	100	Good
4105	APC	40,910	30	Very Poor
4107	PCC	220,315	97	Good
4110	APC	56,000	74	Satisfactory
4123	APC	43,794	83	Satisfactory
4150	AAC	14,083	79	Satisfactory
4155	AAC	33,689	59	Fair
4157	AAC	92,541	67	Fair
4160	PCC	59,640	96	Good
4165	PCC	66,649	95	Good
4170	AAC	18,816	87	Good
4175	PCC	14,910	5	Failed
4176	AC	3,573	28	Very Poor
4177	APC	20,899	80	Satisfactory
4178	APC	59,522	62	Fair
4179	APC	77,111	66	Fair
4180	AAC	126,695	45	Poor
4183	AAC	39,947	68	Fair
4185	APC	12,820	51	Poor
4190	PCC	18,650	14	Serious
4195	PCC	11,250	9	Failed
4198	PCC	18,579	20	Serious
4199	PCC	25,200	100	Good

AP MAIN consists of 14 flexible and 9 rigid pavement sections, totaling 1,197,983 sf. The last major construction dates range from 1942 to 2023, resulting in an area-weighted average age at inspection of 15 years old. Overall, AP MAIN is in Satisfactory condition with an area-weighted average PCI of 74.

Chapter 5: SAPMP Customization



Chapter 5 – SAPMP Customization

Once the PAVER™ database is populated with inventory and condition data (including PCI and rank), it is further customized with key elements such as network-level attributes, performance models, critical PCI, maintenance policies, and unit costs that are specific to the FDOT SAPMP. Each of these factors play a role in the development of rehabilitation strategies as they help to identify maintenance and rehabilitation needs for long-term management.

The FDOT SAPMP is organized to provide airports with planning-level data and does not intend to preclude the responsible engineer from performing the appropriate level of investigation and analysis in determining the appropriate design details of a pavement rehabilitation. It would not be advisable to solely base design-level rehabilitation without the appropriate level of investigation and determination of pavement deterioration beyond that of a visual functional condition assessment.

5.1 Network-Level Customization

The network-level attribute fields used in the FDOT SAPMP PAVER™ database consist of the Network, Airport Classification, District, FAA ADO Area, Inspection Phase, and Continuing Florida Aviation System Planning Process (CFASPP) Center. Each of these elements are briefly defined below.

- The “Network” field identifies the airport being analyzed;
- The “Airport Classification” field classifies the Airport according to the type and volume of aircraft traffic;
  - “GA” for General Aviation, community airports
  - “RL” for Regional Relievers
  - “PR” for Primary/Commercial airports
- The “District” field identifies the FDOT District to which the Airport belongs;
- The “FAA ADO Area” is an area used by the Orlando ADO to assign airports within those areas to the responsible FAA ADO personnel (planners, engineers, and environmentalists);
- The “Inspection Phase” denotes which phase of the SAPMP the Airport is surveyed (Phase 1 or Phase 2); and
- The “CFASPP Center” identifies which Region or Metropolitan Area of the Continuing Florida Aviation Systems Planning Process an Airport falls within.

5.2 Pavement Condition Forecasts

Pavement performance models, alternatively known as forecast models, prediction curves, or family curves, are developed from past and current distress data, as well as age data. These prediction curves are used to develop forecasts of PCI values that then help determine optimum timing for pavement maintenance and rehabilitation.

5.2.1 Forecasting PCI Considerations

Performance models will continue to be refined as the FDOT updates the SAPMP with subsequent PCI surveys. With the refinement of additional PCI and age data points, the forecasting of pavement conditions will continue to better reflect the performance trends of airfield pavements in the FAS. As a reminder, forecasting of pavement condition for the Airport is intended for planning purposes only. **The estimation of forecasted PCI values gives no assurance of future pavement conditions as PCI values represent an engineering estimation to be used as a planning tool. Forecasted PCI data should not be the sole metric for determining the year in which a project should be planned. Design-level planning should be undertaken by the responsible engineer prior to the development of airfield design plans.** Design-level recommendations for pavement rehabilitation and/or reconstruction will require the appropriate application of the procedures defined in the FAA AC 150/5320-6F.

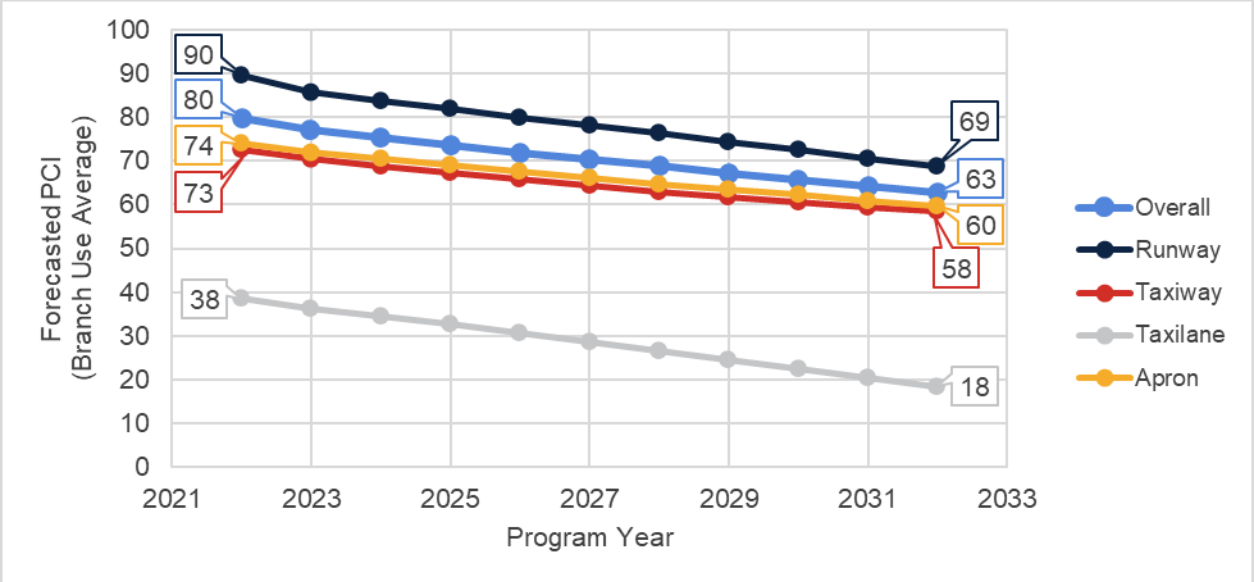
5.2.2 Performance Models

To develop pavement performance models, data for each section is combined into "groups" or "families" according to pavement type, traffic, and functional use. For the FDOT SAPMP, the models were defined for both PCC- and AC-surfaced pavements and further divided according to functional use. Based on average deterioration rates for different pavement types, each pavement section is assigned to a specific deterioration family to forecast the condition over a 10-year period.

5.2.3 Branch-Level Pavement Condition Forecast

Figure 5.2.3 depicts the branch-level pavement condition forecast for each branch use (Runway, Taxiway, Taxilane, and/or Apron) as well as the overall network. The condition forecasts are for a 10-year duration, starting in 2023 through 2032.

Figure 5.2.3: Forecasted Branch-Level Pavement Performance



5.2.4 Section-Level Pavement Condition Forecast

Table 5.2.4 provides section-level details for PCI forecasts. Pavement condition forecasts should be used for planning purposes only, as actual condition of sections is subject to the sensitivities in changes of traffic and maintenance frequency.

Table 5.2.4: Forecasted PCI Values 2023-2032 – Section-Level

Network ID	Branch ID	Section ID	Current PCI	2023	2024	2025	2026	2027	2028	2029	2030	2031	2032
PIE	RW 4-22	6205	73	71	69	67	65	63	61	59	57	55	53
PIE	RW 4-22	6210	79	77	75	73	71	69	67	65	63	61	59
PIE	RW 4-22	6215	63	61	59	57	55	53	51	49	47	45	43
PIE	RW 4-22	6220	70	68	66	64	62	60	58	56	54	52	50
PIE	RW 4-22	6225	54	52	51	49	48	46	45	43	42	40	39
PIE	RW 4-22	6230	24	22	21	19	18	16	15	13	12	10	9
PIE	RW 4-22	6235	100	95	94	92	91	89	88	86	85	83	82
PIE	RW 4-22	6240	100	95	94	92	91	89	88	86	85	83	82
PIE	RW 18-36	6115	100	96	95	93	92	90	89	87	86	84	83
PIE	RW 18-36	6120	100	95	93	91	89	87	85	83	81	80	78
PIE	RW 18-36	6155	100	95	93	91	89	87	85	83	81	80	78
PIE	RW 18-36	6156	100	96	95	93	92	90	89	87	86	84	83
PIE	RW 18-36	6157	100	96	95	93	92	90	89	87	86	84	83
PIE	RW 18-36	6160	100	95	93	91	89	87	85	83	81	80	78
PIE	RW 18-36	6165	100	96	95	93	92	90	89	87	86	84	83
PIE	RW 18-36	6170	100	95	93	91	89	87	85	83	81	80	78
PIE	RW 18-36	6175	100	95	93	91	89	87	85	83	81	80	78
PIE	RW 18-36	6180	100	95	93	91	89	87	85	83	81	80	78
PIE	RW 18-36	6185	100	95	93	91	89	87	85	83	81	80	78
PIE	RW 18-36	6190	100	95	93	91	89	87	85	83	81	80	78
PIE	RW 18-36	6196	100	95	93	91	89	87	85	83	81	80	78
PIE	FBO CONN	107	45	44	43	41	40	38	36	34	32	30	27
PIE	FBO CONN	108	32	29	27	25	23	21	19	17	15	13	11
PIE	FBO CONN	112	42	40	39	37	35	33	30	28	25	21	18
PIE	FBO CONN	114	59	58	57	56	55	55	54	53	52	51	50
PIE	FBO CONN	117	77	75	73	71	69	67	66	64	63	62	60
PIE	FBO CONN	119	29	26	24	22	20	18	16	14	12	10	8
PIE	FBO CONN	125	58	57	56	55	54	54	53	52	52	51	50
PIE	FBO CONN	127	82	79	77	75	73	71	70	68	66	65	63
PIE	TW A	115	65	63	62	61	59	58	57	56	56	55	54
PIE	TW A	130	71	69	67	66	64	63	61	60	59	58	57
PIE	TW A	132	88	85	83	81	79	76	74	73	71	69	67
PIE	TW A	155	89	86	84	82	79	77	75	73	71	70	68
PIE	TW A	158	63	61	60	59	58	57	56	55	54	54	53
PIE	TW A	160	82	79	77	75	73	71	70	68	66	65	63
PIE	TW A1	135	64	62	61	60	59	58	57	56	55	54	53
PIE	TW A1	140	67	65	64	62	61	60	59	58	57	56	55
PIE	TW A1	145	100	94	92	90	88	86	84	83	81	80	78
PIE	TW A2	165	74	72	71	70	69	68	67	66	65	64	63

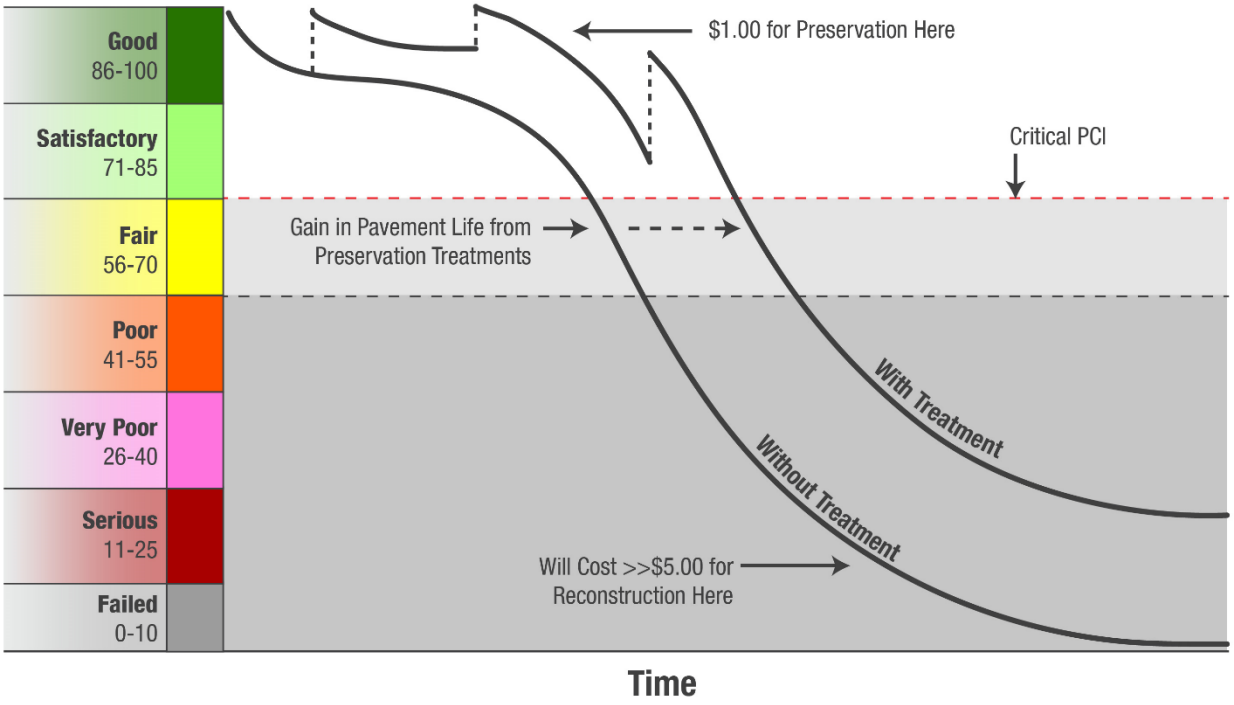
Network ID	Branch ID	Section ID	Current PCI	2023	2024	2025	2026	2027	2028	2029	2030	2031	2032
PIE	TW A3	168	77	75	74	73	72	70	69	68	67	66	65
PIE	TW A4	170	85	83	81	80	78	77	75	74	73	72	71
PIE	TW A5	175	82	80	79	77	76	74	73	72	71	70	69
PIE	TW A6	180	86	84	82	81	79	78	76	75	74	72	71
PIE	TW A7	162	83	80	78	76	74	72	70	69	67	65	64
PIE	TW B	205	91	88	86	83	81	79	77	75	73	71	69
PIE	TW B	207	90	87	85	82	80	78	76	74	72	70	69
PIE	TW B	210	88	85	83	81	79	76	74	73	71	69	67
PIE	TW B	212	80	77	75	73	72	70	68	66	65	63	62
PIE	TW B	215	85	83	81	80	78	77	75	74	73	72	71
PIE	TW B	220	16	13	11	9	7	5	3	1	0	0	0
PIE	TW B	225	89	87	85	83	82	80	78	77	76	74	73
PIE	TW F	610	76	74	73	72	71	70	69	68	67	66	65
PIE	TW F	615	100	94	91	89	86	84	82	80	78	76	74
PIE	TW G	1315	69	68	67	66	65	64	63	62	61	61	60
PIE	TW G	1320	62	60	59	58	57	56	55	55	54	53	52
PIE	TW G	1325	65	63	62	61	59	58	57	56	56	55	54
PIE	TW G	1340	82	79	77	75	73	71	70	68	66	65	63
PIE	TW G1	1330	65	63	62	61	59	58	57	56	56	55	54
PIE	TW G1	1335	63	61	60	59	58	57	56	55	54	54	53
PIE	TW G2	1005	89	86	84	82	79	77	75	73	71	70	68
PIE	TW G2	1010	89	86	84	82	79	77	75	73	71	70	68
PIE	TW G3	605	24	20	16	12	7	2	0	0	0	0	0
PIE	TW G3	607	88	85	83	81	79	76	74	73	71	69	67
PIE	TW H	810	5	0	0	0	0	0	0	0	0	0	0
PIE	TW H	815	87	85	83	81	80	78	77	76	74	73	72
PIE	TW H	820	88	86	84	82	81	79	78	76	75	74	72
PIE	TW J	905	100	93	91	89	87	85	83	82	80	79	77
PIE	TW L	1205	77	75	74	73	72	70	69	68	67	66	65
PIE	TW L	1215	76	74	73	72	71	70	69	68	67	66	65
PIE	TW L	1245	79	77	76	75	73	72	71	70	69	68	67
PIE	TW P	1250	85	83	81	80	78	77	75	74	73	72	71
PIE	TW P	1255	91	89	87	85	83	82	80	78	77	76	74
PIE	TW Q	1705	94	91	89	86	84	82	80	77	75	73	72
PIE	TW Q	1710	94	91	89	88	86	84	82	81	79	78	76
PIE	TW T	2010	87	84	82	80	78	76	74	72	70	68	67
PIE	TW T	2020	86	83	81	79	77	75	73	71	69	68	66
PIE	TW T	2045	74	72	70	68	67	65	64	62	61	60	59
PIE	TW T	2050	90	87	85	82	80	78	76	74	72	70	69
PIE	TL T-HANG	1120	39	37	35	33	31	29	27	25	23	21	19
PIE	TL T-HANG	1125	38	36	34	32	30	28	26	24	22	20	18
PIE	AP MAIN	4103	100	99	98	97	96	94	93	92	91	90	89
PIE	AP MAIN	4105	30	27	25	22	20	17	14	11	9	6	3
PIE	AP MAIN	4107	97	96	94	93	92	91	90	89	89	88	87
PIE	AP MAIN	4110	74	72	70	68	66	65	63	62	60	59	57
PIE	AP MAIN	4123	83	80	78	76	74	72	70	68	66	65	63

Network ID	Branch ID	Section ID	Current PCI	2023	2024	2025	2026	2027	2028	2029	2030	2031	2032
PIE	AP MAIN	4150	79	76	74	72	70	69	67	65	64	62	61
PIE	AP MAIN	4155	59	57	56	54	53	52	50	49	47	46	44
PIE	AP MAIN	4157	67	65	63	62	60	59	57	56	55	53	52
PIE	AP MAIN	4160	96	95	93	92	91	90	90	89	88	87	86
PIE	AP MAIN	4165	95	94	93	92	91	90	89	88	87	86	86
PIE	AP MAIN	4170	87	84	81	79	77	75	73	71	69	67	66
PIE	AP MAIN	4175	5	1	0	0	0	0	0	0	0	0	0
PIE	AP MAIN	4176	28	26	24	23	21	19	18	16	14	13	11
PIE	AP MAIN	4177	80	77	75	73	71	69	68	66	64	63	61
PIE	AP MAIN	4178	62	60	59	57	56	54	53	52	50	49	47
PIE	AP MAIN	4179	66	64	62	61	59	58	57	55	54	52	51
PIE	AP MAIN	4180	45	43	41	40	38	36	34	32	30	28	26
PIE	AP MAIN	4183	68	66	64	63	61	60	58	57	55	54	53
PIE	AP MAIN	4185	51	49	48	46	45	43	42	40	38	36	34
PIE	AP MAIN	4190	14	10	7	3	0	0	0	0	0	0	0
PIE	AP MAIN	4195	9	5	1	0	0	0	0	0	0	0	0
PIE	AP MAIN	4198	20	16	13	10	7	3	0	0	0	0	0
PIE	AP MAIN	4199	100	99	98	97	96	94	93	92	91	90	89

5.3 Critical PCI Value

An important concept in pavement management is the critical PCI value, a value that prompts major rehabilitation activities. It serves as a condition threshold that helps determine a section's suitability to receive major work. As soon as a section's PCI reaches the critical PCI value, the rate of PCI loss (deterioration) is expected to increase. The critical PCI concept assumes that once a pavement section deteriorates to this critical level, it is more cost-effective to complete a major rehabilitation project rather than continuing to apply preventive maintenance or deferring major work until more costly reconstruction activities are required. **Figure 5.3 (a)** illustrates the benefit of applying lower cost preventive maintenance to extend the life of the pavement.

Figure 5.3 (a): Pavement Life and the Effect of Treatments



FAA Eligibilty Thresholds: ☐ **>70:** Routine Maintenance ☐ **55-70:** Rehabilitation Eligible ☐ **<55:** Reconstruction Eligible

**Figure is for conceptual purposes only – unit costs are not specific to airfield pavements.*

Critical PCI values vary and are typically based on a pavement's surface type, functional use, and importance, or priority, in daily operations. Pavement priority is generally assigned based on the branch use of a pavement section. In previous System Updates, the critical PCI value was set to 65 for all functional uses. Now, based on FAA Order 5100.38D Change 1 Airport Improvement Handbook, issued February 26, 2019, the FAA has established pavement construction based on thresholds that distinguish Rehabilitation and Reconstruction. Pavement sections between PCI Values 55 and 70 will be considered for Rehabilitation and sections less than 55 will be considered for Reconstruction at the planning-level, as shown in **Table 5.3 (a)**. The FDOT SAPMP will

integrate the PCI thresholds for airfield pavement projects to maintain alignment with the FAA AIP and/or PFC eligibility for project planning. Moving forward, the critical PCI value will be defined at 70 for the FDOT SAPMP. Critical PCI values for this SAPMP System Update are shown in **Table 5.3 (b)**.

Table 5.3 (a): AIP Handbook PCI Requirements for Airfield Pavement Projects

Airfield Pavement Project Type	PCI Requirement
Reconstruction	PCI < 55 (Poor)
Rehabilitation	PCI < 70 (Fair)
Maintenance	N/A

**Source:* AIP Handbook, in reference to Runways, Taxiways, and Aprons as seen in table G-2, H-1, and I-1 respectively

Table 5.3 (b): Critical PCI Values by Branch Use

Runway	Taxiway	Apron
70	70	70

Figures 5.3 (b) and **5.3 (c)** depict the decision process for major rehabilitation project identification with the assumption of available funds (Shahin). Should funding be unavailable for pavement sections in need of major rehabilitation, the Airport may elect to apply appropriate localized stopgap repair strategies. As the figures show, once major rehabilitation has been applied, the PCI of the section is reset to 100.

Figure 5.3 (b): Major Rehabilitation Planning Decision Diagram, PCI < Critical PCI

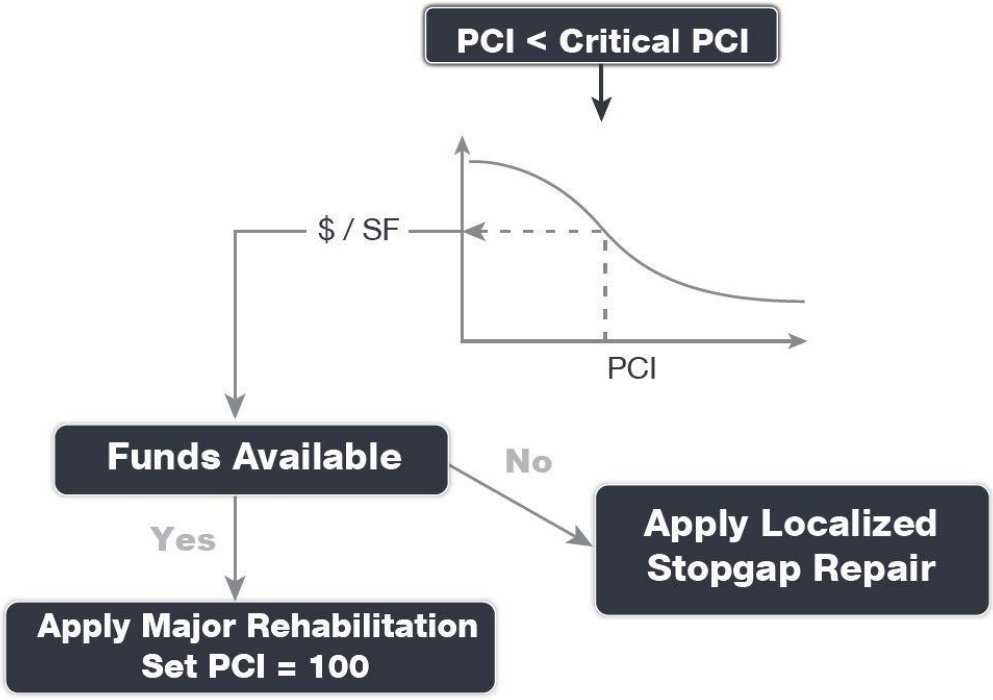
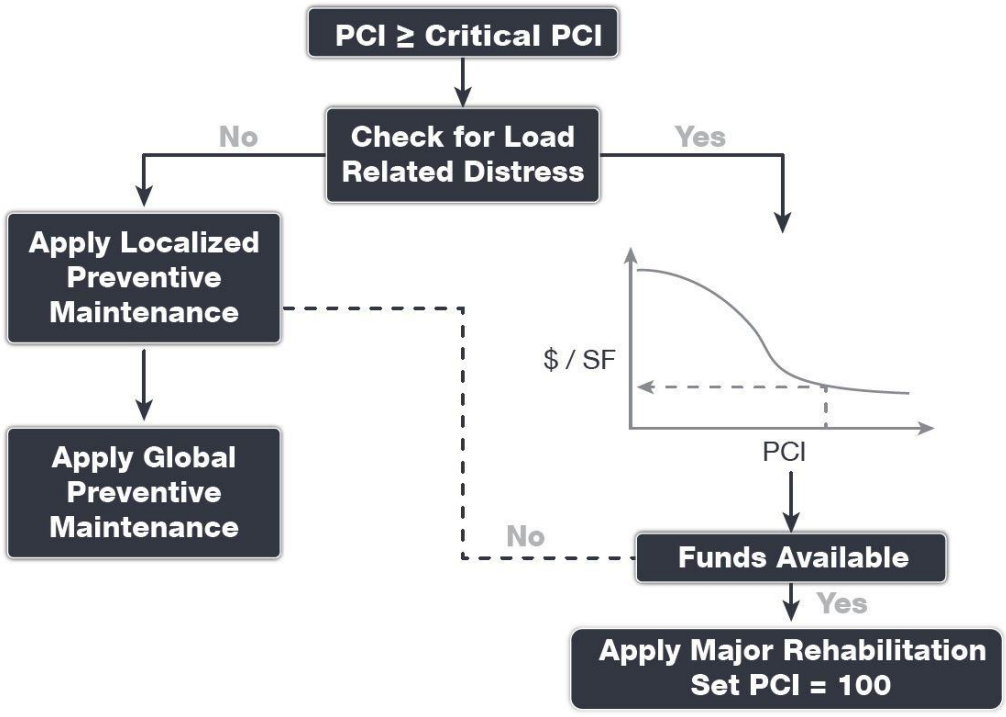


Figure 5.3 (c): Major Rehabilitation Planning Decision Diagram, PCI ≥ Critical PCI



5.4 Localized Maintenance and Repair

This section discusses both localized maintenance and major rehabilitation methods and how they may be most effectively applied to extend the life of the pavement network. General maintenance and rehabilitation (M&R) methods are characterized under two (2) broad categories: localized maintenance and major rehabilitation.

Localized maintenance is best applied as a conservation measure and is applied to slow the rate of pavement deterioration. It may, however, be applied as a temporary corrective measure in isolated areas. Proactive localized maintenance, and specifically preservation, is highly recommended to the Airport. However, it is recognized that once pavements have deteriorated below a certain condition threshold (the critical PCI value), the pavement benefits from more substantial rehabilitation in lieu of localized repairs.

Major rehabilitation is recommended when a pavement section falls below the critical PCI value or if a pavement section has a significant presence of load-related distress. Major rehabilitation efforts can correct or improve structural deficiencies and/or functional deterioration for pavement sections within a network.

M&R planning combines methods of repair to address the cause of the problem rather than just treating the symptom. For example, a PCC corner break may require slab under-sealing, full-depth patching, and joint sealing. While these repair methods apply to specific distress and pavement types, they also consider the impact of Foreign Object Debris (FOD) on aircraft operations. Untidy or improperly constructed repair activities may disintegrate and potentially create FOD at or near the repair site. Therefore, maintenance activities must include quality control monitoring to ensure that repairs are conducted properly and clean-up activities are undertaken to address this potential. The current version of the FAA Advisory Circular 150/5210-24 “Airport Foreign Object Debris (FOD) Management” provides additional guidance for developing and managing an airport FOD program.

5.4.1 Localized Maintenance and Repair Approach

Localized maintenance differs from major rehabilitation in that localized maintenance is applied based on the distresses observed and not an averaged or forecasted PCI value. Treatments are selected based on the appropriate corrective measure for a given distress type and severity level. Localized maintenance can be applied either as a preventive measure or a safety (“stopgap”) measure. The two (2) types of localized maintenance are described below in further detail.

- Localized Preventive Maintenance and Repair
  - Distress maintenance activities performed with the primary objective of slowing the rate of deterioration. These activities typically include crack sealing and patching.
- Localized Stopgap/Safety Maintenance and Repair
  - Defined as the localized distress repair needed to keep a pavement in a safe and operational condition. These activities are typically applied to high-severity distresses or distresses impacting operations.

5.4.2 Localized Work Types

The following sections provide detailed descriptions of the maintenance policy work types identified in the Localized Maintenance Policy.

AC Crack Sealing

Crack sealing is the process of cleaning and sealing (or resealing) cracks in AC pavements. This repair is used to fill longitudinal and transverse cracks, including reflective cracks and block cracks that are wider than 1/8-inch. The purpose of this treatment is to prevent water and incompressible materials from entering cracks and causing further deterioration of the pavement structure. Accumulation of incompressible materials in cracks may lead to spalling and is a source of FOD. Crack sealing is cost-effective when used as a preventive measure. Depending on the size of the crack, routing and cleaning the crack may be necessary to remove the loose material within the crack for better adherence of the crack sealant to the crack face. Measurement of this work type is typically in linear feet.

AC Full-Depth Patching

This technique involves replacing the full thickness of the AC layer and may include replacement of the base and subbase layers. Full-depth patching is used to repair structural and material-related distresses, such as alligator cracking, corrugation, depressions, rutting, slippage cracking, and swelling in AC pavements. This repair may be limited to the top AC layer (partial-depth patch) if the base and subbase layers exhibit no signs of deterioration. Measurement of this work type is typically in square feet or square yards.

AC Partial-Depth AC Patching

This technique involves the removal of a given thickness of the surface layer using a milling machine and adding back a layer of AC pavement. This technique removes the deteriorated layer and provides a good bond for an overlay. It can correct or improve the structural capacity or functional requirement, such as skid resistance and ride quality. This repair is used for surface distresses that can occur over a large area, such as raveling, shoving, and bleeding. While mill and replace can be a major rehabilitation M&R method when applied at a large scale, its application in a localized capacity to treat specific distress types also classifies it under localized maintenance for the purpose of this study. After milling operations are completed, any cracks still present should be cleaned and sealed prior to the placement of a tack coat and AC overlay layer(s). Measurement of this work type is typically in square feet or square yards.

Grinding

Grinding is the process of removing a thin layer of the existing concrete by grinding it with a series of closely spaced, rotating saw blades. This method is used to re-profile jointed concrete pavements with poor ride quality due to faulting or warping. Grinding is also used to restore transverse drainage and to provide a textured pavement surface. The concern with this type of maintenance is that if too much material is removed, the overall structural composition of the pavement section may change, potentially reducing the overall life of the pavement. Measurement of this work type is typically in square feet or square yards.

Monitor Pavement

Monitor pavement is recommended when the distresses do not interfere with ride quality, do not have FOD potential, and do not pose an immediate safety concern.

PCC Crack Sealing

Crack sealing is the process of routing, cleaning, and sealing (or resealing) cracks in PCC pavement to prevent water from infiltrating into the pavement foundation and to stop the accumulation of incompressible materials in the cracks. Water entering cracks can weaken the subgrade, potentially leading to pumping, corner breaks, and/or shattered slabs. Accumulation of incompressible materials in cracks may lead to spalling and is a source of FOD. Routing and cleaning of the crack is often necessary to adhere the crack sealant to both sides of the crack. Measurement of this work type is typically in linear feet.

PCC Full-Depth Patching

This type of M&R activity involves full-depth replacement of a portion of a PCC slab. This repair is used for medium- and high-severity corner breaks, medium-severity durability cracking, medium-severity blowups and buckling, and high-severity large patches. This repair requires restoring load transfer if near a joint or crack. Measurement of this work type is typically in square feet or square yards.

PCC Joint Seal

Joint sealing is the process of cleaning and sealing (or resealing) joints in PCC pavement to prevent water from infiltrating into the pavement foundation and to stop the accumulation of incompressible materials in the joints. Water entering joints can weaken the subgrade, potentially leading to pumping, corner breaks, and/or shattered slabs. Accumulation of incompressible materials in joints leads to spalling of the concrete and is a source of FOD. In some cases, it may be necessary to re-saw the pavement joints to remove old material prior to resealing. Measurement of this work type is typically in linear feet.

PCC Partial-Depth Patching

Partial-depth patching involves removing shallow, localized areas of deteriorated or spalled PCC pavement and replacing them with a suitable patch-like cement concrete or epoxy concrete. This method is used to repair distresses that are confined to the top few inches of the slab, such as joint and corner spalling. This repair would require restoring the joint sealant if near a joint. Measurement of this work type is typically in square feet or square yards.

PCC Slab Replacement

This type of M&R activity involves full-depth replacement of an entire PCC slab. This repair is used to repair high-severity blowups and buckling, high-severity durability cracking, medium- and high-severity shattered slabs, and medium- and high-severity ASR. This repair requires restoring load transfer with adjacent slabs through dowels or similar means. Measurement of this work type is typically in square feet or square yards.

Surface Seal

Application of a surface treatment provides AC-surfaced pavements with an unoxidized layer of bituminous material that can help extend the life of a pavement that is experiencing climate-related distresses such as weathering and raveling. The surface treatment can also serve as a repair that re-establishes a bond between aggregates, slowing pavement deterioration and reducing FOD potential. Measurement of this work type is typically in square feet or square yards.

5.4.3 Localized Maintenance Planning-Level Unit Costs

The activities identified here are based on research of practical pavement treatments in consideration of the FAA AC 150/5380-6C. The Localized Maintenance Policies and associated planning-level unit costs are developed in consideration of a network-level analysis.

The Localized Maintenance and Repair Policies and associated planning-level unit costs are based on a statewide consideration of pavement treatments and construction costs from both airfield pavements and the FDOT Historical Cost Information archives. Furthermore, a consideration of limited repair quantities is factored into the determination of conservative planning-level unit costs. Neither the FDOT nor the Consultant team have control over the cost of labor, materials, equipment, the Contractor's methods of determining prices, or over competitive bidding or market conditions. Opinions of probable construction costs provided herein are based on the information known to the FDOT at this time and represent only the Consultant team's judgment as a design professional familiar with the construction industry. This Report cannot and does not guarantee that proposals, bids, or actual construction costs will not vary from its opinions of probable construction costs.

Tables 5.4.3 (a) and **(b)** display the cost by maintenance activity for AC and PCC pavement types, respectively. Because the localized maintenance activities identified for both preventive and stopgap work types are based on a statewide network approach, project-specific evaluations and maintenance quantities should be developed prior to construction.

Table 5.4.3 (a): Localized M&R Planning-Level Unit Costs – Asphalt Concrete

Localized Work Type	Primary/Commercial Costs		Work Type Unit
AC Crack Sealing	$	4.00	LF
AC Full-Depth Patching	$	18.75	SF
AC Partial-Depth Patching	$	6.50	SF
Surface Seal	$	0.75	SF

Table 5.4.3 (b): Localized M&R Planning-Level Unit Costs – Portland Cement Concrete

Localized Work Type	Primary/Commercial Costs		Work Type Unit
Grinding	$	2.00	SF
PCC Crack Sealing	$	7.00	LF
PCC Joint Seal	$	4.25	LF
PCC Full-Depth Patching	$	75.00	SF
PCC Partial-Depth Patching	$	169.00	SF
PCC Slab Replacement	$	51.50	SF

**PCC Partial-Depth Patching considers high-early-strength and high-performing repair material.*

5.4.4 Localized Maintenance and Repair Policy

Table 5.4.4 and **Table 5.4.5** depicts the Localized Preventive Maintenance Policy and the Localized Stopgap Maintenance Policy for AC and PCC pavements. The resulting Localized Maintenance recommendations for this program are identified based on this policy.

Table 5.4.4: AC Pavement Localized Preventive& Stopgap Maintenance & Repair Policy

Distress	Severity	Description	AC Preventive Work Type	AC Stopgap Work Type
41	Low	Alligator Cracking	Monitor Pavement	Monitor Pavement
41	Medium	Alligator Cracking	AC Full Depth Patching	AC Full Depth Patching
41	High	Alligator Cracking	AC Full Depth Patching	AC Full Depth Patching
42	N/A	Bleeding	Monitor Pavement	Monitor Pavement
43	Low	Block Cracking	Monitor Pavement	Monitor Pavement
43	Medium	Block Cracking	AC Crack Sealing	Monitor Pavement
43	High	Block Cracking	AC Crack Sealing	AC Crack Sealing
44	Low	Corrugation	Monitor Pavement	Monitor Pavement
44	Medium	Corrugation	AC Full Depth Patching	Monitor Pavement
44	High	Corrugation	AC Full Depth Patching	AC Full Depth Patching
45	Low	Depression	Monitor Pavement	Monitor Pavement
45	Medium	Depression	AC Full Depth Patching	Monitor Pavement
45	High	Depression	AC Full Depth Patching	AC Full Depth Patching
46	N/A	Jet Blast	Monitor Pavement	Monitor Pavement
47	Low	Jt. Reflective Cracking	Monitor Pavement	Monitor Pavement
47	Medium	Jt. Reflective Cracking	AC Crack Sealing	Monitor Pavement
47	High	Jt. Reflective Cracking	AC Full Depth Patching	AC Full Depth Patching
48	Low	L&T Cracking	Monitor Pavement	Monitor Pavement
48	Medium	L&T Cracking	AC Crack Sealing	Monitor Pavement
48	High	L&T Cracking	AC Full Depth Patching	AC Full Depth Patching
49	N/A	Oil Spillage	Monitor Pavement	Monitor Pavement
50	Low	Patching	Monitor Pavement	Monitor Pavement
50	Medium	Patching	AC Full Depth Patching	Monitor Pavement
50	High	Patching	AC Full Depth Patching	AC Full Depth Patching
51	N/A	Polished Aggregate	Monitor Pavement	Monitor Pavement
52	Low	Raveling	Surface Seal	Monitor Pavement
52	Medium	Raveling	Surface Seal	Monitor Pavement
52	High	Raveling	AC Partial Depth Patching	AC Partial Depth Patching
53	Low	Rutting	Monitor Pavement	Monitor Pavement
53	Medium	Rutting	AC Full Depth Patching	Monitor Pavement
53	High	Rutting	AC Full Depth Patching	AC Full Depth Patching
54	Low	Shoving	Monitor Pavement	Monitor Pavement
54	Medium	Shoving	AC Partial Depth Patching	Monitor Pavement
54	High	Shoving	AC Full Depth Patching	AC Full Depth Patching
55	N/A	Slippage Cracking	AC Full Depth Patching	AC Full Depth Patching
56	Low	Swelling	Monitor Pavement	Monitor Pavement
56	Medium	Swelling	AC Full Depth Patching	Monitor Pavement
56	High	Swelling	AC Full Depth Patching	AC Full Depth Patching

Distress	Severity	Description	AC Preventive Work Type	AC Stopgap Work Type
57	Low	Weathering	Monitor Pavement	Monitor Pavement
57	Medium	Weathering	Surface Seal	Monitor Pavement
57	High	Weathering	AC Partial Depth Patching	Surface Seal

Table 5.4.5: PCC Pavement Localized Preventive& Stopgap Maintenance & Repair Policy

Distress	Severity	Description	PCC Preventive Work Type	PCC Stopgap Work Type
61	Low	Blow-up	PCC Full Depth Patching	Monitor Pavement
61	Medium	Blow-up	PCC Full Depth Patching	PCC Full Depth Patching
61	High	Blow-up	PCC Slab Replacement	PCC Slab Replacement
62	Low	Corner Break	Monitor Pavement	Monitor Pavement
62	Medium	Corner Break	PCC Full Depth Patching	PCC Full Depth Patching
62	High	Corner Break	PCC Full Depth Patching	PCC Full Depth Patching
63	Low	Linear Cracking	Monitor Pavement	Monitor Pavement
63	Medium	Linear Cracking	PCC Crack Sealing	PCC Crack Sealing
63	High	Linear Cracking	PCC Full Depth Patching	PCC Crack Sealing
64	Low	Durability Cracking	Monitor Pavement	Monitor Pavement
64	Medium	Durability Cracking	PCC Full Depth Patching	PCC Full Depth Patching
64	High	Durability Cracking	PCC Slab Replacement	PCC Slab Replacement
65	Low	Jt. Seal Damage	PCC Joint Seal	Monitor Pavement
65	Medium	Jt. Seal Damage	PCC Joint Seal	Monitor Pavement
65	High	Jt. Seal Damage	PCC Joint Seal	PCC Joint Seal
66	Low	Small Patch	Monitor Pavement	Monitor Pavement
66	Medium	Small Patch	PCC Partial Depth Patching	Monitor Pavement
66	High	Small Patch	PCC Partial Depth Patching	PCC Partial Depth Patching
67	Low	Large Patch	Monitor Pavement	Monitor Pavement
67	Medium	Large Patch	PCC Full Depth Patching	Monitor Pavement
67	High	Large Patch	PCC Full Depth Patching	PCC Full Depth Patching
68	N/A	Popouts	Monitor Pavement	Monitor Pavement
69	N/A	Pumping	Monitor Pavement	Monitor Pavement
70	Low	Scaling	Monitor Pavement	Monitor Pavement
70	Medium	Scaling	PCC Slab Replacement	Monitor Pavement
70	High	Scaling	PCC Slab Replacement	PCC Slab Replacement
71	Low	Faulting	Monitor Pavement	Monitor Pavement
71	Medium	Faulting	Grinding	Monitor Pavement
71	High	Faulting	PCC Slab Replacement	PCC Slab Replacement
72	Low	Shattered Slab	PCC Crack Sealing	Monitor Pavement
72	Medium	Shattered Slab	PCC Slab Replacement	PCC Crack Sealing
72	High	Shattered Slab	PCC Slab Replacement	PCC Slab Replacement
73	N/A	Shrinkage Cracking	Monitor Pavement	Monitor Pavement

Distress	Severity	Description	PCC Preventive Work Type	PCC Stopgap Work Type
74	Low	Joint Spall	Monitor Pavement	Monitor Pavement
74	Medium	Joint Spall	PCC Partial Depth Patching	PCC Partial Depth Patching
74	High	Joint Spall	PCC Partial Depth Patching	PCC Partial Depth Patching
75	Low	Corner Spall	Monitor Pavement	Monitor Pavement
75	Medium	Corner Spall	PCC Partial Depth Patching	PCC Partial Depth Patching
75	High	Corner Spall	PCC Partial Depth Patching	PCC Partial Depth Patching
76	Low	ASR	Monitor Pavement	Monitor Pavement
76	Medium	ASR	PCC Slab Replacement	PCC Slab Replacement
76	High	ASR	PCC Slab Replacement	PCC Slab Replacement

5.5 Major Rehabilitation

Major rehabilitation is recommended to correct or improve structural deficiencies and/or functional deterioration. Often, when pavements are subject to significant changes in the aircraft fleet mix (frequency and type), major rehabilitation is required to provide a pavement section that can meet the structural demands of traffic loading. Major rehabilitation is generally described as a pavement construction that removes and replaces the pavement surface, thus resetting the PCI value to 100 and the pavement age to zero. Typical policies include full- and partial-depth reconstruction and mill and overlay.

5.5.1 Major Rehabilitation Pavement Section Development

Once the timing of the major rehabilitation activity is determined based on the PCI value, existing as-built record documentation is used to determine typical rehabilitation processes and pavement sections. Refinement of the pavement section layers is performed in consideration of the FAA AC 150/5320-6F. It should be noted that no subsurface geotechnical investigation, American Land Title Association (ALTA)/American Congress on Surveying and Mapping (ACSM) Survey, topographic survey, utilities survey, environmental, or site-specific air traffic study(s) have been utilized in the development of the design criteria. No warranty or assurance is implied in this document for final design nor construction for any airfield pavements discussed within this Report.

Major rehabilitation is divided into two (2) policy categories as part of this System Update: Full-Depth Reconstruction (Reconstruction) and Intermediate Major Rehabilitation (Rehabilitation). Based on the pavement type, the general categories are defined as AC Reconstruction and AC Rehabilitation for AC, AAC, and APC pavement types, and PCC Reconstruction and PCC Rehabilitation for PCC pavement types. The pavement sections are based on the average Primary/Commercial Airport Type requirements; no pavement design has been performed in accordance with the FAA AC 150/5320-6F for the determined conceptual sections. **Table 5.5.1** provide details on the conceptual pavement sections developed for this study.

Table 5.5.1: Conceptual Pavement Sections for Major Rehabilitation

Rehabilitation Type	Primary/Commercial Pavement Section
AC Reconstruction	
Full-depth asphalt pavement section reconstruction. Removal of existing pavement section and construction of a new section. ***PCI < 55***	Pavement Removal
	Unclassified Excavation
	Subgrade Stabilization (12")
	Limerock Base Course (8")
	Prime Coat
	Tack Coat
	P-403 Stabilized Base Course (5")
	P-401 Surface Course (4")
	Excludes any paved shoulder features
AC Rehabilitation	
Combination of asphalt pavement milling and replacement overlay with 15% of the areas subject to full-depth reconstruction. ***PCI = 55 to 70***	**15% AC Reconstruction**
	Mill and Overlay
	AC Milling (4")
	Tack Coat
	P-401 Surface Course (4")
	Excludes any paved shoulder features
PCC Reconstruction	
Full-depth rigid pavement section reconstruction. ***PCI < 55***	Pavement Removal
	Unclassified Excavation
	Subgrade Stabilization (12")
	Limerock Base Course (6")
	Prime Coat
	Tack Coat
	P-403 Stabilized Base Course (5")
	P-501 PCC Pavement (17")
	PCC Joint Seal
PCC Rehabilitation	
Rehabilitation of PCC pavement with a combination of crack sealing, joint seal replacement, limited patching, and replacement of 15% of slab panels. ***PCI = 55 to 70***	**15% Slab Replacement**
	Joint and Crack Seal
	Limited Patching

The identification of rehabilitation needs and conceptual pavement sections have been determined at the planning level. Design-level investigation is recommended prior to developing construction-level design documents and budgets. **This type of construction typically warrants consideration for non-pavement efforts that may include drainage, turfing, electrical lighting, pavement marking, construction contingency, mobilization costs, and project soft costs.**

Reconstruction (AC or PCC)

Reconstruction is the removal and replacement of the existing AC or PCC pavement and base layer and includes preparation of the existing subgrade material. This technique is utilized when the pavement is badly deteriorated or a structural improvement is required. Reconstruction is used when the pavements are structurally deficient and an overlay is not possible due to adjacent pavement grades.

AC Rehabilitation

AC Rehabilitation, for the purposes of this SAPMP, is a removal of all or a portion of the asphalt surface through milling and replacing the milled depth with an overlay of asphalt. This rehabilitation activity is typically applied to pavement that does not require a structural improvement and does not display an extensive amount of load-related distresses. However, this work type conservatively accounts for 15% of the planned area to receive a full-depth replacement of the pavement structure. This is meant to capture any deficiencies that may not be apparent from a visual evaluation of the surface of the pavement. This work type occurs on pavement sections with a PCI value between 55 and 70. As a general rule of thumb, intermediate rehabilitation activities have a shorter pavement life compared to a full-depth reconstruction, but AC Rehabilitation will still reset the pavement to a PCI of 100.

PCC Rehabilitation

PCC Rehabilitation, for the purposes of this SAPMP, is a planning-level estimate of several concurrent PCC maintenance activities intended to raise the PCI above Critical without reconstructing the entire area. This work type accounts for the replacement of 15% of the slabs as well as a PCC patching, crack sealing, and joint sealing for areas outside of the panel replacement. This work type occurs on pavement sections with a PCI value between 55 and 70.

5.5.2 Major Rehabilitation Planning-Level Unit Costs

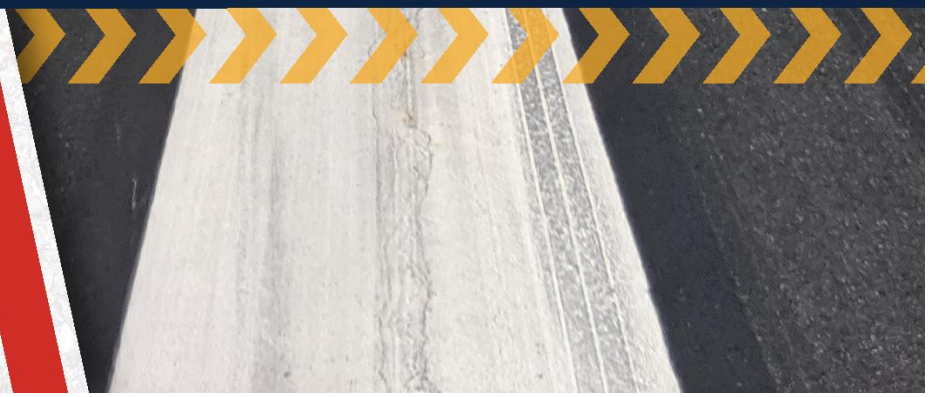
Planning-level opinions of probable construction cost developed for this System Update are based on archived bid tabulations and records from airfield pavement projects provided by participating airports. A review of cost trends and cost factors have been incorporated to assist airports in planning for project budgets.

Neither the FDOT nor the Consultant team have control over the cost of labor, materials, equipment, Contractor's methods of determining prices, or over competitive bidding or market conditions. Opinions of probable construction costs provided herein are based on the information known to the FDOT at this time and represent only the Consultant team's judgment as a design professional familiar with the construction industry. This Report cannot and does not guarantee that proposals, bids, or actual construction costs will not vary from its opinions of probable construction costs. **Table 5.5.2** depicts the associated work type planning-level unit costs for Major Rehabilitation for each pavement type.

Table 5.5.2: PR Major Rehabilitation Planning-Level Unit Cost by Pavement Type

Rehabilitation Type	PCI Range	Asphalt Concrete Cost per SF	Portland Cement Concrete Cost Per SF
Rehabilitation	55 to 70	$14.00	$30.50
Reconstruction	0 to 55	$30.50	$60.00

Chapter 6: M&R Planning and Budget Scenario Analysis



Chapter 6 – M&R Planning and Budget Scenario Analysis

6.1 Localized Maintenance and Repair Analysis and Recommendations

This FDOT SAPMP System Update provides a planning-level estimation of Localized Maintenance and Repair costs based on the results of the latest PCI assessment performed at the Airport. Due to the limited sample units inspected in certain pavement sections, a statistical extrapolation of distresses is used to estimate the quantities of recommended repair activities at the section level, based the policies defined in **5.4.4 Localized Maintenance and Repair Policy**. These work quantities are limited to a near-term application since they were determined directly from the PCI assessment efforts. As pavements continue to deteriorate year-to-year, quantities and/or distress severities may increase, which will affect the amount and type of localized maintenance required. This analysis can be utilized as a planning tool to assist Airport staff in determining an annual budget allocation for maintenance activities that will help maintain Airport pavements above the critical PCI value and extend the life of the pavement.

Table 6.1 (a) provides a summary of the anticipated planning-level costs for Year 1 Localized Preventive Maintenance and Localized Stopgap Maintenance. The following table depicts planning-level costs rounded up to the next 10-dollar increment.

Table 6.1 (a): Year 1 Summary of Localized Maintenance

Work Category	Cost	
Preventive	$	74,380
Stopgap	$	271,310
Planning-Level Localized M&R Needs =	***$***	***345,690***

Localized Preventive Maintenance is typically applied to pavements that are in a condition above the critical PCI value of the pavement section. Localized Stopgap Maintenance is typically applied to pavement sections that are at or below the critical PCI value. Application of localized maintenance and repair should be coordinated with the planning of major rehabilitation efforts identified through the Major Rehabilitation analysis. Pavements with stopgap recommendations that are subject to near-term major rehabilitation efforts may remove the need to perform localized (stopgap) maintenance efforts in subsequent years.

Table 6.1 (b) summarizes the anticipated Year 1 Localized Maintenance recommendations by work type, based on the PCI assessment efforts performed as part of this SAPMP System Update. The following table depicts planning-level costs rounded up to the next 10-dollar increment.

Table 6.1 (b): Year 1 Localized Maintenance by Work Type Summary

Localized Maintenance Category	Localized Work Type	Rough Estimate of Work Quantity	Work Units	Planning Material Cost
Localized Preventive Maintenance	AC Crack Sealing	2,233	LF	$ 8,970
	Surface Seal	72,436	SF	$ 54,420
	AC Full-Depth Patching	250	SF	$ 4,710
	PCC Partial-Depth Patching	37	SF	$ 6,280
Localized Stopgap Maintenance	AC Partial-Depth Patching	243	SF	$ 1,600
	AC Full-Depth Patching	9,596	SF	$ 180,010
	PCC Crack Sealing	3,700	LF	$ 25,940
	PCC Joint Seal	4,771	LF	$ 20,290
	PCC Partial-Depth Patching	191	SF	$ 32,320
	PCC Full-Depth Patching	149	SF	$ 11,150

Table 6.1 (c) provides a breakdown of the anticipated planning-level costs by section for those areas exhibiting distresses that would benefit from Year 1 Localized M&R. The table shows the approximate improved "End Condition" PCI value of the section after the application of Localized M&R. This approximation is intended to depict a planning-level estimate of the effect of the localized M&R on the section-level PCI; the performance of the work does not guarantee the pavement will not deteriorate in other ways outside of the described treatment. The following table depicts planning-level costs rounded up to the next 10-dollar increment.

Table 6.1 (c): Section-Level Year 1 Localized M&R Planning Cost Summary

Network ID	Branch ID	Section ID	Area (SF)	Start PCI	End PCI	Cost
PIE	RW 4-22	6205	474,873	73	78	$ 19,720
PIE	RW 4-22	6210	237,436	79	80	$ 2,900
PIE	RW 4-22	6215	50,072	63	63	$ -
PIE	RW 4-22	6220	25,036	70	70	$ -
PIE	RW 4-22	6225	45,300	54	54	$ -
PIE	RW 4-22	6230	22,650	24	24	$ -
PIE	RW 4-22	6235	9,700	100	100	$ -
PIE	RW 4-22	6240	4,850	100	100	$ -
PIE	RW 18-36	6115	135,960	100	100	$ -
PIE	RW 18-36	6120	176,940	100	100	$ -
PIE	RW 18-36	6155	99,000	100	100	$ -
PIE	RW 18-36	6156	18,000	100	100	$ -
PIE	RW 18-36	6157	12,000	100	100	$ -
PIE	RW 18-36	6160	148,500	100	100	$ -
PIE	RW 18-36	6165	40,500	100	100	$ -
PIE	RW 18-36	6170	60,750	100	100	$ -
PIE	RW 18-36	6175	170,280	100	100	$ -
PIE	RW 18-36	6180	255,420	100	100	$ -
PIE	RW 18-36	6185	126,000	100	100	$ -
PIE	RW 18-36	6190	189,000	100	100	$ -
PIE	RW 18-36	6196	27,000	100	100	$ -
PIE	FBO CONN	107	3,297	45	45	$ -

Network ID	Branch ID	Section ID	Area (SF)	Start PCI	End PCI	Cost
PIE	FBO CONN	108	3,361	32	32	$ -
PIE	FBO CONN	112	4,221	42	42	$ -
PIE	FBO CONN	114	2,361	59	59	$ -
PIE	FBO CONN	117	6,019	77	77	$ -
PIE	FBO CONN	119	3,041	29	34	$ 570
PIE	FBO CONN	125	4,598	58	58	$ -
PIE	FBO CONN	127	12,891	82	87	$ 970
PIE	TW A	115	224,709	65	66	$ 15,410
PIE	TW A	130	358,395	71	75	$ 17,950
PIE	TW A	132	23,007	88	88	$ -
PIE	TW A	155	6,259	89	89	$ -
PIE	TW A	158	16,692	63	63	$ -
PIE	TW A	160	99,856	82	82	$ -
PIE	TW A1	135	40,056	64	64	$ -
PIE	TW A1	140	14,541	67	67	$ -
PIE	TW A1	145	2,945	100	100	$ -
PIE	TW A2	165	60,458	74	79	$ 5,550
PIE	TW A3	168	60,311	77	83	$ 3,310
PIE	TW A4	170	58,588	85	89	$ 2,200
PIE	TW A5	175	56,987	82	83	$ 960
PIE	TW A6	180	58,658	86	89	$ 2,200
PIE	TW A7	162	52,089	83	83	$ -
PIE	TW B	205	6,200	91	91	$ -
PIE	TW B	207	7,750	90	90	$ -
PIE	TW B	210	6,353	88	88	$ -
PIE	TW B	212	18,000	80	80	$ -
PIE	TW B	215	15,387	85	85	$ -
PIE	TW B	220	40,670	16	18	$ 15,830
PIE	TW B	225	17,624	89	89	$ -
PIE	TW F	610	43,041	76	76	$ -
PIE	TW F	615	4,165	100	100	$ -
PIE	TW G	1315	19,536	69	69	$ -
PIE	TW G	1320	15,822	62	62	$ -
PIE	TW G	1325	199,036	65	65	$ -
PIE	TW G	1340	14,004	82	82	$ -
PIE	TW G1	1330	13,135	65	65	$ -
PIE	TW G1	1335	12,530	63	63	$ -
PIE	TW G2	1005	15,843	89	89	$ -
PIE	TW G2	1010	8,964	89	89	$ -
PIE	TW G3	605	10,930	24	24	$ -
PIE	TW G3	607	8,732	88	88	$ -
PIE	TW H	810	59,729	5	14	$ 144,930
PIE	TW H	815	57,784	87	91	$ 2,170
PIE	TW H	820	4,760	88	88	$ -
PIE	TW J	905	8,851	100	100	$ -
PIE	TW L	1205	22,175	77	82	$ 1,760
PIE	TW L	1215	13,483	76	81	$ 1,020

Network ID	Branch ID	Section ID	Area (SF)	Start PCI	End PCI	Cost
PIE	TW L	1245	52,150	79	79	$ -
PIE	TW P	1250	27,739	85	88	$ 1,040
PIE	TW P	1255	52,339	91	94	$ 1,970
PIE	TW Q	1705	4,449	94	94	$ -
PIE	TW Q	1710	3,632	94	94	$ -
PIE	TW T	2010	12,963	87	87	$ -
PIE	TW T	2020	14,337	86	86	$ -
PIE	TW T	2045	16,549	74	84	$ 1,110
PIE	TW T	2050	149,440	90	90	$ -
PIE	TL T-HANG	1120	1,346	39	39	$ -
PIE	TL T-HANG	1125	1,472	38	38	$ -
PIE	AP MAIN	4103	122,390	100	100	$ -
PIE	AP MAIN	4105	40,910	30	30	$ -
PIE	AP MAIN	4107	220,315	97	97	$ -
PIE	AP MAIN	4110	56,000	74	77	$ 2,410
PIE	AP MAIN	4123	43,794	83	83	$ -
PIE	AP MAIN	4150	14,083	79	79	$ -
PIE	AP MAIN	4155	33,689	59	59	$ -
PIE	AP MAIN	4157	92,541	67	67	$ 1,160
PIE	AP MAIN	4160	59,640	96	98	$ 6,280
PIE	AP MAIN	4165	66,649	95	95	$ -
PIE	AP MAIN	4170	18,816	87	87	$ -
PIE	AP MAIN	4175	14,910	5	40	$ 36,340
PIE	AP MAIN	4176	3,573	28	28	$ 740
PIE	AP MAIN	4177	20,899	80	83	$ 790
PIE	AP MAIN	4178	59,522	62	62	$ -
PIE	AP MAIN	4179	77,111	66	66	$ -
PIE	AP MAIN	4180	126,695	45	45	$ 2,950
PIE	AP MAIN	4183	39,947	68	68	$ -
PIE	AP MAIN	4185	12,820	51	51	$ -
PIE	AP MAIN	4190	18,650	14	67	$ 24,900
PIE	AP MAIN	4195	11,250	9	28	$ 25,980
PIE	AP MAIN	4198	18,579	20	35	$ 2,430
PIE	AP MAIN	4199	25,200	100	100	$ -

6.2 Major Rehabilitation Needs

Major rehabilitation is identified within the FDOT SAPMP as a major construction activity that results in a substantial improvement to the pavement condition and resets the pavement section's PCI value to 100. Major rehabilitation recommendations (AC Rehabilitation, AC Reconstruction, PCC Rehabilitation, and PCC Reconstruction) should be considered as planning-level only. Additional design-level investigation in accordance with FAA Advisory Circulars is required. Recommendations identified within this planning document do not imply final design.

The objective of the Major Pavement Rehabilitation Needs analysis is to develop planning-level projects within an Airport's airfield pavement network. As depicted in **Figures 5.3 (b)** and **(c)** in **Chapter 5**, major rehabilitation activities are recommended when a pavement section has deteriorated below the critical PCI value, a point at which localized maintenance and repair activities may not be a cost-effective solution. In addition, major rehabilitation is also recommended when the section's PCI value is above the critical PCI value with the section exhibiting a significant amount of load-related distresses. Identification of rehabilitation needs is done at the section-level. This, however, does not limit the Airport from further refining limits of project planning areas.

6.2.1 10-Year Unconstrained Budget Major Rehabilitation Needs

Major rehabilitation needs are identified by analyzing the Airport's pavement condition in relationship to critical PCI values, major rehabilitation policies, and unit costs, assuming there are no budget constraints. This is done over a 10-year analysis period. While this is financially impractical, it does yield the unbiased pavement needs over a 10-year time frame at the Airport given current and forecasted pavement conditions. The FDOT recognizes that airports are constrained by budgets and does not intend to convey an unrealistic approach of addressing pavement rehabilitation. Each airport has a unique set of challenges and FDOT's goals are to provide it with the data needed to formulate a practical Capital Improvement Program and identify needs in the Joint Automated Capital Improvement Program (JACIP). This includes:

- An estimation of current pavement condition;
- Major pavement rehabilitation needs based on condition and policies; and
- Planning-level cost estimates for the major rehabilitation needs.

Table 6.2.1 (a) summarizes section-level major rehabilitation needs forecasted for a 10-year period. It should be noted that the following table depicts planning-level costs and has been rounded up to the nearest $1,000 for planning purposes.

Table 6.2.1 (a): Section-Level 10-Year Major Rehabilitation Needs

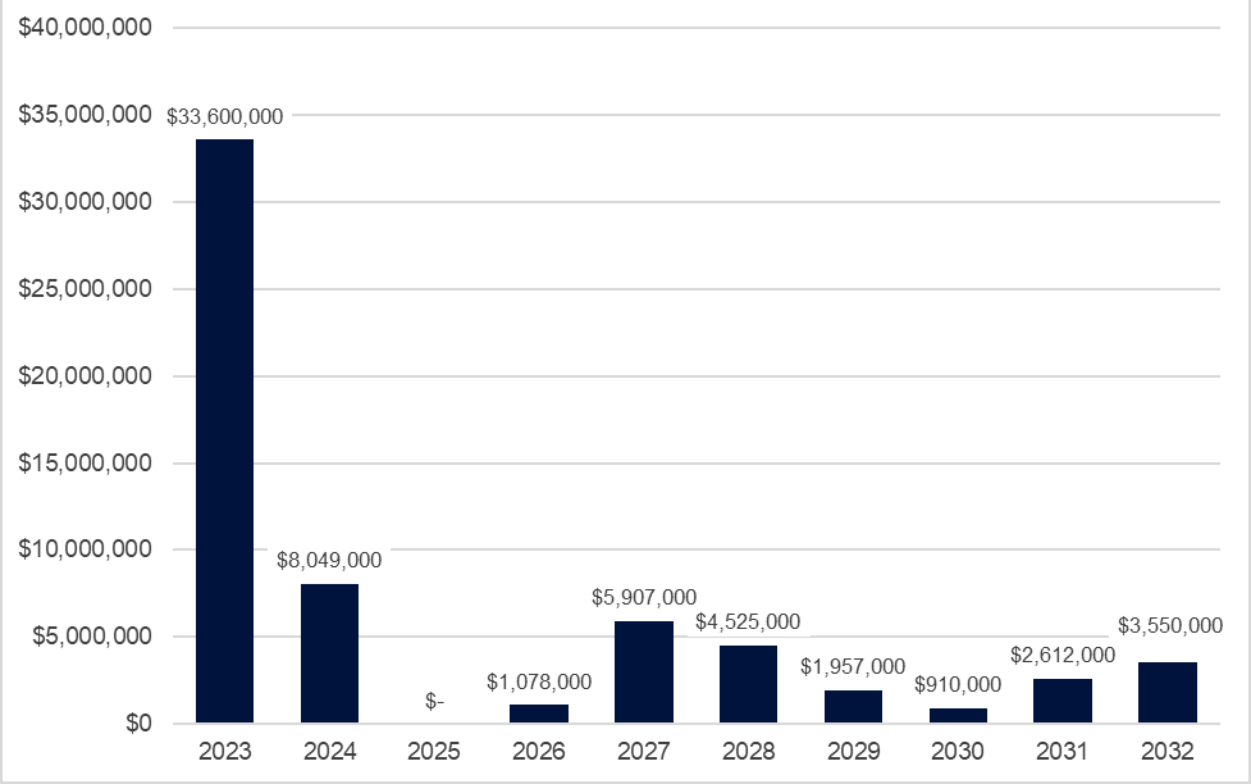
Program Year	Network ID	Branch ID	Section ID	Surface	Area (SF)	PCI Before	Rehabilitation Type	Planning Cost Estimate
2023	**PIE**	RW 4-22	6215	AAC	50,072	61	AC Rehabilitation	$ 701,000
2023	**PIE**	RW 4-22	6220	AAC	25,036	68	AC Rehabilitation	$ 351,000
2023	**PIE**	RW 4-22	6225	AC	45,300	52	AC Reconstruction	$ 1,382,000
2023	**PIE**	RW 4-22	6230	AC	22,650	22	AC Reconstruction	$ 691,000
2023	**PIE**	FBO CONN	107	AAC	3,297	44	AC Reconstruction	$ 101,000
2023	**PIE**	FBO CONN	108	AC	3,361	29	AC Reconstruction	$ 103,000
2023	**PIE**	FBO CONN	112	AAC	4,221	40	AC Reconstruction	$ 129,000
2023	**PIE**	FBO CONN	114	AC	2,361	58	AC Rehabilitation	$ 34,000
2023	**PIE**	FBO CONN	119	AC	3,041	26	AC Reconstruction	$ 93,000
2023	**PIE**	FBO CONN	125	APC	4,598	57	AC Rehabilitation	$ 65,000
2023	**PIE**	TW A	115	AAC	224,709	63	AC Rehabilitation	$ 3,146,000
2023	**PIE**	TW A	130	AAC	358,395	69	AC Rehabilitation	$ 5,018,000
2023	**PIE**	TW A	158	AAC	16,692	61	AC Rehabilitation	$ 234,000
2023	**PIE**	TW A1	135	AAC	40,056	62	AC Rehabilitation	$ 561,000

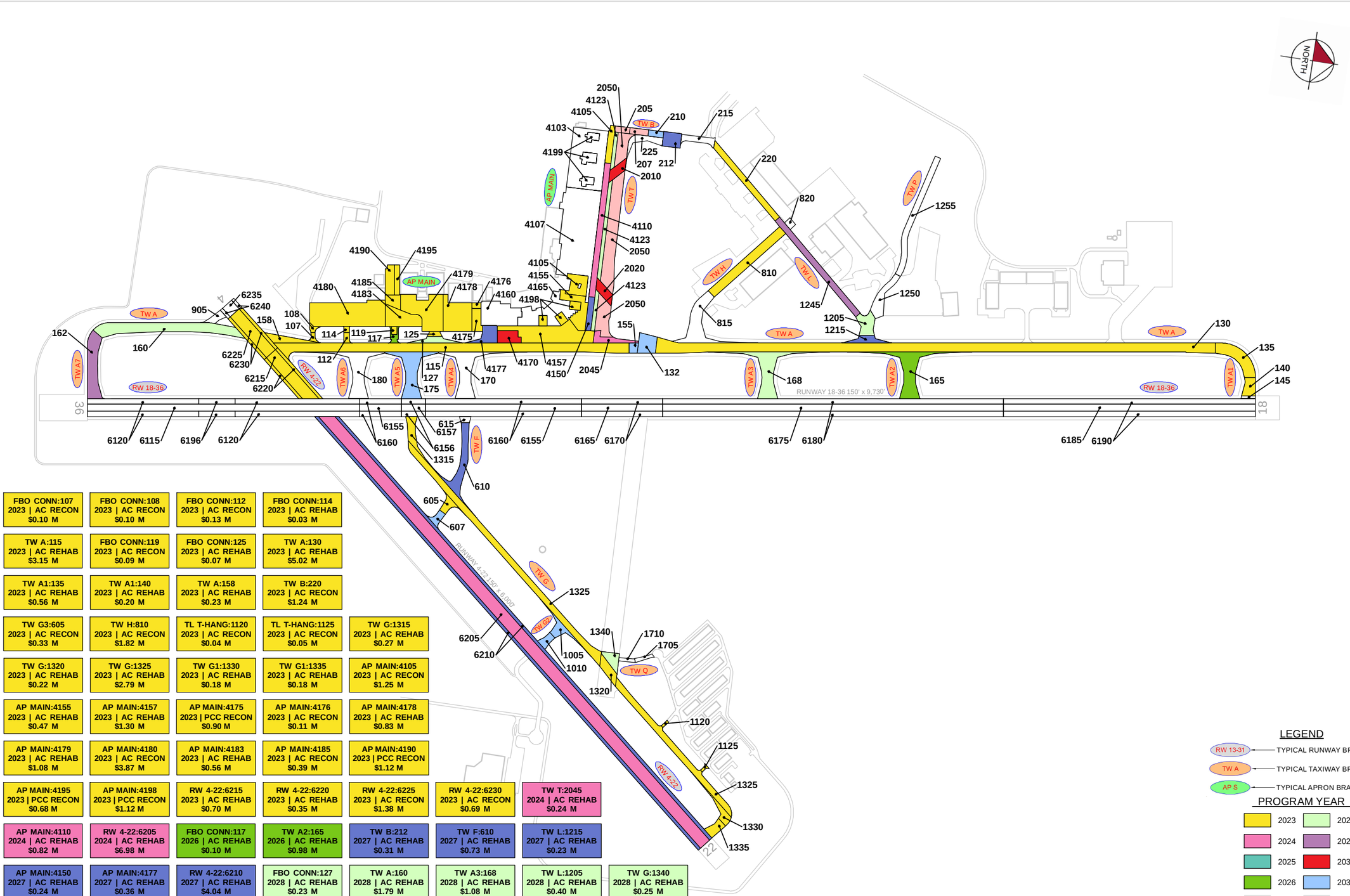
Program Year	Network ID	Branch ID	Section ID	Surface	Area (SF)	PCI Before	Rehabilitation Type	Planning Cost Estimate
2023	PIE	TW A1	140	AAC	14,541	65	AC Rehabilitation	$ 204,000
2023	PIE	TW B	220	AC	40,670	13	AC Reconstruction	$ 1,241,000
2023	PIE	TW G	1315	AC	19,536	68	AC Rehabilitation	$ 274,000
2023	PIE	TW G	1320	AAC	15,822	60	AC Rehabilitation	$ 222,000
2023	PIE	TW G	1325	AAC	199,036	63	AC Rehabilitation	$ 2,787,000
2023	PIE	TW G1	1330	AAC	13,135	63	AC Rehabilitation	$ 184,000
2023	PIE	TW G1	1335	AAC	12,530	61	AC Rehabilitation	$ 176,000
2023	PIE	TW G3	605	AAC	10,930	20	AC Reconstruction	$ 334,000
2023	PIE	TW H	810	AAC	59,729	0	AC Reconstruction	$ 1,822,000
2023	PIE	TL T-HANG	1120	AC	1,346	37	AC Reconstruction	$ 42,000
2023	PIE	TL T-HANG	1125	AC	1,472	36	AC Reconstruction	$ 45,000
2023	PIE	AP MAIN	4105	APC	40,910	27	AC Reconstruction	$ 1,248,000
2023	PIE	AP MAIN	4155	AAC	33,689	57	AC Rehabilitation	$ 472,000
2023	PIE	AP MAIN	4157	AAC	92,541	65	AC Rehabilitation	$ 1,296,000
2023	PIE	AP MAIN	4175	PCC	14,910	1	PCC Reconstruction	$ 895,000
2023	PIE	AP MAIN	4176	AC	3,573	26	AC Reconstruction	$ 109,000
2023	PIE	AP MAIN	4178	APC	59,522	60	AC Rehabilitation	$ 834,000
2023	PIE	AP MAIN	4179	APC	77,111	64	AC Rehabilitation	$ 1,080,000
2023	PIE	AP MAIN	4180	AAC	126,695	43	AC Reconstruction	$ 3,865,000
2023	PIE	AP MAIN	4183	AAC	39,947	66	AC Rehabilitation	$ 560,000
2023	PIE	AP MAIN	4185	APC	12,820	49	AC Reconstruction	$ 392,000
2023	PIE	AP MAIN	4190	PCC	18,650	10	PCC Reconstruction	$ 1,119,000
2023	PIE	AP MAIN	4195	PCC	11,250	5	PCC Reconstruction	$ 675,000
2023	PIE	AP MAIN	4198	PCC	18,579	16	PCC Reconstruction	$ 1,115,000
2024	PIE	RW 4-22	6205	AAC	474,873	69	AC Rehabilitation	$ 6,981,000
2024	PIE	TW T	2045	AAC	16,549	70	AC Rehabilitation	$ 244,000
2024	PIE	AP MAIN	4110	APC	56,000	70	AC Rehabilitation	$ 824,000
2026	PIE	FBO CONN	117	AAC	6,019	69	AC Rehabilitation	$ 98,000
2026	PIE	TW A2	165	AC	60,458	69	AC Rehabilitation	$ 980,000
2027	PIE	RW 4-22	6210	AAC	237,436	69	AC Rehabilitation	$ 4,041,000
2027	PIE	TW B	212	AAC	18,000	70	AC Rehabilitation	$ 307,000
2027	PIE	TW F	610	AC	43,041	70	AC Rehabilitation	$ 733,000
2027	PIE	TW L	1215	AC	13,483	70	AC Rehabilitation	$ 230,000
2027	PIE	AP MAIN	4150	AAC	14,083	69	AC Rehabilitation	$ 240,000
2027	PIE	AP MAIN	4177	APC	20,899	69	AC Rehabilitation	$ 356,000
2028	PIE	FBO CONN	127	APC	12,891	70	AC Rehabilitation	$ 231,000
2028	PIE	TW A	160	AAC	99,856	70	AC Rehabilitation	$ 1,785,000
2028	PIE	TW A3	168	AC	60,311	69	AC Rehabilitation	$ 1,078,000
2028	PIE	TW G	1340	AAC	14,004	70	AC Rehabilitation	$ 251,000
2028	PIE	TW L	1205	AC	22,175	69	AC Rehabilitation	$ 397,000
2028	PIE	AP MAIN	4123	APC	43,794	70	AC Rehabilitation	$ 783,000
2029	PIE	TW A7	162	AAC	52,089	69	AC Rehabilitation	$ 978,000
2029	PIE	TW L	1245	AC	52,150	70	AC Rehabilitation	$ 979,000
2030	PIE	TW T	2010	AAC	12,963	70	AC Rehabilitation	$ 256,000
2030	PIE	TW T	2020	AAC	14,337	69	AC Rehabilitation	$ 283,000
2030	PIE	AP MAIN	4170	AAC	18,816	69	AC Rehabilitation	$ 371,000

Program Year	Network ID	Branch ID	Section ID	Surface	Area (SF)	PCI Before	Rehabilitation Type	Planning Cost Estimate
2031	PIE	TW A	132	AAC	23,007	69	AC Rehabilitation	$ 476,000
2031	PIE	TW A	155	AAC	6,259	70	AC Rehabilitation	$ 130,000
2031	PIE	TW A5	175	AC	56,987	70	AC Rehabilitation	$ 1,179,000
2031	PIE	TW B	210	AAC	6,353	69	AC Rehabilitation	$ 132,000
2031	PIE	TW G2	1005	AAC	15,843	70	AC Rehabilitation	$ 328,000
2031	PIE	TW G2	1010	AAC	8,964	70	AC Rehabilitation	$ 186,000
2031	PIE	TW G3	607	AAC	8,732	69	AC Rehabilitation	$ 181,000
2032	PIE	TW B	205	AAC	6,200	69	AC Rehabilitation	$ 135,000
2032	PIE	TW B	207	AAC	7,750	69	AC Rehabilitation	$ 169,000
2032	PIE	TW T	2050	AAC	149,440	69	AC Rehabilitation	$ 3,246,000

Figure 6.2.1 (a) summarizes the section-level major rehabilitation needs for a 10-year period between 2023 and 2032. **Figure 6.2.1 (b)**, the Airfield Pavement Major Rehabilitation Exhibit, graphically depicts the major rehabilitation needs with rounded costs. As suggested previously, this is planning-level data that can be used by the Airport to support developing a practical CIP.

Figure 6.2.1 (a): 10-Year Major Rehabilitation Needs by Program Year





FBO CONN:107 2023 | AC RECON $0.10 M	FBO CONN:108 2023 | AC RECON $0.10 M	FBO CONN:112 2023 | AC RECON $0.13 M	FBO CONN:114 2023 | AC REHAB $0.03 M
TW A:115 2023 | AC REHAB $3.15 M	FBO CONN:119 2023 | AC RECON $0.09 M	FBO CONN:125 2023 | AC REHAB $0.07 M	TW A:130 2023 | AC REHAB $5.02 M
TW A1:135 2023 | AC REHAB $0.56 M	TW A1:140 2023 | AC REHAB $0.20 M	TW A:158 2023 | AC REHAB $0.23 M	TW B:220 2023 | AC RECON $1.24 M
TW G3:605 2023 | AC RECON $0.33 M	TW H:810 2023 | AC RECON $1.82 M	TL T-HANG:1120 2023 | AC RECON $0.04 M	TL T-HANG:1125 2023 | AC RECON $0.05 M	TW G:1315 2023 | AC REHAB $0.27 M
TW G:1320 2023 | AC REHAB $0.22 M	TW G:1325 2023 | AC REHAB $2.79 M	TW G1:1330 2023 | AC REHAB $0.18 M	TW G1:1335 2023 | AC REHAB $0.18 M	AP MAIN:4105 2023 | AC RECON $1.25 M
AP MAIN:4155 2023 | AC REHAB $0.47 M	AP MAIN:4157 2023 | AC REHAB $1.30 M	AP MAIN:4175 2023 | PCC RECON $0.90 M	AP MAIN:4176 2023 | AC RECON $0.11 M	AP MAIN:4178 2023 | AC REHAB $0.83 M
AP MAIN:4179 2023 | AC REHAB $1.08 M	AP MAIN:4180 2023 | AC RECON $3.87 M	AP MAIN:4183 2023 | AC REHAB $0.56 M	AP MAIN:4185 2023 | AC RECON $0.39 M	AP MAIN:4190 2023 | PCC RECON $1.12 M
AP MAIN:4195 2023 | PCC RECON $0.68 M	AP MAIN:4198 2023 | PCC RECON $1.12 M	RW 4-22:6215 2023 | AC REHAB $0.70 M	RW 4-22:6220 2023 | AC REHAB $0.35 M	RW 4-22:6225 2023 | AC RECON $1.38 M	RW 4-22:6230 2023 | AC RECON $0.69 M	TW T:2045 2024 | AC REHAB $0.24 M
AP MAIN:4110 2024 | AC REHAB $0.82 M	RW 4-22:6205 2024 | AC REHAB $6.98 M	FBO CONN:117 2026 | AC REHAB $0.10 M	TW A2:165 2026 | AC REHAB $0.98 M	TW B:212 2027 | AC REHAB $0.31 M	TW F:610 2027 | AC REHAB $0.73 M	TW L:1215 2027 | AC REHAB $0.23 M
AP MAIN:4150 2027 | AC REHAB $0.24 M	AP MAIN:4177 2027 | AC REHAB $0.36 M	RW 4-22:6210 2027 | AC REHAB $4.04 M	FBO CONN:127 2028 | AC REHAB $0.23 M	TW A:160 2028 | AC REHAB $1.79 M	TW A3:168 2028 | AC REHAB $1.08 M	TW L:1205 2028 | AC REHAB $0.40 M	TW G:1340 2028 | AC REHAB $0.25 M
AP MAIN:4123 2028 | AC REHAB $0.78 M	TW A7:162 2029 | AC REHAB $0.98 M	TW L:1245 2029 | AC REHAB $0.98 M	TW T:2010 2030 | AC REHAB $0.26 M	TW T:2020 2030 | AC REHAB $0.28 M	AP MAIN:4170 2030 | AC REHAB $0.37 M	TW A:132 2031 | AC REHAB $0.48 M	TW A:155 2031 | AC REHAB $0.13 M
TW A5:175 2031 | AC REHAB $1.18 M	TW B:210 2031 | AC REHAB $0.13 M	TW G3:607 2031 | AC REHAB $0.18 M	TW G2:1005 2031 | AC REHAB $0.33 M	TW G2:1010 2031 | AC REHAB $0.19 M	TW B:205 2032 | AC REHAB $0.14 M	TW B:207 2032 | AC REHAB $0.17 M	TW T:2050 2032 | AC REHAB $3.25 M

LEGEND

RW 13-31 — TYPICAL RUNWAY BRANCH ID

TW A — TYPICAL TAXIWAY BRANCH ID

AP S — TYPICAL APRON BRANCH ID

PROGRAM YEAR

2023	2028
2024	2029
2025	2030
2026	2031
2027	2032

"BRANCH":"SECTION"
"YEAR"|"REHAB ACTIVITY"
"EST. COST"

RUNWAY LENGTHS DEPICTED IN THIS DRAWING ARE FOR PAVEMENT MANAGEMENT PURPOSES ONLY AND MAY NOT MATCH PUBLISHED RUNWAY LENGTHS. DRAWING NOT TO SCALE.

PIE

AIRFIELD PAVEMENT MAJOR REHABILITATION EXHIBIT

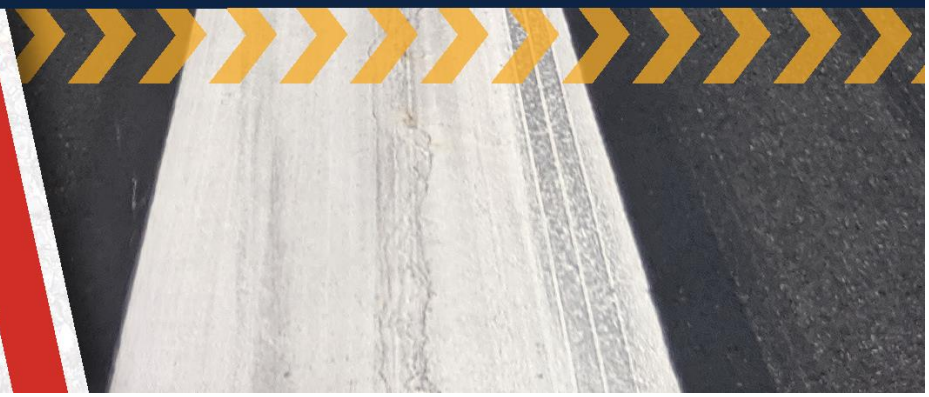
Statewide Airfield Pavement Management Program

ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

FDOT

2022

Chapter 7: Conclusion



Chapter 7 – Conclusion

7.1 Recommendations

7.1.1 Continued PCI Surveys

It is recommended that the Airport continue to perform regularly scheduled PCI surveys in accordance with the ASTM D5340-20 (or latest edition) to monitor the condition of airfield pavement facilities.

A high priority should be placed on maintaining good record keeping and re-inspecting the Airport's maintained pavement facilities to ensure continued safe aircraft operations. Per the FAA AC 150/5380-7B, a series of scheduled periodic inspections must be carried out for an effective maintenance program. Re-inspection of pavements should be scheduled in a timely manner to ensure that all areas, particularly those that may not come under day-to-day observation, are thoroughly evaluated and reported.

7.1.2 Localized Maintenance and Repair

While deterioration of the pavements due to usage and exposure to the environment cannot be prevented, applying timely and effective maintenance efforts can slow the anticipated rate of deterioration. Lack of adequate and timely maintenance is a significant factor in pavement deterioration. **Chapter 6** identified localized maintenance and repair needs. It is recommended that Airport sponsors coordinate with their respective Airport maintenance staff and Airport engineer when developing project-level maintenance and repair efforts.

7.1.3 Major Rehabilitation

Chapter 6 also identified major pavement rehabilitation project needs from 2023-2032. Identification of these rehabilitation needs are performed at the section level for manageable project areas and assume an unconstrained budget scenario. Given the uncertainty in Airport-specific budget information and prioritization goals, the unconstrained budget scenario represents a conservative scenario and identifies pavement needs over a 10-year period. Certainly, it is understood that most airports are faced with constrained budgets, thus further evaluation of projects based on prioritization, operational criticality, funding availability, and practicality is recommended.

7.1.4 Pavement Management System

The following recommendations are made to fully implement an effective pavement management program for the Airport:

- Develop a detailed preventive maintenance program for the Airport based on the recommendations provided in **Section 6.1**;
- Further refine and implement the identified 10-year major rehabilitation needs provided in **Section 6.2**;
- Maintain detailed records on pavement maintenance, construction, and inspection; and
- Maintain records on major pavement construction projects (year, scope, cost, and construction documents).

7.2 Supporting Documents

Airfield Pavement Network Definition Exhibit

The Airfield Pavement Network Definition Exhibit is located in **Chapter 3** and **Appendix C**. The Exhibit depicts the airfield layout in a manner that defines the airfield pavement infrastructure as branches, sections, and sample units in accordance with the ASTM D5340-20. The Exhibit is intended for planning purposes only. Further details can be found on the Airport's adopted Airport Layout Plan. Detailed characteristics are tabulated in **Appendix A.**

Airfield Pavement System Inventory Exhibit

The Airfield Pavement System Inventory Exhibit is located in **Chapter 3** and **Appendix C**. The Exhibit depicts recent and/or anticipated construction activity within the airfield pavement facilities reported by Airport staff. The Exhibit is intended to schematically identify the pavement limits of work and general work description. The information reported on the Airport Response Form provided by each participating airport was used as the basis of the changes. Furthermore, changes are confirmed at the Airport with Airport staff during the in-brief and debrief meeting.

Airfield Pavement Estimated Age Exhibit

The Airfield Pavement Estimated Age Exhibit is located in **Chapter 3** and **Appendix C**. Based on the review of historic airfield pavement construction activities, the Exhibit provides the approximate limits of the age of the pavement sections since the last major construction activity has occurred. This is intended to be a rough estimate based on interpretation of the limited data available at the time of report.

Airfield Pavement Condition Index Exhibit

The Airfield Pavement Condition Index Exhibit is located in **Chapter 4** and **Appendix C**. The Exhibit is a visual summary of the latest conditions reported from the PCI assessment performed at the Airport. Distress analysis occurred in accordance with ASTM D5340-20 (referenced in **Appendix E**), with results being analyzed using PAVER™ software to determine PCI values. The PCI values are identified in the Exhibit and graphically represented using the standard ASTM D5340-20 condition rating categories.

Airfield Pavement Major Rehabilitation Exhibit

The Airfield Pavement Major Rehabilitation Exhibit is located in **Chapter 6** and **Appendix C**. The Exhibit has been prepared based on the section condition analysis, pavement condition forecasts, and major rehabilitation needs analysis. The Exhibit graphically depicts the inventory with the associated rehabilitation type activity, program year, and the planning-level costs. Area limits, rehabilitation type, and planning-level costs should not be considered a design-level recommendation. A tabulation of the 10-Year Major Rehabilitation is located in **Appendix B**.

Inspection Photograph Documentation

Representative field conditions from the PCI assessment are documented with digital photographs located in **Appendix D**. Select photographs are provided with a limited caption on the distress(es) observed. "Vicinity" photos refer to the approximate boundaries of an inspected sample unit within the section and provide an overview of the section condition but are not focused on a specific distress. The Appendix does not contain photographs for every section and sample unit.

7.3 Conclusion

The FDOT SAPMP System Update Phase 2 2021-2023 was completed for the Airport on behalf of the FDOT AO in accordance with the FAA AC 150/5380-7B and 150/5380-6C. FDOT's implementation of the SAPMP has assisted public airports with this requirement in performing PCI survey inspections and analysis in accordance with the ASTM D5340-20.

7.4 References

The following documents are referenced as specific guidelines and procedures for maintaining Airport pavements, establishing an effective pavement maintenance program, and identifying specific pavement distresses, probable causes of distresses, survey guidelines, and recommended methods of repair.

- ASTM D5340-20, Standard Test Method for Airport Pavement Condition Index Surveys, American Society for Testing and Materials, West Conshohocken, PA, 2018.
- AC 150/5210-24 Airport Foreign Object Debris (FOD) Management, Federal Aviation Administration, Washington, D.C., 2010.
- AC 150/5320-6F, Airport Pavement Design and Evaluation, Federal Aviation Administration, Washington, D.C., 2016.
- AC 150/5380-7B, Airport Pavement Management Program (PMP), Federal Aviation Administration, Washington, D.C., 2014.
- AC 150/5380-6C, Guidelines and Procedures for Maintenance of Airport Pavements, Federal Aviation Administration, Washington, D.C., 2014.
- AC 150/5370-10H, Standard Specifications for Construction of Airports, Federal Aviation Administration, Washington, D.C., 2018.
- Airport Improvement Program Handbook, Order 5100.38D, Change 1, Federal Aviation Administration, Washington, D.C., 2019.
- Tri-Service Pavements Working Group (TSPWG) Manual 3-270-08. 14-03, Preventive Maintenance Plan (PMP) for Airfield Pavements, Department of Defense, Washington, D.C., 2019.
- Unified Facilities Criteria (UFC) 3-260-16, O&M Manual: Standard Practice for Airfield Pavement Condition Surveys, Department of Defense, Washington, D.C., 2019.
- Unified Facilities Criteria (UFC) 3-260-03, Airfield Pavement Evaluation, Department of Defense, Washington, D.C., 2001.
- Shahin, Mohamed Y., Pavement Management for Airports, Roads, and Parking Lots, Springer, 2005.

Appendix A: Airfield Pavement Analysis

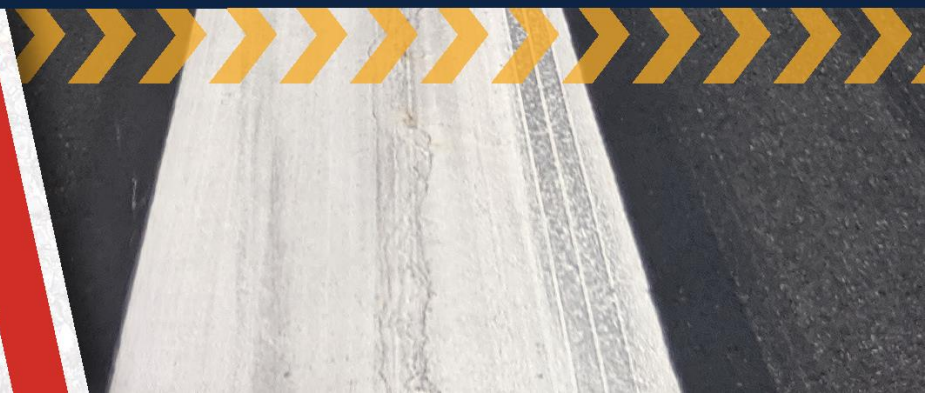


Table A.1: Pavement System Inventory Details

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	Surface Type	Estimate of Last Construction Date
PIE	RW 4-22	Runway	6205	474,873	AAC	1/1/2012
PIE	RW 4-22	Runway	6210	237,436	AAC	1/1/2012
PIE	RW 4-22	Runway	6215	50,072	AAC	1/1/2012
PIE	RW 4-22	Runway	6220	25,036	AAC	1/1/2012
PIE	RW 4-22	Runway	6225	45,300	AC	1/1/2006
PIE	RW 4-22	Runway	6230	22,650	AC	1/1/2006
PIE	RW 4-22	Runway	6235	9,700	AC	4/1/2020
PIE	RW 4-22	Runway	6240	4,850	AC	4/1/2020
PIE	RW 18-36	Runway	6115	135,960	AC	12/1/2020
PIE	RW 18-36	Runway	6120	176,940	AAC	12/1/2020
PIE	RW 18-36	Runway	6155	99,000	AAC	12/1/2020
PIE	RW 18-36	Runway	6156	18,000	AC	12/1/2020
PIE	RW 18-36	Runway	6157	12,000	AC	12/1/2020
PIE	RW 18-36	Runway	6160	148,500	AAC	12/1/2020
PIE	RW 18-36	Runway	6165	40,500	AC	12/1/2020
PIE	RW 18-36	Runway	6170	60,750	AAC	12/1/2020
PIE	RW 18-36	Runway	6175	170,280	AAC	12/1/2020
PIE	RW 18-36	Runway	6180	255,420	AAC	12/1/2020
PIE	RW 18-36	Runway	6185	126,000	AAC	12/1/2020
PIE	RW 18-36	Runway	6190	189,000	AAC	12/1/2020
PIE	RW 18-36	Runway	6196	27,000	AAC	12/1/2020
PIE	FBO CONN	Taxiway	107	3,297	AAC	1/1/1990
PIE	FBO CONN	Taxiway	108	3,361	AC	1/1/1994
PIE	FBO CONN	Taxiway	112	4,221	AAC	1/1/1990
PIE	FBO CONN	Taxiway	114	2,361	AC	1/1/1968
PIE	FBO CONN	Taxiway	117	6,019	AAC	8/1/2016
PIE	FBO CONN	Taxiway	119	3,041	AC	1/1/1968
PIE	FBO CONN	Taxiway	125	4,598	APC	8/1/2016
PIE	FBO CONN	Taxiway	127	12,891	APC	8/1/2016
PIE	TW A	Taxiway	115	224,709	AAC	8/1/2016
PIE	TW A	Taxiway	130	358,395	AAC	8/1/2016
PIE	TW A	Taxiway	132	23,007	AAC	5/1/2019
PIE	TW A	Taxiway	155	6,259	AAC	8/1/2016
PIE	TW A	Taxiway	158	16,692	AAC	8/1/2016
PIE	TW A	Taxiway	160	99,856	AAC	1/1/2017
PIE	TW A1	Taxiway	135	40,056	AAC	8/1/2016
PIE	TW A1	Taxiway	140	14,541	AAC	8/1/2016
PIE	TW A1	Taxiway	145	2,945	AC	12/1/2020
PIE	TW A2	Taxiway	165	60,458	AC	8/1/2016
PIE	TW A3	Taxiway	168	60,311	AC	8/1/2016
PIE	TW A4	Taxiway	170	58,588	AC	8/1/2016
PIE	TW A5	Taxiway	175	56,987	AC	8/1/2016
PIE	TW A6	Taxiway	180	58,658	AC	8/1/2016
PIE	TW A7	Taxiway	162	52,089	AAC	1/1/2017

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	Surface Type	Estimate of Last Construction Date
PIE	TW B	Taxiway	205	6,200	AAC	6/1/2018
PIE	TW B	Taxiway	207	7,750	AAC	6/1/2018
PIE	TW B	Taxiway	210	6,353	AAC	6/1/2018
PIE	TW B	Taxiway	212	18,000	AAC	6/1/2018
PIE	TW B	Taxiway	215	15,387	AC	1/1/2012
PIE	TW B	Taxiway	220	40,670	AC	1/1/1965
PIE	TW B	Taxiway	225	17,624	AC	6/1/2018
PIE	TW F	Taxiway	610	43,041	AC	6/1/2018
PIE	TW F	Taxiway	615	4,165	AAC	12/1/2020
PIE	TW G	Taxiway	1315	19,536	AC	6/1/2018
PIE	TW G	Taxiway	1320	15,822	AAC	6/1/2018
PIE	TW G	Taxiway	1325	199,036	AAC	6/1/2018
PIE	TW G	Taxiway	1340	14,004	AAC	6/1/2018
PIE	TW G1	Taxiway	1330	13,135	AAC	6/1/2018
PIE	TW G1	Taxiway	1335	12,530	AAC	6/1/2018
PIE	TW G2	Taxiway	1005	15,843	AAC	6/1/2018
PIE	TW G2	Taxiway	1010	8,964	AAC	6/1/2018
PIE	TW G3	Taxiway	605	10,930	AAC	1/1/1984
PIE	TW G3	Taxiway	607	8,732	AAC	1/1/2012
PIE	TW H	Taxiway	810	59,729	AAC	1/2/1965
PIE	TW H	Taxiway	815	57,784	AC	8/1/2016
PIE	TW H	Taxiway	820	4,760	AC	1/1/2017
PIE	TW J	Taxiway	905	8,851	AC	4/1/2020
PIE	TW L	Taxiway	1205	22,175	AC	8/1/2016
PIE	TW L	Taxiway	1215	13,483	AC	8/1/2016
PIE	TW L	Taxiway	1245	52,150	AC	8/1/2016
PIE	TW P	Taxiway	1250	27,739	AC	1/1/2016
PIE	TW P	Taxiway	1255	52,339	AC	1/1/2016
PIE	TW Q	Taxiway	1705	4,449	AAC	6/1/2018
PIE	TW Q	Taxiway	1710	3,632	AC	6/1/2018
PIE	TW T	Taxiway	2010	12,963	AAC	6/1/2018
PIE	TW T	Taxiway	2020	14,337	AAC	6/1/2018
PIE	TW T	Taxiway	2045	16,549	AAC	8/1/2016
PIE	TW T	Taxiway	2050	149,440	AAC	6/1/2018
PIE	TL T-HANG	Taxilane	1120	1,346	AC	1/1/1984
PIE	TL T-HANG	Taxilane	1125	1,472	AC	1/1/1984
PIE	AP MAIN	Apron	4103	122,390	PCC	1/1/2023
PIE	AP MAIN	Apron	4105	40,910	APC	1/2/2003
PIE	AP MAIN	Apron	4107	220,315	PCC	1/1/2016
PIE	AP MAIN	Apron	4110	56,000	APC	6/1/2018
PIE	AP MAIN	Apron	4123	43,794	APC	6/1/2018
PIE	AP MAIN	Apron	4150	14,083	AAC	6/1/2018
PIE	AP MAIN	Apron	4155	33,689	AAC	1/1/2003
PIE	AP MAIN	Apron	4157	92,541	AAC	8/1/2016
PIE	AP MAIN	Apron	4160	59,640	PCC	1/1/2016
PIE	AP MAIN	Apron	4165	66,649	PCC	1/1/2012

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	Surface Type	Estimate of Last Construction Date
PIE	AP MAIN	Apron	4170	18,816	AAC	8/1/2016
PIE	AP MAIN	Apron	4175	14,910	PCC	1/1/1942
PIE	AP MAIN	Apron	4176	3,573	AC	12/25/1955
PIE	AP MAIN	Apron	4177	20,899	APC	8/1/2016
PIE	AP MAIN	Apron	4178	59,522	APC	1/1/2013
PIE	AP MAIN	Apron	4179	77,111	APC	10/1/2011
PIE	AP MAIN	Apron	4180	126,695	AAC	1/2/1968
PIE	AP MAIN	Apron	4183	39,947	AAC	1/1/2013
PIE	AP MAIN	Apron	4185	12,820	APC	1/1/2013
PIE	AP MAIN	Apron	4190	18,650	PCC	1/1/1942
PIE	AP MAIN	Apron	4195	11,250	PCC	1/1/1942
PIE	AP MAIN	Apron	4198	18,579	PCC	1/1/2003
PIE	AP MAIN	Apron	4199	25,200	PCC	1/1/2023

Table A.2: Pavement Condition Index Summary (Current PCI Survey) – Section Level

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	PCI	Condition Rating
PIE	RW 4-22	Runway	6205	474,873	73	Satisfactory
PIE	RW 4-22	Runway	6210	237,436	79	Satisfactory
PIE	RW 4-22	Runway	6215	50,072	63	Fair
PIE	RW 4-22	Runway	6220	25,036	70	Fair
PIE	RW 4-22	Runway	6225	45,300	54	Poor
PIE	RW 4-22	Runway	6230	22,650	24	Serious
PIE	RW 4-22	Runway	6235	9,700	100	Good
PIE	RW 4-22	Runway	6240	4,850	100	Good
PIE	RW 18-36	Runway	6115	135,960	100	Good
PIE	RW 18-36	Runway	6120	176,940	100	Good
PIE	RW 18-36	Runway	6155	99,000	100	Good
PIE	RW 18-36	Runway	6156	18,000	100	Good
PIE	RW 18-36	Runway	6157	12,000	100	Good
PIE	RW 18-36	Runway	6160	148,500	100	Good
PIE	RW 18-36	Runway	6165	40,500	100	Good
PIE	RW 18-36	Runway	6170	60,750	100	Good
PIE	RW 18-36	Runway	6175	170,280	100	Good
PIE	RW 18-36	Runway	6180	255,420	100	Good
PIE	RW 18-36	Runway	6185	126,000	100	Good
PIE	RW 18-36	Runway	6190	189,000	100	Good
PIE	RW 18-36	Runway	6196	27,000	100	Good
PIE	FBO CONN	Taxiway	107	3,297	45	Poor
PIE	FBO CONN	Taxiway	108	3,361	32	Very Poor
PIE	FBO CONN	Taxiway	112	4,221	42	Poor
PIE	FBO CONN	Taxiway	114	2,361	59	Fair
PIE	FBO CONN	Taxiway	117	6,019	77	Satisfactory
PIE	FBO CONN	Taxiway	119	3,041	29	Very Poor
PIE	FBO CONN	Taxiway	125	4,598	58	Fair
PIE	FBO CONN	Taxiway	127	12,891	82	Satisfactory
PIE	TW A	Taxiway	115	224,709	65	Fair
PIE	TW A	Taxiway	130	358,395	71	Satisfactory
PIE	TW A	Taxiway	132	23,007	88	Good
PIE	TW A	Taxiway	155	6,259	89	Good
PIE	TW A	Taxiway	158	16,692	63	Fair
PIE	TW A	Taxiway	160	99,856	82	Satisfactory
PIE	TW A1	Taxiway	135	40,056	64	Fair
PIE	TW A1	Taxiway	140	14,541	67	Fair
PIE	TW A1	Taxiway	145	2,945	100	Good
PIE	TW A2	Taxiway	165	60,458	74	Satisfactory
PIE	TW A3	Taxiway	168	60,311	77	Satisfactory
PIE	TW A4	Taxiway	170	58,588	85	Satisfactory
PIE	TW A5	Taxiway	175	56,987	82	Satisfactory
PIE	TW A6	Taxiway	180	58,658	86	Good
PIE	TW A7	Taxiway	162	52,089	83	Satisfactory
PIE	TW B	Taxiway	205	6,200	91	Good

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	PCI	Condition Rating
PIE	TW B	Taxiway	207	7,750	90	Good
PIE	TW B	Taxiway	210	6,353	88	Good
PIE	TW B	Taxiway	212	18,000	80	Satisfactory
PIE	TW B	Taxiway	215	15,387	85	Satisfactory
PIE	TW B	Taxiway	220	40,670	16	Serious
PIE	TW B	Taxiway	225	17,624	89	Good
PIE	TW F	Taxiway	610	43,041	76	Satisfactory
PIE	TW F	Taxiway	615	4,165	100	Good
PIE	TW G	Taxiway	1315	19,536	69	Fair
PIE	TW G	Taxiway	1320	15,822	62	Fair
PIE	TW G	Taxiway	1325	199,036	65	Fair
PIE	TW G	Taxiway	1340	14,004	82	Satisfactory
PIE	TW G1	Taxiway	1330	13,135	65	Fair
PIE	TW G1	Taxiway	1335	12,530	63	Fair
PIE	TW G2	Taxiway	1005	15,843	89	Good
PIE	TW G2	Taxiway	1010	8,964	89	Good
PIE	TW G3	Taxiway	605	10,930	24	Serious
PIE	TW G3	Taxiway	607	8,732	88	Good
PIE	TW H	Taxiway	810	59,729	5	Failed
PIE	TW H	Taxiway	815	57,784	87	Good
PIE	TW H	Taxiway	820	4,760	88	Good
PIE	TW J	Taxiway	905	8,851	100	Good
PIE	TW L	Taxiway	1205	22,175	77	Satisfactory
PIE	TW L	Taxiway	1215	13,483	76	Satisfactory
PIE	TW L	Taxiway	1245	52,150	79	Satisfactory
PIE	TW P	Taxiway	1250	27,739	85	Satisfactory
PIE	TW P	Taxiway	1255	52,339	91	Good
PIE	TW Q	Taxiway	1705	4,449	94	Good
PIE	TW Q	Taxiway	1710	3,632	94	Good
PIE	TW T	Taxiway	2010	12,963	87	Good
PIE	TW T	Taxiway	2020	14,337	86	Good
PIE	TW T	Taxiway	2045	16,549	74	Satisfactory
PIE	TW T	Taxiway	2050	149,440	90	Good
PIE	TL T-HANG	Taxilane	1120	1,346	39	Very Poor
PIE	TL T-HANG	Taxilane	1125	1,472	38	Very Poor
PIE	AP MAIN	Apron	4103	122,390	100	Good
PIE	AP MAIN	Apron	4105	40,910	30	Very Poor
PIE	AP MAIN	Apron	4107	220,315	97	Good
PIE	AP MAIN	Apron	4110	56,000	74	Satisfactory
PIE	AP MAIN	Apron	4123	43,794	83	Satisfactory
PIE	AP MAIN	Apron	4150	14,083	79	Satisfactory
PIE	AP MAIN	Apron	4155	33,689	59	Fair
PIE	AP MAIN	Apron	4157	92,541	67	Fair
PIE	AP MAIN	Apron	4160	59,640	96	Good
PIE	AP MAIN	Apron	4165	66,649	95	Good
PIE	AP MAIN	Apron	4170	18,816	87	Good
PIE	AP MAIN	Apron	4175	14,910	5	Failed

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	PCI	Condition Rating
PIE	AP MAIN	Apron	4176	3,573	28	Very Poor
PIE	AP MAIN	Apron	4177	20,899	80	Satisfactory
PIE	AP MAIN	Apron	4178	59,522	62	Fair
PIE	AP MAIN	Apron	4179	77,111	66	Fair
PIE	AP MAIN	Apron	4180	126,695	45	Poor
PIE	AP MAIN	Apron	4183	39,947	68	Fair
PIE	AP MAIN	Apron	4185	12,820	51	Poor
PIE	AP MAIN	Apron	4190	18,650	14	Serious
PIE	AP MAIN	Apron	4195	11,250	9	Failed
PIE	AP MAIN	Apron	4198	18,579	20	Serious
PIE	AP MAIN	Apron	4199	25,200	100	Good

Table A.3: Forecasted PCI Values 2023-2032 – Section-Level

Network ID	Branch ID	Section ID	Current PCI	2023	2024	2025	2026	2027	2028	2029	2030	2031	2032
PIE	RW 4-22	6205	73	71	69	67	65	63	61	59	57	55	53
PIE	RW 4-22	6210	79	77	75	73	71	69	67	65	63	61	59
PIE	RW 4-22	6215	63	61	59	57	55	53	51	49	47	45	43
PIE	RW 4-22	6220	70	68	66	64	62	60	58	56	54	52	50
PIE	RW 4-22	6225	54	52	51	49	48	46	45	43	42	40	39
PIE	RW 4-22	6230	24	22	21	19	18	16	15	13	12	10	9
PIE	RW 4-22	6235	100	95	94	92	91	89	88	86	85	83	82
PIE	RW 4-22	6240	100	95	94	92	91	89	88	86	85	83	82
PIE	RW 18-36	6115	100	96	95	93	92	90	89	87	86	84	83
PIE	RW 18-36	6120	100	95	93	91	89	87	85	83	81	80	78
PIE	RW 18-36	6155	100	95	93	91	89	87	85	83	81	80	78
PIE	RW 18-36	6156	100	96	95	93	92	90	89	87	86	84	83
PIE	RW 18-36	6157	100	96	95	93	92	90	89	87	86	84	83
PIE	RW 18-36	6160	100	95	93	91	89	87	85	83	81	80	78
PIE	RW 18-36	6165	100	96	95	93	92	90	89	87	86	84	83
PIE	RW 18-36	6170	100	95	93	91	89	87	85	83	81	80	78
PIE	RW 18-36	6175	100	95	93	91	89	87	85	83	81	80	78
PIE	RW 18-36	6180	100	95	93	91	89	87	85	83	81	80	78
PIE	RW 18-36	6185	100	95	93	91	89	87	85	83	81	80	78
PIE	RW 18-36	6190	100	95	93	91	89	87	85	83	81	80	78
PIE	RW 18-36	6196	100	95	93	91	89	87	85	83	81	80	78
PIE	FBO CONN	107	45	44	43	41	40	38	36	34	32	30	27
PIE	FBO CONN	108	32	29	27	25	23	21	19	17	15	13	11
PIE	FBO CONN	112	42	40	39	37	35	33	30	28	25	21	18
PIE	FBO CONN	114	59	58	57	56	55	55	54	53	52	51	50
PIE	FBO CONN	117	77	75	73	71	69	67	66	64	63	62	60
PIE	FBO CONN	119	29	26	24	22	20	18	16	14	12	10	8
PIE	FBO CONN	125	58	57	56	55	54	54	53	52	52	51	50
PIE	FBO CONN	127	82	79	77	75	73	71	70	68	66	65	63
PIE	TW A	115	65	63	62	61	59	58	57	56	56	55	54
PIE	TW A	130	71	69	67	66	64	63	61	60	59	58	57
PIE	TW A	132	88	85	83	81	79	76	74	73	71	69	67
PIE	TW A	155	89	86	84	82	79	77	75	73	71	70	68
PIE	TW A	158	63	61	60	59	58	57	56	55	54	54	53
PIE	TW A	160	82	79	77	75	73	71	70	68	66	65	63
PIE	TW A1	135	64	62	61	60	59	58	57	56	55	54	53
PIE	TW A1	140	67	65	64	62	61	60	59	58	57	56	55
PIE	TW A1	145	100	94	92	90	88	86	84	83	81	80	78
PIE	TW A2	165	74	72	71	70	69	68	67	66	65	64	63
PIE	TW A3	168	77	75	74	73	72	70	69	68	67	66	65
PIE	TW A4	170	85	83	81	80	78	77	75	74	73	72	71
PIE	TW A5	175	82	80	79	77	76	74	73	72	71	70	69
PIE	TW A6	180	86	84	82	81	79	78	76	75	74	72	71
PIE	TW A7	162	83	80	78	76	74	72	70	69	67	65	64

Network ID	Branch ID	Section ID	Current PCI	2023	2024	2025	2026	2027	2028	2029	2030	2031	2032
PIE	TW B	205	91	88	86	83	81	79	77	75	73	71	69
PIE	TW B	207	90	87	85	82	80	78	76	74	72	70	69
PIE	TW B	210	88	85	83	81	79	76	74	73	71	69	67
PIE	TW B	212	80	77	75	73	72	70	68	66	65	63	62
PIE	TW B	215	85	83	81	80	78	77	75	74	73	72	71
PIE	TW B	220	16	13	11	9	7	5	3	1	0	0	0
PIE	TW B	225	89	87	85	83	82	80	78	77	76	74	73
PIE	TW F	610	76	74	73	72	71	70	69	68	67	66	65
PIE	TW F	615	100	94	91	89	86	84	82	80	78	76	74
PIE	TW G	1315	69	68	67	66	65	64	63	62	61	61	60
PIE	TW G	1320	62	60	59	58	57	56	55	55	54	53	52
PIE	TW G	1325	65	63	62	61	59	58	57	56	56	55	54
PIE	TW G	1340	82	79	77	75	73	71	70	68	66	65	63
PIE	TW G1	1330	65	63	62	61	59	58	57	56	56	55	54
PIE	TW G1	1335	63	61	60	59	58	57	56	55	54	54	53
PIE	TW G2	1005	89	86	84	82	79	77	75	73	71	70	68
PIE	TW G2	1010	89	86	84	82	79	77	75	73	71	70	68
PIE	TW G3	605	24	20	16	12	7	2	0	0	0	0	0
PIE	TW G3	607	88	85	83	81	79	76	74	73	71	69	67
PIE	TW H	810	5	0	0	0	0	0	0	0	0	0	0
PIE	TW H	815	87	85	83	81	80	78	77	76	74	73	72
PIE	TW H	820	88	86	84	82	81	79	78	76	75	74	72
PIE	TW J	905	100	93	91	89	87	85	83	82	80	79	77
PIE	TW L	1205	77	75	74	73	72	70	69	68	67	66	65
PIE	TW L	1215	76	74	73	72	71	70	69	68	67	66	65
PIE	TW L	1245	79	77	76	75	73	72	71	70	69	68	67
PIE	TW P	1250	85	83	81	80	78	77	75	74	73	72	71
PIE	TW P	1255	91	89	87	85	83	82	80	78	77	76	74
PIE	TW Q	1705	94	91	89	86	84	82	80	77	75	73	72
PIE	TW Q	1710	94	91	89	88	86	84	82	81	79	78	76
PIE	TW T	2010	87	84	82	80	78	76	74	72	70	68	67
PIE	TW T	2020	86	83	81	79	77	75	73	71	69	68	66
PIE	TW T	2045	74	72	70	68	67	65	64	62	61	60	59
PIE	TW T	2050	90	87	85	82	80	78	76	74	72	70	69
PIE	TL T-HANG	1120	39	37	35	33	31	29	27	25	23	21	19
PIE	TL T-HANG	1125	38	36	34	32	30	28	26	24	22	20	18
PIE	AP MAIN	4103	100	99	98	97	96	94	93	92	91	90	89
PIE	AP MAIN	4105	30	27	25	22	20	17	14	11	9	6	3
PIE	AP MAIN	4107	97	96	94	93	92	91	90	89	89	88	87
PIE	AP MAIN	4110	74	72	70	68	66	65	63	62	60	59	57
PIE	AP MAIN	4123	83	80	78	76	74	72	70	68	66	65	63
PIE	AP MAIN	4150	79	76	74	72	70	69	67	65	64	62	61
PIE	AP MAIN	4155	59	57	56	54	53	52	50	49	47	46	44
PIE	AP MAIN	4157	67	65	63	62	60	59	57	56	55	53	52
PIE	AP MAIN	4160	96	95	93	92	91	90	90	89	88	87	86
PIE	AP MAIN	4165	95	94	93	92	91	90	89	88	87	86	86

Network ID	Branch ID	Section ID	Current PCI	2023	2024	2025	2026	2027	2028	2029	2030	2031	2032
PIE	AP MAIN	4170	87	84	81	79	77	75	73	71	69	67	66
PIE	AP MAIN	4175	5	1	0	0	0	0	0	0	0	0	0
PIE	AP MAIN	4176	28	26	24	23	21	19	18	16	14	13	11
PIE	AP MAIN	4177	80	77	75	73	71	69	68	66	64	63	61
PIE	AP MAIN	4178	62	60	59	57	56	54	53	52	50	49	47
PIE	AP MAIN	4179	66	64	62	61	59	58	57	55	54	52	51
PIE	AP MAIN	4180	45	43	41	40	38	36	34	32	30	28	26
PIE	AP MAIN	4183	68	66	64	63	61	60	58	57	55	54	53
PIE	AP MAIN	4185	51	49	48	46	45	43	42	40	38	36	34
PIE	AP MAIN	4190	14	10	7	3	0	0	0	0	0	0	0
PIE	AP MAIN	4195	9	5	1	0	0	0	0	0	0	0	0
PIE	AP MAIN	4198	20	16	13	10	7	3	0	0	0	0	0
PIE	AP MAIN	4199	100	99	98	97	96	94	93	92	91	90	89

Network: ST. PETE-CLEARW **Branch:** AP MAIN MAIN APRON **Section:** 4103 **Surface:**PCC

L.C.D. 1/1/2023 **Use:** APRON **Rank:** P **Length:** 295.00 (Ft) **Width:** 500.00 (Ft) **True Area:** 122390.0000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2023	CR-PC	Complete Reconstruction - PCC	0.00	0.00	☑	
1/2/2003	OL-AS	Overlay - AC Structural	0.00	0.00	☑	
1/1/2003	ML-OVL	Mill and Overlay	0.00	0.00	☑	
1/2/1942	OL-AS	Overlay - AC Structural	221,841.00	0.00	☑	ASSUME 1942 PCC PAVEMENT
1/1/1942	NC-PC	New Construction - PCC	0.00	0.00	☑	EST.

Network: ST. PETE-CLEARW **Branch:** AP MAIN MAIN APRON **Section:** 4105 **Surface:**APC

L.C.D. 1/2/2003 **Use:** APRON **Rank:** P **Length:** 315.00 (Ft) **Width:** 50.00 (Ft) **True Area:** 40910.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/2/2003	OL-AS	Overlay - AC Structural	0.00	0.00	☑	
1/1/2003	ML-OVL	Mill and Overlay	0.00	0.00	☑	
1/2/1942	OL-AS	Overlay - AC Structural	221,841.00	0.00	☑	ASSUME 1942 PCC PAVEMENT
1/1/1942	NC-PC	New Construction - PCC	0.00	0.00	☑	EST.

Network: ST. PETE-CLEARW **Branch:** AP MAIN MAIN APRON **Section:** 4107 **Surface:**PCC

L.C.D. 1/1/2016 **Use:** APRON **Rank:** P **Length:** 730.00 (Ft) **Width:** 295.00 (Ft) **True Area:** 220315.0000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2016	CR-PC	Complete Reconstruction - PCC	0.00	0.00	☑	
1/1/2003	ML-OVL	Mill and Overlay	0.00	2.00	☑	
1/1/1942	IMPORTED	BUILT	0.00	0.00	☑	AC OIVERLAY ON
1/1/1942	IMPORTED	OVERLAY	0.00	0.00	☑	ASSUME 1942 PCC PAVEMENT

Network: ST. PETE-CLEARW **Branch:** AP MAIN MAIN APRON **Section:** 4110 **Surface:**APC

L.C.D. 6/1/2018 **Use:** APRON **Rank:** P **Length:** 1,120.00 (Ft) **Width:** 50.00 (Ft) **True Area:** 56000.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
6/1/2018	ML-OVL	Mill and Overlay	0.00	0.00	☑	0"-2" Mill, 2" Overlay P-401
1/1/2003	ML-OVL	Mill and Overlay	0.00	2.00	☑	
1/2/1942	OL-AS	Overlay - AC Structural	0.00	0.00	☑	AC OVERLAY ON
1/1/1942	NC-PC	New Construction - PCC	0.00	0.00	☑	ASSUME 1942 PCC PAVEMENT

Network: ST. PETE-CLEARW **Branch:** AP MAIN MAIN APRON **Section:** 4123 **Surface:**APC

L.C.D. 6/1/2018 **Use:** APRON **Rank:** P **Length:** 1,460.00 (Ft) **Width:** 30.00 (Ft) **True Area:** 43794.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
6/1/2018	ML-OVL	Mill and Overlay	0.00	0.00	☑	
1/1/2003	ML-OVL	Mill and Overlay	0.00	2.00	☑	
1/1/1997	IMPORTED	BUILT	0.00	0.00	☑	1997 AC OVERLAY ON
1/1/1997	IMPORTED	OVERLAY	0.00	0.00	☑	EXISTING PAVEMENT SECTION

Network: ST. PETE-CLEARW **Branch:** AP MAIN MAIN APRON **Section:** 4150 **Surface:**AAC

L.C.D. 6/1/2018 **Use:** APRON **Rank:** P **Length:** 285.00 (Ft) **Width:** 50.00 (Ft) **True Area:** 14083.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
6/1/2018	OL-AS	Overlay - AC Structural	0.00	0.00	☑	0"-2" Mill, 2" Overlay P-401
1/1/2003	ML-OVL	Mill and Overlay	0.00	2.00	☑	
1/2/1990	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	THIS FEATURE HAS AN EMULSIO
1/1/1990	IMPORTED	OVERLAY	0.00	3.00	☑	1990: 3" P-401 OVERLAY
1/1/1955	IMPORTED	BUILT	0.00	1.50	☑	1955: 1.5" P-401 ON 1.5" P-201 ON 10" P-211

Network: ST. PETE-CLEARW **Branch:** AP MAIN MAIN APRON **Section:** 4155 **Surface:**AAC

L.C.D. 1/1/2003 **Use:** APRON **Rank:** P **Length:** 275.00 (Ft) **Width:** 125.00 (Ft) **True Area:** 33689.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2003	ML-OVL	Mill and Overlay	0.00	2.00	☑	
1/2/1990	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	THIS FEATURE HAS AN EMULSIO
1/1/1990	IMPORTED	OVERLAY	0.00	3.00	☑	1990: 3" P-401 OVERLAY
1/1/1955	IMPORTED	BUILT	0.00	1.50	☑	1955: 1.5" P-401 ON 1.5" P-201 ON 10" P-211

Network: ST. PETE-CLEARW **Branch:** AP MAIN MAIN APRON **Section:** 4157 **Surface:**AAC

L.C.D. 8/1/2016 **Use:** APRON **Rank:** P **Length:** 597.00 (Ft) **Width:** 300.00 (Ft) **True Area:** 92541.00002 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
8/1/2016	ML-OVL	Mill and Overlay	0.00	0.00	☑	2" MILL AND 3" P-401SP OVERLA
1/1/2003	ML-OVL	Mill and Overlay	0.00	2.00	☑	
1/2/1990	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	THIS FEATURE HAS AN EMULSIO
1/1/1990	IMPORTED	OVERLAY	0.00	3.00	☑	1990: 3" P-401 OVERLAY
1/1/1955	IMPORTED	BUILT	0.00	1.50	☑	1955: 1.5" P-401 ON 1.5" P-201 ON 10" P-211

Network: ST. PETE-CLEARW **Branch:** AP MAIN MAIN APRON **Section:** 4160 **Surface:**PCC

L.C.D. 1/1/2016 **Use:** APRON **Rank:** P **Length:** 305.00 (Ft) **Width:** 190.00 (Ft) **True Area:** 59640.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2016	CR-PC	Complete Reconstruction - PCC	0.00	0.00	☑	Unknown
1/1/2003	ML-OVL	Mill and Overlay	0.00	2.00	☑	
1/2/1990	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	THIS FEATURE HAS AN EMULSIO
1/1/1990	IMPORTED	OVERLAY	0.00	3.00	☑	1990: 3" P-401 OVERLAY
1/1/1955	IMPORTED	BUILT	0.00	1.50	☑	1955: 1.5" P-401 ON 1.5" P-201 ON 10" P-211

Network: ST. PETE-CLEARW **Branch:** AP MAIN MAIN APRON **Section:** 4165 **Surface:**PCC

L.C.D. 1/1/2012 **Use:** APRON **Rank:** P **Length:** 800.00 (Ft) **Width:** 300.00 (Ft) **True Area:** 66649.00002 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2012	CR-PC	Complete Reconstruction - PCC	0.00	0.00	☑	EXPAND APRON HARDSTAND @
1/1/2003	ML-OVL	Mill and Overlay	0.00	2.00	☑	
1/2/1990	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	THIS FEATURE HAS AN EMULSIO
1/1/1990	IMPORTED	OVERLAY	0.00	3.00	☑	1990: 3" P-401 OVERLAY
1/1/1955	IMPORTED	BUILT	0.00	1.50	☑	1955: 1.5" P-401 ON 1.5" P-201 ON 10" P-211

Network: ST. PETE-CLEARW **Branch:** AP MAIN MAIN APRON **Section:** 4170 **Surface:**AAC

L.C.D. 8/1/2016 **Use:** APRON **Rank:** P **Length:** 170.00 (Ft) **Width:** 90.00 (Ft) **True Area:** 18816.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
8/1/2016	ML-OVL	Mill and Overlay	0.00	0.00	☑	2" MILL AND 3" P-401SP OVERLA
1/1/2003	ML-OVL	Mill and Overlay	0.00	2.00	☑	
1/2/1990	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	THIS FEATURE HAS AN EMULSIO
1/1/1990	IMPORTED	OVERLAY	0.00	3.00	☑	1990: 3" P-401 OVERLAY
1/1/1979	IMPORTED	BUILT	0.00	3.00	☑	1979: 3" P-401 ON 13.5" P-211

Network: ST. PETE-CLEARW **Branch:** AP MAIN MAIN APRON **Section:** 4175 **Surface:**PCC

L.C.D. 1/1/1942 **Use:** APRON **Rank:** P **Length:** 189.00 (Ft) **Width:** 75.00 (Ft) **True Area:** 14910.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/1942	IMPORTED	BUILT	0.00	0.00	☑	ESTIMATE 1942 CONCRETE PAVEMENT

Network: ST. PETE-CLEARW **Branch:** AP MAIN MAIN APRON **Section:** 4176 **Surface:**AC

L.C.D. 12/25/195 **Use:** APRON **Rank:** P **Length:** 75.00 (Ft) **Width:** 48.00 (Ft) **True Area:** 3573.000001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
12/25/1955	NU-IN	New Construction - Initial	0.00	0.00	☑	

Network: ST. PETE-CLEARW **Branch:** AP MAIN MAIN APRON **Section:** 4177 **Surface:**APC

L.C.D. 8/1/2016 **Use:** APRON **Rank:** P **Length:** 145.00 (Ft) **Width:** 123.00 (Ft) **True Area:** 20899.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
8/1/2016	ML-OVL	Mill and Overlay	0.00	0.00	☑	2" MILL AND 3" P-401SP OVERLA
1/1/1990	IMPORTED	OVERLAY	0.00	3.00	☑	1990: 3" P-401 OVERLAY
1/1/1990	IMPORTED	OVERLAY	0.00	7.00	☑	EXISTING 7" CONCRETE PAVEMENT
1/1/1978	IMPORTED	BUILT	0.00	0.00	☑	1978: P-401 OVERLAY

Network: ST. PETE-CLEARW **Branch:** AP MAIN MAIN APRON **Section:** 4178 **Surface:**APC

L.C.D. 1/1/2013 **Use:** APRON **Rank:** P **Length:** 240.00 (Ft) **Width:** 240.00 (Ft) **True Area:** 59522.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
11/1/2019	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	
1/1/2014	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	
1/1/2013	OL-AS	Overlay - AC Structural	0.00	0.00	☑	
1/1/1942	IMPORTED	BUILT	0.00	0.00	☑	ESTIMATE 1942 CONCRETE PAVEMENT

Network: ST. PETE-CLEARW **Branch:** AP MAIN MAIN APRON **Section:** 4179 **Surface:**APC

L.C.D. 10/1/2011 **Use:** APRON **Rank:** P **Length:** 350.00 (Ft) **Width:** 306.00 (Ft) **True Area:** 77111.00002 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
11/1/2019	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	
1/1/2014	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	
10/1/2011	OL-AS	Overlay - AC Structural	0.00	0.00	☑	
1/1/1942	NC-PC	New Construction - PCC	0.00	0.00	☑	

Network: ST. PETE-CLEARW **Branch:** AP MAIN MAIN APRON **Section:** 4180 **Surface:**AAC

L.C.D. 1/2/1968 **Use:** APRON **Rank:** P **Length:** 625.00 (Ft) **Width:** 197.00 (Ft) **True Area:** 126695.0000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
7/1/2019	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	
1/1/2014	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	
1/1/2012	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	
1/2/1968	OL-AS	Overlay - AC Structural	0.00	0.00	☑	THIS FEATURE HAS A CHIP SEAL
1/1/1968	NC-AC	New Construction - AC	0.00	1.00	☑	1968: 1" TYPE-I AC ON 6" LIME R

Network: ST. PETE-CLEARW **Branch:** AP MAIN MAIN APRON **Section:** 4183 **Surface:**AAC

L.C.D. 1/1/2013 **Use:** APRON **Rank:** P **Length:** 100.00 (Ft) **Width:** 308.00 (Ft) **True Area:** 39947.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
11/1/2019	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	
1/1/2014	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	
1/1/2013	ML-OVL	Mill and Overlay	0.00	0.00	☑	
1/2/1968	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	THIS FEATURE HAS A CHIP SEAL
1/1/1968	IMPORTED	BUILT	0.00	1.00	☑	1968: 1" TYPE-I AC ON 6" LIME ROCK BASE

Network: ST. PETE-CLEARW **Branch:** AP MAIN MAIN APRON **Section:** 4185 **Surface:**APC

L.C.D. 1/1/2013 **Use:** APRON **Rank:** P **Length:** 126.00 (Ft) **Width:** 55.00 (Ft) **True Area:** 12820.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
11/1/2019	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	
1/1/2014	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	
1/1/2013	OL-AS	Overlay - AC Structural	0.00	0.00	☑	PCC paved over as of 3/14/2013
1/1/1942	IMPORTED	BUILT	0.00	0.00	☑	ASSUME 1942 CONCRETE PAVEMENT

Network: ST. PETE-CLEARW **Branch:** AP MAIN MAIN APRON **Section:** 4190 **Surface:**PCC

L.C.D. 1/1/1942 **Use:** APRON **Rank:** P **Length:** 250.00 (Ft) **Width:** 77.00 (Ft) **True Area:** 18650.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/1942	IMPORTED	BUILT	0.00	0.00	☑	ASSUME 1942 CONCRETE PAVEMENT

Network: ST. PETE-CLEARW **Branch:** AP MAIN MAIN APRON **Section:** 4195 **Surface:**PCC

L.C.D. 1/1/1942 **Use:** APRON **Rank:** P **Length:** 250.00 (Ft) **Width:** 45.00 (Ft) **True Area:** 11250.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/1942	IMPORTED	BUILT	0.00	0.00	☑	ASSUME 1942 CONCRETE PAVEMENT

Network: ST. PETE-CLEARW **Branch:** AP MAIN MAIN APRON **Section:** 4198 **Surface:**PCC

L.C.D. 1/1/2003 **Use:** APRON **Rank:** P **Length:** 270.00 (Ft) **Width:** 70.00 (Ft) **True Area:** 18579.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2003	NU-IN	New Construction - Initial	0.00	0.00	☑	

Network: ST. PETE-CLEARW **Branch:** AP MAIN MAIN APRON **Section:** 4199 **Surface:**PCC

L.C.D. 1/1/2023 **Use:** APRON **Rank:** P **Length:** 360.00 (Ft) **Width:** 80.00 (Ft) **True Area:** 25200.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2023	CR-PC	Complete Reconstruction - PCC	0.00	0.00	☑	
1/1/2003	NU-IN	New Construction - Initial	0.00	0.00	☑	

Network: ST. PETE-CLEARW **Branch:** FBO CONN FBO CONNECTO **Section:** 107 **Surface:**AAC

L.C.D. 1/1/1990 **Use:** TAXIWAY **Rank:** P **Length:** 75.00 (Ft) **Width:** 32.00 (Ft) **True Area:** 3297.000001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
11/1/2021	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	
1/1/1990	OL-AS	Overlay - AC Structural	0.00	0.00	☑	
1/1/1968	NC-AC	New Construction - AC	0.00	0.00	☑	

Network: ST. PETE-CLEARW **Branch:** FBO CONN FBO CONNECTO **Section:** 108 **Surface:**AC

L.C.D. 1/1/1994 **Use:** TAXIWAY **Rank:** P **Length:** 50.00 (Ft) **Width:** 42.00 (Ft) **True Area:** 3361.000001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
11/1/2021	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	
1/1/1994	NC-AC	New Construction - AC	0.00	0.00	☑	

Network: ST. PETE-CLEARW **Branch:** FBO CONN FBO CONNECTO **Section:** 112 **Surface:**AAC

L.C.D. 1/1/1990 **Use:** TAXIWAY **Rank:** P **Length:** 87.00 (Ft) **Width:** 47.00 (Ft) **True Area:** 4221.000001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
11/1/2021	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	
1/1/1990	IMPORTED	OVERLAY	0.00	0.00	☑	1990: FEATHERED P-401 OVERLAY
1/1/1968	IMPORTED	BUILT	0.00	1.00	☑	1968: 1" TYPE-I AC ON 6" LIME ROCK BASE

Network: ST. PETE-CLEARW **Branch:** FBO CONN FBO CONNECTO **Section:** 114 **Surface:**AC

L.C.D. 1/1/1968 **Use:** TAXIWAY **Rank:** P **Length:** 45.00 (Ft) **Width:** 43.00 (Ft) **True Area:** 2361.000000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
11/1/2021	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	
1/1/1968	IMPORTED	BUILT	0.00	1.00	☑	1968: 1" TYPE-I AC ON 6" LIME ROCK BASE

Network: ST. PETE-CLEARW **Branch:** FBO CONN FBO CONNECTO **Section:** 117 **Surface:**AAC

L.C.D. 8/1/2016 **Use:** TAXIWAY **Rank:** P **Length:** 137.00 (Ft) **Width:** 68.00 (Ft) **True Area:** 6019.000001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
11/1/2021	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	
8/1/2016	ML-OVL	Mill and Overlay	0.00	0.00	☑	
1/1/1990	IMPORTED	OVERLAY	0.00	0.00	☑	1990: FEATHERED P-401 OVERLAY
1/1/1968	IMPORTED	BUILT	0.00	1.00	☑	1968: 1" TYPE-I AC ON 6" LIME ROCK BASE

Network: ST. PETE-CLEARW **Branch:** FBO CONN FBO CONNECTO **Section:** 119 **Surface:**AC

L.C.D. 1/1/1968 **Use:** TAXIWAY **Rank:** P **Length:** 68.00 (Ft) **Width:** 45.00 (Ft) **True Area:** 3041.000000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
11/1/2021	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	
1/1/1968	IMPORTED	BUILT	0.00	1.00	☑	1968: 1" TYPE-I AC ON 6" LIME ROCK BASE

Network: ST. PETE-CLEARW **Branch:** FBO CONN FBO CONNECTO **Section:** 125 **Surface:**APC

L.C.D. 8/1/2016 **Use:** TAXIWAY **Rank:** P **Length:** 44.00 (Ft) **Width:** 125.00 (Ft) **True Area:** 4598.000001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
8/1/2016	ML-OVL	Mill and Overlay	0.00	0.00	☑	2" MILL AND 3" P-401SP OVERLA
10/1/2011	OL-AS	Overlay - AC Structural	0.00	0.00	☑	
1/1/1942	NC-PC	New Construction - PCC	0.00	0.00	☑	

Network: ST. PETE-CLEARW **Branch:** FBO CONN FBO CONNECTO **Section:** 127 **Surface:**APC

L.C.D. 8/1/2016 **Use:** TAXIWAY **Rank:** P **Length:** 53.00 (Ft) **Width:** 125.00 (Ft) **True Area:** 12891.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
8/1/2016	ML-OVL	Mill and Overlay	0.00	0.00	☑	2" MILL AND 3" P-401SP OVERLA
1/1/1990	IMPORTED	OVERLAY	0.00	3.00	☑	1990: 3" P-401 OVERLAY
1/1/1990	IMPORTED	OVERLAY	0.00	7.00	☑	EXISTING 7" CONCRETE PAVEMENT
1/1/1978	IMPORTED	BUILT	0.00	0.00	☑	1978: P-401 OVERLAY

Network: ST. PETE-CLEARW **Branch:** RW 18-36 RUNWAY 18-36 **Section:** 6115 **Surface:**AC

L.C.D. 12/1/2020 **Use:** RUNWAY **Rank:** P **Length:** 2,266.00 (Ft) **Width:** 60.00 (Ft) **True Area:** 135960.0000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
12/1/2020	CR-AC	Complete Reconstruction - AC	0.00	0.00	☑	4" P-401, 5" P-401 Base, 6" P-211
1/1/2003	ML-OVL	Mill and Overlay	0.00	3.00	☑	MILL 3/4"; 3" OVERLAY
1/1/1988	IMPORTED	BUILT	0.00	4.00	☑	1988 4" P-401 SURFACE ON 4" P-401 BASE ON 14" P-211

Network: ST. PETE-CLEARW **Branch:** RW 18-36 RUNWAY 18-36 **Section:** 6120 **Surface:**AAC

L.C.D. 12/1/2020 **Use:** RUNWAY **Rank:** P **Length:** 1,966.00 (Ft) **Width:** 90.00 (Ft) **True Area:** 176940.0000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
12/1/2020	ML-OVL	Mill and Overlay	0.00	0.00	☑	Variable depth mill, 4" P-401 overlay;
1/1/2003	ML-OVL	Mill and Overlay	0.00	1.50	☑	MILL 3/4"; 1.5" OVERLAY
1/1/1988	IMPORTED	BUILT	0.00	4.00	☑	1988: 4" P-401 SURFACE ON 4" P-401 BASE ON 14" P-211

Network: ST. PETE-CLEARW **Branch:** RW 18-36 RUNWAY 18-36 **Section:** 6155 **Surface:**AAC

L.C.D. 12/1/2020 **Use:** RUNWAY **Rank:** P **Length:** 1,650.00 (Ft) **Width:** 60.00 (Ft) **True Area:** 99000.00003 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
12/1/2020	ML-OVL	Mill and Overlay	0.00	0.00	☑	Variable depth mill, 4" P-401 overlay
1/1/2003	ML-OVL	Mill and Overlay	0.00	3.00	☑	MILL 3/4"; 3" OVERLAY
1/1/1988	IMPORTED	OVERLAY	0.00	4.00	☑	1988: 4" P-401 OVERLAY
1/1/1988	IMPORTED	OVERLAY	0.00	0.00	☑	EXISTING PAVEMENT
1/1/1977	IMPORTED	OVERLAY	0.00	0.00	☑	1977: P-401 OVERLAY
1/1/1958	IMPORTED	BUILT	0.00	2.00	☑	1958: 2" AC ON 5" - 6" LIME ROCK BASE

Network: ST. PETE-CLEARW **Branch:** RW 18-36 RUNWAY 18-36 **Section:** 6156 **Surface:**AC

L.C.D. 12/1/2020 **Use:** RUNWAY **Rank:** P **Length:** 200.00 (Ft) **Width:** 90.00 (Ft) **True Area:** 18000.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
12/1/2020	CR-AC	Complete Reconstruction - AC	0.00	0.00	☑	4" P-401, 5" P-401 Base, 6" P-211
1/1/2003	ML-OVL	Mill and Overlay	0.00	1.50	☑	MILL 3/4"; 1.5" OVERLAY
1/1/1988	IMPORTED	OVERLAY	0.00	4.00	☑	1988: 4" P-401 OVERLAY
1/1/1988	IMPORTED	OVERLAY	0.00	0.00	☑	EXISTING PAVEMENT
1/1/1977	IMPORTED	OVERLAY	0.00	0.00	☑	1977: P-401 OVERLAY
1/1/1958	IMPORTED	BUILT	0.00	2.00	☑	1958: 2" AC ON 4" - 5" LIME ROCK BASE

Network: ST. PETE-CLEARW **Branch:** RW 18-36 RUNWAY 18-36 **Section:** 6157 **Surface:**AC

L.C.D. 12/1/2020 **Use:** RUNWAY **Rank:** P **Length:** 200.00 (Ft) **Width:** 60.00 (Ft) **True Area:** 12000.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
12/1/2020	CR-AC	Complete Reconstruction - AC	0.00	0.00	☑	4" P-401, 5" P-401 Base, 6" P-211
1/1/2003	ML-OVL	Mill and Overlay	0.00	3.00	☑	MILL 3/4"; 3" OVERLAY
1/1/1988	IMPORTED	OVERLAY	0.00	4.00	☑	1988: 4" P-401 OVERLAY
1/1/1988	IMPORTED	OVERLAY	0.00	0.00	☑	EXISTING PAVEMENT
1/1/1977	IMPORTED	OVERLAY	0.00	0.00	☑	1977: P-401 OVERLAY
1/1/1958	IMPORTED	BUILT	0.00	2.00	☑	1958: 2" AC ON 5" - 6" LIME ROCK BASE

Network: ST. PETE-CLEARW **Branch:** RW 18-36 RUNWAY 18-36 **Section:** 6160 **Surface:**AAC

L.C.D. 12/1/2020 **Use:** RUNWAY **Rank:** P **Length:** 1,650.00 (Ft) **Width:** 90.00 (Ft) **True Area:** 148500.0000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
12/1/2020	ML-OVL	Mill and Overlay	0.00	0.00	☑	Variable depth mill, 4" P-401 overlay
1/1/2003	ML-OVL	Mill and Overlay	0.00	1.50	☑	MILL 3/4"; 1.5" OVERLAY
1/1/1988	IMPORTED	OVERLAY	0.00	4.00	☑	1988: 4" P-401 OVERLAY
1/1/1988	IMPORTED	OVERLAY	0.00	0.00	☑	EXISTING PAVEMENT
1/1/1977	IMPORTED	OVERLAY	0.00	0.00	☑	1977: P-401 OVERLAY
1/1/1958	IMPORTED	BUILT	0.00	2.00	☑	1958: 2" AC ON 4" - 5" LIME ROCK BASE

Network: ST. PETE-CLEARW **Branch:** RW 18-36 RUNWAY 18-36 **Section:** 6165 **Surface:**AC

L.C.D. 12/1/2020 **Use:** RUNWAY **Rank:** P **Length:** 675.00 (Ft) **Width:** 60.00 (Ft) **True Area:** 40500.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
12/1/2020	CR-AC	Complete Reconstruction - AC	0.00	0.00	☑	4" P-401, 5" P-401 Base, 6" P-211; Iso
1/1/2003	ML-OVL	Mill and Overlay	0.00	3.00	☑	MILL 3/4"; 3" OVERLAY
1/1/1988	IMPORTED	OVERLAY	0.00	4.00	☑	1988: 4" P-401 OVERLAY
1/1/1988	IMPORTED	OVERLAY	0.00	0.00	☑	EXISTING PAVEMENT
1/1/1977	IMPORTED	OVERLAY	0.00	0.00	☑	1977: P-401 OVERLAY
1/1/1958	IMPORTED	BUILT	0.00	3.00	☑	1958: 3" AC ON 6" - 8" LIME ROCK BASE

Network: ST. PETE-CLEARW **Branch:** RW 18-36 RUNWAY 18-36 **Section:** 6170 **Surface:**AAC

L.C.D. 12/1/2020 **Use:** RUNWAY **Rank:** P **Length:** 675.00 (Ft) **Width:** 90.00 (Ft) **True Area:** 60750.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
12/1/2020	ML-OVL	Mill and Overlay	0.00	0.00	☑	Variable depth mill, 4" P-401 overlay
1/1/2003	ML-OVL	Mill and Overlay	0.00	1.50	☑	MILL 3/4"; 1.5" OVERLAY
1/1/1988	IMPORTED	OVERLAY	0.00	4.00	☑	1988: 4" P-401 OVERLAY
1/1/1988	IMPORTED	OVERLAY	0.00	0.00	☑	EXISTING PAVEMENT
1/1/1977	IMPORTED	OVERLAY	0.00	0.00	☑	1977: P-401 OVERLAY
1/1/1958	IMPORTED	BUILT	0.00	3.00	☑	1958: 3" AC ON 4" - 6" LIME ROCK BASE

Network: ST. PETE-CLEARW **Branch:** RW 18-36 RUNWAY 18-36 **Section:** 6175 **Surface:**AAC

L.C.D. 12/1/2020 **Use:** RUNWAY **Rank:** P **Length:** 2,838.00 (Ft) **Width:** 60.00 (Ft) **True Area:** 170280.0000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
12/1/2020	ML-OVL	Mill and Overlay	0.00	0.00	☑	Variable depth mill, 4" P-401 overlay;
1/1/2003	ML-OVL	Mill and Overlay	0.00	3.00	☑	MILL 3/4"; 3" OVERLAY
1/1/1988	IMPORTED	OVERLAY	0.00	4.00	☑	1988: 4" P-401 OVERLAY
1/1/1988	IMPORTED	OVERLAY	0.00	0.00	☑	EXISTING PAVEMENT
1/1/1977	IMPORTED	OVERLAY	0.00	0.00	☑	1977: P-401 OVERLAY
1/1/1958	IMPORTED	BUILT	0.00	2.00	☑	1958: 2" AC ON 5" - 6" LIME ROCK BASE

Network: ST. PETE-CLEARW **Branch:** RW 18-36 RUNWAY 18-36 **Section:** 6180 **Surface:**AAC

L.C.D. 12/1/2020 **Use:** RUNWAY **Rank:** P **Length:** 2,838.00 (Ft) **Width:** 90.00 (Ft) **True Area:** 255420.0000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
12/1/2020	ML-OVL	Mill and Overlay	0.00	0.00	☑	Variable depth mill, 4" P-401 overlay
1/1/2003	ML-OVL	Mill and Overlay	0.00	1.50	☑	MILL 3/4"; 1.5" OVERLAY
1/1/1988	IMPORTED	OVERLAY	0.00	4.00	☑	1988: 4" P-401 OVERLAY
1/1/1988	IMPORTED	OVERLAY	0.00	0.00	☑	EXISTING PAVEMENT
1/1/1977	IMPORTED	OVERLAY	0.00	0.00	☑	1977: P-401 OVERLAY
1/1/1958	IMPORTED	BUILT	0.00	2.00	☑	1958: 2" AC ON 4" -5" LIME ROCK BASE

Network: ST. PETE-CLEARW **Branch:** RW 18-36 RUNWAY 18-36 **Section:** 6185 **Surface:**AAC

L.C.D. 12/1/2020 **Use:** RUNWAY **Rank:** P **Length:** 2,100.00 (Ft) **Width:** 60.00 (Ft) **True Area:** 126000.0000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
12/1/2020	ML-OVL	Mill and Overlay	0.00	0.00	☑	Variable depth mill, 2" P-401 overlay
1/1/2003	ML-OVL	Mill and Overlay	0.00	3.00	☑	MILL 3/4"; 3" OVERLAY
1/1/1988	IMPORTED	OVERLAY	0.00	4.00	☑	1988: 4" P-401 OVERLAY
1/1/1977	IMPORTED	OVERLAY	0.00	0.00	☑	1977: P-401 OVERLAY
1/1/1960	IMPORTED	BUILT	0.00	2.00	☑	1960: 2" - 3" AC ON 8" - 10" LIME ROCK BASE

Network: ST. PETE-CLEARW **Branch:** RW 18-36 RUNWAY 18-36 **Section:** 6190 **Surface:**AAC

L.C.D. 12/1/2020 **Use:** RUNWAY **Rank:** P **Length:** 2,100.00 (Ft) **Width:** 90.00 (Ft) **True Area:** 189000.0000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
12/1/2020	ML-OVL	Mill and Overlay	0.00	0.00	☑	Variable depth mill, 2" P-401 overlay
1/1/2003	ML-OVL	Mill and Overlay	0.00	1.50	☑	MILL 3/4"; 1.5" OVERLAY
1/1/1988	IMPORTED	OVERLAY	0.00	4.00	☑	1988: 4" P-401 OVERLAY
1/1/1977	IMPORTED	OVERLAY	0.00	0.00	☑	1977: P-401 OVERLAY
1/1/1960	IMPORTED	BUILT	0.00	2.00	☑	1960: 2" - 3" AC ON 8" - 10" LIME ROCK BASE

Network: ST. PETE-CLEARW **Branch:** RW 18-36 RUNWAY 18-36 **Section:** 6196 **Surface:**AAC

L.C.D. 12/1/2020 **Use:** RUNWAY **Rank:** P **Length:** 300.00 (Ft) **Width:** 90.00 (Ft) **True Area:** 27000.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
12/1/2020	ML-OVL	Mill and Overlay	0.00	0.00	☑	Variable depth mill, 4" P-401 overlay
1/1/2013	ML-OVL	Mill and Overlay	0.00	0.00	☑	3" MILL AND OVERLAY TO CORR
1/1/2002	NC-AC	New Construction - AC	0.00	0.00	☑	

Network: ST. PETE-CLEARW **Branch:** RW 4-22 RUNWAY 4-22 **Section:** 6205 **Surface:**AAC

L.C.D. 1/1/2012 **Use:** RUNWAY **Rank:** P **Length:** 4,700.00 (Ft) **Width:** 100.00 (Ft) **True Area:** 474873.0001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2020	CS-AC	Crack Sealing - AC	0.00	0.00	☐	
1/1/2012	ML-OVL	Mill and Overlay	0.00	0.00	☑	
1/1/1983	IMPORTED	BUILT	0.00	0.00	☑	1983: P-401 OVERLAY

Network: ST. PETE-CLEARW **Branch:** RW 4-22 RUNWAY 4-22 **Section:** 6210 **Surface:**AAC

L.C.D. 1/1/2012 **Use:** RUNWAY **Rank:** P **Length:** 9,400.00 (Ft) **Width:** 25.00 (Ft) **True Area:** 237436.0000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2020	CS-AC	Crack Sealing - AC	0.00	0.00	☐	
1/1/2012	ML-OVL	Mill and Overlay	0.00	0.00	☑	
1/1/1983	IMPORTED	BUILT	0.00	0.00	☑	1983: P-401 OVERLAY
1/1/1983	IMPORTED	OVERLAY	0.00	0.00	☑	EXISTING AC PAVEMENT

Network: ST. PETE-CLEARW **Branch:** RW 4-22 RUNWAY 4-22 **Section:** 6215 **Surface:**AAC

L.C.D. 1/1/2012 **Use:** RUNWAY **Rank:** P **Length:** 495.00 (Ft) **Width:** 100.00 (Ft) **True Area:** 50072.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2020	CS-AC	Crack Sealing - AC	0.00	0.00	☐	
1/1/2012	ML-OVL	Mill and Overlay	0.00	0.00	☑	
1/1/1988	IMPORTED	OVERLAY	0.00	4.00	☑	1988: 4" P-401 OVERLAY
1/1/1988	IMPORTED	OVERLAY	0.00	3.00	☑	EXISTING 3" AC ON 10" LIME ROCK ON 1" SAND-ASPHALT
1/1/1978	IMPORTED	BUILT	0.00	4.00	☑	1978: 4" P-401 OVERLAY

Network: ST. PETE-CLEARW **Branch:** RW 4-22 RUNWAY 4-22 **Section:** 6220 **Surface:**AAC

L.C.D. 1/1/2012 **Use:** RUNWAY **Rank:** P **Length:** 495.00 (Ft) **Width:** 50.00 (Ft) **True Area:** 25036.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2020	CS-AC	Crack Sealing - AC	0.00	0.00	☐	
1/1/2012	ML-OVL	Mill and Overlay	0.00	0.00	☑	
1/1/1988	IMPORTED	OVERLAY	0.00	4.00	☑	1988: 4" P-401 OVERLAY
1/1/1988	IMPORTED	OVERLAY	0.00	3.00	☑	EXISTING 3" AC ON 10" LIME ROCK ON 1" SAND-ASPHALT
1/1/1978	IMPORTED	BUILT	0.00	4.00	☑	1978: 4" P-401 OVERLAY

Network: ST. PETE-CLEARW **Branch:** RW 4-22 RUNWAY 4-22 **Section:** 6225 **Surface:**AC

L.C.D. 1/1/2006 **Use:** RUNWAY **Rank:** P **Length:** 453.00 (Ft) **Width:** 100.00 (Ft) **True Area:** 45300.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2006	NC-AC	New Construction - AC	0.00	0.00	☑	

Network: ST. PETE-CLEARW **Branch:** RW 4-22 RUNWAY 4-22 **Section:** 6230 **Surface:**AC

L.C.D. 1/1/2006 **Use:** RUNWAY **Rank:** P **Length:** 453.00 (Ft) **Width:** 50.00 (Ft) **True Area:** 22650.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2006	NC-AC	New Construction - AC	0.00	0.00	☑	

Network: ST. PETE-CLEARW **Branch:** RW 4-22 RUNWAY 4-22 **Section:** 6235 **Surface:**AC

L.C.D. 4/1/2020 **Use:** RUNWAY **Rank:** P **Length:** 97.00 (Ft) **Width:** 100.00 (Ft) **True Area:** 9700.000002 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
4/1/2020	NC-AC	New Construction - AC	0.00	0.00	☑	

Network: ST. PETE-CLEARW **Branch:** RW 4-22 RUNWAY 4-22 **Section:** 6240 **Surface:**AC

L.C.D. 4/1/2020 **Use:** RUNWAY **Rank:** P **Length:** 97.00 (Ft) **Width:** 50.00 (Ft) **True Area:** 4850.000001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
4/1/2020	NC-AC	New Construction - AC	0.00	0.00	☑	

Network: ST. PETE-CLEARW **Branch:** TL T-HANG T-HANGAR TAX **Section:** 1120 **Surface:**AC

L.C.D. 1/1/1984 **Use:** TAXILAN **Rank:** P **Length:** 67.00 (Ft) **Width:** 20.00 (Ft) **True Area:** 1346.000000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/1984	IMPORTED	BUILT	0.00	3.00	☑	1984: 3" P-401 SURFACE ON 7.5" P-401 BASE

Network: ST. PETE-CLEARW **Branch:** TL T-HANG T-HANGAR TAX **Section:** 1125 **Surface:**AC

L.C.D. 1/1/1984 **Use:** TAXILAN **Rank:** P **Length:** 62.00 (Ft) **Width:** 20.00 (Ft) **True Area:** 1472.000000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/1984	IMPORTED	BUILT	0.00	3.00	☑	1984: 3" P-401 SURFACE ON 7.5" P-401 BASE

Network: ST. PETE-CLEARW **Branch:** TW A1 TAXIWAY A1 **Section:** 135 **Surface:**AAC

L.C.D. 8/1/2016 **Use:** TAXIWAY **Rank:** P **Length:** 2,475.00 (Ft) **Width:** 75.00 (Ft) **True Area:** 40056.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
8/1/2016	ML-OVL	Mill and Overlay	0.00	0.00	☑	3.5" MILL, 4" P-401SP OVERLAY
1/1/2012	ML-OVL	Mill and Overlay	0.00	0.00	☑	2012: MILL & RESURFACE TO AD
1/1/1992	IMPORTED	OVERLAY	0.00	0.00	☑	1992: P-401 OVERLAY
1/1/1978	IMPORTED	BUILT	0.00	4.00	☑	1978: 4" NOMINAL P-401 ON 3" AC ON 10" LIME ROCK BASE

Network: ST. PETE-CLEARW **Branch:** TW A1 TAXIWAY A1 **Section:** 140 **Surface:**AAC

L.C.D. 8/1/2016 **Use:** TAXIWAY **Rank:** P **Length:** 150.00 (Ft) **Width:** 75.00 (Ft) **True Area:** 14541.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
8/1/2016	ML-OVL	Mill and Overlay	0.00	0.00	☑	3.5" MILL & 4" P-401SP OVERLAY
1/1/2003	ML-OVL	Mill and Overlay	0.00	3.00	☑	MILL 3/4"; 3" OVERLAY
1/1/1988	IMPORTED	OVERLAY	0.00	4.00	☑	1988: 4" P-401 OVERLAY
1/1/1978	IMPORTED	BUILT	0.00	4.00	☑	1978: 4" P-401 ON 3" AC ON 10" LIME ROCK BASE

Network: ST. PETE-CLEARW **Branch:** TW A1 TAXIWAY A1 **Section:** 145 **Surface:**AC

L.C.D. 12/1/2020 **Use:** TAXIWAY **Rank:** P **Length:** 120.00 (Ft) **Width:** 25.00 (Ft) **True Area:** 2945.000000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
12/1/2020	CR-AC	Complete Reconstruction - AC	0.00	0.00	☑	4" P-401, 5" P-401 Base, 6" P-211
8/1/2016	ML-OVL	Mill and Overlay	0.00	0.00	☑	3.5" MILL & 4" P-401SP OVERLAY
1/1/2003	ML-OVL	Mill and Overlay	0.00	3.00	☑	MILL 3/4"; 3" OVERLAY
1/1/1988	IMPORTED	OVERLAY	0.00	4.00	☑	1988: 4" P-401 OVERLAY
1/1/1978	IMPORTED	BUILT	0.00	4.00	☑	1978: 4" P-401 ON 3" AC ON 10" LIME ROCK BASE

Network: ST. PETE-CLEARW **Branch:** TW A TAXIWAY A **Section:** 115 **Surface:**AAC

L.C.D. 8/1/2016 **Use:** TAXIWAY **Rank:** P **Length:** 2,940.00 (Ft) **Width:** 75.00 (Ft) **True Area:** 224709.0000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
7/1/2020	PA-AC	Patching - AC	0.00	0.00	☐	
8/1/2016	ML-OVL	Mill and Overlay	0.00	0.00	☑	2" MILL & 3" P-401SP OVERLAY
1/1/1990	IMPORTED	OVERLAY	0.00	0.00	☑	1990: P-401 OVERLAY
1/1/1978	IMPORTED	OVERLAY	0.00	5.50	☑	1978: 5.5" P-401 OVERLAY
1/1/1958	IMPORTED	BUILT	0.00	1.50	☑	1958: 1.5" AC AND 4" LIME ROCK PLACED ON EXISTING PAVEMEN

Network: ST. PETE-CLEARW **Branch:** TW A TAXIWAY A **Section:** 130 **Surface:**AAC

L.C.D. 8/1/2016 **Use:** TAXIWAY **Rank:** P **Length:** 4,660.00 (Ft) **Width:** 75.00 (Ft) **True Area:** 358395.0001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2020	CS-AC	Crack Sealing - AC	0.00	0.00	☐	
8/1/2016	ML-OVL	Mill and Overlay	0.00	0.00	☑	3.5" MILL & 4" P-401SP OVERLAY
1/1/1992	IMPORTED	OVERLAY	0.00	0.00	☑	1992: P-401 OVERLAY
1/1/1978	IMPORTED	BUILT	0.00	4.00	☑	1978: 4" NOMINAL P-401 ON 3" AC ON 10" LIME ROCK BASE

Network: ST. PETE-CLEARW **Branch:** TW A TAXIWAY A **Section:** 132 **Surface:** AAC

L.C.D. 5/1/2019 **Use:** TAXIWAY **Rank:** P **Length:** 150.00 (Ft) **Width:** 150.00 (Ft) **True Area:** 23007.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
5/1/2019	ML-OVL	Mill and Overlay	0.00	0.00	☑	
1/1/1992	IMPORTED	OVERLAY	0.00	4.00	☑	1992: 4" P-401 OVERLAY
1/1/1992	IMPORTED	OVERLAY	0.00	0.00	☑	EXISTING PAVEMENT
1/1/1958	IMPORTED	BUILT	0.00	3.00	☑	1958: 3" AC ON 6" - 8" LIME ROCK BASE

Network: ST. PETE-CLEARW **Branch:** TW A TAXIWAY A **Section:** 155 **Surface:** AAC

L.C.D. 8/1/2016 **Use:** TAXIWAY **Rank:** P **Length:** 75.00 (Ft) **Width:** 85.00 (Ft) **True Area:** 6259.000001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
8/1/2016	ML-OVL	Mill and Overlay	0.00	0.00	☑	2" MILL & 3" P-401SP OVERLAY
1/1/1992	IMPORTED	OVERLAY	0.00	0.00	☑	1992: FEATHERED P-401 OVERLAY
1/1/1990	IMPORTED	OVERLAY	0.00	0.00	☑	1990: P-401 OVERLAY
1/1/1978	IMPORTED	BUILT	0.00	9.00	☑	1978: 9" P-211 - ASSUME 1978 2" P-401 MILLED OFF IN 1990

Network: ST. PETE-CLEARW **Branch:** TW A TAXIWAY A **Section:** 158 **Surface:** AAC

L.C.D. 8/1/2016 **Use:** TAXIWAY **Rank:** P **Length:** 1,700.00 (Ft) **Width:** 125.00 (Ft) **True Area:** 16692.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
8/1/2016	ML-OVL	Mill and Overlay	0.00	0.00	☑	2" MILL & 3" P-401SP OVERLAY
1/1/2006	NC-AC	New Construction - AC	0.00	0.00	☑	

Network: ST. PETE-CLEARW **Branch:** TW A TAXIWAY A **Section:** 160 **Surface:** AAC

L.C.D. 1/1/2017 **Use:** TAXIWAY **Rank:** P **Length:** 1,330.00 (Ft) **Width:** 75.00 (Ft) **True Area:** 99856.00003 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2017	ML-OVL	Mill and Overlay	0.00	0.00	☑	
1/1/2006	NC-AC	New Construction - AC	0.00	0.00	☑	

Network: ST. PETE-CLEARW **Branch:** TW A2 TAXIWAY A2 **Section:** 165 **Surface:** AC

L.C.D. 8/1/2016 **Use:** TAXIWAY **Rank:** P **Length:** 600.00 (Ft) **Width:** 100.00 (Ft) **True Area:** 60458.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2020	CS-AC	Crack Sealing - AC	0.00	0.00	☐	
8/1/2016	NU-IN	New Construction - Initial	0.00	0.00	☑	5" P-401SP, 12" P-211, COMP STAB.

Network: ST. PETE-CLEARW **Branch:** TW A3 TAXIWAY A3 **Section:** 168 **Surface:** AC

L.C.D. 8/1/2016 **Use:** TAXIWAY **Rank:** P **Length:** 400.00 (Ft) **Width:** 100.00 (Ft) **True Area:** 60311.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
8/1/2016	NU-IN	New Construction - Initial	0.00	0.00	☑	5" P-401SP, 12" P-211, COMP STAB.

Network: ST. PETE-CLEARW **Branch:** TW A4 TAXIWAY A4 **Section:** 170 **Surface:**AC

L.C.D. 8/1/2016 **Use:** TAXIWAY **Rank:** P **Length:** 400.00 (Ft) **Width:** 100.00 (Ft) **True Area:** 58588.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
8/1/2016	NU-IN	New Construction - Initial	0.00	0.00	☑	5" P-401SP, 12" P-211, COMP STAB.

Network: ST. PETE-CLEARW **Branch:** TW A5 TAXIWAY A5 **Section:** 175 **Surface:**AC

L.C.D. 8/1/2016 **Use:** TAXIWAY **Rank:** P **Length:** 400.00 (Ft) **Width:** 100.00 (Ft) **True Area:** 56987.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
8/1/2016	NU-IN	New Construction - Initial	0.00	0.00	☑	5" P-401SP, 12" P-211, COMP STAB.

Network: ST. PETE-CLEARW **Branch:** TW A6 TAXIWAY A6 **Section:** 180 **Surface:**AC

L.C.D. 8/1/2016 **Use:** TAXIWAY **Rank:** P **Length:** 400.00 (Ft) **Width:** 100.00 (Ft) **True Area:** 58658.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
8/1/2016	NU-IN	New Construction - Initial	0.00	0.00	☑	5" P-401SP, 12" P-211, COMP STAB.

Network: ST. PETE-CLEARW **Branch:** TW A7 TAXIWAY A7 **Section:** 162 **Surface:**AAC

L.C.D. 1/1/2017 **Use:** TAXIWAY **Rank:** P **Length:** 700.00 (Ft) **Width:** 75.00 (Ft) **True Area:** 52089.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2017	ML-OVL	Mill and Overlay	0.00	0.00	☑	
1/1/2006	NC-AC	New Construction - AC	0.00	0.00	☑	

Network: ST. PETE-CLEARW **Branch:** TW B TAXIWAY B **Section:** 205 **Surface:**AAC

L.C.D. 6/1/2018 **Use:** TAXIWAY **Rank:** P **Length:** 124.00 (Ft) **Width:** 50.00 (Ft) **True Area:** 6200.000001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
6/1/2018	ML-OVL	Mill and Overlay	0.00	0.00	☑	
1/1/1958	IMPORTED	BUILT	0.00	1.50	☑	1958: 1.5" AC ON 4" LIME ROCK BASE ON EXISTING AC PAVEME

Network: ST. PETE-CLEARW **Branch:** TW B TAXIWAY B **Section:** 207 **Surface:**AAC

L.C.D. 6/1/2018 **Use:** TAXIWAY **Rank:** P **Length:** 155.00 (Ft) **Width:** 50.00 (Ft) **True Area:** 7750.000002 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
6/1/2018	ML-OVL	Mill and Overlay	0.00	0.00	☑	
1/1/1958	IMPORTED	BUILT	0.00	1.50	☑	1958: 1.5" AC ON 4" LIME ROCK BASE ON EXISTING AC PAVEME

Network: ST. PETE-CLEARW **Branch:** TW B TAXIWAY B **Section:** 210 **Surface:**AAC

L.C.D. 6/1/2018 **Use:** TAXIWAY **Rank:** P **Length:** 130.00 (Ft) **Width:** 50.00 (Ft) **True Area:** 6353.000001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
6/1/2018	ML-OVL	Mill and Overlay	0.00	0.00	☑	
1/1/1992	IMPORTED	OVERLAY	0.00	0.00	☑	1992: P-401 OVERLAY
1/1/1983	IMPORTED	OVERLAY	0.00	0.00	☑	1983: P-401 OVERLAY
1/1/1958	IMPORTED	BUILT	0.00	1.50	☑	1958: 1.5" AC AND 4" LIME ROCK PLACED ON EXISTING PAVEMEN

Network: ST. PETE-CLEARW **Branch:** TW B TAXIWAY B **Section:** 212 **Surface:**AAC

L.C.D. 6/1/2018 **Use:** TAXIWAY **Rank:** P **Length:** 150.00 (Ft) **Width:** 120.00 (Ft) **True Area:** 18000.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
6/1/2018	ML-OVL	Mill and Overlay	0.00	0.00	☑	2" Mill and Overlay P-401
1/1/1994	IMPORTED	OVERLAY	0.00	2.00	☑	1994 2" P401 OVERLAY ON
1/1/1958	IMPORTED	BUILT	0.00	3.00	☑	1958 3" P401 ON 6" -8" P211

Network: ST. PETE-CLEARW **Branch:** TW B TAXIWAY B **Section:** 215 **Surface:**AC

L.C.D. 1/1/2012 **Use:** TAXIWAY **Rank:** P **Length:** 300.00 (Ft) **Width:** 50.00 (Ft) **True Area:** 15387.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2012	NU-IN	New Construction - Initial	0.00	0.00	☑	2012: RECONSTRUCTION

Network: ST. PETE-CLEARW **Branch:** TW B TAXIWAY B **Section:** 220 **Surface:**AC

L.C.D. 1/1/1965 **Use:** TAXIWAY **Rank:** P **Length:** 835.00 (Ft) **Width:** 50.00 (Ft) **True Area:** 40670.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/1965	NU-IN	New Construction - Initial	0.00	0.00	☑	

Network: ST. PETE-CLEARW **Branch:** TW B TAXIWAY B **Section:** 225 **Surface:**AC

L.C.D. 6/1/2018 **Use:** TAXIWAY **Rank:** P **Length:** 280.00 (Ft) **Width:** 40.00 (Ft) **True Area:** 17624.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
6/1/2018	NC-AC	New Construction - AC	0.00	0.00	☑	

Network: ST. PETE-CLEARW **Branch:** TW F TAXIWAY F **Section:** 610 **Surface:**AC

L.C.D. 6/1/2018 **Use:** TAXIWAY **Rank:** P **Length:** 590.00 (Ft) **Width:** 75.00 (Ft) **True Area:** 43041.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
6/1/2018	CR-AC	Complete Reconstruction - AC	0.00	0.00	☑	
1/1/1989	IMPORTED	OVERLAY	0.00	0.00	☑	1989: P-401 OVERLAY
1/1/1989	IMPORTED	OVERLAY	0.00	0.00	☑	EXISTING PAVEMENT
1/1/1984	IMPORTED	BUILT	0.00	1.50	☑	1984: 1.5" P-401 OVERLAY

Network: ST. PETE-CLEARW **Branch:** TW F TAXIWAY F **Section:** 615 **Surface:**AAC

L.C.D. 12/1/2020 **Use:** TAXIWAY **Rank:** P **Length:** 75.00 (Ft) **Width:** 50.00 (Ft) **True Area:** 4165.000001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
12/1/2020	ML-OVL	Mill and Overlay	0.00	0.00	☑	Variable depth mill, 4" P-401 overlay
6/1/2018	CR-AC	Complete Reconstruction - AC	0.00	0.00	☑	
1/1/1989	IMPORTED	OVERLAY	0.00	0.00	☑	1989: P-401 OVERLAY
1/1/1989	IMPORTED	OVERLAY	0.00	0.00	☑	EXISTING PAVEMENT
1/1/1984	IMPORTED	BUILT	0.00	1.50	☑	1984: 1.5" P-401 OVERLAY

Network: ST. PETE-CLEARW **Branch:** TW G1 TAXIWAY G1 **Section:** 1330 **Surface:**AAC

L.C.D. 6/1/2018 **Use:** TAXIWAY **Rank:** P **Length:** 85.00 (Ft) **Width:** 85.00 (Ft) **True Area:** 13135.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2022	PA-AC	Patching - AC	0.00	0.00	☐	
6/1/2018	ML-OVL	Mill and Overlay	0.00	0.00	☑	
1/1/1984	IMPORTED	BUILT	0.00	3.00	☑	1984: 3" P-401 SURFACE ON 7.5" P-401 BASE

Network: ST. PETE-CLEARW **Branch:** TW G1 TAXIWAY G1 **Section:** 1335 **Surface:**AAC

L.C.D. 6/1/2018 **Use:** TAXIWAY **Rank:** P **Length:** 150.00 (Ft) **Width:** 85.00 (Ft) **True Area:** 12530.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
6/1/2018	ML-OVL	Mill and Overlay	0.00	0.00	☑	
1/1/1984	IMPORTED	BUILT	0.00	3.00	☑	1984: 3" P-401 SURFACE ON 7.5" P-401 BASE

Network: ST. PETE-CLEARW **Branch:** TW G TAXIWAY G **Section:** 1315 **Surface:**AC

L.C.D. 6/1/2018 **Use:** TAXIWAY **Rank:** P **Length:** 255.00 (Ft) **Width:** 65.00 (Ft) **True Area:** 19536.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
6/1/2018	NC-AC	New Construction - AC	0.00	0.00	☑	

Network: ST. PETE-CLEARW **Branch:** TW G TAXIWAY G **Section:** 1320 **Surface:**AAC

L.C.D. 6/1/2018 **Use:** TAXIWAY **Rank:** P **Length:** 190.00 (Ft) **Width:** 90.00 (Ft) **True Area:** 15822.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
6/1/2018	ML-OVL	Mill and Overlay	0.00	0.00	☑	1/2"-2" Var Mill, 2"-3" Overlay
1/1/1994	IMPORTED	OVERLAY	0.00	2.00	☑	1994 2" P401 AC OVERLAY
1/1/1994	IMPORTED	OVERLAY	0.00	0.00	☑	EXISTING PAVEMENT
1/1/1958	IMPORTED	BUILT	0.00	3.00	☑	1958 3" AC ON 6"-8" LIMEROCK ON

Network: ST. PETE-CLEARW **Branch:** TW G TAXIWAY G **Section:** 1325 **Surface:**AAC

L.C.D. 6/1/2018 **Use:** TAXIWAY **Rank:** P **Length:** 3,875.00 (Ft) **Width:** 50.00 (Ft) **True Area:** 199036.0000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2022	PA-AC	Patching - AC	0.00	0.00	☐	
6/1/2018	ML-OVL	Mill and Overlay	0.00	0.00	☑	
1/1/1984	IMPORTED	BUILT	0.00	3.00	☑	1984: 3" P-401 SURFACE ON 7.5" P-401 BASE

Network: ST. PETE-CLEARW **Branch:** TW G TAXIWAY G **Section:** 1340 **Surface:**AAC

L.C.D. 6/1/2018 **Use:** TAXIWAY **Rank:** P **Length:** 150.00 (Ft) **Width:** 100.00 (Ft) **True Area:** 14004.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
6/1/2018	ML-OVL	Mill and Overlay	0.00	0.00	☑	1/2"-2" Var Mill, 2"-3" Overlay
1/1/1994	IMPORTED	OVERLAY	0.00	2.00	☑	1994 2" P401 AC OVERLAY
1/1/1994	IMPORTED	OVERLAY	0.00	0.00	☑	EXISTING PAVEMENT
1/1/1958	IMPORTED	BUILT	0.00	3.00	☑	1958 3" AC ON 6"-8" LIMEROCK ON

Network: ST. PETE-CLEARW **Branch:** TW G2 TAXIWAY G2 **Section:** 1005 **Surface:**AAC

L.C.D. 6/1/2018 **Use:** TAXIWAY **Rank:** P **Length:** 125.00 (Ft) **Width:** 65.00 (Ft) **True Area:** 15843.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
6/1/2018	ML-OVL	Mill and Overlay	0.00	0.00	☑	
1/1/1984	IMPORTED	BUILT	0.00	3.00	☑	1984: 3" P-401 SURFACE ON 7.5" P-401 BASE

Network: ST. PETE-CLEARW **Branch:** TW G2 TAXIWAY G2 **Section:** 1010 **Surface:**AAC

L.C.D. 6/1/2018 **Use:** TAXIWAY **Rank:** P **Length:** 125.00 (Ft) **Width:** 65.00 (Ft) **True Area:** 8964.000002 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
6/1/2018	ML-OVL	Mill and Overlay	0.00	0.00	☑	
1/1/2012	ML-OVL	Mill and Overlay	0.00	0.00	☑	
1/1/1984	IMPORTED	BUILT	0.00	3.00	☑	1984: 3" P-401 SURFACE ON 7.5" P-401 BASE

Network: ST. PETE-CLEARW **Branch:** TW G3 TAXIWAY G3 **Section:** 605 **Surface:**AAC

L.C.D. 1/1/1984 **Use:** TAXIWAY **Rank:** P **Length:** 132.00 (Ft) **Width:** 61.00 (Ft) **True Area:** 10930.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/1984	IMPORTED	BUILT	0.00	1.50	☑	1984: 1.5" P-401 OVERLAY PLACED ON
1/1/1984	IMPORTED	OVERLAY	0.00	0.00	☑	EXISTING AC PAVEMENT

Network: ST. PETE-CLEARW **Branch:** TW G3 TAXIWAY G3 **Section:** 607 **Surface:**AAC

L.C.D. 1/1/2012 **Use:** TAXIWAY **Rank:** P **Length:** 122.00 (Ft) **Width:** 61.00 (Ft) **True Area:** 8732.000002 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2012	ML-OVL	Mill and Overlay	0.00	0.00	☑	
1/1/1984	IMPORTED	BUILT	0.00	1.50	☑	1984: 1.5" P-401 OVERLAY PLACED ON
1/1/1984	IMPORTED	OVERLAY	0.00	0.00	☑	EXISTING AC PAVEMENT

Network: ST. PETE-CLEARW **Branch:** TW H TAXIWAY H **Section:** 810 **Surface:**AAC

L.C.D. 1/2/1965 **Use:** TAXIWAY **Rank:** P **Length:** 798.00 (Ft) **Width:** 75.00 (Ft) **True Area:** 59729.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/2/1965	OL-AS	Overlay - AC Structural	0.00	0.00	☑	THERE IS A SLURRY SEAL ON PO
1/1/1965	NC-AC	New Construction - AC	0.00	0.00	☑	ESTIMATE 1965 AC PAVEMENT

Network: ST. PETE-CLEARW **Branch:** TW H TAXIWAY H **Section:** 815 **Surface:**AC

L.C.D. 8/1/2016 **Use:** TAXIWAY **Rank:** P **Length:** 500.00 (Ft) **Width:** 100.00 (Ft) **True Area:** 57784.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
8/1/2016	NU-IN	New Construction - Initial	0.00	0.00	☑	5" P-401SP, 12" P-211, COMP STAB.

Network: ST. PETE-CLEARW **Branch:** TW H TAXIWAY H **Section:** 820 **Surface:**AC

L.C.D. 1/1/2017 **Use:** TAXIWAY **Rank:** P **Length:** 75.00 (Ft) **Width:** 64.00 (Ft) **True Area:** 4760.000001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2017	CR-AC	Complete Reconstruction - AC	0.00	0.00	☑	
1/2/1965	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	THERE IS A SLURRY SEAL ON PO
1/1/1965	IMPORTED	BUILT	0.00	0.00	☑	ESTIMATE 1965 AC PAVEMENT

Network: ST. PETE-CLEARW **Branch:** TW J TAXIWAY J **Section:** 905 **Surface:**AC

L.C.D. 4/1/2020 **Use:** TAXIWAY **Rank:** P **Length:** 175.00 (Ft) **Width:** 60.00 (Ft) **True Area:** 8851.000002 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
4/1/2020	NC-AC	New Construction - AC	0.00	0.00	☑	

Network: ST. PETE-CLEARW **Branch:** TW L TAXIWAY L **Section:** 1205 **Surface:**AC

L.C.D. 8/1/2016 **Use:** TAXIWAY **Rank:** P **Length:** 164.00 (Ft) **Width:** 100.00 (Ft) **True Area:** 22175.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
8/1/2016	CR-AC	Complete Reconstruction - AC	0.00	0.00	☑	MILL TO BASE COURSE. 4" AC, R
1/1/1986	IMPORTED	BUILT	0.00	3.00	☑	1986: 3" P-401 ON 14" P-211

Network: ST. PETE-CLEARW **Branch:** TW L TAXIWAY L **Section:** 1215 **Surface:**AC

L.C.D. 8/1/2016 **Use:** TAXIWAY **Rank:** P **Length:** 150.00 (Ft) **Width:** 80.00 (Ft) **True Area:** 13483.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
8/1/2016	CR-AC	Complete Reconstruction - AC	0.00	0.00	☑	MILL TO BASE COURSE. 4" AC, R
1/1/1992	IMPORTED	OVERLAY	0.00	0.00	☑	1992: FEATHERED P-401 OVERLAY
1/1/1986	IMPORTED	BUILT	0.00	3.00	☑	1986: 3" P-401 ON 14" P-211

Network: ST. PETE-CLEARW **Branch:** TW L TAXIWAY L **Section:** 1245 **Surface:**AC

L.C.D. 8/1/2016 **Use:** TAXIWAY **Rank:** P **Length:** 1,043.00 (Ft) **Width:** 50.00 (Ft) **True Area:** 52150.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
8/1/2016	CR-AC	Complete Reconstruction - AC	0.00	0.00	☑	
1/1/1986	NU-IN	New Construction - Initial	0.00	0.00	☑	

Network: ST. PETE-CLEARW **Branch:** TW P TAXIWAY P **Section:** 1250 **Surface:**AC

L.C.D. 1/1/2016 **Use:** TAXIWAY **Rank:** P **Length:** 415.00 (Ft) **Width:** 50.00 (Ft) **True Area:** 27739.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2016	CR-AC	Complete Reconstruction - AC	0.00	0.00	☑	MILL TO BASE COURSE. 4" AC, R
1/1/1986	NU-IN	New Construction - Initial	0.00	0.00	☑	

Network: ST. PETE-CLEARW **Branch:** TW P TAXIWAY P **Section:** 1255 **Surface:**AC

L.C.D. 1/1/2016 **Use:** TAXIWAY **Rank:** P **Length:** 1,100.00 (Ft) **Width:** 50.00 (Ft) **True Area:** 52339.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2016	CR-AC	Complete Reconstruction - AC	0.00	0.00	☑	MILL TO BASE COURSE. 4" AC, R
1/1/1986	NU-IN	New Construction - Initial	0.00	0.00	☑	

Network: ST. PETE-CLEARW **Branch:** TW Q TAXIWAY Q **Section:** 1705 **Surface:**AAC

L.C.D. 6/1/2018 **Use:** TAXIWAY **Rank:** P **Length:** 155.00 (Ft) **Width:** 30.00 (Ft) **True Area:** 4449.000001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
6/1/2018	ML-OVL	Mill and Overlay	0.00	0.00	☑	
1/1/1984	NU-IN	New Construction - Initial	0.00	0.00	☑	

Network: ST. PETE-CLEARW **Branch:** TW Q TAXIWAY Q **Section:** 1710 **Surface:**AC

L.C.D. 6/1/2018 **Use:** TAXIWAY **Rank:** P **Length:** 125.00 (Ft) **Width:** 25.00 (Ft) **True Area:** 3632.000001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
6/1/2018	NC-AC	New Construction - AC	0.00	0.00	☑	

Network: ST. PETE-CLEARW **Branch:** TW T TAXIWAY T **Section:** 2010 **Surface:**AAC

L.C.D. 6/1/2018 **Use:** TAXIWAY **Rank:** P **Length:** 173.00 (Ft) **Width:** 75.00 (Ft) **True Area:** 12963.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
6/1/2018	ML-OVL	Mill and Overlay	0.00	0.00	☑	
1/1/1992	IMPORTED	OVERLAY	0.00	0.00	☑	1992: P-401 OVERLAY
1/1/1983	IMPORTED	OVERLAY	0.00	0.00	☑	1983: P-401 OVERLAY
1/1/1960	IMPORTED	BUILT	0.00	3.00	☑	1960: 3" BIT. SURFACE ON 10" LIME ROCK BASE

Network: ST. PETE-CLEARW **Branch:** TW T TAXIWAY T **Section:** 2020 **Surface:**AAC

L.C.D. 6/1/2018 **Use:** TAXIWAY **Rank:** P **Length:** 200.00 (Ft) **Width:** 75.00 (Ft) **True Area:** 14337.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
6/1/2018	ML-OVL	Mill and Overlay	0.00	0.00	☑	0"-2" Mill, 2" Overlay P-401
1/1/1996	IMPORTED	BUILT	0.00	0.00	☑	ESTIMATE 1996 OVERLAY

Network: ST. PETE-CLEARW **Branch:** TW T TAXIWAY T **Section:** 2045 **Surface:**AAC

L.C.D. 8/1/2016 **Use:** TAXIWAY **Rank:** P **Length:** 375.00 (Ft) **Width:** 50.00 (Ft) **True Area:** 16549.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
8/1/2016	ML-OVL	Mill and Overlay	0.00	0.00	☑	2" Mill, 3" P-401SP Overlay
1/1/1997	IMPORTED	BUILT	0.00	13.00	☑	13" 95% COMPACTED SUBGRADE
1/1/1997	IMPORTED	OVERLAY	0.00	18.00	☑	18" 100% COMPACTED SUBGRADE ON
1/1/1997	IMPORTED	OVERLAY	0.00	4.00	☑	1997 4" P401 ON 12" P211 ON 12" P160 ON

Network: ST. PETE-CLEARW **Branch:** TW T TAXIWAY T **Section:** 2050 **Surface:**AAC

L.C.D. 6/1/2018 **Use:** TAXIWAY **Rank:** P **Length:** 1,700.00 (Ft) **Width:** 94.00 (Ft) **True Area:** 149440.0000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
6/1/2018	ML-OVL	Mill and Overlay	0.00	0.00	☑	
1/1/1997	IMPORTED	BUILT	0.00	13.00	☑	13" 95% COMPACTED SUBGRADE
1/1/1997	IMPORTED	OVERLAY	0.00	18.00	☑	18" 100% COMPACTED SUBGRADE ON
1/1/1997	IMPORTED	OVERLAY	0.00	4.00	☑	1997 4" P401 ON 12" P211 ON 12" P160 ON

Summary:

Work Description	Section Count	Area Total (SqFt)	Thickness Avg (in)	Thickness STD (in)
BUILT	67	4,266,767.00	2.37	2.42
Complete Reconstruction - AC	13	429,257.00	0.00	0.00
Complete Reconstruction - PCC	5	494,194.00	0.00	0.00
Crack Sealing - AC	6	1,206,270.00	0.00	0.00
Mill and Overlay	78	5,997,316.00	0.65	1.06
New Construction - AC	16	520,862.00	0.06	0.24
New Construction - Initial	15	592,872.00	0.00	0.00
New Construction - PCC	5	301,009.00	0.00	0.00
OVERLAY	77	5,558,055.00	2.07	3.26
Overlay - AC Structural	13	740,455.00	0.00	0.00
Patching - AC	3	436,880.00	0.00	0.00
Surface Treatment - Seal Coat	25	1,111,310.00	0.00	0.00

Branch Condition Report

Pavement Database: FDOT

Branch ID	Number of Sections	Sum Section Length (Ft)	Avg Section Width (Ft)	True Area (SqFt)	Use	Average PCI	Standard Deviation PCI	Weighted Average PCI
AP MAIN	23	9,332.00	156.70	1,197,983.00	APRON	61.52	30.20	73.93
FBO CONN	8	559.00	65.87	39,789.00	TAXIWAY	53.00	18.30	61.52
RW 18-36	13	19,458.00	76.15	1,459,350.00	RUNWAY	100.00	0.00	100.00
RW 4-22	8	16,190.00	71.87	869,917.00	RUNWAY	70.38	23.21	72.16
TL T-HANG	2	129.00	20.00	2,818.00	TAXILANE	38.50	0.50	38.48
TW A	6	10,855.00	97.50	728,918.00	TAXIWAY	76.33	10.51	71.17
TW A1	3	2,745.00	58.33	57,542.00	TAXIWAY	77.00	16.31	66.60
TW A2	1	600.00	100.00	60,458.00	TAXIWAY	74.00	0.00	74.00
TW A3	1	400.00	100.00	60,311.00	TAXIWAY	77.00	0.00	77.00
TW A4	1	400.00	100.00	58,588.00	TAXIWAY	85.00	0.00	85.00
TW A5	1	400.00	100.00	56,987.00	TAXIWAY	82.00	0.00	82.00
TW A6	1	400.00	100.00	58,658.00	TAXIWAY	86.00	0.00	86.00
TW A7	1	700.00	75.00	52,089.00	TAXIWAY	83.00	0.00	83.00
TW B	7	1,974.00	58.57	111,984.00	TAXIWAY	77.00	25.14	60.62
TW F	2	665.00	62.50	47,206.00	TAXIWAY	88.00	12.00	78.12
TW G	4	4,470.00	76.25	248,398.00	TAXIWAY	69.50	7.63	66.08
TW G1	2	235.00	85.00	25,665.00	TAXIWAY	64.00	1.00	64.02
TW G2	2	250.00	65.00	24,807.00	TAXIWAY	89.00	0.00	89.00
TW G3	2	254.00	61.00	19,662.00	TAXIWAY	56.00	32.00	52.42
TW H	3	1,373.00	79.67	122,273.00	TAXIWAY	60.00	38.89	46.98
TW J	1	175.00	60.00	8,851.00	TAXIWAY	100.00	0.00	100.00
TW L	3	1,357.00	76.67	87,808.00	TAXIWAY	77.33	1.25	78.03
TW P	2	1,515.00	50.00	80,078.00	TAXIWAY	88.00	3.00	88.92
TW Q	2	280.00	27.50	8,081.00	TAXIWAY	94.00	0.00	94.00
TW T	4	2,448.00	73.50	193,289.00	TAXIWAY	84.25	6.10	88.13

Pavement Database: FDOT

Use Category	Number of Sections	Total Area (SqFt)	Arithmetic Average PCI	Average STD PCI	Weighted Average PCI
APRON	23	1,197,983.00	61.52	30.20	73.93
RUNWAY	21	2,329,267.00	88.71	20.30	89.60
TAXILANE	2	2,818.00	38.50	0.50	38.48
TAXIWAY	57	2,151,442.00	74.11	20.77	72.72
ALL	103	5,681,510.00	73.58	25.09	79.88

Pavement Database: FDOT *NetworkId: PIE*

Branch ID	Section ID	Last Const. Date	Surface	Use	Rank	Lanes	True Area (SqFt)	Last Inspection Date	Age At Inspection	PCI
AP MAIN	4103	1/1/2023	PCC	APRON	P	0	122,390.00	1/1/2023	0	100
AP MAIN	4105	1/2/2003	APC	APRON	P	0	40,910.00	4/4/2022	19	30
AP MAIN	4107	1/1/2016	PCC	APRON	P	0	220,315.00	4/4/2022	6	97
AP MAIN	4110	6/1/2018	APC	APRON	P	0	56,000.00	4/4/2022	4	74
AP MAIN	4123	6/1/2018	APC	APRON	P	0	43,794.00	4/4/2022	4	83
AP MAIN	4150	6/1/2018	AAC	APRON	P	0	14,083.00	4/4/2022	4	79
AP MAIN	4155	1/1/2003	AAC	APRON	P	0	33,689.00	4/4/2022	19	59
AP MAIN	4157	8/1/2016	AAC	APRON	P	0	92,541.00	4/4/2022	6	67
AP MAIN	4160	1/1/2016	PCC	APRON	P	0	59,640.00	4/4/2022	6	96
AP MAIN	4165	1/1/2012	PCC	APRON	P	0	66,649.00	4/4/2022	10	95
AP MAIN	4170	8/1/2016	AAC	APRON	P	0	18,816.00	4/4/2022	6	87
AP MAIN	4175	1/1/1942	PCC	APRON	P	0	14,910.00	4/4/2022	80	5
AP MAIN	4176	12/25/1955	AC	APRON	P	0	3,573.00	4/4/2022	67	28
AP MAIN	4177	8/1/2016	APC	APRON	P	0	20,899.00	4/4/2022	6	80
AP MAIN	4178	1/1/2013	APC	APRON	P	0	59,522.00	4/4/2022	9	62
AP MAIN	4179	10/1/2011	APC	APRON	P	0	77,111.00	4/4/2022	11	66
AP MAIN	4180	1/2/1968	AAC	APRON	P	0	126,695.00	4/4/2022	54	45
AP MAIN	4183	1/1/2013	AAC	APRON	P	0	39,947.00	4/4/2022	9	68
AP MAIN	4185	1/1/2013	APC	APRON	P	0	12,820.00	4/4/2022	9	51
AP MAIN	4190	1/1/1942	PCC	APRON	P	0	18,650.00	4/4/2022	80	14
AP MAIN	4195	1/1/1942	PCC	APRON	P	0	11,250.00	4/4/2022	80	9
AP MAIN	4198	1/1/2003	PCC	APRON	P	0	18,579.00	4/4/2022	19	20
AP MAIN	4199	1/1/2023	PCC	APRON	P	0	25,200.00	1/1/2023	0	100
FBO CONN	107	1/1/1990	AAC	TAXIWAY	P	0	3,297.00	4/4/2022	32	45
FBO CONN	108	1/1/1994	AC	TAXIWAY	P	0	3,361.00	4/4/2022	28	32
FBO CONN	112	1/1/1990	AAC	TAXIWAY	P	0	4,221.00	4/4/2022	32	42
FBO CONN	114	1/1/1968	AC	TAXIWAY	P	0	2,361.00	4/4/2022	54	59
FBO CONN	117	8/1/2016	AAC	TAXIWAY	P	0	6,019.00	4/4/2022	6	77
FBO CONN	119	1/1/1968	AC	TAXIWAY	P	0	3,041.00	4/4/2022	54	29
FBO CONN	125	8/1/2016	APC	TAXIWAY	P	0	4,598.00	4/4/2022	6	58
FBO CONN	127	8/1/2016	APC	TAXIWAY	P	0	12,891.00	4/4/2022	6	82
RW 18-36	6115	12/1/2020	AC	RUNWAY	P	0	135,960.00	12/1/2020	0	100
RW 18-36	6120	12/1/2020	AAC	RUNWAY	P	0	176,940.00	12/1/2020	0	100
RW 18-36	6155	12/1/2020	AAC	RUNWAY	P	0	99,000.00	12/1/2020	0	100
RW 18-36	6156	12/1/2020	AC	RUNWAY	P	0	18,000.00	12/1/2020	0	100
RW 18-36	6157	12/1/2020	AC	RUNWAY	P	0	12,000.00	12/1/2020	0	100
RW 18-36	6160	12/1/2020	AAC	RUNWAY	P	0	148,500.00	12/1/2020	0	100
RW 18-36	6165	12/1/2020	AC	RUNWAY	P	0	40,500.00	12/1/2020	0	100
RW 18-36	6170	12/1/2020	AAC	RUNWAY	P	0	60,750.00	12/1/2020	0	100
RW 18-36	6175	12/1/2020	AAC	RUNWAY	P	0	170,280.00	12/1/2020	0	100
RW 18-36	6180	12/1/2020	AAC	RUNWAY	P	0	255,420.00	12/1/2020	0	100
RW 18-36	6185	12/1/2020	AAC	RUNWAY	P	0	126,000.00	12/1/2020	0	100
RW 18-36	6190	12/1/2020	AAC	RUNWAY	P	0	189,000.00	12/1/2020	0	100
RW 18-36	6196	12/1/2020	AAC	RUNWAY	P	0	27,000.00	12/1/2020	0	100
RW 4-22	6205	1/1/2012	AAC	RUNWAY	P	0	474,873.00	4/4/2022	10	73
RW 4-22	6210	1/1/2012	AAC	RUNWAY	P	0	237,436.00	4/4/2022	10	79
RW 4-22	6215	1/1/2012	AAC	RUNWAY	P	0	50,072.00	4/4/2022	10	63
RW 4-22	6220	1/1/2012	AAC	RUNWAY	P	0	25,036.00	4/4/2022	10	70
RW 4-22	6225	1/1/2006	AC	RUNWAY	P	0	45,300.00	4/4/2022	16	54
RW 4-22	6230	1/1/2006	AC	RUNWAY	P	0	22,650.00	4/4/2022	16	24
RW 4-22	6235	4/1/2020	AC	RUNWAY	P	0	9,700.00	4/1/2020	0	100
RW 4-22	6240	4/1/2020	AC	RUNWAY	P	0	4,850.00	4/1/2020	0	100

TL T-HANG	1120	1/1/1984	AC	TAXILANE	P	0	1,346.00	4/4/2022	38	39
TL T-HANG	1125	1/1/1984	AC	TAXILANE	P	0	1,472.00	4/4/2022	38	38
TW A	115	8/1/2016	AAC	TAXIWAY	P	0	224,709.00	4/4/2022	6	65
TW A	130	8/1/2016	AAC	TAXIWAY	P	0	358,395.00	4/4/2022	6	71
TW A	132	5/1/2019	AAC	TAXIWAY	P	0	23,007.00	4/4/2022	3	88
TW A	155	8/1/2016	AAC	TAXIWAY	P	0	6,259.00	4/4/2022	6	89
TW A	158	8/1/2016	AAC	TAXIWAY	P	0	16,692.00	4/4/2022	6	63
TW A	160	1/1/2017	AAC	TAXIWAY	P	0	99,856.00	4/4/2022	5	82
TW A1	135	8/1/2016	AAC	TAXIWAY	P	0	40,056.00	4/4/2022	6	64
TW A1	140	8/1/2016	AAC	TAXIWAY	P	0	14,541.00	4/4/2022	6	67
TW A1	145	12/1/2020	AC	TAXIWAY	P	0	2,945.00	12/1/2020	0	100
TW A2	165	8/1/2016	AC	TAXIWAY	P	0	60,458.00	4/4/2022	6	74
TW A3	168	8/1/2016	AC	TAXIWAY	P	0	60,311.00	4/4/2022	6	77
TW A4	170	8/1/2016	AC	TAXIWAY	P	0	58,588.00	4/4/2022	6	85
TW A5	175	8/1/2016	AC	TAXIWAY	P	0	56,987.00	4/4/2022	6	82
TW A6	180	8/1/2016	AC	TAXIWAY	P	0	58,658.00	4/4/2022	6	86
TW A7	162	1/1/2017	AAC	TAXIWAY	P	0	52,089.00	4/4/2022	5	83
TW B	205	6/1/2018	AAC	TAXIWAY	P	0	6,200.00	4/4/2022	4	91
TW B	207	6/1/2018	AAC	TAXIWAY	P	0	7,750.00	4/4/2022	4	90
TW B	210	6/1/2018	AAC	TAXIWAY	P	0	6,353.00	4/4/2022	4	88
TW B	212	6/1/2018	AAC	TAXIWAY	P	0	18,000.00	4/4/2022	4	80
TW B	215	1/1/2012	AC	TAXIWAY	P	0	15,387.00	4/4/2022	10	85
TW B	220	1/1/1965	AC	TAXIWAY	P	0	40,670.00	4/4/2022	57	16
TW B	225	6/1/2018	AC	TAXIWAY	P	0	17,624.00	4/4/2022	4	89
TW F	610	6/1/2018	AC	TAXIWAY	P	0	43,041.00	4/4/2022	4	76
TW F	615	12/1/2020	AAC	TAXIWAY	P	0	4,165.00	12/1/2020	0	100
TW G	1315	6/1/2018	AC	TAXIWAY	P	0	19,536.00	4/4/2022	4	69
TW G	1320	6/1/2018	AAC	TAXIWAY	P	0	15,822.00	4/4/2022	4	62
TW G	1325	6/1/2018	AAC	TAXIWAY	P	0	199,036.00	4/4/2022	4	65
TW G	1340	6/1/2018	AAC	TAXIWAY	P	0	14,004.00	4/4/2022	4	82
TW G1	1330	6/1/2018	AAC	TAXIWAY	P	0	13,135.00	4/4/2022	4	65
TW G1	1335	6/1/2018	AAC	TAXIWAY	P	0	12,530.00	4/4/2022	4	63
TW G2	1005	6/1/2018	AAC	TAXIWAY	P	0	15,843.00	4/4/2022	4	89
TW G2	1010	6/1/2018	AAC	TAXIWAY	P	0	8,964.00	4/4/2022	4	89
TW G3	605	1/1/1984	AAC	TAXIWAY	P	0	10,930.00	4/4/2022	38	24
TW G3	607	1/1/2012	AAC	TAXIWAY	P	0	8,732.00	4/4/2022	10	88
TW H	810	1/2/1965	AAC	TAXIWAY	P	0	59,729.00	4/4/2022	57	5
TW H	815	8/1/2016	AC	TAXIWAY	P	0	57,784.00	4/4/2022	6	87
TW H	820	1/1/2017	AC	TAXIWAY	P	0	4,760.00	4/4/2022	5	88
TW J	905	4/1/2020	AC	TAXIWAY	P	0	8,851.00	4/1/2020	0	100
TW L	1205	8/1/2016	AC	TAXIWAY	P	0	22,175.00	4/4/2022	6	77
TW L	1215	8/1/2016	AC	TAXIWAY	P	0	13,483.00	4/4/2022	6	76
TW L	1245	8/1/2016	AC	TAXIWAY	P	0	52,150.00	4/4/2022	6	79
TW P	1250	1/1/2016	AC	TAXIWAY	P	0	27,739.00	4/4/2022	6	85
TW P	1255	1/1/2016	AC	TAXIWAY	P	0	52,339.00	4/4/2022	6	91
TW Q	1705	6/1/2018	AAC	TAXIWAY	P	0	4,449.00	4/4/2022	4	94
TW Q	1710	6/1/2018	AC	TAXIWAY	P	0	3,632.00	4/4/2022	4	94
TW T	2010	6/1/2018	AAC	TAXIWAY	P	0	12,963.00	4/4/2022	4	87
TW T	2020	6/1/2018	AAC	TAXIWAY	P	0	14,337.00	4/4/2022	4	86
TW T	2045	8/1/2016	AAC	TAXIWAY	P	0	16,549.00	4/4/2022	6	74
TW T	2050	6/1/2018	AAC	TAXIWAY	P	0	149,440.00	4/4/2022	4	90

Pavement Database: FDOT

Age Category	Average Age at Inspection	Total Area (SqFt)	Number of Sections	Arithmetic Average PCI	Standard Deviation PCI	Weighted Average PCI
00-02		1,637,451.00	20	100.00	0.00	100.00
03-05	4	876,248.00	26	81.77	9.64	78.50
06-10	7	2,624,066.00	36	76.94	11.12	76.76
11-15	11	77,111.00	1	66.00	0.00	66.00
16-20	18	161,128.00	5	37.40	15.99	40.81
26-30	28	3,361.00	1	32.00	0.00	32.00
31-35	32	7,518.00	2	43.50	1.50	43.32
36-40	38	13,748.00	3	33.67	6.85	26.97
50+	65	280,879.00	9	23.33	17.68	26.40
ALL	12	5,681,510.00	103	73.58	25.09	79.88

Appendix B: Maintenance and Rehabilitation Planning Needs

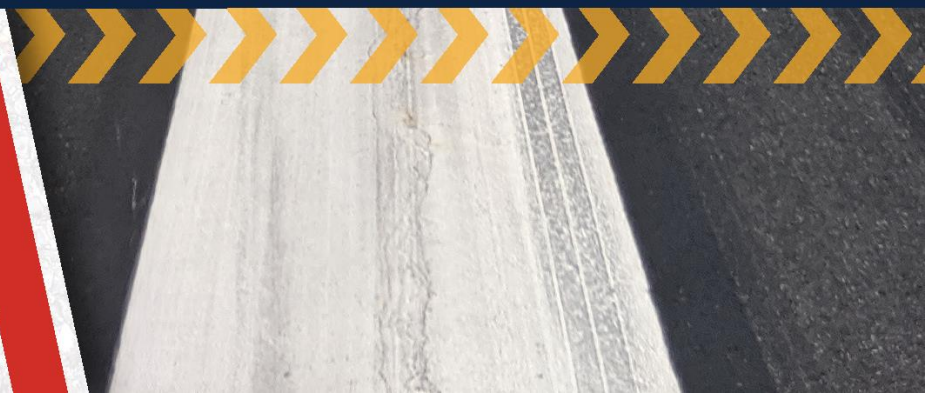


Table B.1: Localized Maintenance and Repair Needs Based on Current Distresses

Network ID	Branch ID	Section ID	Description	Severity	Distress Qty	Distress Unit	Distress Density	Policy Type	Localized Work Type	Work Qty	Work Unit	Unit Cost	Work Cost
PIE	RW 4-22	6205	L & T CR	Medium	1,308	LF	0.3%	Preventive	AC Crack Sealing	1,307	LF	$ 4.00	$ 5,240
PIE	RW 4-22	6205	RAVELING	Low	2,330	SF	0.5%	Preventive	Surface Seal	2,329	SF	$ 0.75	$ 1,750
PIE	RW 4-22	6205	WEATHERING	Medium	16,989	SF	3.6%	Preventive	Surface Seal	16,989	SF	$ 0.75	$ 12,750
PIE	RW 4-22	6210	L & T CR	Medium	57	LF	0.0%	Preventive	AC Crack Sealing	57	LF	$ 4.00	$ 230
PIE	RW 4-22	6210	RAVELING	Low	1,187	SF	0.5%	Preventive	Surface Seal	1,187	SF	$ 0.75	$ 900
PIE	RW 4-22	6210	WEATHERING	Medium	2,374	SF	1.0%	Preventive	Surface Seal	2,375	SF	$ 0.75	$ 1,790
PIE	FBO CONN	127	WEATHERING	Medium	1,290	SF	10.0%	Preventive	Surface Seal	1,290	SF	$ 0.75	$ 970
PIE	TW A	130	L & T CR	Medium	470	LF	0.1%	Preventive	AC Crack Sealing	470	LF	$ 4.00	$ 1,890
PIE	TW A	130	RAVELING	Low	5,528	SF	1.5%	Preventive	Surface Seal	5,527	SF	$ 0.75	$ 4,150
PIE	TW A	130	WEATHERING	Medium	15,897	SF	4.4%	Preventive	Surface Seal	15,897	SF	$ 0.75	$ 11,930
PIE	TW A2	165	SLIPPAGE CR	N/A	126	SF	0.2%	Preventive	AC Full-Depth Patching	174	SF	$ 18.75	$ 3,280
PIE	TW A2	165	WEATHERING	Medium	3,023	SF	5.0%	Preventive	Surface Seal	3,024	SF	$ 0.75	$ 2,270
PIE	TW A3	168	L & T CR	Medium	261	LF	0.4%	Preventive	AC Crack Sealing	261	LF	$ 4.00	$ 1,050
PIE	TW A3	168	WEATHERING	Medium	3,013	SF	5.0%	Preventive	Surface Seal	3,013	SF	$ 0.75	$ 2,260
PIE	TW A4	170	WEATHERING	Medium	2,927	SF	5.0%	Preventive	Surface Seal	2,927	SF	$ 0.75	$ 2,200
PIE	TW A5	175	WEATHERING	Medium	1,277	SF	2.2%	Preventive	Surface Seal	1,277	SF	$ 0.75	$ 960
PIE	TW A6	180	WEATHERING	Medium	2,931	SF	5.0%	Preventive	Surface Seal	2,931	SF	$ 0.75	$ 2,200
PIE	TW H	815	WEATHERING	Medium	2,889	SF	5.0%	Preventive	Surface Seal	2,888	SF	$ 0.75	$ 2,170
PIE	TW L	1205	WEATHERING	Medium	2,337	SF	10.5%	Preventive	Surface Seal	2,337	SF	$ 0.75	$ 1,760
PIE	TW L	1215	WEATHERING	Medium	1,347	SF	10.0%	Preventive	Surface Seal	1,348	SF	$ 0.75	$ 1,020
PIE	TW P	1250	WEATHERING	Medium	1,385	SF	5.0%	Preventive	Surface Seal	1,384	SF	$ 0.75	$ 1,040
PIE	TW P	1255	WEATHERING	Medium	2,620	SF	5.0%	Preventive	Surface Seal	2,620	SF	$ 0.75	$ 1,970
PIE	TW T	2045	L & T CR	Medium	138	LF	0.8%	Preventive	AC Crack Sealing	138	LF	$ 4.00	$ 560
PIE	TW T	2045	WEATHERING	Medium	743	SF	4.5%	Preventive	Surface Seal	744	SF	$ 0.75	$ 560
PIE	AP MAIN	4110	RAVELING	Low	1,307	SF	2.3%	Preventive	Surface Seal	1,307	SF	$ 0.75	$ 980
PIE	AP MAIN	4110	SLIPPAGE CR	N/A	45	SF	0.1%	Preventive	AC Full-Depth Patching	75	SF	$ 18.75	$ 1,430
PIE	AP MAIN	4160	JOINT SPALL	Medium	6	Slabs	3.1%	Preventive	PCC Partial-Depth Patching	37	SF	$ 169.00	$ 6,280
PIE	AP MAIN	4177	WEATHERING	Medium	1,044	SF	5.0%	Preventive	Surface Seal	1,044	SF	$ 0.75	$ 790
PIE	FBO CONN	119	ALLIGATOR CR	Medium	12	SF	0.4%	Stopgap	AC Full-Depth Patching	30	SF	$ 18.75	$ 570
PIE	TW A	115	SLIPPAGE CR	N/A	710	SF	0.3%	Stopgap	AC Full-Depth Patching	821	SF	$ 18.75	$ 15,410
PIE	TW B	220	ALLIGATOR CR	Medium	607	SF	1.5%	Stopgap	AC Full-Depth Patching	710	SF	$ 18.75	$ 13,330
PIE	TW B	220	L & T CR	High	27	LF	0.1%	Stopgap	AC Full-Depth Patching	87	SF	$ 18.75	$ 1,640
PIE	TW B	220	RAVELING	High	133	SF	0.3%	Stopgap	AC Partial-Depth Patching	132	SF	$ 6.50	$ 870
PIE	TW H	810	ALLIGATOR CR	Medium	7,348	SF	12.3%	Stopgap	AC Full-Depth Patching	7,696	SF	$ 18.75	$ 144,320
PIE	TW H	810	RAVELING	High	95	SF	0.2%	Stopgap	AC Partial-Depth Patching	95	SF	$ 6.50	$ 620
PIE	AP MAIN	4157	SLIPPAGE CR	N/A	34	SF	0.0%	Stopgap	AC Full-Depth Patching	61	SF	$ 18.75	$ 1,160
PIE	AP MAIN	4175	LINEAR CR	Medium	35	Slabs	70.8%	Stopgap	PCC Crack Sealing	655	LF	$ 7.00	$ 4,590
PIE	AP MAIN	4175	JT SEAL DMG	High	50	Slabs	100.0%	Stopgap	PCC Joint Seal	1,484	LF	$ 4.25	$ 6,310
PIE	AP MAIN	4175	SHAT. SLAB	Medium	10	Slabs	20.8%	Stopgap	PCC Crack Sealing	386	LF	$ 7.00	$ 2,700
PIE	AP MAIN	4175	JOINT SPALL	Medium	10	Slabs	20.8%	Stopgap	PCC Partial-Depth Patching	68	SF	$ 169.00	$ 11,370
PIE	AP MAIN	4175	JOINT SPALL	High	6	Slabs	12.5%	Stopgap	PCC Partial-Depth Patching	51	SF	$ 169.00	$ 8,530
PIE	AP MAIN	4175	CORNER SPALL	Medium	2	Slabs	4.2%	Stopgap	PCC Partial-Depth Patching	5	SF	$ 169.00	$ 950
PIE	AP MAIN	4175	CORNER SPALL	High	4	Slabs	8.3%	Stopgap	PCC Partial-Depth Patching	11	SF	$ 169.00	$ 1,900
PIE	AP MAIN	4176	PATCHING	High	18	SF	0.5%	Stopgap	AC Full-Depth Patching	39	SF	$ 18.75	$ 740
PIE	AP MAIN	4180	ALLIGATOR CR	Medium	106	SF	0.1%	Stopgap	AC Full-Depth Patching	151	SF	$ 18.75	$ 2,840

Network ID	Branch ID	Section ID	Description	Severity	Distress Qty	Distress Unit	Distress Density	Policy Type	Localized Work Type	Work Qty	Work Unit	Unit Cost	Work Cost
PIE	AP MAIN	4180	RAVELING	High	16	SF	0.0%	Stopgap	AC Partial-Depth Patching	16	SF	$ 6.50	$ 110
PIE	AP MAIN	4190	LINEAR CR	Medium	43	Slabs	69.6%	Stopgap	PCC Crack Sealing	798	LF	$ 7.00	$ 5,590
PIE	AP MAIN	4190	LINEAR CR	High	8	Slabs	13.0%	Stopgap	PCC Crack Sealing	150	LF	$ 7.00	$ 1,050
PIE	AP MAIN	4190	JT SEAL DMG	High	62	Slabs	100.0%	Stopgap	PCC Joint Seal	2,047	LF	$ 4.25	$ 8,710
PIE	AP MAIN	4190	JOINT SPALL	Medium	5	Slabs	8.7%	Stopgap	PCC Partial-Depth Patching	34	SF	$ 169.00	$ 5,890
PIE	AP MAIN	4190	JOINT SPALL	High	3	Slabs	4.4%	Stopgap	PCC Partial-Depth Patching	22	SF	$ 169.00	$ 3,680
PIE	AP MAIN	4195	CORNER BREAK	Medium	5	Slabs	10.0%	Stopgap	PCC Full-Depth Patching	149	SF	$ 75.00	$ 11,150
PIE	AP MAIN	4195	LINEAR CR	Medium	9	Slabs	20.0%	Stopgap	PCC Crack Sealing	152	LF	$ 7.00	$ 1,070
PIE	AP MAIN	4195	JT SEAL DMG	High	46	Slabs	100.0%	Stopgap	PCC Joint Seal	1,239	LF	$ 4.25	$ 5,270
PIE	AP MAIN	4195	SHAT. SLAB	Medium	37	Slabs	80.0%	Stopgap	PCC Crack Sealing	1,214	LF	$ 7.00	$ 8,510
PIE	AP MAIN	4198	SHAT. SLAB	Medium	8	Slabs	18.8%	Stopgap	PCC Crack Sealing	346	LF	$ 7.00	$ 2,430

Table B.2: Section-Level 10-Year Major Rehabilitation Needs

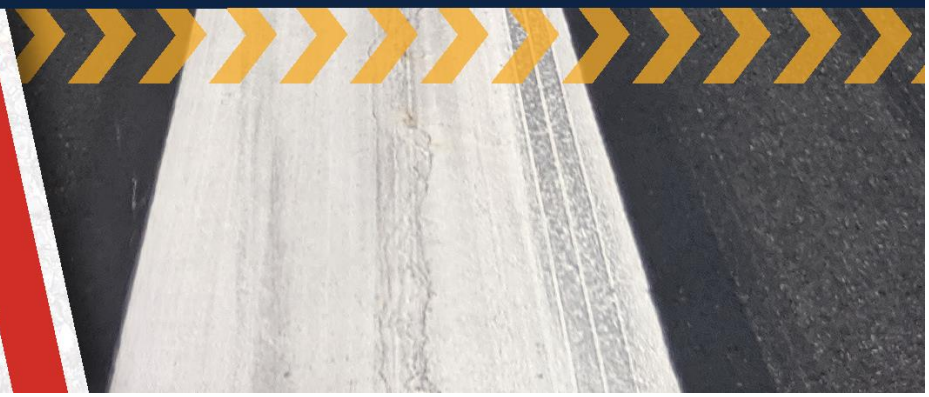
Program Year	Network ID	Branch ID	Section ID	Surface	Area (SF)	PCI Before	Rehabilitation Type	Planning Cost Estimate
2023	PIE	RW 4-22	6215	AAC	50,072	61	AC Rehabilitation	$ 701,000
2023	PIE	RW 4-22	6220	AAC	25,036	68	AC Rehabilitation	$ 351,000
2023	PIE	RW 4-22	6225	AC	45,300	52	AC Reconstruction	$ 1,382,000
2023	PIE	RW 4-22	6230	AC	22,650	22	AC Reconstruction	$ 691,000
2023	PIE	FBO CONN	107	AAC	3,297	44	AC Reconstruction	$ 101,000
2023	PIE	FBO CONN	108	AC	3,361	29	AC Reconstruction	$ 103,000
2023	PIE	FBO CONN	112	AAC	4,221	40	AC Reconstruction	$ 129,000
2023	PIE	FBO CONN	114	AC	2,361	58	AC Rehabilitation	$ 34,000
2023	PIE	FBO CONN	119	AC	3,041	26	AC Reconstruction	$ 93,000
2023	PIE	FBO CONN	125	APC	4,598	57	AC Rehabilitation	$ 65,000
2023	PIE	TW A	115	AAC	224,709	63	AC Rehabilitation	$ 3,146,000
2023	PIE	TW A	130	AAC	358,395	69	AC Rehabilitation	$ 5,018,000
2023	PIE	TW A	158	AAC	16,692	61	AC Rehabilitation	$ 234,000
2023	PIE	TW A1	135	AAC	40,056	62	AC Rehabilitation	$ 561,000
2023	PIE	TW A1	140	AAC	14,541	65	AC Rehabilitation	$ 204,000
2023	PIE	TW B	220	AC	40,670	13	AC Reconstruction	$ 1,241,000
2023	PIE	TW G	1315	AC	19,536	68	AC Rehabilitation	$ 274,000
2023	PIE	TW G	1320	AAC	15,822	60	AC Rehabilitation	$ 222,000
2023	PIE	TW G	1325	AAC	199,036	63	AC Rehabilitation	$ 2,787,000
2023	PIE	TW G1	1330	AAC	13,135	63	AC Rehabilitation	$ 184,000
2023	PIE	TW G1	1335	AAC	12,530	61	AC Rehabilitation	$ 176,000
2023	PIE	TW G3	605	AAC	10,930	20	AC Reconstruction	$ 334,000
2023	PIE	TW H	810	AAC	59,729	0	AC Reconstruction	$ 1,822,000
2023	PIE	TL T-HANG	1120	AC	1,346	37	AC Reconstruction	$ 42,000
2023	PIE	TL T-HANG	1125	AC	1,472	36	AC Reconstruction	$ 45,000
2023	PIE	AP MAIN	4105	APC	40,910	27	AC Reconstruction	$ 1,248,000
2023	PIE	AP MAIN	4155	AAC	33,689	57	AC Rehabilitation	$ 472,000
2023	PIE	AP MAIN	4157	AAC	92,541	65	AC Rehabilitation	$ 1,296,000
2023	PIE	AP MAIN	4175	PCC	14,910	1	PCC Reconstruction	$ 895,000
2023	PIE	AP MAIN	4176	AC	3,573	26	AC Reconstruction	$ 109,000

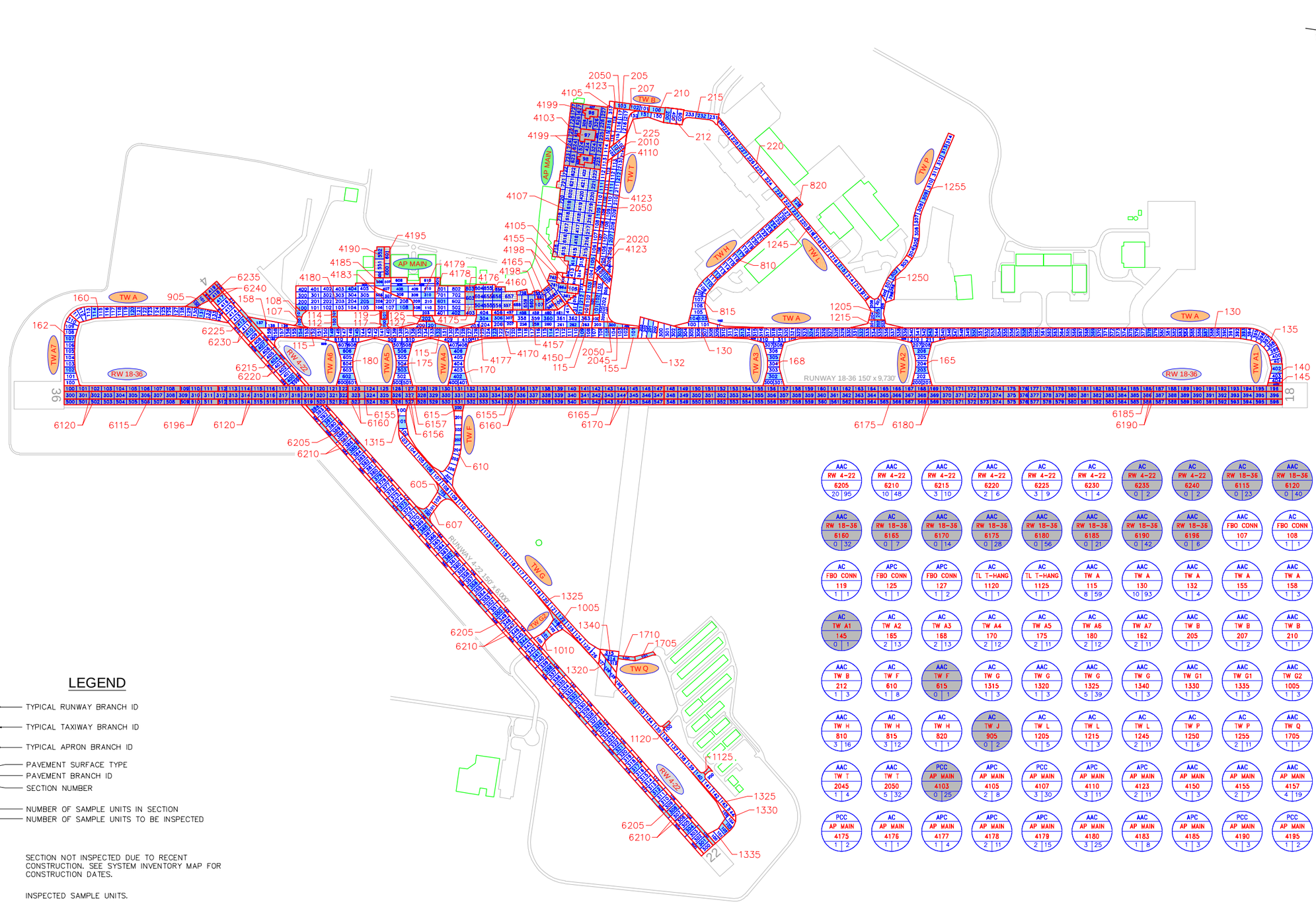
Program Year	Network ID	Branch ID	Section ID	Surface	Area (SF)	PCI Before	Rehabilitation Type	Planning Cost Estimate
2023	PIE	AP MAIN	4178	APC	59,522	60	AC Rehabilitation	$ 834,000
2023	PIE	AP MAIN	4179	APC	77,111	64	AC Rehabilitation	$ 1,080,000
2023	PIE	AP MAIN	4180	AAC	126,695	43	AC Reconstruction	$ 3,865,000
2023	PIE	AP MAIN	4183	AAC	39,947	66	AC Rehabilitation	$ 560,000
2023	PIE	AP MAIN	4185	APC	12,820	49	AC Reconstruction	$ 392,000
2023	PIE	AP MAIN	4190	PCC	18,650	10	PCC Reconstruction	$ 1,119,000
2023	PIE	AP MAIN	4195	PCC	11,250	5	PCC Reconstruction	$ 675,000
2023	PIE	AP MAIN	4198	PCC	18,579	16	PCC Reconstruction	$ 1,115,000
2024	PIE	RW 4-22	6205	AAC	474,873	69	AC Rehabilitation	$ 6,981,000
2024	PIE	TW T	2045	AAC	16,549	70	AC Rehabilitation	$ 244,000
2024	PIE	AP MAIN	4110	APC	56,000	70	AC Rehabilitation	$ 824,000
2026	PIE	FBO CONN	117	AAC	6,019	69	AC Rehabilitation	$ 98,000
2026	PIE	TW A2	165	AC	60,458	69	AC Rehabilitation	$ 980,000
2027	PIE	RW 4-22	6210	AAC	237,436	69	AC Rehabilitation	$ 4,041,000
2027	PIE	TW B	212	AAC	18,000	70	AC Rehabilitation	$ 307,000
2027	PIE	TW F	610	AC	43,041	70	AC Rehabilitation	$ 733,000
2027	PIE	TW L	1215	AC	13,483	70	AC Rehabilitation	$ 230,000
2027	PIE	AP MAIN	4150	AAC	14,083	69	AC Rehabilitation	$ 240,000
2027	PIE	AP MAIN	4177	APC	20,899	69	AC Rehabilitation	$ 356,000
2028	PIE	FBO CONN	127	APC	12,891	70	AC Rehabilitation	$ 231,000
2028	PIE	TW A	160	AAC	99,856	70	AC Rehabilitation	$ 1,785,000
2028	PIE	TW A3	168	AC	60,311	69	AC Rehabilitation	$ 1,078,000
2028	PIE	TW G	1340	AAC	14,004	70	AC Rehabilitation	$ 251,000
2028	PIE	TW L	1205	AC	22,175	69	AC Rehabilitation	$ 397,000
2028	PIE	AP MAIN	4123	APC	43,794	70	AC Rehabilitation	$ 783,000
2029	PIE	TW A7	162	AAC	52,089	69	AC Rehabilitation	$ 978,000
2029	PIE	TW L	1245	AC	52,150	70	AC Rehabilitation	$ 979,000
2030	PIE	TW T	2010	AAC	12,963	70	AC Rehabilitation	$ 256,000
2030	PIE	TW T	2020	AAC	14,337	69	AC Rehabilitation	$ 283,000
2030	PIE	AP MAIN	4170	AAC	18,816	69	AC Rehabilitation	$ 371,000
2031	PIE	TW A	132	AAC	23,007	69	AC Rehabilitation	$ 476,000
2031	PIE	TW A	155	AAC	6,259	70	AC Rehabilitation	$ 130,000

Program Year	Network ID	Branch ID	Section ID	Surface	Area (SF)	PCI Before	Rehabilitation Type	Planning Cost Estimate
2031	**PIE**	TW A5	175	AC	56,987	70	AC Rehabilitation	$ 1,179,000
2031	**PIE**	TW B	210	AAC	6,353	69	AC Rehabilitation	$ 132,000
2031	**PIE**	TW G2	1005	AAC	15,843	70	AC Rehabilitation	$ 328,000
2031	**PIE**	TW G2	1010	AAC	8,964	70	AC Rehabilitation	$ 186,000
2031	**PIE**	TW G3	607	AAC	8,732	69	AC Rehabilitation	$ 181,000
2032	**PIE**	TW B	205	AAC	6,200	69	AC Rehabilitation	$ 135,000
2032	**PIE**	TW B	207	AAC	7,750	69	AC Rehabilitation	$ 169,000
2032	**PIE**	TW T	2050	AAC	149,440	69	AC Rehabilitation	$ 3,246,000

**All planning cost values have been rounded up to the nearest thousand dollars.*

Appendix C: Technical Exhibits





LEGEND

- RW 13-31 — TYPICAL RUNWAY BRANCH ID
- TW A — TYPICAL TAXIWAY BRANCH ID
- AP S — TYPICAL APRON BRANCH ID
- AAC / AP MAIN / 4105 / 10 | 100 — PAVEMENT SURFACE TYPE; PAVEMENT BRANCH ID; SECTION NUMBER; NUMBER OF SAMPLE UNITS IN SECTION; NUMBER OF SAMPLE UNITS TO BE INSPECTED
- AAC / AP MAIN / 4105 / 0 | 100 — SECTION NOT INSPECTED DUE TO RECENT CONSTRUCTION. SEE SYSTEM INVENTORY MAP FOR CONSTRUCTION DATES.
- 100 — INSPECTED SAMPLE UNITS.

TOTAL SAMPLES INSPECTED = 171
AC: 160 PCC: 11

RUNWAYS LENGTHS DEPICTED IN THIS DRAWING ARE FOR PAVEMENT MANAGEMENT PURPOSES ONLY AND MAY NOT MATCH PUBLISHED RUNWAY LENGTHS. DRAWING NOT TO SCALE.

AAC RW 4-22 6205 20 | 95	AAC RW 4-22 6210 10 | 48	AAC RW 4-22 6215 3 | 10	AAC RW 4-22 6220 2 | 6	AC RW 4-22 6225 3 | 9	AC RW 4-22 6230 1 | 4	AC RW 4-22 6235 0 | 2	AC RW 4-22 6240 0 | 2	AC RW 18-36 6115 0 | 23	AAC RW 18-36 6120 0 | 40	AAC RW 18-36 6155 0 | 16	AC RW 18-36 6156 0 | 4	AC RW 18-36 6157 0 | 2
AAC RW 18-36 6160 0 | 32	AC RW 18-36 6165 0 | 7	AAC RW 18-36 6170 0 | 14	AAC RW 18-36 6175 0 | 28	AAC RW 18-36 6180 0 | 56	AAC RW 18-36 6185 0 | 21	AAC RW 18-36 6190 0 | 42	AAC RW 18-36 6195 0 | 8	AAC FBO CONN 107 1 | 1	AC FBO CONN 108 1 | 1	AAC FBO CONN 112 1 | 1	AC FBO CONN 114 1 | 1	AAC FBO CONN 117 1 | 1
AC FBO CONN 119 1 | 1	APC FBO CONN 125 1 | 1	APC FBO CONN 127 1 | 2	AC TL T-HANG 1120 1 | 1	AC TL T-HANG 1125 1 | 1	AAC TW A 115 8 | 59	AAC TW A 130 10 | 93	AAC TW A 132 1 | 4	AAC TW A 155 1 | 1	AAC TW A 158 1 | 3	AAC TW A 160 3 | 24	AC TW A1 135 1 | 9	AAC TW A1 140 1 | 3
AC TW A1 145 0 | 1	AC TW A2 165 2 | 13	AC TW A3 168 2 | 13	AC TW A4 170 2 | 12	AC TW A5 175 2 | 11	AC TW A6 180 2 | 12	AAC TW A7 162 2 | 11	AAC TW B 205 1 | 1	AAC TW B 207 1 | 2	AAC TW B 210 1 | 1	AAC TW B 215 1 | 3	AC TW B 220 2 | 8	AC TW B 225 1 | 3
AAC TW B 212 1 | 3	AC TW F 610 1 | 8	AAC TW F 615 0 | 1	AC TW G 1315 1 | 3	AAC TW G 1320 1 | 3	AAC TW G 1325 5 | 39	AAC TW G 1340 1 | 3	AAC TW G1 1330 1 | 3	AAC TW G1 1335 1 | 3	AAC TW G2 1005 1 | 3	AAC TW G2 1010 1 | 2	AAC TW G3 605 1 | 2	AAC TW G3 607 1 | 2
AAC TW H 810 3 | 16	AC TW H 815 3 | 12	AC TW H 820 1 | 1	AC TW J 905 0 | 2	AC TW L 1205 1 | 5	AC TW L 1215 1 | 3	AC TW L 1245 2 | 11	AC TW P 1250 1 | 6	AC TW P 1255 2 | 11	AAC TW Q 1705 1 | 1	AC TW Q 1710 1 | 1	AAC TW T 2010 1 | 3	AAC TW T 2020 1 | 4
AAC TW T 2045 1 | 4	AAC TW T 2050 5 | 32	PCC AP MAIN 4103 0 | 25	APC AP MAIN 4105 2 | 8	PCC AP MAIN 4107 3 | 30	APC AP MAIN 4110 3 | 11	APC AP MAIN 4123 2 | 11	AAC AP MAIN 4150 1 | 3	AAC AP MAIN 4155 2 | 7	AAC AP MAIN 4157 4 | 19	PCC AP MAIN 4160 2 | 11	PCC AP MAIN 4165 2 | 14	AAC AP MAIN 4170 1 | 4
PCC AP MAIN 4175 1 | 2	AC AP MAIN 4176 1 | 1	APC AP MAIN 4177 1 | 4	APC AP MAIN 4178 2 | 11	APC AP MAIN 4179 2 | 15	AAC AP MAIN 4180 3 | 25	AAC AP MAIN 4183 1 | 8	APC AP MAIN 4185 1 | 3	PCC AP MAIN 4190 1 | 3	PCC AP MAIN 4195 1 | 2	PCC AP MAIN 4198 1 | 3	PCC AP MAIN 4199 0 | 3	

PIE

AIRFIELD PAVEMENT NETWORK DEFINITION EXHIBIT

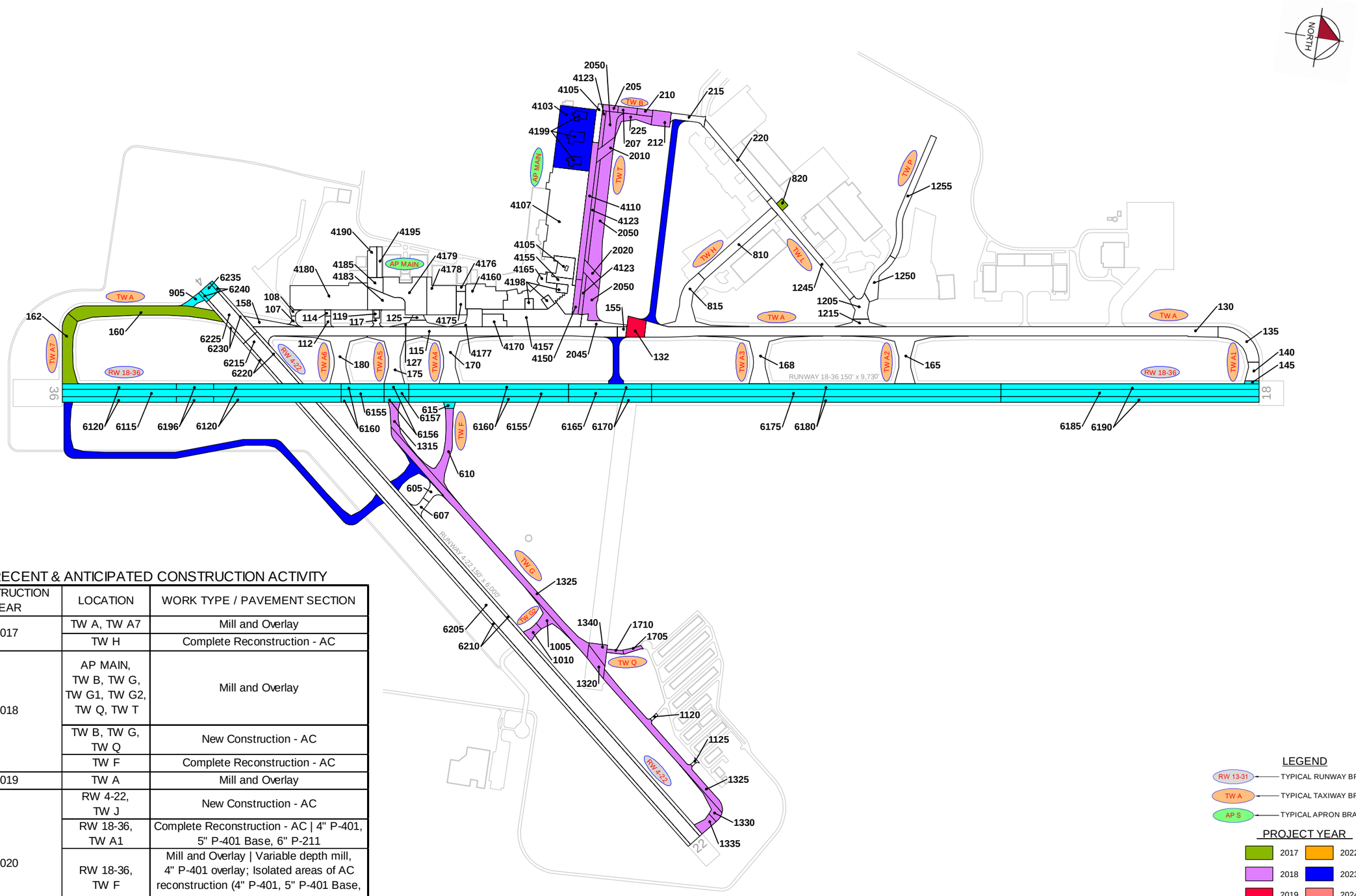
Statewide Airfield Pavement Management Program

ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

FDOT

2022

PIE



RECENT & ANTICIPATED CONSTRUCTION ACTIVITY

CONSTRUCTION YEAR	LOCATION	WORK TYPE / PAVEMENT SECTION
2017	TW A, TW A7	Mill and Overlay
	TW H	Complete Reconstruction - AC
2018	AP MAIN, TW B, TW G, TW G1, TW G2, TW Q, TW T	Mill and Overlay
	TW B, TW G, TW Q	New Construction - AC
	TW F	Complete Reconstruction - AC
2019	TW A	Mill and Overlay
2020	RW 4-22, TW J	New Construction - AC
	RW 18-36, TW A1	Complete Reconstruction - AC | 4" P-401, 5" P-401 Base, 6" P-211
	RW 18-36, TW F	Mill and Overlay | Variable depth mill, 4" P-401 overlay; Isolated areas of AC reconstruction (4" P-401, 5" P-401 Base, 6" P-211)
	RW 18-36	Mill and Overlay | Variable depth mill, 2" P-401 overlay
2023	AP MAIN	Complete Reconstruction - PCC
	TW C, TW D, TW G3	New Construction

LEGEND

RW 13-31 — TYPICAL RUNWAY BRANCH ID

TW A — TYPICAL TAXIWAY BRANCH ID

AP S — TYPICAL APRON BRANCH ID

PROJECT YEAR

2017		2022	
2018		2023	
2019		2024	
2020		2025	
2021		2026	

RUNWAY LENGTHS DEPICTED IN THIS DRAWING ARE FOR PAVEMENT MANAGEMENT PURPOSES ONLY AND MAY NOT MATCH PUBLISHED RUNWAY LENGTHS. DRAWING NOT TO SCALE.

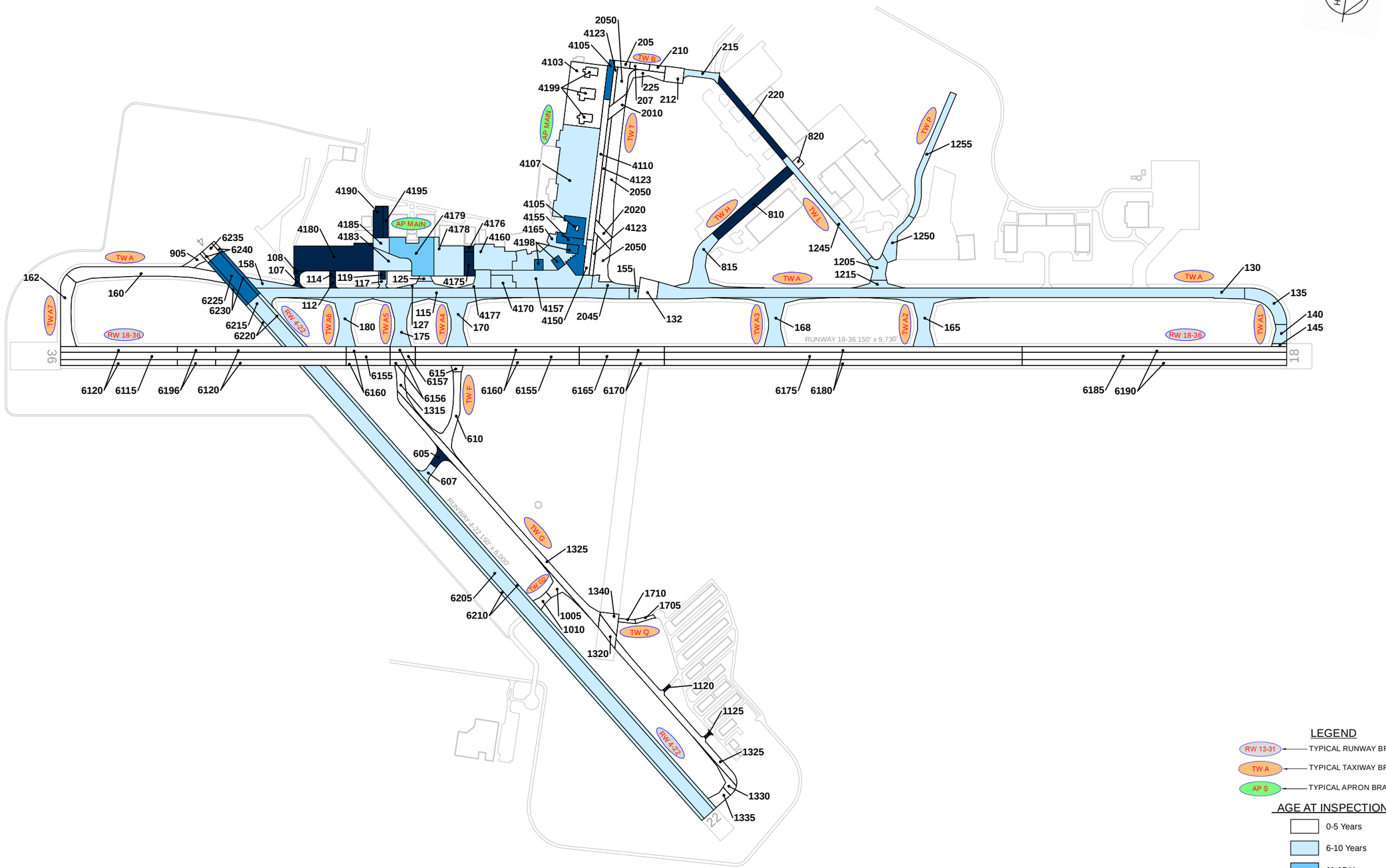
AIRFIELD PAVEMENT SYSTEM INVENTORY EXHIBIT

Statewide Airfield Pavement Management Program

ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

FDOT

2022



LEGEND

- RW 13-31 — TYPICAL RUNWAY BRANCH ID
- TW A — TYPICAL TAXIWAY BRANCH ID
- AP S — TYPICAL APRON BRANCH ID

AGE AT INSPECTION

- 0-5 Years
- 6-10 Years
- 11-15 Years
- 16-20 Years
- > 20 Years

RUNWAY LENGTHS DEPICTED IN THIS DRAWING ARE FOR PAVEMENT MANAGEMENT PURPOSES ONLY AND MAY NOT MATCH PUBLISHED RUNWAY LENGTHS. DRAWING NOT TO SCALE.

PIE

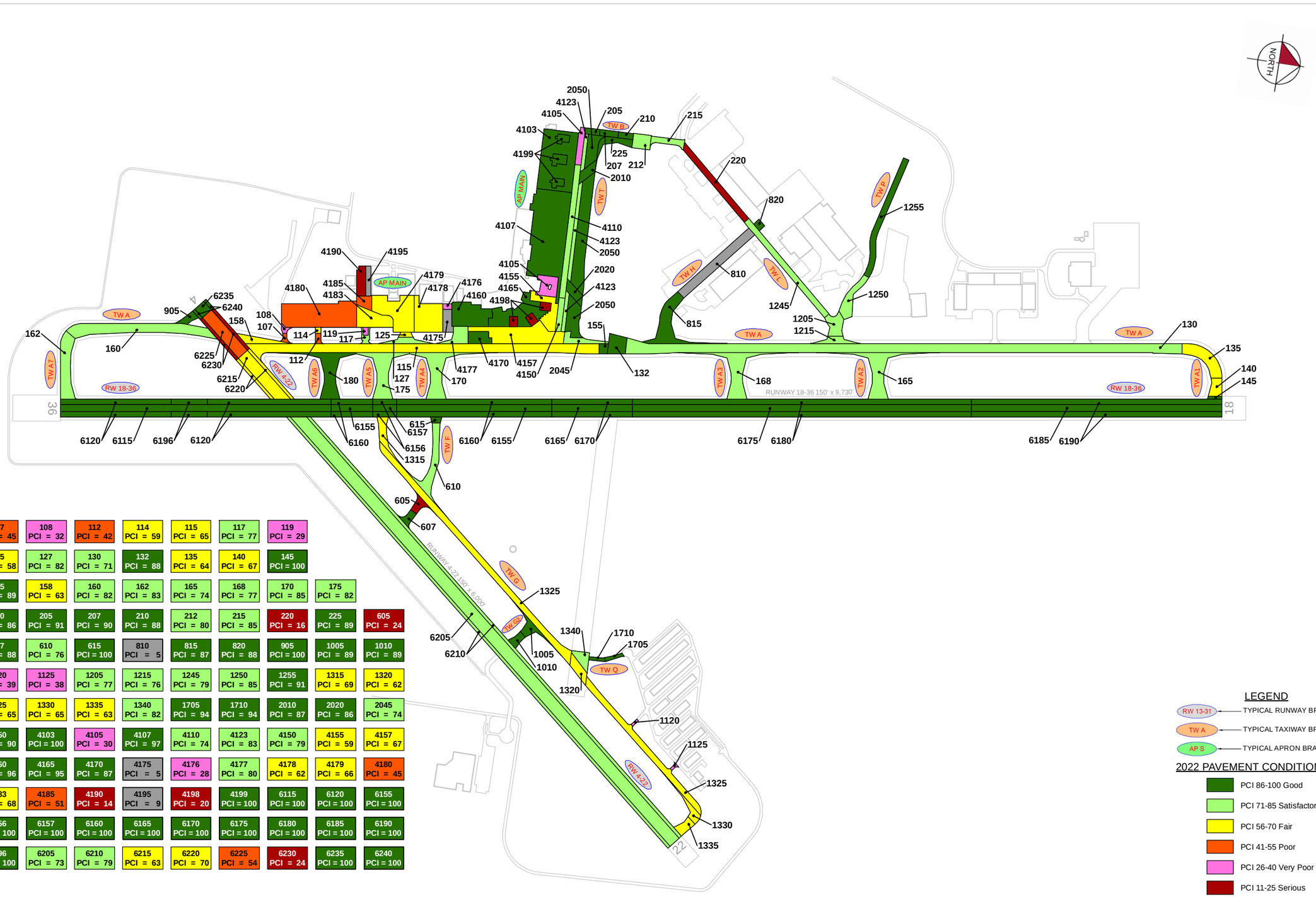
AIRFIELD PAVEMENT
ESTIMATED AGE EXHIBIT

Statewide Airfield Pavement Management Program

ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

FDOT

2022



107 PCI = 45	108 PCI = 32	112 PCI = 42	114 PCI = 59	115 PCI = 65	117 PCI = 77	119 PCI = 29
125 PCI = 58	127 PCI = 82	130 PCI = 71	132 PCI = 88	135 PCI = 64	140 PCI = 67	145 PCI = 100
155 PCI = 89	158 PCI = 63	160 PCI = 82	162 PCI = 83	165 PCI = 74	168 PCI = 77	170 PCI = 85	175 PCI = 82
180 PCI = 86	205 PCI = 91	207 PCI = 90	210 PCI = 88	212 PCI = 80	215 PCI = 85	220 PCI = 16	225 PCI = 89	605 PCI = 24
607 PCI = 88	610 PCI = 76	615 PCI = 100	810 PCI = 5	815 PCI = 87	820 PCI = 88	905 PCI = 100	1005 PCI = 89	1010 PCI = 89
1120 PCI = 39	1125 PCI = 38	1205 PCI = 77	1215 PCI = 76	1245 PCI = 79	1250 PCI = 85	1255 PCI = 91	1315 PCI = 69	1320 PCI = 62
1325 PCI = 65	1330 PCI = 65	1335 PCI = 63	1340 PCI = 82	1705 PCI = 94	1710 PCI = 94	2010 PCI = 87	2020 PCI = 86	2045 PCI = 74
2050 PCI = 90	4103 PCI = 100	4105 PCI = 30	4107 PCI = 97	4110 PCI = 74	4123 PCI = 83	4150 PCI = 79	4155 PCI = 59	4157 PCI = 67
4160 PCI = 96	4165 PCI = 95	4170 PCI = 87	4175 PCI = 5	4176 PCI = 28	4177 PCI = 80	4178 PCI = 62	4179 PCI = 66	4180 PCI = 45
4183 PCI = 68	4185 PCI = 51	4190 PCI = 14	4195 PCI = 9	4198 PCI = 20	4199 PCI = 100	6115 PCI = 100	6120 PCI = 100	6155 PCI = 100
6156 PCI = 100	6157 PCI = 100	6160 PCI = 100	6165 PCI = 100	6170 PCI = 100	6175 PCI = 100	6180 PCI = 100	6185 PCI = 100	6190 PCI = 100
6196 PCI = 100	6205 PCI = 73	6210 PCI = 79	6215 PCI = 63	6220 PCI = 70	6225 PCI = 54	6230 PCI = 24	6235 PCI = 100	6240 PCI = 100

LEGEND

- RW 13-31 — TYPICAL RUNWAY BRANCH ID
- TW A — TYPICAL TAXIWAY BRANCH ID
- AP S — TYPICAL APRON BRANCH ID

2022 PAVEMENT CONDITION INDEX

- PCI 86-100 Good
- PCI 71-85 Satisfactory
- PCI 56-70 Fair
- PCI 41-55 Poor
- PCI 26-40 Very Poor
- PCI 11-25 Serious
- PCI 0-10 Failed

"SECTION ID"
"PCI VALUE"

RUNWAY LENGTHS DEPICTED IN THIS DRAWING ARE FOR PAVEMENT MANAGEMENT PURPOSES ONLY AND MAY NOT MATCH PUBLISHED RUNWAY LENGTHS. DRAWING NOT TO SCALE.

PIE

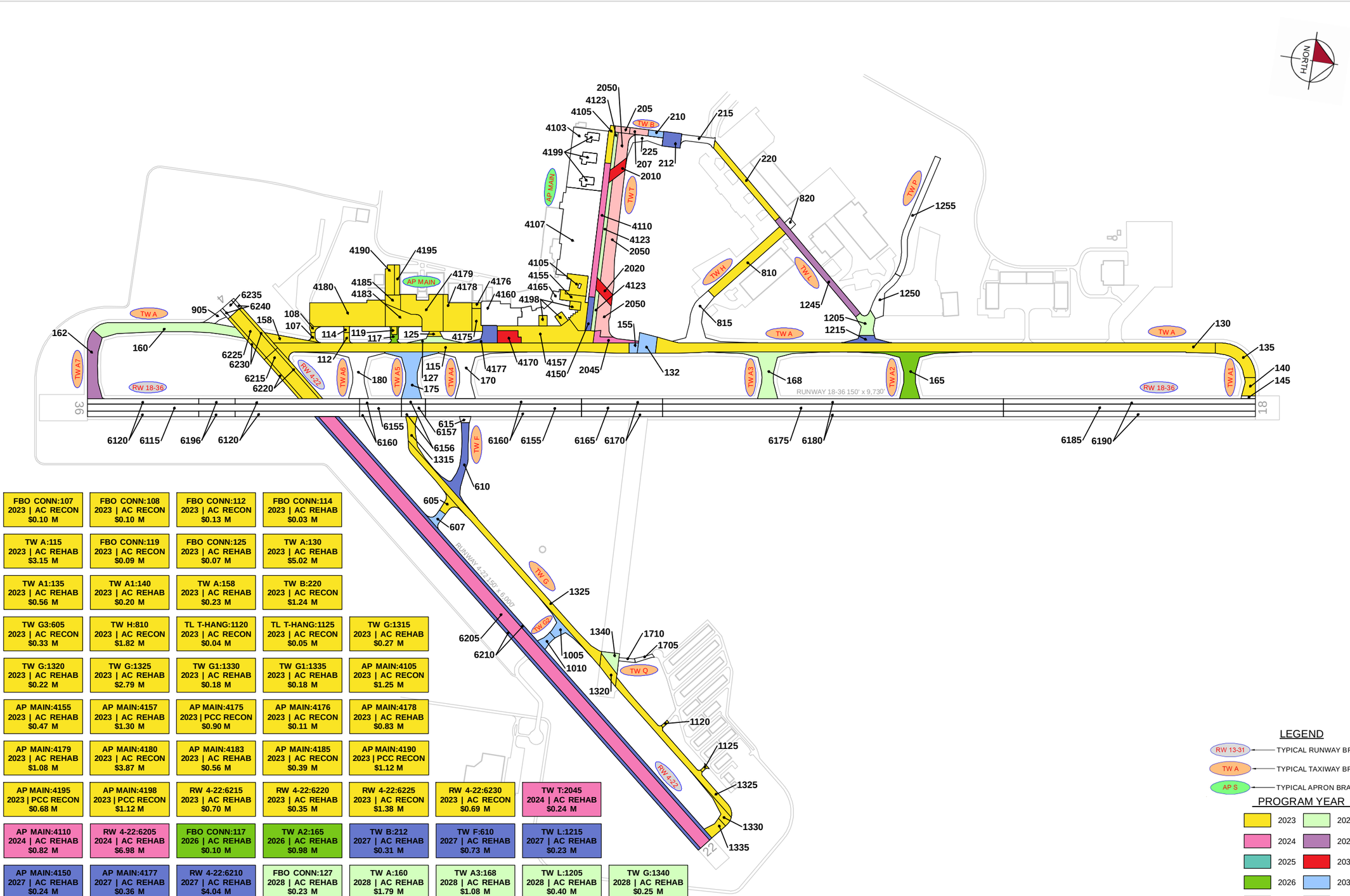
AIRFIELD PAVEMENT CONDITION INDEX EXHIBIT

Statewide Airfield Pavement Management Program

ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

FDOT

2022



FBO CONN:107 2023 | AC RECON $0.10 M	FBO CONN:108 2023 | AC RECON $0.10 M	FBO CONN:112 2023 | AC RECON $0.13 M	FBO CONN:114 2023 | AC REHAB $0.03 M
TW A:115 2023 | AC REHAB $3.15 M	FBO CONN:119 2023 | AC RECON $0.09 M	FBO CONN:125 2023 | AC REHAB $0.07 M	TW A:130 2023 | AC REHAB $5.02 M
TW A1:135 2023 | AC REHAB $0.56 M	TW A1:140 2023 | AC REHAB $0.20 M	TW A:158 2023 | AC REHAB $0.23 M	TW B:220 2023 | AC RECON $1.24 M
TW G3:605 2023 | AC RECON $0.33 M	TW H:810 2023 | AC RECON $1.82 M	TL T-HANG:1120 2023 | AC RECON $0.04 M	TL T-HANG:1125 2023 | AC RECON $0.05 M	TW G:1315 2023 | AC REHAB $0.27 M
TW G:1320 2023 | AC REHAB $0.22 M	TW G:1325 2023 | AC REHAB $2.79 M	TW G1:1330 2023 | AC REHAB $0.18 M	TW G1:1335 2023 | AC REHAB $0.18 M	AP MAIN:4105 2023 | AC RECON $1.25 M
AP MAIN:4155 2023 | AC REHAB $0.47 M	AP MAIN:4157 2023 | AC REHAB $1.30 M	AP MAIN:4175 2023 | PCC RECON $0.90 M	AP MAIN:4176 2023 | AC RECON $0.11 M	AP MAIN:4178 2023 | AC REHAB $0.83 M
AP MAIN:4179 2023 | AC REHAB $1.08 M	AP MAIN:4180 2023 | AC RECON $3.87 M	AP MAIN:4183 2023 | AC REHAB $0.56 M	AP MAIN:4185 2023 | AC RECON $0.39 M	AP MAIN:4190 2023 | PCC RECON $1.12 M
AP MAIN:4195 2023 | PCC RECON $0.68 M	AP MAIN:4198 2023 | PCC RECON $1.12 M	RW 4-22:6215 2023 | AC REHAB $0.70 M	RW 4-22:6220 2023 | AC REHAB $0.35 M	RW 4-22:6225 2023 | AC RECON $1.38 M	RW 4-22:6230 2023 | AC RECON $0.69 M	TW T:2045 2024 | AC REHAB $0.24 M
AP MAIN:4110 2024 | AC REHAB $0.82 M	RW 4-22:6205 2024 | AC REHAB $6.98 M	FBO CONN:117 2026 | AC REHAB $0.10 M	TW A2:165 2026 | AC REHAB $0.98 M	TW B:212 2027 | AC REHAB $0.31 M	TW F:610 2027 | AC REHAB $0.73 M	TW L:1215 2027 | AC REHAB $0.23 M
AP MAIN:4150 2027 | AC REHAB $0.24 M	AP MAIN:4177 2027 | AC REHAB $0.36 M	RW 4-22:6210 2027 | AC REHAB $4.04 M	FBO CONN:127 2028 | AC REHAB $0.23 M	TW A:160 2028 | AC REHAB $1.79 M	TW A3:168 2028 | AC REHAB $1.08 M	TW L:1205 2028 | AC REHAB $0.40 M	TW G:1340 2028 | AC REHAB $0.25 M
AP MAIN:4123 2028 | AC REHAB $0.78 M	TW A7:162 2029 | AC REHAB $0.98 M	TW L:1245 2029 | AC REHAB $0.98 M	TW T:2010 2030 | AC REHAB $0.26 M	TW T:2020 2030 | AC REHAB $0.28 M	AP MAIN:4170 2030 | AC REHAB $0.37 M	TW A:132 2031 | AC REHAB $0.48 M	TW A:155 2031 | AC REHAB $0.13 M
TW A5:175 2031 | AC REHAB $1.18 M	TW B:210 2031 | AC REHAB $0.13 M	TW G3:607 2031 | AC REHAB $0.18 M	TW G2:1005 2031 | AC REHAB $0.33 M	TW G2:1010 2031 | AC REHAB $0.19 M	TW B:205 2032 | AC REHAB $0.14 M	TW B:207 2032 | AC REHAB $0.17 M	TW T:2050 2032 | AC REHAB $3.25 M

LEGEND

RW 13-31 — TYPICAL RUNWAY BRANCH ID

TW A — TYPICAL TAXIWAY BRANCH ID

AP S — TYPICAL APRON BRANCH ID

PROGRAM YEAR

2023	2028
2024	2029
2025	2030
2026	2031
2027	2032

"BRANCH":"SECTION"
"YEAR"|"REHAB ACTIVITY"
"EST. COST"

RUNWAY LENGTHS DEPICTED IN THIS DRAWING ARE FOR PAVEMENT MANAGEMENT PURPOSES ONLY AND MAY NOT MATCH PUBLISHED RUNWAY LENGTHS. DRAWING NOT TO SCALE.

PIE

AIRFIELD PAVEMENT MAJOR REHABILITATION EXHIBIT

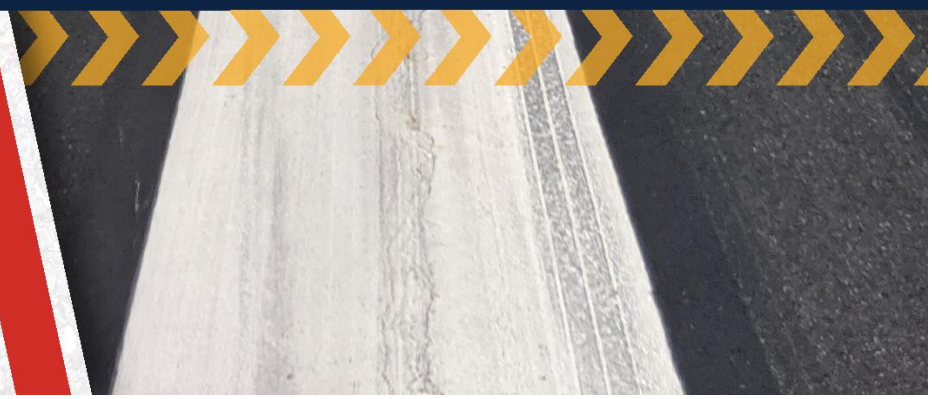
Statewide Airfield Pavement Management Program

ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

FDOT

2022

Appendix D: Inspection Photograph Documentation



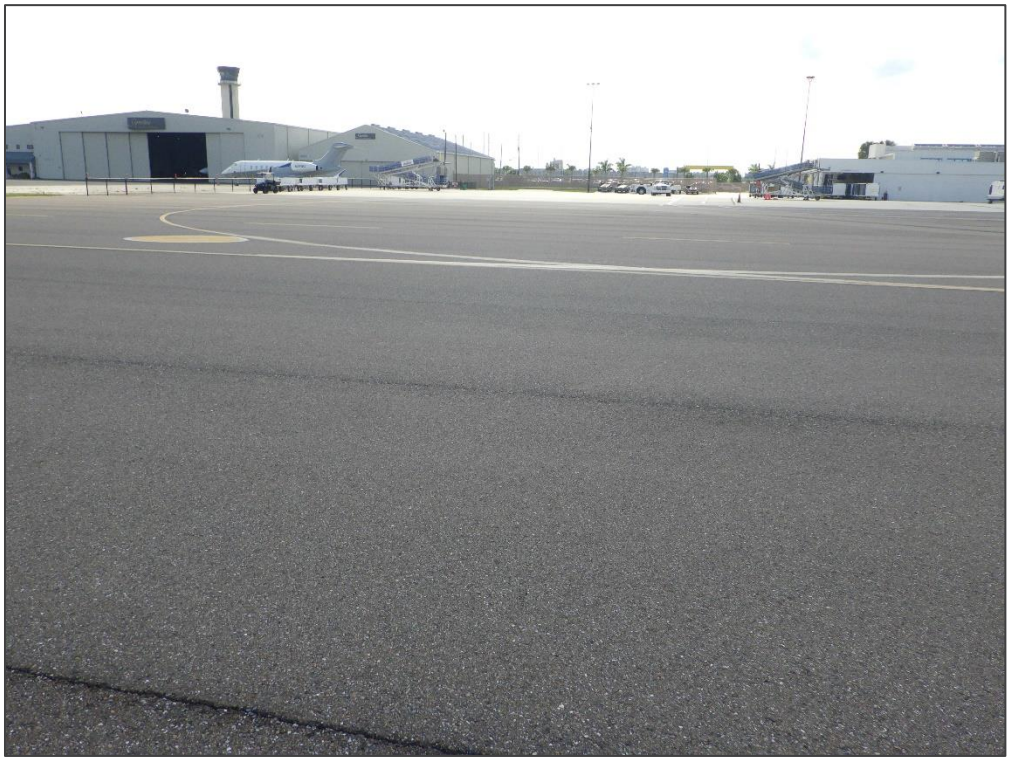
RW 4-22, Section 6205, Sample Unit 308 – Alligator Cracking

RW 4-22, Section 6205, Sample Unit 373 – Longitudinal & Transverse Cracking

RW 4-22, Section 6225, Sample Unit 412 – Bleeding and Rutting

RW 4-22, Section 6230, Sample Unit 210 – Bleeding

TW A, Section 115, Sample Unit 114 – Slippage Cracking



TW A, Section 115, Sample Unit 123 – Vicinity

TW A, Section 130, Sample Unit 343 – Longitudinal & Transverse Cracking and Swelling

TW G, Section 1315, Sample Unit 101 – Rutting

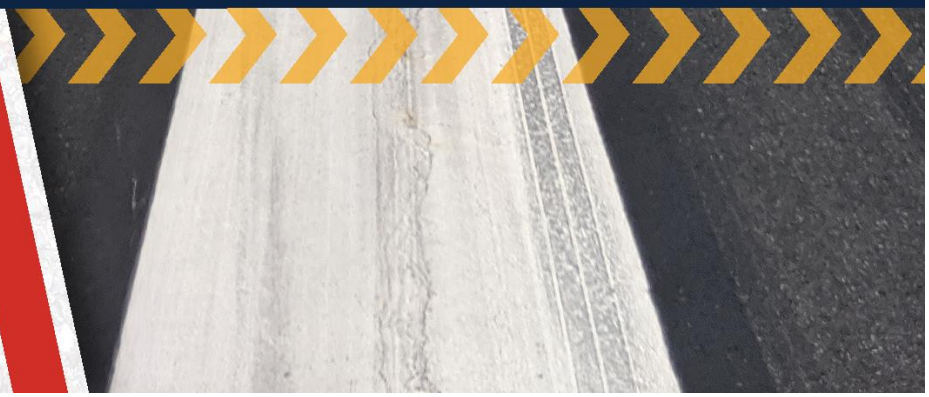
TW H, Section 810, Sample Unit 112 – Vicinity

AP MAIN, Section 4110, Sample Unit 603 – Slippage Cracking

AP MAIN, Section 4175, Sample Unit 603 – Shattered Slab

AP MAIN, Section 4180, Sample Unit 404 – Patching

Appendix E: Inspection Distress Details



Re-Inspection Report

VRB_12-08-2022
Generated Date 12/14/2022

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: AP MAIN **Name:** MAIN APRON **Use:** APRON **Area:** 1,197,983 SqFt

Section: 4103 of 23 **From:** - **To:** - **Last Const.:** 1/1/2023

Surface: PCC **Family:** CA653-PR-AP-PCC **Zone:** **Category:** **Rank:** P

Area: 122,390 SqFt **Length:** 295 Ft **Width:** 500 Ft

Slabs: 306 **Slab Length:** 20 Ft **Slab Width:** 20 Ft **Joint Length:** 13,955 Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1942	**Work Type:** New Construction - PCC	**Code:** NC-PC	**Is Major M&R:** True
Work Date: 1/2/1942	**Work Type:** Overlay - AC Structural	**Code:** OL-AS	**Is Major M&R:** True
Work Date: 1/1/2003	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 1/2/2003	**Work Type:** Overlay - AC Structural	**Code:** OL-AS	**Is Major M&R:** True
Work Date: 1/1/2023	**Work Type:** Complete Reconstruction - PCC	**Code:** CR-PC	**Is Major M&R:** True

Last Insp. Date: 12/10/2018 **TotalSamples:** 33 **Surveyed:** 3

Conditions: **PCI:** 36 **NOTE: *** Pre-Construction PCI *****

Inspection Comments:

Sample Number: 223 **Type:** R **Area:** 5750.00 SqFt **PCI:** 46

Sample Comments:

43	BLOCK CR	L	2800.00	SqFt
48	L & T CR	L	200.00	Ft
48	L & T CR	M	25.00	Ft
52	RAVELING	L	5066.00	SqFt
52	RAVELING	M	684.00	SqFt

Sample Number: 227 **Type:** R **Area:** 4352.00 SqFt **PCI:** 38

Sample Comments:

43	BLOCK CR	L	1737.00	SqFt
45	DEPRESSION	L	280.00	SqFt
48	L & T CR	L	125.00	Ft
48	L & T CR	M	73.00	Ft
52	RAVELING	L	3615.00	SqFt
52	RAVELING	M	737.00	SqFt

Sample Number: 414 **Type:** R **Area:** 6004.00 SqFt **PCI:** 25

Sample Comments:

43	BLOCK CR	L	4450.00	SqFt
43	BLOCK CR	M	1484.00	SqFt
45	DEPRESSION	L	174.00	SqFt
50	PATCHING	L	70.00	SqFt
52	RAVELING	L	1483.00	SqFt
52	RAVELING	M	4451.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: AP MAIN **Name:** MAIN APRON **Use:** APRON **Area:** 1,197,983 SqFt

Section: 4105 of 23 **From:** - **To:** - **Last Const.:** 1/2/2003

Surface: APC **Family:** CA653-PR-AP-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 40,910 SqFt **Length:** 315 Ft **Width:** 50 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1942	**Work Type:** New Construction - PCC	**Code:** NC-PC	**Is Major M&R:** True
Work Date: 1/2/1942	**Work Type:** Overlay - AC Structural	**Code:** OL-AS	**Is Major M&R:** True
Work Date: 1/1/2003	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 1/2/2003	**Work Type:** Overlay - AC Structural	**Code:** OL-AS	**Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 8 **Surveyed:** 2

Conditions: **PCI:** 30

Inspection Comments:

Sample Number: 316 **Type:** R **Area:** 5000.00 SqFt **PCI:** 37

Sample Comments:

43	BLOCK CR	L	2500.00	SqFt
43	BLOCK CR	M	2500.00	SqFt
52	RAVELING	L	4750.00	SqFt
52	RAVELING	M	250.00	SqFt

Sample Number: 414 **Type:** R **Area:** 6004.00 SqFt **PCI:** 25

Sample Comments:

43	BLOCK CR	L	4450.00	SqFt
43	BLOCK CR	M	1484.00	SqFt
45	DEPRESSION	L	200.00	SqFt
50	PATCHING	L	70.00	SqFt
52	RAVELING	L	1484.00	SqFt
52	RAVELING	M	4450.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: AP MAIN **Name:** MAIN APRON **Use:** APRON **Area:** 1,197,983 SqFt

Section: 4107 of 23 **From:** - **To:** - **Last Const.:** 1/1/2016

Surface: PCC **Family:** CA653-PR-AP-PCC **Zone:** **Category:** **Rank:** P

Area: 220,315 SqFt **Length:** 730 Ft **Width:** 295 Ft

Slabs: 680 **Slab Length:** 18 Ft **Slab Width:** 18 Ft **Joint Length:** 22,903 Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1942	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1942	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2003	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 1/1/2016	**Work Type:** Complete Reconstruction - PCC	**Code:** CR-PC	**Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 30 **Surveyed:** 3

Conditions: **PCI:** 97

Inspection Comments:

Sample Number: 221 **Type:** R **Area:** 25.00 Slabs **PCI:** 98

Sample Comments:

73	SHRINKAGE CR	N	3.00	Slabs

Sample Number: 416 **Type:** R **Area:** 25.00 Slabs **PCI:** 96

Sample Comments:

73	SHRINKAGE CR	N	7.00	Slabs

Sample Number: 619 **Type:** R **Area:** 25.00 Slabs **PCI:** 98

Sample Comments:

73	SHRINKAGE CR	N	1.00	Slabs
74	JOINT SPALL	L	1.00	Slabs

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: AP MAIN **Name:** MAIN APRON **Use:** APRON **Area:** 1,197,983 SqFt

Section: 4110 of 23 **From:** - **To:** - **Last Const.:** 6/1/2018

Surface: APC **Family:** CA653-PR-AP-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 56,000 SqFt **Length:** 1,120 Ft **Width:** 50 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1942	**Work Type:** New Construction - PCC	**Code:** NC-PC	**Is Major M&R:** True
Work Date: 1/2/1942	**Work Type:** Overlay - AC Structural	**Code:** OL-AS	**Is Major M&R:** True
Work Date: 1/1/2003	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 6/1/2018	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 11 **Surveyed:** 3

Conditions: **PCI:** 74

Inspection Comments:

Sample Number: 105 **Type:** R **Area:** 5000.00 SqFt **PCI:** 69

Sample Comments:

48	L & T CR	L	418.00	Ft
52	RAVELING	L	300.00	SqFt
57	WEATHERING	L	4700.00	SqFt

Sample Number: 109 **Type:** R **Area:** 5000.00 SqFt **PCI:** 74

Sample Comments:

42	BLEEDING	N	53.00	SqFt
48	L & T CR	L	125.00	Ft
52	RAVELING	L	50.00	SqFt
55	SLIPPAGE CR	N	12.00	SqFt
57	WEATHERING	L	4950.00	SqFt

Sample Number: 111 **Type:** R **Area:** 5000.00 SqFt **PCI:** 79

Sample Comments:

48	L & T CR	L	247.00	Ft
56	SWELLING	L	20.00	SqFt
57	WEATHERING	L	5000.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: AP MAIN **Name:** MAIN APRON **Use:** APRON **Area:** 1,197,983 SqFt

Section: 4123 of 23 **From:** - **To:** - **Last Const.:** 6/1/2018

Surface: APC **Family:** CA653-PR-AP-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 43,794 SqFt **Length:** 1,460 Ft **Width:** 30 Ft

Slabs: 14 **Slab Length:** 55 Ft **Slab Width:** 55 Ft **Joint Length:** 103 Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1997	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1997	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2003	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 6/1/2018	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 11 **Surveyed:** 2

Conditions: **PCI:** 83

Inspection Comments:

Sample Number: 101 **Type:** R **Area:** 5337.00 SqFt **PCI:** 81

Sample Comments:

48	L & T CR	L	216.00	Ft
56	SWELLING	L	15.00	SqFt
57	WEATHERING	L	5337.00	SqFt

Sample Number: 110 **Type:** R **Area:** 3000.00 SqFt **PCI:** 86

Sample Comments:

48	L & T CR	L	54.00	Ft
56	SWELLING	L	10.00	SqFt
57	WEATHERING	L	3000.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: AP MAIN **Name:** MAIN APRON **Use:** APRON **Area:** 1,197,983 SqFt

Section: 4150 of 23 **From:** - **To:** - **Last Const.:** 6/1/2018

Surface: AAC **Family:** CA653-PR-AP-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 14,083 SqFt **Length:** 285 Ft **Width:** 50 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1955	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1990	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/2/1990	**Work Type:** Surface Treatment - Seal Coat	**Code:** ST-SC	**Is Major M&R:** False
Work Date: 1/1/2003	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 6/1/2018	**Work Type:** Overlay - AC Structural	**Code:** OL-AS	**Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 3 **Surveyed:** 1

Conditions: **PCI:** 79

Inspection Comments:

Sample Number: 103 **Type:** R **Area:** 5000.00 SqFt **PCI:** 79

Sample Comments:

48	L & T CR	L	283.00	Ft
57	WEATHERING	L	5000.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: AP MAIN **Name:** MAIN APRON **Use:** APRON **Area:** 1,197,983 SqFt

Section: 4155 of 23 **From:** - **To:** - **Last Const.:** 1/1/2003

Surface: AAC **Family:** CA653-PR-AP-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 33,689 SqFt **Length:** 275 Ft **Width:** 125 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1955	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1990	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/2/1990	**Work Type:** Surface Treatment - Seal Coat	**Code:** ST-SC	**Is Major M&R:** False
Work Date: 1/1/2003	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 7 **Surveyed:** 2

Conditions: **PCI:** 59

Inspection Comments:

Sample Number: 211 **Type:** R **Area:** 4368.00 SqFt **PCI:** 54

Sample Comments:

42	BLEEDING	N	45.00	SqFt
48	L & T CR	L	234.00	Ft
48	L & T CR	M	150.00	Ft
52	RAVELING	L	250.00	SqFt
56	SWELLING	L	20.00	SqFt
57	WEATHERING	L	3912.00	SqFt
57	WEATHERING	M	206.00	SqFt

Sample Number: 212 **Type:** R **Area:** 5000.00 SqFt **PCI:** 63

Sample Comments:

42	BLEEDING	N	53.00	SqFt
48	L & T CR	L	216.00	Ft
48	L & T CR	M	50.00	Ft
52	RAVELING	L	500.00	SqFt
57	WEATHERING	L	4250.00	SqFt
57	WEATHERING	M	250.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: AP MAIN **Name:** MAIN APRON **Use:** APRON **Area:** 1,197,983 SqFt

Section: 4157 of 23 **From:** - **To:** - **Last Const.:** 8/1/2016

Surface: AAC **Family:** CA653-PR-AP-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 92,541 SqFt **Length:** 597 Ft **Width:** 300 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1955	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1990	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/2/1990	**Work Type:** Surface Treatment - Seal Coat	**Code:** ST-SC	**Is Major M&R:** False
Work Date: 1/1/2003	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 8/1/2016	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 19 **Surveyed:** 4

Conditions: **PCI:** 67

Inspection Comments:

Sample Number: 259 **Type:** R **Area:** 4850.00 SqFt **PCI:** 65

Sample Comments:

48	L & T CR	L	217.00	Ft
48	L & T CR	M	200.00	Ft
56	SWELLING	L	33.00	SqFt
57	WEATHERING	L	4850.00	SqFt

Sample Number: 262 **Type:** R **Area:** 4850.00 SqFt **PCI:** 71

Sample Comments:

48	L & T CR	L	528.00	Ft
57	WEATHERING	L	4850.00	SqFt

Sample Number: 360 **Type:** R **Area:** 5300.00 SqFt **PCI:** 61

Sample Comments:

48	L & T CR	L	229.00	Ft
48	L & T CR	M	100.00	Ft
52	RAVELING	L	300.00	SqFt
55	SLIPPAGE CR	N	7.00	SqFt
57	WEATHERING	L	4750.00	SqFt
57	WEATHERING	M	250.00	SqFt

Sample Number: 458 **Type:** R **Area:** 4058.00 SqFt **PCI:** 71

Sample Comments:

48	L & T CR	L	267.00	Ft
56	SWELLING	L	30.00	SqFt
57	WEATHERING	L	3855.00	SqFt
57	WEATHERING	M	203.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: AP MAIN **Name:** MAIN APRON **Use:** APRON **Area:** 1,197,983 SqFt

Section: 4160 of 23 **From:** - **To:** - **Last Const.:** 1/1/2016

Surface: PCC **Family:** CA653-PR-AP-PCC **Zone:** **Category:** **Rank:** P

Area: 59,640 SqFt **Length:** 305 Ft **Width:** 190 Ft

Slabs: 184 **Slab Length:** 18 Ft **Slab Width:** 18 Ft **Joint Length:** 5,944 Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1955	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1990	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/2/1990	**Work Type:** Surface Treatment - Seal Coat	**Code:** ST-SC	**Is Major M&R:** False
Work Date: 1/1/2003	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 1/1/2016	**Work Type:** Complete Reconstruction - PCC	**Code:** CR-PC	**Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 11 **Surveyed:** 2

Conditions: **PCI:** 96

Inspection Comments:

Sample Number: 504 **Type:** R **Area:** 16.00 Slabs **PCI:** 98

Sample Comments:

74	JOINT SPALL	L	1.00	Slabs

Sample Number: 656 **Type:** R **Area:** 16.00 Slabs **PCI:** 94

Sample Comments:

66	SMALL PATCH	L	1.00	Slabs
74	JOINT SPALL	M	1.00	Slabs

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: AP MAIN **Name:** MAIN APRON **Use:** APRON **Area:** 1,197,983 SqFt

Section: 4165 of 23 **From:** - **To:** - **Last Const.:** 1/1/2012

Surface: PCC **Family:** CA653-PR-AP-PCC **Zone:** **Category:** **Rank:** P

Area: 66,649 SqFt **Length:** 800 Ft **Width:** 300 Ft

Slabs: 206 **Slab Length:** 18 Ft **Slab Width:** 18 Ft **Joint Length:** 25,567 Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1955	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1990	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/2/1990	**Work Type:** Surface Treatment - Seal Coat	**Code:** ST-SC	**Is Major M&R:** False
Work Date: 1/1/2003	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 1/1/2012	**Work Type:** Complete Reconstruction - PCC	**Code:** CR-PC	**Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 14 **Surveyed:** 2

Conditions: **PCI:** 95

Inspection Comments:

Sample Number: 660 **Type:** R **Area:** 22.00 Slabs **PCI:** 95

Sample Comments:

63	LINEAR CR	L	1.00	Slabs
73	SHRINKAGE CR	N	1.00	Slabs

Sample Number: 664 **Type:** R **Area:** 16.00 Slabs **PCI:** 97

Sample Comments:

73	SHRINKAGE CR	N	1.00	Slabs
74	JOINT SPALL	L	1.00	Slabs

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: AP MAIN **Name:** MAIN APRON **Use:** APRON **Area:** 1,197,983 SqFt

Section: 4170 of 23 **From:** - **To:** - **Last Const.:** 8/1/2016

Surface: AAC **Family:** CA653-PR-AP-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 18,816 SqFt **Length:** 170 Ft **Width:** 90 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1979	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1990	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/2/1990	**Work Type:** Surface Treatment - Seal Coat	**Code:** ST-SC	**Is Major M&R:** False
Work Date: 1/1/2003	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 8/1/2016	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 4 **Surveyed:** 1

Conditions: **PCI:** 87

Inspection Comments:

Sample Number: 306 **Type:** R **Area:** 5300.00 SqFt **PCI:** 87

Sample Comments:

48	L & T CR	L	118.00	Ft
57	WEATHERING	L	5300.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: AP MAIN **Name:** MAIN APRON **Use:** APRON **Area:** 1,197,983 SqFt

Section: 4175 of 23 **From:** - **To:** - **Last Const.:** 1/1/1942

Surface: PCC **Family:** CA653-PR-AP-PCC **Zone:** **Category:** **Rank:** P

Area: 14,910 SqFt **Length:** 189 Ft **Width:** 75 Ft

Slabs: 50 **Slab Length:** 25 Ft **Slab Width:** 12 Ft **Joint Length:** 1,484 Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1942 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 2 **Surveyed:** 1

Conditions: **PCI:** 5

Inspection Comments:

Sample Number: 603 **Type:** R **Area:** 24.00 Slabs **PCI:** 5

Sample Comments:

62	CORNER BREAK	L	2.00	Slabs
63	LINEAR CR	M	17.00	Slabs
65	JT SEAL DMG	H	24.00	Slabs
72	SHAT. SLAB	L	1.00	Slabs
72	SHAT. SLAB	M	5.00	Slabs
73	SHRINKAGE CR	N	15.00	Slabs
74	JOINT SPALL	M	5.00	Slabs
74	JOINT SPALL	H	3.00	Slabs
75	CORNER SPALL	L	2.00	Slabs
75	CORNER SPALL	M	1.00	Slabs
75	CORNER SPALL	H	2.00	Slabs

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: AP MAIN **Name:** MAIN APRON **Use:** APRON **Area:** 1,197,983 SqFt

Section: 4176 of 23 **From:** - **To:** - **Last Const.:** 12/25/1955

Surface: AC **Family:** CA653-PR-AP-AC **Zone:** **Category:** **Rank:** P

Area: 3,573 SqFt **Length:** 75 Ft **Width:** 48 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 12/25/1955 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 1 **Surveyed:** 1

Conditions: **PCI:** 28

Inspection Comments:

Sample Number: 803 **Type:** R **Area:** 3573.00 SqFt **PCI:** 28

Sample Comments:

41	ALLIGATOR CR	L	16.00	SqFt
43	BLOCK CR	L	75.00	SqFt
43	BLOCK CR	M	2375.00	SqFt
45	DEPRESSION	L	120.00	SqFt
48	L & T CR	L	45.00	Ft
50	PATCHING	M	55.00	SqFt
50	PATCHING	H	18.00	SqFt
52	RAVELING	L	3500.00	SqFt
53	RUTTING	L	30.00	SqFt
56	SWELLING	M	6.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: AP MAIN **Name:** MAIN APRON **Use:** APRON **Area:** 1,197,983 SqFt

Section: 4177 of 23 **From:** - **To:** - **Last Const.:** 8/1/2016

Surface: APC **Family:** CA653-PR-AP-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 20,899 SqFt **Length:** 145 Ft **Width:** 123 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1978	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1990	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1990	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 8/1/2016	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 4 **Surveyed:** 1

Conditions: **PCI:** 80

Inspection Comments:

Sample Number: 304 **Type:** R **Area:** 6704.00 SqFt **PCI:** 80

Sample Comments:

45	DEPRESSION	L	54.00	SqFt
48	L & T CR	L	123.00	Ft
57	WEATHERING	L	6369.00	SqFt
57	WEATHERING	M	335.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: AP MAIN **Name:** MAIN APRON **Use:** APRON **Area:** 1,197,983 SqFt

Section: 4178 of 23 **From:** - **To:** - **Last Const.:** 1/1/2013

Surface: APC **Family:** CA653-PR-AP-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 59,522 SqFt **Length:** 240 Ft **Width:** 240 Ft

Slabs: 198 **Slab Length:** 12 Ft **Slab Width:** 25 Ft **Joint Length:** 6,624 Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1942	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2013	**Work Type:** Overlay - AC Structural	**Code:** OL-AS	**Is Major M&R:** True
Work Date: 1/1/2014	**Work Type:** Surface Treatment - Seal Coat	**Code:** ST-SC	**Is Major M&R:** False
Work Date: 11/1/2019	**Work Type:** Surface Treatment - Seal Coat	**Code:** ST-SC	**Is Major M&R:** False

Last Insp. Date: 4/4/2022 **TotalSamples:** 11 **Surveyed:** 2

Conditions: **PCI:** 62

Inspection Comments:

Sample Number: 402 **Type:** R **Area:** 5580.00 SqFt **PCI:** 62

Sample Comments:

47	JT REF. CR	L	537.00	Ft
47	JT REF. CR	M	100.00	Ft
48	L & T CR	L	282.00	Ft
48	L & T CR	M	25.00	Ft
56	SWELLING	L	50.00	SqFt
57	WEATHERING	L	5580.00	SqFt

Sample Number: 601 **Type:** R **Area:** 5000.00 SqFt **PCI:** 61

Sample Comments:

47	JT REF. CR	L	600.00	Ft
47	JT REF. CR	M	100.00	Ft
48	L & T CR	L	304.00	Ft
48	L & T CR	M	100.00	Ft
56	SWELLING	L	25.00	SqFt
57	WEATHERING	L	5000.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: AP MAIN **Name:** MAIN APRON **Use:** APRON **Area:** 1,197,983 SqFt

Section: 4179 of 23 **From:** - **To:** - **Last Const.:** 10/1/2011

Surface: APC **Family:** CA653-PR-AP-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 77,111 SqFt **Length:** 350 Ft **Width:** 306 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1942	**Work Type:** New Construction - PCC	**Code:** NC-PC	**Is Major M&R:** True
Work Date: 10/1/2011	**Work Type:** Overlay - AC Structural	**Code:** OL-AS	**Is Major M&R:** True
Work Date: 1/1/2014	**Work Type:** Surface Treatment - Seal Coat	**Code:** ST-SC	**Is Major M&R:** False
Work Date: 11/1/2019	**Work Type:** Surface Treatment - Seal Coat	**Code:** ST-SC	**Is Major M&R:** False

Last Insp. Date: 4/4/2022 **TotalSamples:** 15 **Surveyed:** 2

Conditions: **PCI:** 66

Inspection Comments:

Sample Number: 310 **Type:** R **Area:** 5630.00 SqFt **PCI:** 69

Sample Comments:

47	JT REF. CR	L	426.00	Ft
47	JT REF. CR	M	50.00	Ft
48	L & T CR	L	34.00	Ft
48	L & T CR	M	25.00	Ft
56	SWELLING	L	50.00	SqFt
57	WEATHERING	L	5630.00	SqFt

Sample Number: 408 **Type:** R **Area:** 6599.00 SqFt **PCI:** 63

Sample Comments:

47	JT REF. CR	L	558.00	Ft
47	JT REF. CR	M	50.00	Ft
48	L & T CR	L	77.00	Ft
48	L & T CR	M	50.00	Ft
56	SWELLING	L	50.00	SqFt
57	WEATHERING	L	6599.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: AP MAIN **Name:** MAIN APRON **Use:** APRON **Area:** 1,197,983 SqFt

Section: 4180 of 23 **From:** - **To:** - **Last Const.:** 1/2/1968

Surface: AAC **Family:** CA653-PR-AP-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 126,695 SqFt **Length:** 625 Ft **Width:** 197 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1968	**Work Type:** New Construction - AC	**Code:** NC-AC	**Is Major M&R:** True
Work Date: 1/2/1968	**Work Type:** Overlay - AC Structural	**Code:** OL-AS	**Is Major M&R:** True
Work Date: 1/1/2012	**Work Type:** Surface Treatment - Seal Coat	**Code:** ST-SC	**Is Major M&R:** False
Work Date: 1/1/2014	**Work Type:** Surface Treatment - Seal Coat	**Code:** ST-SC	**Is Major M&R:** False
Work Date: 7/1/2019	**Work Type:** Surface Treatment - Seal Coat	**Code:** ST-SC	**Is Major M&R:** False

Last Insp. Date: 4/4/2022 **TotalSamples:** 25 **Surveyed:** 3

Conditions: **PCI:** 45

Inspection Comments:

Sample Number: 100 **Type:** R **Area:** 4388.00 SqFt **PCI:** 63

Sample Comments:

43	BLOCK CR	L	70.00	SqFt
45	DEPRESSION	L	1.00	SqFt
48	L & T CR	L	188.00	Ft
52	RAVELING	L	4388.00	SqFt

Sample Number: 205 **Type:** R **Area:** 6480.00 SqFt **PCI:** 46

Sample Comments:

41	ALLIGATOR CR	L	36.00	SqFt
43	BLOCK CR	L	252.00	SqFt
48	L & T CR	L	255.00	Ft
48	L & T CR	M	155.00	Ft
52	RAVELING	L	5832.00	SqFt
52	RAVELING	M	648.00	SqFt

Sample Number: 404 **Type:** R **Area:** 4700.00 SqFt **PCI:** 26

Sample Comments:

41	ALLIGATOR CR	L	48.00	SqFt
41	ALLIGATOR CR	M	13.00	SqFt
43	BLOCK CR	L	217.00	SqFt
43	BLOCK CR	M	40.00	SqFt
45	DEPRESSION	L	36.00	SqFt
48	L & T CR	L	42.00	Ft
48	L & T CR	M	321.00	Ft
50	PATCHING	L	438.00	SqFt
50	PATCHING	M	523.00	SqFt
52	RAVELING	L	3363.00	SqFt
52	RAVELING	M	374.00	SqFt
52	RAVELING	H	2.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: AP MAIN **Name:** MAIN APRON **Use:** APRON **Area:** 1,197,983 SqFt

Section: 4183 of 23 **From:** - **To:** - **Last Const.:** 1/1/2013

Surface: AAC **Family:** CA653-PR-AP-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 39,947 SqFt **Length:** 100 Ft **Width:** 308 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1968	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/2/1968	**Work Type:** Surface Treatment - Seal Coat	**Code:** ST-SC	**Is Major M&R:** False
Work Date: 1/1/2013	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 1/1/2014	**Work Type:** Surface Treatment - Seal Coat	**Code:** ST-SC	**Is Major M&R:** False
Work Date: 11/1/2019	**Work Type:** Surface Treatment - Seal Coat	**Code:** ST-SC	**Is Major M&R:** False

Last Insp. Date: 4/4/2022 **TotalSamples:** 8 **Surveyed:** 1

Conditions: **PCI:** 68

Inspection Comments:

Sample Number: 108 **Type:** R **Area:** 6880.00 SqFt **PCI:** 68

Sample Comments:

48	L & T CR	L	402.00	Ft
48	L & T CR	M	55.00	Ft
56	SWELLING	L	20.00	SqFt
57	WEATHERING	M	6880.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: AP MAIN **Name:** MAIN APRON **Use:** APRON **Area:** 1,197,983 SqFt

Section: 4185 of 23 **From:** - **To:** - **Last Const.:** 1/1/2013

Surface: APC **Family:** CA653-PR-AP-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 12,820 SqFt **Length:** 126 Ft **Width:** 55 Ft

Slabs: 43 **Slab Length:** 12 Ft **Slab Width:** 25 Ft **Joint Length:** 674 Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1942	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2013	**Work Type:** Overlay - AC Structural	**Code:** OL-AS	**Is Major M&R:** True
Work Date: 1/1/2014	**Work Type:** Surface Treatment - Seal Coat	**Code:** ST-SC	**Is Major M&R:** False
Work Date: 11/1/2019	**Work Type:** Surface Treatment - Seal Coat	**Code:** ST-SC	**Is Major M&R:** False

Last Insp. Date: 4/4/2022 **TotalSamples:** 3 **Surveyed:** 1

Conditions: **PCI:** 51

Inspection Comments:

Sample Number: 506 **Type:** R **Area:** 4506.00 SqFt **PCI:** 51

Sample Comments:

43	BLOCK CR	L	641.00	SqFt
47	JT REF. CR	L	385.00	Ft
47	JT REF. CR	M	75.00	Ft
48	L & T CR	L	248.00	Ft
48	L & T CR	M	50.00	Ft
50	PATCHING	L	232.00	SqFt
57	WEATHERING	L	4274.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: AP MAIN **Name:** MAIN APRON **Use:** APRON **Area:** 1,197,983 SqFt

Section: 4190 of 23 **From:** - **To:** - **Last Const.:** 1/1/1942

Surface: PCC **Family:** CA653-PR-AP-PCC **Zone:** **Category:** **Rank:** P

Area: 18,650 SqFt **Length:** 250 Ft **Width:** 77 Ft

Slabs: 62 **Slab Length:** 25 Ft **Slab Width:** 12 Ft **Joint Length:** 2,047 Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1942 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 3 **Surveyed:** 1

Conditions: **PCI:** 14

Inspection Comments:

Sample Number: 552 **Type:** R **Area:** 23.00 Slabs **PCI:** 14

Sample Comments:

63	LINEAR CR	L	4.00	Slabs
63	LINEAR CR	M	16.00	Slabs
63	LINEAR CR	H	3.00	Slabs
65	JT SEAL DMG	H	23.00	Slabs
71	FAULTING	L	1.00	Slabs
73	SHRINKAGE CR	N	11.00	Slabs
74	JOINT SPALL	M	2.00	Slabs
74	JOINT SPALL	H	1.00	Slabs

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: AP MAIN **Name:** MAIN APRON **Use:** APRON **Area:** 1,197,983 SqFt

Section: 4195 of 23 **From:** - **To:** - **Last Const.:** 1/1/1942

Surface: PCC **Family:** CA653-PR-AP-PCC **Zone:** **Category:** **Rank:** P

Area: 11,250 SqFt **Length:** 250 Ft **Width:** 45 Ft

Slabs: 46 **Slab Length:** 22 Ft **Slab Width:** 11 Ft **Joint Length:** 1,239 Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1942 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 2 **Surveyed:** 1

Conditions: **PCI:** 9

Inspection Comments:

Sample Number: 600 **Type:** R **Area:** 20.00 Slabs **PCI:** 9

Sample Comments:

62	CORNER BREAK	M	2.00	Slabs
63	LINEAR CR	M	4.00	Slabs
65	JT SEAL DMG	H	20.00	Slabs
66	SMALL PATCH	M	3.00	Slabs
71	FAULTING	L	1.00	Slabs
72	SHAT. SLAB	M	16.00	Slabs

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: AP MAIN **Name:** MAIN APRON **Use:** APRON **Area:** 1,197,983 SqFt

Section: 4198 of 23 **From:** - **To:** - **Last Const.:** 1/1/2003

Surface: PCC **Family:** CA653-PR-AP-PCC **Zone:** **Category:** **Rank:** P

Area: 18,579 SqFt **Length:** 270 Ft **Width:** 70 Ft

Slabs: 45 **Slab Length:** 23 Ft **Slab Width:** 18 Ft **Joint Length:** 1,532 Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/2003 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 3 **Surveyed:** 1

Conditions: **PCI:** 20

Inspection Comments:

Sample Number: 107 **Type:** R **Area:** 16.00 Slabs **PCI:** 20

Sample Comments:

63	LINEAR CR	L	5.00	Slabs
72	SHAT. SLAB	L	8.00	Slabs
72	SHAT. SLAB	M	3.00	Slabs
73	SHRINKAGE CR	N	12.00	Slabs
74	JOINT SPALL	L	1.00	Slabs
75	CORNER SPALL	L	1.00	Slabs

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: AP MAIN **Name:** MAIN APRON **Use:** APRON **Area:** 1,197,983 SqFt

Section: 4199 of 23 **From:** - **To:** - **Last Const.:** 1/1/2023

Surface: PCC **Family:** CA653-PR-AP-PCC **Zone:** **Category:** **Rank:** P

Area: 25,200 SqFt **Length:** 360 Ft **Width:** 80 Ft

Slabs: 61 **Slab Length:** 23 Ft **Slab Width:** 18 Ft **Joint Length:** 2,412 Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/2003 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Work Date: 1/1/2023 **Work Type:** Complete Reconstruction - PCC **Code:** CR-PC **Is Major M&R:** True

Last Insp. Date: 12/10/2018 **TotalSamples:** 3 **Surveyed:** 1

Conditions: **PCI:** 75 **NOTE: *** Pre-Construction PCI *****

Inspection Comments:

Sample Number: 097 **Type:** R **Area:** 27.00 Slabs **PCI:** 75

Sample Comments:

63	LINEAR CR	L	2.00	Slabs
65	JT SEAL DMG	L	27.00	Slabs
72	SHAT. SLAB	L	1.00	Slabs
73	SHRINKAGE CR	N	24.00	Slabs

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: FBO CONN **Name:** FBO CONNECTOR **Use:** TAXIWAY **Area:** 39,789 SqFt

Section: 107 of 8 **From:** - **To:** - **Last Const.:** 1/1/1990

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 3,297 SqFt **Length:** 75 Ft **Width:** 32 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1968	**Work Type:** New Construction - AC	**Code:** NC-AC	**Is Major M&R:** True
Work Date: 1/1/1990	**Work Type:** Overlay - AC Structural	**Code:** OL-AS	**Is Major M&R:** True
Work Date: 11/1/2021	**Work Type:** Surface Treatment - Seal Coat	**Code:** ST-SC	**Is Major M&R:** False

Last Insp. Date: 4/4/2022 **TotalSamples:** 1 **Surveyed:** 1

Conditions: **PCI:** 45

Inspection Comments:

Sample Number: 100 **Type:** R **Area:** 3297.00 SqFt **PCI:** 45

Sample Comments:

41	ALLIGATOR CR	L	17.00	SqFt
45	DEPRESSION	L	30.00	SqFt
48	L & T CR	L	78.00	Ft
48	L & T CR	M	75.00	Ft
50	PATCHING	L	2108.00	SqFt
57	WEATHERING	L	594.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: FBO CONN **Name:** FBO CONNECTOR **Use:** TAXIWAY **Area:** 39,789 SqFt

Section: 108 of 8 **From:** - **To:** - **Last Const.:** 1/1/1994

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 3,361 SqFt **Length:** 50 Ft **Width:** 42 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1994 **Work Type:** New Construction - AC **Code:** NC-AC **Is Major M&R:** True

Work Date: 11/1/2021 **Work Type:** Surface Treatment - Seal Coat **Code:** ST-SC **Is Major M&R:** False

Last Insp. Date: 4/4/2022 **TotalSamples:** 1 **Surveyed:** 1

Conditions: **PCI:** 32

Inspection Comments:

Sample Number: 101 **Type:** R **Area:** 3361.00 SqFt **PCI:** 32

Sample Comments:

41	ALLIGATOR CR	L	36.00	SqFt
43	BLOCK CR	L	1662.00	SqFt
43	BLOCK CR	M	1663.00	SqFt
52	RAVELING	L	3025.00	SqFt
52	RAVELING	M	336.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: FBO CONN **Name:** FBO CONNECTOR **Use:** TAXIWAY **Area:** 39,789 SqFt

Section: 112 of 8 **From:** - **To:** - **Last Const.:** 1/1/1990

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 4,221 SqFt **Length:** 87 Ft **Width:** 47 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1968	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1990	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 11/1/2021	**Work Type:** Surface Treatment - Seal Coat	**Code:** ST-SC	**Is Major M&R:** False

Last Insp. Date: 4/4/2022 **TotalSamples:** 1 **Surveyed:** 1

Conditions: **PCI:** 42

Inspection Comments:

Sample Number: 100 **Type:** R **Area:** 4221.00 SqFt **PCI:** 42

Sample Comments:

41	ALLIGATOR CR	L	136.00	SqFt
48	L & T CR	L	100.00	Ft
48	L & T CR	M	139.00	Ft
50	PATCHING	L	1320.00	SqFt
53	RUTTING	L	40.00	SqFt
57	WEATHERING	L	2901.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: FBO CONN **Name:** FBO CONNECTOR **Use:** TAXIWAY **Area:** 39,789 SqFt

Section: 114 of 8 **From:** - **To:** - **Last Const.:** 1/1/1968

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 2,361 SqFt **Length:** 45 Ft **Width:** 43 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1968 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Work Date: 11/1/2021 **Work Type:** Surface Treatment - Seal Coat **Code:** ST-SC **Is Major M&R:** False

Last Insp. Date: 4/4/2022 **TotalSamples:** 1 **Surveyed:** 1

Conditions: **PCI:** 59

Inspection Comments:

Sample Number: 101 **Type:** R **Area:** 2361.00 SqFt **PCI:** 59

Sample Comments:

45	DEPRESSION	L	75.00	SqFt
48	L & T CR	L	93.00	Ft
48	L & T CR	M	25.00	Ft
52	RAVELING	L	2361.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: FBO CONN **Name:** FBO CONNECTOR **Use:** TAXIWAY **Area:** 39,789 SqFt

Section: 117 of 8 **From:** - **To:** - **Last Const.:** 8/1/2016

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 6,019 SqFt **Length:** 137 Ft **Width:** 68 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1968	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1990	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 8/1/2016	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 11/1/2021	**Work Type:** Surface Treatment - Seal Coat	**Code:** ST-SC	**Is Major M&R:** False

Last Insp. Date: 4/4/2022 **TotalSamples:** 1 **Surveyed:** 1

Conditions: **PCI:** 77

Inspection Comments:

Sample Number: 100 **Type:** R **Area:** 6019.00 SqFt **PCI:** 77

Sample Comments:

48	L & T CR	L	32.00	Ft
50	PATCHING	L	975.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: FBO CONN **Name:** FBO CONNECTOR **Use:** TAXIWAY **Area:** 39,789 SqFt

Section: 119 of 8 **From:** - **To:** - **Last Const.:** 1/1/1968

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 3,041 SqFt **Length:** 68 Ft **Width:** 45 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1968 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Work Date: 11/1/2021 **Work Type:** Surface Treatment - Seal Coat **Code:** ST-SC **Is Major M&R:** False

Last Insp. Date: 4/4/2022 **TotalSamples:** 1 **Surveyed:** 1

Conditions: **PCI:** 29

Inspection Comments:

Sample Number: 101 **Type:** R **Area:** 3041.00 SqFt **PCI:** 29

Sample Comments:

41	ALLIGATOR CR	L	238.00	SqFt
41	ALLIGATOR CR	M	12.00	SqFt
45	DEPRESSION	L	60.00	SqFt
48	L & T CR	L	149.00	Ft
50	PATCHING	L	374.00	SqFt
52	RAVELING	L	2667.00	SqFt
53	RUTTING	L	228.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: FBO CONN **Name:** FBO CONNECTOR **Use:** TAXIWAY **Area:** 39,789 SqFt

Section: 125 of 8 **From:** - **To:** - **Last Const.:** 8/1/2016

Surface: APC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 4,598 SqFt **Length:** 44 Ft **Width:** 125 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1942	**Work Type:** New Construction - PCC	**Code:** NC-PC	**Is Major M&R:** True
Work Date: 10/1/2011	**Work Type:** Overlay - AC Structural	**Code:** OL-AS	**Is Major M&R:** True
Work Date: 8/1/2016	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 1 **Surveyed:** 1

Conditions: **PCI:** 58

Inspection Comments:

Sample Number: 202 **Type:** R **Area:** 4598.00 SqFt **PCI:** 58

Sample Comments:

45	DEPRESSION	L	135.00	SqFt
48	L & T CR	L	26.00	Ft
50	PATCHING	L	1134.00	SqFt
57	WEATHERING	L	3004.00	SqFt
57	WEATHERING	M	460.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: FBO CONN **Name:** FBO CONNECTOR **Use:** TAXIWAY **Area:** 39,789 SqFt

Section: 127 of 8 **From:** - **To:** - **Last Const.:** 8/1/2016

Surface: APC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 12,891 SqFt **Length:** 53 Ft **Width:** 125 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1978	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1990	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1990	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 8/1/2016	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 2 **Surveyed:** 1

Conditions: **PCI:** 82

Inspection Comments:

Sample Number: 201 **Type:** R **Area:** 6446.00 SqFt **PCI:** 82

Sample Comments:

45	DEPRESSION	L	24.00	SqFt
48	L & T CR	L	49.00	Ft
57	WEATHERING	L	5801.00	SqFt
57	WEATHERING	M	645.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: RW 18-36 **Name:** RUNWAY 18-36 **Use:** RUNWAY **Area:** 1,459,350 SqFt

Section: 6115 of 13 **From:** - **To:** - **Last Const.:** 12/1/2020

Surface: AC **Family:** CA653-PR-RW-AC **Zone:** **Category:** **Rank:** P

Area: 135,960 SqFt **Length:** 2,266 Ft **Width:** 60 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1988 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Work Date: 1/1/2003 **Work Type:** Mill and Overlay **Code:** ML-OVL **Is Major M&R:** True

Work Date: 12/1/2020 **Work Type:** Complete Reconstruction - AC **Code:** CR-AC **Is Major M&R:** True

Last Insp. Date: 12/10/2018 **TotalSamples:** 10 **Surveyed:** 10

Conditions: **PCI:** 53 **NOTE: *** Pre-Construction PCI *****

Inspection Comments:

Sample Number: 275 **Type:** R **Area:** 5000.00 SqFt **PCI:** 43

Sample Comments:

42	BLEEDING	N	831.00	SqFt
48	L & T CR	L	95.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 277 **Type:** R **Area:** 5000.00 SqFt **PCI:** 30

Sample Comments:

42	BLEEDING	N	2750.00	SqFt
45	DEPRESSION	L	9.00	SqFt
48	L & T CR	L	18.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 286 **Type:** R **Area:** 5000.00 SqFt **PCI:** 42

Sample Comments:

42	BLEEDING	N	650.00	SqFt
48	L & T CR	L	99.00	Ft
53	RUTTING	L	400.00	SqFt
57	WEATHERING	L	5000.00	SqFt

Sample Number: 294 **Type:** R **Area:** 5000.00 SqFt **PCI:** 82

Sample Comments:

48	L & T CR	L	213.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 299 **Type:** R **Area:** 5000.00 SqFt **PCI:** 76

Sample Comments:

48	L & T CR	L	226.00	Ft
52	RAVELING	L	651.00	SqFt
57	WEATHERING	L	4349.00	SqFt

Sample Number: 302 **Type:** R **Area:** 5000.00 SqFt **PCI:** 43

Sample Comments:

41	ALLIGATOR CR	L	136.00	SqFt
48	L & T CR	L	356.00	Ft
48	L & T CR	M	25.00	Ft
50	PATCHING	L	324.00	SqFt
52	RAVELING	L	4616.00	SqFt
52	RAVELING	M	60.00	SqFt

Sample Number: 306 **Type:** R **Area:** 5000.00 SqFt **PCI:** 53

Sample Comments:

41	ALLIGATOR CR	L	28.00	SqFt
48	L & T CR	L	400.00	Ft

52	RAVELING	L	2965.00	SqFt
52	RAVELING	M	35.00	SqFt
55	SLIPPAGE CR	N	128.00	SqFt

Sample Number: 310 **Type:** R **Area:** 5000.00 SqFt **PCI:** 58

Sample Comments:

41	ALLIGATOR CR	L	38.00	SqFt
48	L & T CR	L	456.00	Ft
52	RAVELING	L	2979.00	SqFt
52	RAVELING	M	35.00	SqFt
56	SWELLING	L	32.00	SqFt

Sample Number: 316 **Type:** R **Area:** 5000.00 SqFt **PCI:** 52

Sample Comments:

41	ALLIGATOR CR	L	109.00	SqFt
48	L & T CR	L	332.00	Ft
48	L & T CR	M	35.00	Ft
52	RAVELING	L	979.00	SqFt
52	RAVELING	M	105.00	SqFt

Sample Number: 319 **Type:** R **Area:** 5000.00 SqFt **PCI:** 50

Sample Comments:

41	ALLIGATOR CR	L	86.00	SqFt
48	L & T CR	L	335.00	Ft
50	PATCHING	L	1500.00	SqFt
52	RAVELING	L	680.00	SqFt
52	RAVELING	M	100.00	SqFt
56	SWELLING	L	7.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: RW 18-36 **Name:** RUNWAY 18-36 **Use:** RUNWAY **Area:** 1,459,350 SqFt

Section: 6120 of 13 **From:** - **To:** - **Last Const.:** 12/1/2020

Surface: AAC **Family:** CA653-PR-RW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 176,940 SqFt **Length:** 1,966 Ft **Width:** 90 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1988	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2003	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 12/1/2020	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 12/10/2018 **TotalSamples:** 6 **Surveyed:** 5

Conditions: **PCI:** 65 **NOTE: *** Pre-Construction PCI *****

Inspection Comments:

Sample Number: 104 **Type:** R **Area:** 3750.00 SqFt **PCI:** 64

Sample Comments:

42	BLEEDING	N	30.00	SqFt
48	L & T CR	L	216.00	Ft
52	RAVELING	L	3750.00	SqFt

Sample Number: 110 **Type:** R **Area:** 5000.00 SqFt **PCI:** 59

Sample Comments:

42	BLEEDING	N	6.00	SqFt
48	L & T CR	L	463.00	Ft
48	L & T CR	M	227.00	Ft
52	RAVELING	L	500.00	SqFt
56	SWELLING	L	25.00	SqFt
57	WEATHERING	L	4500.00	SqFt

Sample Number: 488 **Type:** R **Area:** 5000.00 SqFt **PCI:** 74

Sample Comments:

48	L & T CR	L	230.00	Ft
50	PATCHING	L	4.00	SqFt
52	RAVELING	L	250.00	SqFt
57	WEATHERING	L	4746.00	SqFt

Sample Number: 500 **Type:** R **Area:** 5000.00 SqFt **PCI:** 69

Sample Comments:

48	L & T CR	L	327.00	Ft
52	RAVELING	L	5000.00	SqFt

Sample Number: 516 **Type:** R **Area:** 3750.00 SqFt **PCI:** 59

Sample Comments:

48	L & T CR	L	546.00	Ft
52	RAVELING	L	500.00	SqFt
56	SWELLING	L	35.00	SqFt
57	WEATHERING	L	3250.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: RW 18-36 **Name:** RUNWAY 18-36 **Use:** RUNWAY **Area:** 1,459,350 SqFt

Section: 6155 of 13 **From:** - **To:** - **Last Const.:** 12/1/2020

Surface: AAC **Family:** CA653-PR-RW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 99,000 SqFt **Length:** 1,650 Ft **Width:** 60 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1958	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1977	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1988	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1988	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2003	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 12/1/2020	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 12/10/2018 **TotalSamples:** 36 **Surveyed:** 7

Conditions: **PCI:** 49 **NOTE: *** Pre-Construction PCI *****

Inspection Comments:

Sample Number: 323 **Type:** R **Area:** 5000.00 SqFt **PCI:** 54

Sample Comments:

48	L & T CR	L	316.00	Ft
50	PATCHING	L	2500.00	SqFt
52	RAVELING	L	375.00	SqFt
56	SWELLING	L	22.00	SqFt
57	WEATHERING	L	2125.00	SqFt

Sample Number: 326 **Type:** R **Area:** 5000.00 SqFt **PCI:** 47

Sample Comments:

41	ALLIGATOR CR	L	72.00	SqFt
48	L & T CR	L	440.00	Ft
48	L & T CR	M	59.00	Ft
50	PATCHING	L	204.00	SqFt
52	RAVELING	L	4796.00	SqFt
56	SWELLING	L	32.00	SqFt

Sample Number: 329 **Type:** R **Area:** 5000.00 SqFt **PCI:** 52

Sample Comments:

41	ALLIGATOR CR	L	70.00	SqFt
48	L & T CR	L	451.00	Ft
48	L & T CR	M	50.00	Ft
52	RAVELING	L	2500.00	SqFt
56	SWELLING	L	15.00	SqFt
57	WEATHERING	L	2500.00	SqFt

Sample Number: 337 **Type:** R **Area:** 5000.00 SqFt **PCI:** 38

Sample Comments:

41	ALLIGATOR CR	L	282.00	SqFt
48	L & T CR	L	310.00	Ft
48	L & T CR	M	60.00	Ft
52	RAVELING	L	1493.00	SqFt
52	RAVELING	H	24.00	SqFt
55	SLIPPAGE CR	N	74.00	SqFt
56	SWELLING	L	50.00	SqFt

Sample Number: 343 **Type:** R **Area:** 5000.00 SqFt **PCI:** 47

Sample Comments:

41	ALLIGATOR CR	L	126.00	SqFt
48	L & T CR	L	500.00	Ft
48	L & T CR	M	23.00	Ft
52	RAVELING	L	1425.00	SqFt
52	RAVELING	M	250.00	SqFt
56	SWELLING	L	70.00	SqFt

Sample Number: 350 **Type:** R **Area:** 5000.00 SqFt **PCI:** 51

Sample Comments:

41	ALLIGATOR CR	L	83.00	SqFt
48	L & T CR	L	376.00	Ft
48	L & T CR	M	97.00	Ft
52	RAVELING	L	1466.00	SqFt
52	RAVELING	M	115.00	SqFt
56	SWELLING	L	70.00	SqFt

Sample Number: 355 **Type:** R **Area:** 5000.00 SqFt **PCI:** 57

Sample Comments:

41	ALLIGATOR CR	L	40.00	SqFt
48	L & T CR	L	398.00	Ft
48	L & T CR	M	20.00	Ft
52	RAVELING	L	1900.00	SqFt
52	RAVELING	M	250.00	SqFt
56	SWELLING	L	44.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: RW 18-36 **Name:** RUNWAY 18-36 **Use:** RUNWAY **Area:** 1,459,350 SqFt

Section: 6156 of 13 **From:** - **To:** - **Last Const.:** 12/1/2020

Surface: AC **Family:** CA653-PR-RW-AC **Zone:** **Category:** **Rank:** P

Area: 18,000 SqFt **Length:** 200 Ft **Width:** 90 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1958	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1977	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1988	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1988	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2003	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 12/1/2020	**Work Type:** Complete Reconstruction - AC	**Code:** CR-AC	**Is Major M&R:** True

Last Insp. Date: 12/10/2018 **TotalSamples:** 18 **Surveyed:** 5

Conditions: **PCI:** 70 **NOTE: *** Pre-Construction PCI ***

Inspection Comments:

Sample Number: 120 **Type:** R **Area:** 5000.00 SqFt **PCI:** 54

Sample Comments:

41	ALLIGATOR CR	L	44.00	SqFt
42	BLEEDING	N	45.00	SqFt
45	DEPRESSION	L	15.00	SqFt
48	L & T CR	L	562.00	Ft
52	RAVELING	L	494.00	SqFt
52	RAVELING	M	64.00	SqFt

Sample Number: 132 **Type:** R **Area:** 5000.00 SqFt **PCI:** 78

Sample Comments:

42	BLEEDING	N	2.00	SqFt
48	L & T CR	L	185.00	Ft
52	RAVELING	L	597.00	SqFt
57	WEATHERING	L	4403.00	SqFt

Sample Number: 152 **Type:** R **Area:** 5000.00 SqFt **PCI:** 71

Sample Comments:

48	L & T CR	L	231.00	Ft
48	L & T CR	M	25.00	Ft
52	RAVELING	L	500.00	SqFt
57	WEATHERING	L	4500.00	SqFt

Sample Number: 524 **Type:** R **Area:** 5000.00 SqFt **PCI:** 71

Sample Comments:

48	L & T CR	L	374.00	Ft
52	RAVELING	L	497.00	SqFt
52	RAVELING	M	28.00	SqFt

Sample Number: 544 **Type:** R **Area:** 5000.00 SqFt **PCI:** 79

Sample Comments:

48	L & T CR	L	125.00	Ft
52	RAVELING	L	500.00	SqFt
56	SWELLING	L	15.00	SqFt
57	WEATHERING	L	4500.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: RW 18-36 **Name:** RUNWAY 18-36 **Use:** RUNWAY **Area:** 1,459,350 SqFt

Section: 6157 of 13 **From:** - **To:** - **Last Const.:** 12/1/2020

Surface: AC **Family:** CA653-PR-RW-AC **Zone:** **Category:** **Rank:** P

Area: 12,000 SqFt **Length:** 200 Ft **Width:** 60 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1958	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1977	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1988	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1988	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2003	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 12/1/2020	**Work Type:** Complete Reconstruction - AC	**Code:** CR-AC	**Is Major M&R:** True

Last Insp. Date: 12/10/2018 **TotalSamples:** 36 **Surveyed:** 7

Conditions: **PCI:** 49 **NOTE: *** Pre-Construction PCI ***

Inspection Comments:

Sample Number: 323 **Type:** R **Area:** 5000.00 SqFt **PCI:** 54

Sample Comments:

48	L & T CR	L	316.00	Ft
50	PATCHING	L	2500.00	SqFt
52	RAVELING	L	375.00	SqFt
56	SWELLING	L	22.00	SqFt
57	WEATHERING	L	2125.00	SqFt

Sample Number: 326 **Type:** R **Area:** 5000.00 SqFt **PCI:** 47

Sample Comments:

41	ALLIGATOR CR	L	72.00	SqFt
48	L & T CR	L	440.00	Ft
48	L & T CR	M	59.00	Ft
50	PATCHING	L	204.00	SqFt
52	RAVELING	L	4796.00	SqFt
56	SWELLING	L	32.00	SqFt

Sample Number: 329 **Type:** R **Area:** 5000.00 SqFt **PCI:** 52

Sample Comments:

41	ALLIGATOR CR	L	70.00	SqFt
48	L & T CR	L	451.00	Ft
48	L & T CR	M	50.00	Ft
52	RAVELING	L	2500.00	SqFt
56	SWELLING	L	15.00	SqFt
57	WEATHERING	L	2500.00	SqFt

Sample Number: 337 **Type:** R **Area:** 5000.00 SqFt **PCI:** 38

Sample Comments:

41	ALLIGATOR CR	L	282.00	SqFt
48	L & T CR	L	310.00	Ft
48	L & T CR	M	60.00	Ft
52	RAVELING	L	1493.00	SqFt
52	RAVELING	H	24.00	SqFt
55	SLIPPAGE CR	N	74.00	SqFt
56	SWELLING	L	50.00	SqFt

Sample Number: 343 **Type:** R **Area:** 5000.00 SqFt **PCI:** 47

Sample Comments:

41	ALLIGATOR CR	L	126.00	SqFt

48	L & T CR	L	500.00	Ft
48	L & T CR	M	23.00	Ft
52	RAVELING	L	1425.00	SqFt
52	RAVELING	M	250.00	SqFt
56	SWELLING	L	70.00	SqFt

Sample Number: 350 **Type:** R **Area:** 5000.00 SqFt **PCI:** 51

Sample Comments:

41	ALLIGATOR CR	L	83.00	SqFt
48	L & T CR	L	376.00	Ft
48	L & T CR	M	97.00	Ft
52	RAVELING	L	1466.00	SqFt
52	RAVELING	M	115.00	SqFt
56	SWELLING	L	70.00	SqFt

Sample Number: 355 **Type:** R **Area:** 5000.00 SqFt **PCI:** 57

Sample Comments:

41	ALLIGATOR CR	L	40.00	SqFt
48	L & T CR	L	398.00	Ft
48	L & T CR	M	20.00	Ft
52	RAVELING	L	1900.00	SqFt
52	RAVELING	M	250.00	SqFt
56	SWELLING	L	44.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: RW 18-36 **Name:** RUNWAY 18-36 **Use:** RUNWAY **Area:** 1,459,350 SqFt

Section: 6160 of 13 **From:** - **To:** - **Last Const.:** 12/1/2020

Surface: AAC **Family:** CA653-PR-RW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 148,500 SqFt **Length:** 1,650 Ft **Width:** 90 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1958	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1977	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1988	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1988	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2003	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 12/1/2020	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 12/10/2018 **TotalSamples:** 18 **Surveyed:** 5

Conditions: **PCI:** 70 **NOTE: *** Pre-Construction PCI *****

Inspection Comments:

Sample Number: 120 **Type:** R **Area:** 5000.00 SqFt **PCI:** 54

Sample Comments:

41	ALLIGATOR CR	L	44.00	SqFt
42	BLEEDING	N	45.00	SqFt
45	DEPRESSION	L	15.00	SqFt
48	L & T CR	L	562.00	Ft
52	RAVELING	L	494.00	SqFt
52	RAVELING	M	64.00	SqFt

Sample Number: 132 **Type:** R **Area:** 5000.00 SqFt **PCI:** 78

Sample Comments:

42	BLEEDING	N	2.00	SqFt
48	L & T CR	L	185.00	Ft
52	RAVELING	L	597.00	SqFt
57	WEATHERING	L	4403.00	SqFt

Sample Number: 152 **Type:** R **Area:** 5000.00 SqFt **PCI:** 71

Sample Comments:

48	L & T CR	L	231.00	Ft
48	L & T CR	M	25.00	Ft
52	RAVELING	L	500.00	SqFt
57	WEATHERING	L	4500.00	SqFt

Sample Number: 524 **Type:** R **Area:** 5000.00 SqFt **PCI:** 71

Sample Comments:

48	L & T CR	L	374.00	Ft
52	RAVELING	L	497.00	SqFt
52	RAVELING	M	28.00	SqFt

Sample Number: 544 **Type:** R **Area:** 5000.00 SqFt **PCI:** 79

Sample Comments:

48	L & T CR	L	125.00	Ft
52	RAVELING	L	500.00	SqFt
56	SWELLING	L	15.00	SqFt
57	WEATHERING	L	4500.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: RW 18-36 **Name:** RUNWAY 18-36 **Use:** RUNWAY **Area:** 1,459,350 SqFt

Section: 6165 of 13 **From:** - **To:** - **Last Const.:** 12/1/2020

Surface: AC **Family:** CA653-PR-RW-AC **Zone:** **Category:** **Rank:** P

Area: 40,500 SqFt **Length:** 675 Ft **Width:** 60 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1958	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1977	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1988	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1988	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2003	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 12/1/2020	**Work Type:** Complete Reconstruction - AC	**Code:** CR-AC	**Is Major M&R:** True

Last Insp. Date: 12/10/2018 **TotalSamples:** 14 **Surveyed:** 3

Conditions: **PCI:** 49 **NOTE: *** Pre-Construction PCI *****

Inspection Comments:

Sample Number: 357 **Type:** R **Area:** 5000.00 SqFt **PCI:** 57

Sample Comments:

41	ALLIGATOR CR	L	78.00	SqFt
48	L & T CR	L	252.00	Ft
52	RAVELING	L	2000.00	SqFt
56	SWELLING	L	65.00	SqFt
57	WEATHERING	L	3000.00	SqFt

Sample Number: 362 **Type:** R **Area:** 5000.00 SqFt **PCI:** 46

Sample Comments:

41	ALLIGATOR CR	L	124.00	SqFt
48	L & T CR	L	314.00	Ft
48	L & T CR	M	100.00	Ft
52	RAVELING	L	1738.00	SqFt
52	RAVELING	M	35.00	SqFt
56	SWELLING	L	215.00	SqFt

Sample Number: 368 **Type:** R **Area:** 5000.00 SqFt **PCI:** 44

Sample Comments:

41	ALLIGATOR CR	L	101.00	SqFt
48	L & T CR	L	443.00	Ft
48	L & T CR	M	36.00	Ft
52	RAVELING	L	1473.00	SqFt
52	RAVELING	M	90.00	SqFt
56	SWELLING	L	65.00	SqFt
56	SWELLING	M	54.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: RW 18-36 **Name:** RUNWAY 18-36 **Use:** RUNWAY **Area:** 1,459,350 SqFt

Section: 6170 of 13 **From:** - **To:** - **Last Const.:** 12/1/2020

Surface: AAC **Family:** CA653-PR-RW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 60,750 SqFt **Length:** 675 Ft **Width:** 90 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1958	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1977	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1988	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1988	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2003	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 12/1/2020	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 12/10/2018 **TotalSamples:** 8 **Surveyed:** 2

Conditions: **PCI:** 66 **NOTE: *** Pre-Construction PCI *****

Inspection Comments:

Sample Number: 164 **Type:** R **Area:** 3750.00 SqFt **PCI:** 59

Sample Comments:

48	L & T CR	L	400.00	Ft
48	L & T CR	M	116.00	Ft
52	RAVELING	L	553.00	SqFt
52	RAVELING	M	66.00	SqFt
56	SWELLING	L	13.00	SqFt

Sample Number: 560 **Type:** R **Area:** 5000.00 SqFt **PCI:** 71

Sample Comments:

48	L & T CR	L	136.00	Ft
48	L & T CR	M	10.00	Ft
52	RAVELING	L	484.00	SqFt
56	SWELLING	L	23.00	SqFt
57	WEATHERING	L	4356.00	SqFt
57	WEATHERING	M	160.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: RW 18-36 **Name:** RUNWAY 18-36 **Use:** RUNWAY **Area:** 1,459,350 SqFt

Section: 6175 of 13 **From:** - **To:** - **Last Const.:** 12/1/2020

Surface: AAC **Family:** CA653-PR-RW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 170,280 SqFt **Length:** 2,838 Ft **Width:** 60 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1958	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1977	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1988	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1988	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2003	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 12/1/2020	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 12/10/2018 **TotalSamples:** 58 **Surveyed:** 12

Conditions: **PCI:** 51 **NOTE: *** Pre-Construction PCI *****

Inspection Comments:

Sample Number: 371 **Type:** R **Area:** 5000.00 SqFt **PCI:** 48

Sample Comments:

41	ALLIGATOR CR	L	95.00	SqFt
48	L & T CR	L	558.00	Ft
48	L & T CR	M	10.00	Ft
52	RAVELING	L	1425.00	SqFt
52	RAVELING	M	250.00	SqFt
56	SWELLING	L	124.00	SqFt

Sample Number: 373 **Type:** R **Area:** 5000.00 SqFt **PCI:** 52

Sample Comments:

41	ALLIGATOR CR	L	67.00	SqFt
48	L & T CR	L	322.00	Ft
48	L & T CR	M	12.00	Ft
52	RAVELING	L	1500.00	SqFt
56	SWELLING	L	113.00	SqFt
57	WEATHERING	L	3500.00	SqFt

Sample Number: 377 **Type:** R **Area:** 5000.00 SqFt **PCI:** 49

Sample Comments:

41	ALLIGATOR CR	L	83.00	SqFt
48	L & T CR	L	556.00	Ft
48	L & T CR	M	12.00	Ft
52	RAVELING	L	1500.00	SqFt
56	SWELLING	L	124.00	SqFt
57	WEATHERING	L	3500.00	SqFt

Sample Number: 382 **Type:** R **Area:** 5000.00 SqFt **PCI:** 51

Sample Comments:

41	ALLIGATOR CR	L	18.00	SqFt
48	L & T CR	L	536.00	Ft
48	L & T CR	M	23.00	Ft
52	RAVELING	L	1500.00	SqFt
56	SWELLING	L	130.00	SqFt
57	WEATHERING	L	3500.00	SqFt

Sample Number: 389 **Type:** R **Area:** 5000.00 SqFt **PCI:** 55

Sample Comments:

41	ALLIGATOR CR	L	47.00	SqFt
48	L & T CR	L	571.00	Ft
52	RAVELING	L	996.00	SqFt
52	RAVELING	M	20.00	SqFt
56	SWELLING	L	125.00	SqFt

Sample Number: 395 **Type:** R **Area:** 5000.00 SqFt **PCI:** 57

Sample Comments:

41	ALLIGATOR CR	L	67.00	SqFt
48	L & T CR	L	485.00	Ft
52	RAVELING	L	997.00	SqFt
52	RAVELING	M	15.00	SqFt
56	SWELLING	L	100.00	SqFt

Sample Number: 400 **Type:** R **Area:** 5000.00 SqFt **PCI:** 54

Sample Comments:

41	ALLIGATOR CR	L	97.00	SqFt
48	L & T CR	L	366.00	Ft
52	RAVELING	L	950.00	SqFt
52	RAVELING	M	250.00	SqFt
56	SWELLING	L	85.00	SqFt

Sample Number: 405 **Type:** R **Area:** 5000.00 SqFt **PCI:** 55

Sample Comments:

41	ALLIGATOR CR	L	57.00	SqFt
48	L & T CR	L	550.00	Ft
52	RAVELING	L	991.00	SqFt
52	RAVELING	M	45.00	SqFt
56	SWELLING	L	125.00	SqFt

Sample Number: 412 **Type:** R **Area:** 5000.00 SqFt **PCI:** 48

Sample Comments:

41	ALLIGATOR CR	L	43.00	SqFt
48	L & T CR	L	686.00	Ft
48	L & T CR	M	20.00	Ft
52	RAVELING	L	950.00	SqFt
52	RAVELING	M	250.00	SqFt
56	SWELLING	L	80.00	SqFt

Sample Number: 415 **Type:** R **Area:** 5000.00 SqFt **PCI:** 53

Sample Comments:

41	ALLIGATOR CR	L	69.00	SqFt
48	L & T CR	L	657.00	Ft
52	RAVELING	L	994.00	SqFt
52	RAVELING	M	30.00	SqFt
56	SWELLING	L	120.00	SqFt

Sample Number: 419 **Type:** R **Area:** 5000.00 SqFt **PCI:** 47

Sample Comments:

41	ALLIGATOR CR	L	137.00	SqFt
48	L & T CR	L	623.00	Ft
48	L & T CR	M	50.00	Ft
52	RAVELING	L	993.00	SqFt
52	RAVELING	M	35.00	SqFt
56	SWELLING	L	45.00	SqFt

Sample Number: 425 **Type:** R **Area:** 5000.00 SqFt **PCI:** 49

Sample Comments:

41	ALLIGATOR CR	L	52.00	SqFt
48	L & T CR	L	661.00	Ft
48	L & T CR	M	34.00	Ft
52	RAVELING	L	989.00	SqFt
52	RAVELING	M	55.00	SqFt
56	SWELLING	L	65.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: RW 18-36 **Name:** RUNWAY 18-36 **Use:** RUNWAY **Area:** 1,459,350 SqFt

Section: 6180 of 13 **From:** - **To:** - **Last Const.:** 12/1/2020

Surface: AAC **Family:** CA653-PR-RW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 255,420 SqFt **Length:** 2,838 Ft **Width:** 90 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1958	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1977	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1988	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1988	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2003	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 12/1/2020	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 12/10/2018 **TotalSamples:** 30 **Surveyed:** 5

Conditions: **PCI:** 70 **NOTE: *** Pre-Construction PCI ***

Inspection Comments:

Sample Number: 172 **Type:** R **Area:** 3750.00 SqFt **PCI:** 67

Sample Comments:

48	L & T CR	L	158.00	Ft
48	L & T CR	M	50.00	Ft
52	RAVELING	L	375.00	SqFt
52	RAVELING	H	2.00	SqFt
56	SWELLING	L	63.00	SqFt

Sample Number: 192 **Type:** R **Area:** 5000.00 SqFt **PCI:** 65

Sample Comments:

48	L & T CR	L	369.00	Ft
50	PATCHING	L	4.00	SqFt
52	RAVELING	L	500.00	SqFt
56	SWELLING	L	75.00	SqFt
57	WEATHERING	L	4496.00	SqFt

Sample Number: 208 **Type:** R **Area:** 5000.00 SqFt **PCI:** 64

Sample Comments:

48	L & T CR	L	299.00	Ft
48	L & T CR	M	97.00	Ft
52	RAVELING	L	500.00	SqFt
56	SWELLING	L	76.00	SqFt
57	WEATHERING	L	4500.00	SqFt

Sample Number: 588 **Type:** R **Area:** 5000.00 SqFt **PCI:** 73

Sample Comments:

48	L & T CR	L	180.00	Ft
52	RAVELING	L	500.00	SqFt
56	SWELLING	L	135.00	SqFt
57	WEATHERING	L	4500.00	SqFt

Sample Number: 612 **Type:** R **Area:** 5000.00 SqFt **PCI:** 79

Sample Comments:

48	L & T CR	L	46.00	Ft
52	RAVELING	L	500.00	SqFt
56	SWELLING	L	2.00	SqFt
57	WEATHERING	L	4500.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: RW 18-36 **Name:** RUNWAY 18-36 **Use:** RUNWAY **Area:** 1,459,350 SqFt

Section: 6185 of 13 **From:** - **To:** - **Last Const.:** 12/1/2020

Surface: AAC **Family:** CA653-PR-RW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 126,000 SqFt **Length:** 2,100 Ft **Width:** 60 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1960	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1977	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1988	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2003	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 12/1/2020	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 12/10/2018 **TotalSamples:** 42 **Surveyed:** 8

Conditions: **PCI:** 47 **NOTE: *** Pre-Construction PCI ***

Inspection Comments:

Sample Number: 430 **Type:** R **Area:** 5000.00 SqFt **PCI:** 50

Sample Comments:

41	ALLIGATOR CR	L	128.00	SqFt
48	L & T CR	L	587.00	Ft
52	RAVELING	L	1000.00	SqFt
56	SWELLING	L	115.00	SqFt
57	WEATHERING	L	4000.00	SqFt

Sample Number: 433 **Type:** R **Area:** 5000.00 SqFt **PCI:** 46

Sample Comments:

41	ALLIGATOR CR	L	197.00	SqFt
48	L & T CR	L	545.00	Ft
52	RAVELING	L	740.00	SqFt
52	RAVELING	M	65.00	SqFt
56	SWELLING	L	115.00	SqFt

Sample Number: 437 **Type:** R **Area:** 5000.00 SqFt **PCI:** 37

Sample Comments:

41	ALLIGATOR CR	L	300.00	SqFt
48	L & T CR	L	575.00	Ft
48	L & T CR	M	50.00	Ft
52	RAVELING	L	744.00	SqFt
52	RAVELING	M	40.00	SqFt
56	SWELLING	L	125.00	SqFt

Sample Number: 441 **Type:** R **Area:** 5000.00 SqFt **PCI:** 34

Sample Comments:

41	ALLIGATOR CR	L	635.00	SqFt
48	L & T CR	L	350.00	Ft
52	RAVELING	L	744.00	SqFt
52	RAVELING	M	40.00	SqFt
56	SWELLING	L	105.00	SqFt

Sample Number: 449 **Type:** R **Area:** 5000.00 SqFt **PCI:** 48

Sample Comments:

41	ALLIGATOR CR	L	150.00	SqFt
48	L & T CR	L	283.00	Ft
52	RAVELING	L	4945.00	SqFt
52	RAVELING	M	55.00	SqFt
56	SWELLING	L	75.00	SqFt

Sample Number: 455 **Type:** R **Area:** 5000.00 SqFt **PCI:** 47

Sample Comments:

41	ALLIGATOR CR	L	140.00	SqFt
48	L & T CR	L	364.00	Ft
52	RAVELING	L	4900.00	SqFt
52	RAVELING	M	100.00	SqFt
56	SWELLING	L	65.00	SqFt

Sample Number: 461 **Type:** R **Area:** 5000.00 SqFt **PCI:** 52

Sample Comments:

41	ALLIGATOR CR	L	36.00	SqFt
48	L & T CR	L	529.00	Ft
52	RAVELING	L	4950.00	SqFt
52	RAVELING	M	50.00	SqFt
56	SWELLING	L	157.00	SqFt

Sample Number: 466 **Type:** R **Area:** 5000.00 SqFt **PCI:** 58

Sample Comments:

41	ALLIGATOR CR	L	12.00	SqFt
48	L & T CR	L	724.00	Ft
52	RAVELING	L	5000.00	SqFt
56	SWELLING	L	62.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: RW 18-36 **Name:** RUNWAY 18-36 **Use:** RUNWAY **Area:** 1,459,350 SqFt

Section: 6190 of 13 **From:** - **To:** - **Last Const.:** 12/1/2020

Surface: AAC **Family:** CA653-PR-RW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 189,000 SqFt **Length:** 2,100 Ft **Width:** 90 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1960	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1977	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1988	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2003	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 12/1/2020	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 12/10/2018 **TotalSamples:** 22 **Surveyed:** 5

Conditions: **PCI:** 68 **NOTE: *** Pre-Construction PCI ***

Inspection Comments:

Sample Number: 228 **Type:** R **Area:** 5000.00 SqFt **PCI:** 61

Sample Comments:

48	L & T CR	L	387.00	Ft
48	L & T CR	M	55.00	Ft
52	RAVELING	L	500.00	SqFt
56	SWELLING	L	65.00	SqFt
57	WEATHERING	L	4500.00	SqFt

Sample Number: 248 **Type:** R **Area:** 5000.00 SqFt **PCI:** 73

Sample Comments:

48	L & T CR	L	269.00	Ft
52	RAVELING	L	500.00	SqFt
56	SWELLING	L	20.00	SqFt
57	WEATHERING	L	4500.00	SqFt

Sample Number: 264 **Type:** R **Area:** 3750.00 SqFt **PCI:** 63

Sample Comments:

48	L & T CR	L	141.00	Ft
48	L & T CR	M	100.00	Ft
52	RAVELING	L	500.00	SqFt
56	SWELLING	L	56.00	SqFt
57	WEATHERING	L	3250.00	SqFt

Sample Number: 640 **Type:** R **Area:** 5000.00 SqFt **PCI:** 68

Sample Comments:

48	L & T CR	L	304.00	Ft
52	RAVELING	L	750.00	SqFt
56	SWELLING	L	100.00	SqFt
57	WEATHERING	L	4250.00	SqFt

Sample Number: 656 **Type:** R **Area:** 5000.00 SqFt **PCI:** 72

Sample Comments:

48	L & T CR	L	269.00	Ft
52	RAVELING	L	500.00	SqFt
56	SWELLING	L	30.00	SqFt
57	WEATHERING	L	4500.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: RW 18-36 **Name:** RUNWAY 18-36 **Use:** RUNWAY **Area:** 1,459,350 SqFt

Section: 6196 of 13 **From:** - **To:** - **Last Const.:** 12/1/2020

Surface: AAC **Family:** CA653-PR-RW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 27,000 SqFt **Length:** 300 Ft **Width:** 90 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/2002	**Work Type:** New Construction - AC	**Code:** NC-AC	**Is Major M&R:** True
Work Date: 1/1/2013	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 12/1/2020	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 12/10/2018 **TotalSamples:** 4 **Surveyed:** 1

Conditions: **PCI:** 79 **NOTE: *** Pre-Construction PCI *****

Inspection Comments:

Sample Number: 094 **Type:** R **Area:** 3750.00 SqFt **PCI:** 79

Sample Comments:

48	L & T CR	L	122.00	Ft
48	L & T CR	M	12.00	Ft
57	WEATHERING	L	3750.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: RW 4-22 **Name:** RUNWAY 4-22 **Use:** RUNWAY **Area:** 869,917 SqFt

Section: 6205 of 8 **From:** - **To:** - **Last Const.:** 1/1/2012

Surface: AAC **Family:** CA653-PR-RW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 474,873 SqFt **Length:** 4,700 Ft **Width:** 100 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1983	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2012	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 1/1/2020	**Work Type:** Crack Sealing - AC	**Code:** CS-AC	**Is Major M&R:** False

Last Insp. Date: 4/4/2022 **TotalSamples:** 95 **Surveyed:** 20

Conditions: **PCI:** 73

Inspection Comments:

Sample Number: 301 **Type:** R **Area:** 5000.00 SqFt **PCI:** 74

Sample Comments:

42	BLEEDING	N	3.00	SqFt
48	L & T CR	L	412.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 304 **Type:** R **Area:** 5000.00 SqFt **PCI:** 66

Sample Comments:

48	L & T CR	L	367.00	Ft
48	L & T CR	M	25.00	Ft
52	RAVELING	L	240.00	SqFt
57	WEATHERING	L	4760.00	SqFt

Sample Number: 308 **Type:** R **Area:** 5000.00 SqFt **PCI:** 65

Sample Comments:

41	ALLIGATOR CR	L	14.00	SqFt
48	L & T CR	L	573.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 312 **Type:** R **Area:** 5000.00 SqFt **PCI:** 68

Sample Comments:

42	BLEEDING	N	12.00	SqFt
48	L & T CR	L	458.00	Ft
57	WEATHERING	L	4750.00	SqFt
57	WEATHERING	M	250.00	SqFt

Sample Number: 317 **Type:** R **Area:** 5000.00 SqFt **PCI:** 67

Sample Comments:

48	L & T CR	L	386.00	Ft
52	RAVELING	L	250.00	SqFt
57	WEATHERING	L	4500.00	SqFt
57	WEATHERING	M	250.00	SqFt

Sample Number: 321 **Type:** R **Area:** 5000.00 SqFt **PCI:** 74

Sample Comments:

48	L & T CR	L	332.00	Ft
57	WEATHERING	L	4750.00	SqFt
57	WEATHERING	M	250.00	SqFt

Sample Number: 326 **Type:** R **Area:** 5000.00 SqFt **PCI:** 82

Sample Comments:

48	L & T CR	L	123.00	Ft
48	L & T CR	M	5.00	Ft

57	WEATHERING	L	5000.00	SqFt

Sample Number: 332 **Type:** R **Area:** 5000.00 SqFt **PCI:** 68

Sample Comments:

48	L & T CR	L	352.00	Ft
48	L & T CR	M	30.00	Ft
57	WEATHERING	L	4750.00	SqFt
57	WEATHERING	M	250.00	SqFt

Sample Number: 338 **Type:** R **Area:** 5000.00 SqFt **PCI:** 75

Sample Comments:

48	L & T CR	L	305.00	Ft
57	WEATHERING	L	4750.00	SqFt
57	WEATHERING	M	250.00	SqFt

Sample Number: 341 **Type:** R **Area:** 5000.00 SqFt **PCI:** 75

Sample Comments:

48	L & T CR	L	259.00	Ft
48	L & T CR	M	25.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 344 **Type:** R **Area:** 5000.00 SqFt **PCI:** 75

Sample Comments:

48	L & T CR	L	213.00	Ft
48	L & T CR	M	40.00	Ft
57	WEATHERING	L	4914.00	SqFt
57	WEATHERING	M	86.00	SqFt

Sample Number: 350 **Type:** R **Area:** 5000.00 SqFt **PCI:** 76

Sample Comments:

48	L & T CR	L	272.00	Ft
57	WEATHERING	L	4750.00	SqFt
57	WEATHERING	M	250.00	SqFt

Sample Number: 356 **Type:** R **Area:** 5000.00 SqFt **PCI:** 81

Sample Comments:

48	L & T CR	L	167.00	Ft
57	WEATHERING	L	4750.00	SqFt
57	WEATHERING	M	250.00	SqFt

Sample Number: 362 **Type:** R **Area:** 5000.00 SqFt **PCI:** 80

Sample Comments:

48	L & T CR	L	173.00	Ft
57	WEATHERING	L	4750.00	SqFt
57	WEATHERING	M	250.00	SqFt

Sample Number: 368 **Type:** R **Area:** 5000.00 SqFt **PCI:** 89

Sample Comments:

48	L & T CR	L	71.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 373 **Type:** R **Area:** 5000.00 SqFt **PCI:** 70

Sample Comments:

48	L & T CR	L	429.00	Ft
57	WEATHERING	L	4750.00	SqFt
57	WEATHERING	M	250.00	SqFt

Sample Number: 379 **Type:** R **Area:** 5000.00 SqFt **PCI:** 68

Sample Comments:

48	L & T CR	L	304.00	Ft
48	L & T CR	M	25.00	Ft
56	SWELLING	L	8.00	SqFt
57	WEATHERING	L	4750.00	SqFt
57	WEATHERING	M	250.00	SqFt

Sample Number: 385 **Type:** R **Area:** 5000.00 SqFt **PCI:** 77

Sample Comments:

48	L & T CR	L	252.00	Ft
57	WEATHERING	L	4750.00	SqFt
57	WEATHERING	M	250.00	SqFt

Sample Number: 391 **Type:** R **Area:** 5000.00 SqFt **PCI:** 71

Sample Comments:

48	L & T CR	L	259.00	Ft
48	L & T CR	M	50.00	Ft
57	WEATHERING	L	4750.00	SqFt
57	WEATHERING	M	250.00	SqFt

Sample Number: 394 **Type:** R **Area:** 4873.00 SqFt **PCI:** 66

Sample Comments:

48	L & T CR	L	357.00	Ft
48	L & T CR	M	75.00	Ft
57	WEATHERING	L	4386.00	SqFt
57	WEATHERING	M	487.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: RW 4-22 **Name:** RUNWAY 4-22 **Use:** RUNWAY **Area:** 869,917 SqFt

Section: 6210 of 8 **From:** - **To:** - **Last Const.:** 1/1/2012

Surface: AAC **Family:** CA653-PR-RW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 237,436 SqFt **Length:** 9,400 Ft **Width:** 25 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1983	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1983	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2012	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 1/1/2020	**Work Type:** Crack Sealing - AC	**Code:** CS-AC	**Is Major M&R:** False

Last Insp. Date: 4/4/2022 **TotalSamples:** 48 **Surveyed:** 10

Conditions: **PCI:** 79

Inspection Comments:

Sample Number: 100 **Type:** R **Area:** 5000.00 SqFt **PCI:** 71

Sample Comments:

48	L & T CR	L	362.00	Ft
52	RAVELING	L	250.00	SqFt
57	WEATHERING	L	4750.00	SqFt

Sample Number: 136 **Type:** R **Area:** 5000.00 SqFt **PCI:** 82

Sample Comments:

48	L & T CR	L	212.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 168 **Type:** R **Area:** 5000.00 SqFt **PCI:** 86

Sample Comments:

48	L & T CR	L	135.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 184 **Type:** R **Area:** 5000.00 SqFt **PCI:** 85

Sample Comments:

48	L & T CR	L	147.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 508 **Type:** R **Area:** 5000.00 SqFt **PCI:** 75

Sample Comments:

48	L & T CR	L	403.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 520 **Type:** R **Area:** 5000.00 SqFt **PCI:** 71

Sample Comments:

48	L & T CR	L	366.00	Ft
48	L & T CR	M	12.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 528 **Type:** R **Area:** 5000.00 SqFt **PCI:** 85

Sample Comments:

48	L & T CR	L	154.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 552 **Type:** R **Area:** 5000.00 SqFt **PCI:** 78

Sample Comments:

48	L & T CR	L	294.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 572 **Type:** R **Area:** 5000.00 SqFt **PCI:** 74

Sample Comments:

48	L & T CR	L	412.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 588 **Type:** R **Area:** 5000.00 SqFt **PCI:** 83

Sample Comments:

48	L & T CR	L	85.00	Ft
57	WEATHERING	L	4500.00	SqFt
57	WEATHERING	M	500.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: RW 4-22 **Name:** RUNWAY 4-22 **Use:** RUNWAY **Area:** 869,917 SqFt

Section: 6215 of 8 **From:** - **To:** - **Last Const.:** 1/1/2012

Surface: AAC **Family:** CA653-PR-RW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 50,072 SqFt **Length:** 495 Ft **Width:** 100 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1978	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1988	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1988	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2012	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 1/1/2020	**Work Type:** Crack Sealing - AC	**Code:** CS-AC	**Is Major M&R:** False

Last Insp. Date: 4/4/2022 **TotalSamples:** 10 **Surveyed:** 3

Conditions: **PCI:** 63

Inspection Comments:

Sample Number: 400 **Type:** R **Area:** 5000.00 SqFt **PCI:** 60

Sample Comments:

41	ALLIGATOR CR	L	5.00	SqFt
42	BLEEDING	N	30.00	SqFt
48	L & T CR	L	241.00	Ft
48	L & T CR	M	125.00	Ft
57	WEATHERING	L	4750.00	SqFt
57	WEATHERING	M	250.00	SqFt

Sample Number: 404 **Type:** R **Area:** 5000.00 SqFt **PCI:** 64

Sample Comments:

41	ALLIGATOR CR	L	8.00	SqFt
42	BLEEDING	N	1.00	SqFt
48	L & T CR	L	340.00	Ft
48	L & T CR	M	25.00	Ft
56	SWELLING	L	7.00	SqFt
57	WEATHERING	L	4975.00	SqFt
57	WEATHERING	M	25.00	SqFt

Sample Number: 407 **Type:** R **Area:** 5000.00 SqFt **PCI:** 65

Sample Comments:

48	L & T CR	L	485.00	Ft
52	RAVELING	L	41.00	SqFt
57	WEATHERING	L	4463.00	SqFt
57	WEATHERING	M	496.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: RW 4-22 **Name:** RUNWAY 4-22 **Use:** RUNWAY **Area:** 869,917 SqFt

Section: 6220 of 8 **From:** - **To:** - **Last Const.:** 1/1/2012

Surface: AAC **Family:** CA653-PR-RW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 25,036 SqFt **Length:** 495 Ft **Width:** 50 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1978	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1988	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1988	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2012	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 1/1/2020	**Work Type:** Crack Sealing - AC	**Code:** CS-AC	**Is Major M&R:** False

Last Insp. Date: 4/4/2022 **TotalSamples:** 6 **Surveyed:** 2

Conditions: **PCI:** 70

Inspection Comments:

Sample Number: 202 **Type:** R **Area:** 5000.00 SqFt **PCI:** 71

Sample Comments:

48	L & T CR	L	547.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 606 **Type:** R **Area:** 3750.00 SqFt **PCI:** 70

Sample Comments:

48	L & T CR	L	418.00	Ft
57	WEATHERING	L	3750.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: RW 4-22 **Name:** RUNWAY 4-22 **Use:** RUNWAY **Area:** 869,917 SqFt

Section: 6225 of 8 **From:** - **To:** - **Last Const.:** 1/1/2006

Surface: AC **Family:** CA653-PR-RW-AC **Zone:** **Category:** **Rank:** P

Area: 45,300 SqFt **Length:** 453 Ft **Width:** 100 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/2006 **Work Type:** New Construction - AC **Code:** NC-AC **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 9 **Surveyed:** 3

Conditions: **PCI:** 54

Inspection Comments:

Sample Number: 410 **Type:** R **Area:** 5000.00 SqFt **PCI:** 65

Sample Comments:

42	BLEEDING	N	219.00	SqFt
48	L & T CR	L	13.00	Ft
57	WEATHERING	L	4500.00	SqFt
57	WEATHERING	M	500.00	SqFt

Sample Number: 412 **Type:** R **Area:** 5000.00 SqFt **PCI:** 16

Sample Comments:

42	BLEEDING	N	2176.00	SqFt
48	L & T CR	L	32.00	Ft
53	RUTTING	L	476.00	SqFt
53	RUTTING	M	1162.00	SqFt
57	WEATHERING	L	5000.00	SqFt

Sample Number: 417 **Type:** R **Area:** 5300.00 SqFt **PCI:** 80

Sample Comments:

57	WEATHERING	M	5300.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: RW 4-22 **Name:** RUNWAY 4-22 **Use:** RUNWAY **Area:** 869,917 SqFt

Section: 6230 of 8 **From:** - **To:** - **Last Const.:** 1/1/2006

Surface: AC **Family:** CA653-PR-RW-AC **Zone:** **Category:** **Rank:** P

Area: 22,650 SqFt **Length:** 453 Ft **Width:** 50 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/2006 **Work Type:** New Construction - AC **Code:** NC-AC **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 4 **Surveyed:** 1

Conditions: **PCI:** 24

Inspection Comments:

Sample Number: 210 **Type:** R **Area:** 6250.00 SqFt **PCI:** 24

Sample Comments:

42	BLEEDING	N	1880.00	SqFt
48	L & T CR	L	240.00	Ft
52	RAVELING	L	312.00	SqFt
53	RUTTING	M	504.00	SqFt
57	WEATHERING	L	5938.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TL T-HANG **Name:** T-Hangar Taxilane **Use:** TAXILANE **Area:** 2,818 SqFt

Section: 1120 of 2 **From:** - **To:** - **Last Const.:** 1/1/1984

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 1,346 SqFt **Length:** 67 Ft **Width:** 20 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1984 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 1 **Surveyed:** 1

Conditions: **PCI:** 39

Inspection Comments:

Sample Number: 100 **Type:** R **Area:** 1346.00 SqFt **PCI:** 39

Sample Comments:

48	L & T CR	L	134.00	Ft
48	L & T CR	M	25.00	Ft
52	RAVELING	L	538.00	SqFt
52	RAVELING	M	808.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TL T-HANG **Name:** T-Hangar Taxilane **Use:** TAXILANE **Area:** 2,818 SqFt

Section: 1125 of 2 **From:** - **To:** - **Last Const.:** 1/1/1984

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 1,472 SqFt **Length:** 62 Ft **Width:** 20 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1984 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 1 **Surveyed:** 1

Conditions: **PCI:** 38

Inspection Comments:

Sample Number: 200 **Type:** R **Area:** 1472.00 SqFt **PCI:** 38

Sample Comments:

48	L & T CR	L	30.00	Ft
48	L & T CR	M	90.00	Ft
52	RAVELING	M	1472.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW A **Name:** TAXIWAY A **Use:** TAXIWAY **Area:** 728,918 SqFt

Section: 115 of 6 **From:** - **To:** - **Last Const.:** 8/1/2016

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 224,709 SqFt **Length:** 2,940 Ft **Width:** 75 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1958	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1978	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1990	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 8/1/2016	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 7/1/2020	**Work Type:** Patching - AC	**Code:** PA-AC	**Is Major M&R:** False

Last Insp. Date: 4/4/2022 **TotalSamples:** 59 **Surveyed:** 8

Conditions: **PCI:** 65

Inspection Comments:

Sample Number: 105 **Type:** R **Area:** 4115.00 SqFt **PCI:** 71

Sample Comments:

41	ALLIGATOR CR	L	8.00	SqFt
48	L & T CR	L	127.00	Ft
55	SLIPPAGE CR	N	30.00	SqFt
57	WEATHERING	L	3909.00	SqFt
57	WEATHERING	M	206.00	SqFt

Sample Number: 109 **Type:** R **Area:** 3759.00 SqFt **PCI:** 64

Sample Comments:

48	L & T CR	L	225.00	Ft
53	RUTTING	L	150.00	SqFt
57	WEATHERING	L	3571.00	SqFt
57	WEATHERING	M	188.00	SqFt

Sample Number: 114 **Type:** R **Area:** 3750.00 SqFt **PCI:** 46

Sample Comments:

48	L & T CR	L	164.00	Ft
50	PATCHING	L	567.00	SqFt
53	RUTTING	L	37.00	SqFt
53	RUTTING	M	53.00	SqFt
55	SLIPPAGE CR	N	66.00	SqFt
57	WEATHERING	L	3024.00	SqFt
57	WEATHERING	M	159.00	SqFt

Sample Number: 123 **Type:** R **Area:** 3750.00 SqFt **PCI:** 69

Sample Comments:

42	BLEEDING	N	10.00	SqFt
48	L & T CR	L	114.00	Ft
53	RUTTING	L	90.00	SqFt
57	WEATHERING	L	3750.00	SqFt

Sample Number: 132 **Type:** R **Area:** 3750.00 SqFt **PCI:** 68

Sample Comments:

48	L & T CR	L	80.00	Ft
53	RUTTING	L	150.00	SqFt
57	WEATHERING	M	3750.00	SqFt

Sample Number: 145 **Type:** R **Area:** 3750.00 SqFt **PCI:** 59

Sample Comments:

48	L & T CR	L	25.00	Ft
50	PATCHING	L	2600.00	SqFt
57	WEATHERING	L	1150.00	SqFt

Sample Number: 149 **Type:** R **Area:** 3750.00 SqFt **PCI:** 59

Sample Comments:

48	L & T CR	L	25.00	Ft
50	PATCHING	L	2600.00	SqFt
57	WEATHERING	L	1150.00	SqFt

Sample Number: 154 **Type:** R **Area:** 3750.00 SqFt **PCI:** 81

Sample Comments:

48	L & T CR	L	118.00	Ft
57	WEATHERING	L	3562.00	SqFt
57	WEATHERING	M	188.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW A **Name:** TAXIWAY A **Use:** TAXIWAY **Area:** 728,918 SqFt

Section: 130 of 6 **From:** - **To:** - **Last Const.:** 8/1/2016

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 358,395 SqFt **Length:** 4,660 Ft **Width:** 75 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1978	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1992	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 8/1/2016	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 1/1/2020	**Work Type:** Crack Sealing - AC	**Code:** CS-AC	**Is Major M&R:** False

Last Insp. Date: 4/4/2022 **TotalSamples:** 93 **Surveyed:** 10

Conditions: **PCI:** 71

Inspection Comments:

Sample Number: 303 **Type:** R **Area:** 4373.00 SqFt **PCI:** 69

Sample Comments:

48	L & T CR	L	178.00	Ft
48	L & T CR	M	50.00	Ft
52	RAVELING	L	588.00	SqFt
57	WEATHERING	L	3596.00	SqFt
57	WEATHERING	M	189.00	SqFt

Sample Number: 313 **Type:** R **Area:** 3750.00 SqFt **PCI:** 75

Sample Comments:

48	L & T CR	L	187.00	Ft
56	SWELLING	L	10.00	SqFt
57	WEATHERING	L	3562.00	SqFt
57	WEATHERING	M	188.00	SqFt

Sample Number: 324 **Type:** R **Area:** 3750.00 SqFt **PCI:** 76

Sample Comments:

48	L & T CR	L	163.00	Ft
56	SWELLING	L	25.00	SqFt
57	WEATHERING	L	3562.00	SqFt
57	WEATHERING	M	188.00	SqFt

Sample Number: 332 **Type:** R **Area:** 3750.00 SqFt **PCI:** 85

Sample Comments:

48	L & T CR	L	59.00	Ft
57	WEATHERING	L	3562.00	SqFt
57	WEATHERING	M	188.00	SqFt

Sample Number: 343 **Type:** R **Area:** 3750.00 SqFt **PCI:** 67

Sample Comments:

48	L & T CR	L	314.00	Ft
56	SWELLING	L	50.00	SqFt
57	WEATHERING	L	3562.00	SqFt
57	WEATHERING	M	188.00	SqFt

Sample Number: 350 **Type:** R **Area:** 3750.00 SqFt **PCI:** 73

Sample Comments:

48	L & T CR	L	249.00	Ft
56	SWELLING	L	50.00	SqFt
57	WEATHERING	L	3750.00	SqFt

Sample Number: 358 **Type:** R **Area:** 3750.00 SqFt **PCI:** 66

Sample Comments:

48	L & T CR	L	291.00	Ft
56	SWELLING	L	50.00	SqFt
57	WEATHERING	L	3375.00	SqFt
57	WEATHERING	M	375.00	SqFt

Sample Number: 369 **Type:** R **Area:** 3750.00 SqFt **PCI:** 68

Sample Comments:

48	L & T CR	L	373.00	Ft
56	SWELLING	L	50.00	SqFt
57	WEATHERING	L	3750.00	SqFt

Sample Number: 378 **Type:** R **Area:** 3750.00 SqFt **PCI:** 65

Sample Comments:

48	L & T CR	L	366.00	Ft
56	SWELLING	L	10.00	SqFt
57	WEATHERING	L	3375.00	SqFt
57	WEATHERING	M	375.00	SqFt

Sample Number: 387 **Type:** R **Area:** 3750.00 SqFt **PCI:** 69

Sample Comments:

48	L & T CR	L	325.00	Ft
56	SWELLING	L	70.00	SqFt
57	WEATHERING	L	3750.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW A **Name:** TAXIWAY A **Use:** TAXIWAY **Area:** 728,918 SqFt

Section: 132 of 6 **From:** - **To:** - **Last Const.:** 5/1/2019

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 23,007 SqFt **Length:** 150 Ft **Width:** 150 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1958	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1992	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1992	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 5/1/2019	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 4 **Surveyed:** 1

Conditions: **PCI:** 88

Inspection Comments:

Sample Number: 201 **Type:** R **Area:** 5976.00 SqFt **PCI:** 88

Sample Comments:

42	BLEEDING	N	18.00	SqFt
48	L & T CR	L	16.00	Ft
57	WEATHERING	L	5976.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW A **Name:** TAXIWAY A **Use:** TAXIWAY **Area:** 728,918 SqFt

Section: 155 of 6 **From:** - **To:** - **Last Const.:** 8/1/2016

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 6,259 SqFt **Length:** 75 Ft **Width:** 85 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1978	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1990	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1992	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 8/1/2016	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 1 **Surveyed:** 1

Conditions: **PCI:** 89

Inspection Comments:

Sample Number: 101 **Type:** R **Area:** 6259.00 SqFt **PCI:** 89

Sample Comments:

48	L & T CR	L	83.00	Ft
57	WEATHERING	L	6259.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW A **Name:** TAXIWAY A **Use:** TAXIWAY **Area:** 728,918 SqFt

Section: 158 of 6 **From:** - **To:** - **Last Const.:** 8/1/2016

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 16,692 SqFt **Length:** 1,700 Ft **Width:** 125 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/2006 **Work Type:** New Construction - AC **Code:** NC-AC **Is Major M&R:** True

Work Date: 8/1/2016 **Work Type:** Mill and Overlay **Code:** ML-OVL **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 3 **Surveyed:** 1

Conditions: **PCI:** 63

Inspection Comments:

Sample Number: 137 **Type:** R **Area:** 6795.00 SqFt **PCI:** 63

Sample Comments:

48	L & T CR	L	223.00	Ft
53	RUTTING	L	150.00	SqFt
53	RUTTING	M	50.00	SqFt
57	WEATHERING	L	6795.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW A **Name:** TAXIWAY A **Use:** TAXIWAY **Area:** 728,918 SqFt

Section: 160 of 6 **From:** - **To:** - **Last Const.:** 1/1/2017

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 99,856 SqFt **Length:** 1,330 Ft **Width:** 75 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/2006 **Work Type:** New Construction - AC **Code:** NC-AC **Is Major M&R:** True

Work Date: 1/1/2017 **Work Type:** Mill and Overlay **Code:** ML-OVL **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 24 **Surveyed:** 3

Conditions: **PCI:** 82

Inspection Comments:

Sample Number: 114 **Type:** R **Area:** 4719.00 SqFt **PCI:** 87

Sample Comments:

48	L & T CR	L	113.00	Ft
57	WEATHERING	L	4719.00	SqFt

Sample Number: 120 **Type:** R **Area:** 3890.00 SqFt **PCI:** 73

Sample Comments:

48	L & T CR	L	77.00	Ft
50	PATCHING	L	516.00	SqFt
57	WEATHERING	L	3374.00	SqFt

Sample Number: 131 **Type:** R **Area:** 3947.00 SqFt **PCI:** 86

Sample Comments:

48	L & T CR	L	108.00	Ft
57	WEATHERING	L	3947.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW A1 **Name:** TAXIWAY A1 **Use:** TAXIWAY **Area:** 57,542 SqFt

Section: 135 of 3 **From:** - **To:** - **Last Const.:** 8/1/2016

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 40,056 SqFt **Length:** 2,475 Ft **Width:** 75 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1978	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1992	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2012	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 8/1/2016	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 9 **Surveyed:** 1

Conditions: **PCI:** 64

Inspection Comments:

Sample Number: 393 **Type:** R **Area:** 4007.00 SqFt **PCI:** 64

Sample Comments:

48	L & T CR	L	344.00	Ft
56	SWELLING	L	66.00	SqFt
57	WEATHERING	L	3606.00	SqFt
57	WEATHERING	M	401.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW A1 **Name:** TAXIWAY A1 **Use:** TAXIWAY **Area:** 57,542 SqFt

Section: 140 of 3 **From:** - **To:** - **Last Const.:** 8/1/2016

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 14,541 SqFt **Length:** 150 Ft **Width:** 75 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1978	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1988	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2003	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 8/1/2016	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 3 **Surveyed:** 1

Conditions: **PCI:** 67

Inspection Comments:

Sample Number: 402 **Type:** R **Area:** 6689.00 SqFt **PCI:** 67

Sample Comments:

48	L & T CR	L	571.00	Ft
56	SWELLING	L	20.00	SqFt
57	WEATHERING	L	6020.00	SqFt
57	WEATHERING	M	669.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW A1 **Name:** TAXIWAY A1 **Use:** TAXIWAY **Area:** 57,542 SqFt

Section: 145 of 3 **From:** - **To:** - **Last Const.:** 12/1/2020

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 2,945 SqFt **Length:** 120 Ft **Width:** 25 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1978	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1988	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2003	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 8/1/2016	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 12/1/2020	**Work Type:** Complete Reconstruction - AC	**Code:** CR-AC	**Is Major M&R:** True

Last Insp. Date: 12/10/2018 **TotalSamples:** 3 **Surveyed:** 1

Conditions: **PCI:** 79 **NOTE: *** Pre-Construction PCI ***

Inspection Comments:

Sample Number: 402 **Type:** R **Area:** 6689.00 SqFt **PCI:** 79

Sample Comments:

48	L & T CR	L	358.00	Ft
57	WEATHERING	L	6689.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW A2 **Name:** TAXIWAY A2 **Use:** TAXIWAY **Area:** 60,458 SqFt

Section: 165 of 1 **From:** - **To:** - **Last Const.:** 8/1/2016

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 60,458 SqFt **Length:** 600 Ft **Width:** 100 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 8/1/2016 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Work Date: 1/1/2020 **Work Type:** Crack Sealing - AC **Code:** CS-AC **Is Major M&R:** False

Last Insp. Date: 4/4/2022 **TotalSamples:** 13 **Surveyed:** 2

Conditions: **PCI:** 74

Inspection Comments:

Sample Number: 203 **Type:** R **Area:** 5172.00 SqFt **PCI:** 77

Sample Comments:

48	L & T CR	L	209.00	Ft
56	SWELLING	L	25.00	SqFt
57	WEATHERING	L	4913.00	SqFt
57	WEATHERING	M	259.00	SqFt

Sample Number: 206 **Type:** R **Area:** 6386.00 SqFt **PCI:** 72

Sample Comments:

48	L & T CR	L	262.00	Ft
55	SLIPPAGE CR	N	24.00	SqFt
56	SWELLING	L	25.00	SqFt
57	WEATHERING	L	6067.00	SqFt
57	WEATHERING	M	319.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW A3 **Name:** TAXIWAY A3 **Use:** TAXIWAY **Area:** 60,311 SqFt

Section: 168 of 1 **From:** - **To:** - **Last Const.:** 8/1/2016

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 60,311 SqFt **Length:** 400 Ft **Width:** 100 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 8/1/2016 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 13 **Surveyed:** 2

Conditions: **PCI:** 77

Inspection Comments:

Sample Number: 302 **Type:** R **Area:** 6425.00 SqFt **PCI:** 76

Sample Comments:

48	L & T CR	L	150.00	Ft
48	L & T CR	M	50.00	Ft
57	WEATHERING	L	6104.00	SqFt
57	WEATHERING	M	321.00	SqFt

Sample Number: 305 **Type:** R **Area:** 5145.00 SqFt **PCI:** 77

Sample Comments:

45	DEPRESSION	L	27.00	SqFt
48	L & T CR	L	178.00	Ft
57	WEATHERING	L	4888.00	SqFt
57	WEATHERING	M	257.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW A4 **Name:** TAXIWAY A4 **Use:** TAXIWAY **Area:** 58,588 SqFt

Section: 170 of 1 **From:** - **To:** - **Last Const.:** 8/1/2016

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 58,588 SqFt **Length:** 400 Ft **Width:** 100 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 8/1/2016 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 12 **Surveyed:** 2

Conditions: **PCI:** 85

Inspection Comments:

Sample Number: 402 **Type:** R **Area:** 6426.00 SqFt **PCI:** 86

Sample Comments:

48	L & T CR	L	73.00	Ft
57	WEATHERING	L	6105.00	SqFt
57	WEATHERING	M	321.00	SqFt

Sample Number: 406 **Type:** R **Area:** 6384.00 SqFt **PCI:** 85

Sample Comments:

48	L & T CR	L	102.00	Ft
57	WEATHERING	L	6065.00	SqFt
57	WEATHERING	M	319.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW A5 **Name:** TAXIWAY A5 **Use:** TAXIWAY **Area:** 56,987 SqFt

Section: 175 of 1 **From:** - **To:** - **Last Const.:** 8/1/2016

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 56,987 SqFt **Length:** 400 Ft **Width:** 100 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 8/1/2016 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 11 **Surveyed:** 2

Conditions: **PCI:** 82

Inspection Comments:

Sample Number: 503 **Type:** R **Area:** 5171.00 SqFt **PCI:** 73

Sample Comments:

53	RUTTING	L	99.00	SqFt
57	WEATHERING	L	4912.00	SqFt
57	WEATHERING	M	259.00	SqFt

Sample Number: 506 **Type:** R **Area:** 6387.00 SqFt **PCI:** 89

Sample Comments:

48	L & T CR	L	99.00	Ft
57	WEATHERING	L	6387.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW A6 **Name:** TAXIWAY A6 **Use:** TAXIWAY **Area:** 58,658 SqFt

Section: 180 of 1 **From:** - **To:** - **Last Const.:** 8/1/2016

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 58,658 SqFt **Length:** 400 Ft **Width:** 100 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 8/1/2016 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 12 **Surveyed:** 2

Conditions: **PCI:** 86

Inspection Comments:

Sample Number: 602 **Type:** R **Area:** 6422.00 SqFt **PCI:** 86

Sample Comments:

48	L & T CR	L	50.00	Ft
57	WEATHERING	L	6101.00	SqFt
57	WEATHERING	M	321.00	SqFt

Sample Number: 606 **Type:** R **Area:** 6387.00 SqFt **PCI:** 86

Sample Comments:

48	L & T CR	L	73.00	Ft
57	WEATHERING	L	6068.00	SqFt
57	WEATHERING	M	319.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW A7 **Name:** TAXIWAY A7 **Use:** TAXIWAY **Area:** 52,089 SqFt

Section: 162 of 1 **From:** - **To:** - **Last Const.:** 1/1/2017

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 52,089 SqFt **Length:** 700 Ft **Width:** 75 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/2006 **Work Type:** New Construction - AC **Code:** NC-AC **Is Major M&R:** True

Work Date: 1/1/2017 **Work Type:** Mill and Overlay **Code:** ML-OVL **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 11 **Surveyed:** 2

Conditions: **PCI:** 83

Inspection Comments:

Sample Number: 102 **Type:** R **Area:** 4568.00 SqFt **PCI:** 83

Sample Comments:

42	BLEEDING	N	64.00	SqFt
48	L & T CR	L	31.00	Ft
57	WEATHERING	L	4568.00	SqFt

Sample Number: 107 **Type:** R **Area:** 4392.00 SqFt **PCI:** 83

Sample Comments:

42	BLEEDING	N	64.00	SqFt
48	L & T CR	L	31.00	Ft
57	WEATHERING	L	4392.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW B **Name:** TAXIWAY B **Use:** TAXIWAY **Area:** 111,984 SqFt

Section: 205 of 7 **From:** - **To:** - **Last Const.:** 6/1/2018

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 6,200 SqFt **Length:** 124 Ft **Width:** 50 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1958 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Work Date: 6/1/2018 **Work Type:** Mill and Overlay **Code:** ML-OVL **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 1 **Surveyed:** 1

Conditions: **PCI:** 91

Inspection Comments:

Sample Number: 103 **Type:** R **Area:** 6200.00 SqFt **PCI:** 91

Sample Comments:

48	L & T CR	L	5.00	Ft
56	SWELLING	L	2.00	SqFt
57	WEATHERING	L	6200.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW B **Name:** TAXIWAY B **Use:** TAXIWAY **Area:** 111,984 SqFt

Section: 207 of 7 **From:** - **To:** - **Last Const.:** 6/1/2018

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 7,750 SqFt **Length:** 155 Ft **Width:** 50 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1958	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 6/1/2018	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 2 **Surveyed:** 1

Conditions: **PCI:** 90

Inspection Comments:

Sample Number: 102 **Type:** R **Area:** 4100.00 SqFt **PCI:** 90

Sample Comments:

48	L & T CR	L	20.00	Ft
57	WEATHERING	L	4100.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW B **Name:** TAXIWAY B **Use:** TAXIWAY **Area:** 111,984 SqFt

Section: 210 of 7 **From:** - **To:** - **Last Const.:** 6/1/2018

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 6,353 SqFt **Length:** 130 Ft **Width:** 50 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1958	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1983	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1992	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 6/1/2018	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 1 **Surveyed:** 1

Conditions: **PCI:** 88

Inspection Comments:

Sample Number: 100 **Type:** R **Area:** 6353.00 SqFt **PCI:** 88

Sample Comments:

48	L & T CR	L	62.00	Ft
56	SWELLING	L	13.00	SqFt
57	WEATHERING	L	6353.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW B **Name:** TAXIWAY B **Use:** TAXIWAY **Area:** 111,984 SqFt

Section: 212 of 7 **From:** - **To:** - **Last Const.:** 6/1/2018

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 18,000 SqFt **Length:** 150 Ft **Width:** 120 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1958	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1994	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 6/1/2018	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 3 **Surveyed:** 1

Conditions: **PCI:** 80

Inspection Comments:

Sample Number: 200 **Type:** R **Area:** 6000.00 SqFt **PCI:** 80

Sample Comments:

48	L & T CR	L	229.00	Ft
56	SWELLING	L	50.00	SqFt
57	WEATHERING	L	6000.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW B **Name:** TAXIWAY B **Use:** TAXIWAY **Area:** 111,984 SqFt

Section: 215 of 7 **From:** - **To:** - **Last Const.:** 1/1/2012

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 15,387 SqFt **Length:** 300 Ft **Width:** 50 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/2012 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 3 **Surveyed:** 1

Conditions: **PCI:** 85

Inspection Comments:

Sample Number: 232 **Type:** R **Area:** 5000.00 SqFt **PCI:** 85

Sample Comments:

42	BLEEDING	N	2.00	SqFt
45	DEPRESSION	L	6.00	SqFt
48	L & T CR	L	136.00	Ft
57	WEATHERING	L	5000.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW B **Name:** TAXIWAY B **Use:** TAXIWAY **Area:** 111,984 SqFt

Section: 220 of 7 **From:** - **To:** - **Last Const.:** 1/1/1965

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 40,670 SqFt **Length:** 835 Ft **Width:** 50 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1965 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 8 **Surveyed:** 2

Conditions: **PCI:** 16

Inspection Comments:

Sample Number: 223 **Type:** R **Area:** 5714.00 SqFt **PCI:** 9

Sample Comments:

41	ALLIGATOR CR	L	635.00	SqFt
41	ALLIGATOR CR	M	160.00	SqFt
48	L & T CR	L	95.00	Ft
48	L & T CR	M	40.00	Ft
52	RAVELING	M	5679.00	SqFt
52	RAVELING	H	35.00	SqFt
53	RUTTING	L	740.00	SqFt
53	RUTTING	M	280.00	SqFt

Sample Number: 227 **Type:** R **Area:** 5000.00 SqFt **PCI:** 23

Sample Comments:

41	ALLIGATOR CR	L	80.00	SqFt
48	L & T CR	L	115.00	Ft
48	L & T CR	M	15.00	Ft
48	L & T CR	H	7.00	Ft
52	RAVELING	M	5000.00	SqFt
53	RUTTING	L	1250.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW B **Name:** TAXIWAY B **Use:** TAXIWAY **Area:** 111,984 SqFt

Section: 225 of 7 **From:** - **To:** - **Last Const.:** 6/1/2018

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 17,624 SqFt **Length:** 280 Ft **Width:** 40 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 6/1/2018 **Work Type:** New Construction - AC **Code:** NC-AC **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 3 **Surveyed:** 1

Conditions: **PCI:** 89

Inspection Comments:

Sample Number: 151 **Type:** R **Area:** 5163.00 SqFt **PCI:** 89

Sample Comments:

50	PATCHING	L	117.00	SqFt
57	WEATHERING	L	5046.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW F **Name:** TAXIWAY F **Use:** TAXIWAY **Area:** 47,206 SqFt

Section: 610 of 2 **From:** - **To:** - **Last Const.:** 6/1/2018

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 43,041 SqFt **Length:** 590 Ft **Width:** 75 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1984	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1989	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1989	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 6/1/2018	**Work Type:** Complete Reconstruction - AC	**Code:** CR-AC	**Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 8 **Surveyed:** 1

Conditions: **PCI:** 76

Inspection Comments:

Sample Number: 203 **Type:** R **Area:** 4393.00 SqFt **PCI:** 76

Sample Comments:

48	L & T CR	L	28.00	Ft
50	PATCHING	L	468.00	SqFt
57	WEATHERING	L	3925.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW F **Name:** TAXIWAY F **Use:** TAXIWAY **Area:** 47,206 SqFt

Section: 615 of 2 **From:** - **To:** - **Last Const.:** 12/1/2020

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 4,165 SqFt **Length:** 75 Ft **Width:** 50 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1984	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1989	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1989	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 6/1/2018	**Work Type:** Complete Reconstruction - AC	**Code:** CR-AC	**Is Major M&R:** True
Work Date: 12/1/2020	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 1/30/2015 **TotalSamples:** 2 **Surveyed:** 1

Conditions: **PCI:** 41 **NOTE: *** Pre-Construction PCI ***

Inspection Comments:

Sample Number: 200 **Type:** R **Area:** 3644.00 SqFt **PCI:** 41

Sample Comments:

48	LONGITUDINAL/TRANSVERSE CRACKING	L	60.00	Ft
52	RAVELING	L	1093.00	SqFt
52	RAVELING	M	2551.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW G **Name:** TAXIWAY G **Use:** TAXIWAY **Area:** 248,398 SqFt

Section: 1315 of 4 **From:** - **To:** - **Last Const.:** 6/1/2018

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 19,536 SqFt **Length:** 255 Ft **Width:** 65 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 6/1/2018 **Work Type:** New Construction - AC **Code:** NC-AC **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 3 **Surveyed:** 1

Conditions: **PCI:** 69

Inspection Comments:

Sample Number: 101 **Type:** R **Area:** 6469.00 SqFt **PCI:** 69

Sample Comments:

53	RUTTING	L	500.00	SqFt
57	WEATHERING	L	6469.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW G **Name:** TAXIWAY G **Use:** TAXIWAY **Area:** 248,398 SqFt

Section: 1320 of 4 **From:** - **To:** - **Last Const.:** 6/1/2018

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 15,822 SqFt **Length:** 190 Ft **Width:** 90 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1958	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1994	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1994	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 6/1/2018	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 3 **Surveyed:** 1

Conditions: **PCI:** 62

Inspection Comments:

Sample Number: 128 **Type:** R **Area:** 5378.00 SqFt **PCI:** 62

Sample Comments:

42	BLEEDING	N	16.00	SqFt
48	L & T CR	L	197.00	Ft
50	PATCHING	L	1804.00	SqFt
57	WEATHERING	L	3574.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW G **Name:** TAXIWAY G **Use:** TAXIWAY **Area:** 248,398 SqFt

Section: 1325 of 4 **From:** - **To:** - **Last Const.:** 6/1/2018

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 199,036 SqFt **Length:** 3,875 Ft **Width:** 50 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1984	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 6/1/2018	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 1/1/2022	**Work Type:** Patching - AC	**Code:** PA-AC	**Is Major M&R:** False

Last Insp. Date: 4/4/2022 **TotalSamples:** 39 **Surveyed:** 5

Conditions: **PCI:** 65

Inspection Comments:

Sample Number: 106 **Type:** R **Area:** 5041.00 SqFt **PCI:** 67

Sample Comments:

50	PATCHING	L	2200.00	SqFt
57	WEATHERING	L	2841.00	SqFt

Sample Number: 114 **Type:** R **Area:** 5000.00 SqFt **PCI:** 66

Sample Comments:

50	PATCHING	L	2300.00	SqFt
57	WEATHERING	L	2700.00	SqFt

Sample Number: 122 **Type:** R **Area:** 5000.00 SqFt **PCI:** 63

Sample Comments:

48	L & T CR	L	22.00	Ft
50	PATCHING	L	2200.00	SqFt
57	WEATHERING	L	2800.00	SqFt

Sample Number: 132 **Type:** R **Area:** 5000.00 SqFt **PCI:** 67

Sample Comments:

50	PATCHING	L	2200.00	SqFt
57	WEATHERING	L	2800.00	SqFt

Sample Number: 140 **Type:** R **Area:** 5624.00 SqFt **PCI:** 63

Sample Comments:

48	L & T CR	L	42.00	Ft
50	PATCHING	L	2300.00	SqFt
57	WEATHERING	L	3324.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW G **Name:** TAXIWAY G **Use:** TAXIWAY **Area:** 248,398 SqFt

Section: 1340 of 4 **From:** - **To:** - **Last Const.:** 6/1/2018

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 14,004 SqFt **Length:** 150 Ft **Width:** 100 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1958	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1994	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1994	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 6/1/2018	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 3 **Surveyed:** 1

Conditions: **PCI:** 82

Inspection Comments:

Sample Number: 314 **Type:** R **Area:** 4286.00 SqFt **PCI:** 82

Sample Comments:

48	L & T CR	L	176.00	Ft
57	WEATHERING	L	4286.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW G1 **Name:** TAXIWAY G1 **Use:** TAXIWAY **Area:** 25,665 SqFt

Section: 1330 of 2 **From:** - **To:** - **Last Const.:** 6/1/2018

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 13,135 SqFt **Length:** 85 Ft **Width:** 85 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1984	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 6/1/2018	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 1/1/2022	**Work Type:** Patching - AC	**Code:** PA-AC	**Is Major M&R:** False

Last Insp. Date: 4/4/2022 **TotalSamples:** 3 **Surveyed:** 1

Conditions: **PCI:** 65

Inspection Comments:

Sample Number: 103 **Type:** R **Area:** 3485.00 SqFt **PCI:** 65

Sample Comments:

48	L & T CR	L	58.00	Ft
50	PATCHING	L	528.00	SqFt
53	RUTTING	L	99.00	SqFt
57	WEATHERING	L	2957.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW G1 **Name:** TAXIWAY G1 **Use:** TAXIWAY **Area:** 25,665 SqFt

Section: 1335 of 2 **From:** - **To:** - **Last Const.:** 6/1/2018

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 12,530 SqFt **Length:** 150 Ft **Width:** 85 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1984 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Work Date: 6/1/2018 **Work Type:** Mill and Overlay **Code:** ML-OVL **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 3 **Surveyed:** 1

Conditions: **PCI:** 63

Inspection Comments:

Sample Number: 102 **Type:** R **Area:** 4045.00 SqFt **PCI:** 63

Sample Comments:

48	L & T CR	L	45.00	Ft
53	RUTTING	L	350.00	SqFt
57	WEATHERING	L	4045.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW G2 **Name:** TAXIWAY G2 **Use:** TAXIWAY **Area:** 24,807 SqFt

Section: 1005 of 2 **From:** - **To:** - **Last Const.:** 6/1/2018

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 15,843 SqFt **Length:** 125 Ft **Width:** 65 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1984 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Work Date: 6/1/2018 **Work Type:** Mill and Overlay **Code:** ML-OVL **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 3 **Surveyed:** 1

Conditions: **PCI:** 89

Inspection Comments:

Sample Number: 103 **Type:** R **Area:** 5229.00 SqFt **PCI:** 89

Sample Comments:

42	BLEEDING	N	2.00	SqFt
48	L & T CR	L	85.00	Ft
57	WEATHERING	L	5229.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW G2 **Name:** TAXIWAY G2 **Use:** TAXIWAY **Area:** 24,807 SqFt

Section: 1010 of 2 **From:** - **To:** - **Last Const.:** 6/1/2018

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 8,964 SqFt **Length:** 125 Ft **Width:** 65 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1984	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2012	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True
Work Date: 6/1/2018	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 2 **Surveyed:** 1

Conditions: **PCI:** 89

Inspection Comments:

Sample Number: 101 **Type:** R **Area:** 3805.00 SqFt **PCI:** 89

Sample Comments:

48	L & T CR	L	62.00	Ft
57	WEATHERING	L	3805.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW G3 **Name:** TAXIWAY G3 **Use:** TAXIWAY **Area:** 19,662 SqFt

Section: 605 of 2 **From:** - **To:** - **Last Const.:** 1/1/1984

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 10,930 SqFt **Length:** 132 Ft **Width:** 61 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1984 **Work Type:** OVERLAY **Code:** IMPORTED **Is Major M&R:** True

Work Date: 1/1/1984 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 2 **Surveyed:** 1

Conditions: **PCI:** 24

Inspection Comments:

Sample Number: 103 **Type:** R **Area:** 4889.00 SqFt **PCI:** 24

Sample Comments:

41	ALLIGATOR CR	L	45.00	SqFt
43	BLOCK CR	L	430.00	SqFt
43	BLOCK CR	M	1070.00	SqFt
45	DEPRESSION	L	290.00	SqFt
48	L & T CR	M	270.00	Ft
50	PATCHING	L	215.00	SqFt
52	RAVELING	L	1674.00	SqFt
52	RAVELING	M	3000.00	SqFt
53	RUTTING	L	100.00	SqFt
56	SWELLING	L	240.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW G3 **Name:** TAXIWAY G3 **Use:** TAXIWAY **Area:** 19,662 SqFt

Section: 607 of 2 **From:** - **To:** - **Last Const.:** 1/1/2012

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 8,732 SqFt **Length:** 122 Ft **Width:** 61 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1984	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1984	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2012	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 2 **Surveyed:** 1

Conditions: **PCI:** 88

Inspection Comments:

Sample Number: 100 **Type:** R **Area:** 4450.00 SqFt **PCI:** 88

Sample Comments:

48	L & T CR	L	84.00	Ft
57	WEATHERING	L	4450.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW H **Name:** TAXIWAY H **Use:** TAXIWAY **Area:** 122,273 SqFt

Section: 810 of 3 **From:** - **To:** - **Last Const.:** 1/2/1965

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 59,729 SqFt **Length:** 798 Ft **Width:** 75 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1965 **Work Type:** New Construction - AC **Code:** NC-AC **Is Major M&R:** True

Work Date: 1/2/1965 **Work Type:** Overlay - AC Structural **Code:** OL-AS **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 16 **Surveyed:** 3

Conditions: **PCI:** 5

Inspection Comments:

Sample Number: 112 **Type:** R **Area:** 3214.00 SqFt **PCI:** 2

Sample Comments:

41	ALLIGATOR CR	M	1050.00	SqFt
43	BLOCK CR	M	2164.00	SqFt
52	RAVELING	M	3214.00	SqFt
53	RUTTING	L	875.00	SqFt

Sample Number: 117 **Type:** R **Area:** 3750.00 SqFt **PCI:** 6

Sample Comments:

41	ALLIGATOR CR	L	1000.00	SqFt
41	ALLIGATOR CR	M	50.00	SqFt
43	BLOCK CR	M	2700.00	SqFt
52	RAVELING	M	3750.00	SqFt
53	RUTTING	L	1050.00	SqFt

Sample Number: 124 **Type:** R **Area:** 3750.00 SqFt **PCI:** 7

Sample Comments:

41	ALLIGATOR CR	M	218.00	SqFt
43	BLOCK CR	M	3532.00	SqFt
45	DEPRESSION	L	11.00	SqFt
52	RAVELING	M	3733.00	SqFt
52	RAVELING	H	17.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW H **Name:** TAXIWAY H **Use:** TAXIWAY **Area:** 122,273 SqFt

Section: 815 of 3 **From:** - **To:** - **Last Const.:** 8/1/2016

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 57,784 SqFt **Length:** 500 Ft **Width:** 100 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 8/1/2016 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 12 **Surveyed:** 3

Conditions: **PCI:** 87

Inspection Comments:

Sample Number: 103 **Type:** R **Area:** 3697.00 SqFt **PCI:** 91

Sample Comments:

57	WEATHERING	L	3512.00	SqFt
57	WEATHERING	M	185.00	SqFt

Sample Number: 104 **Type:** R **Area:** 3697.00 SqFt **PCI:** 86

Sample Comments:

48	L & T CR	L	50.00	Ft
57	WEATHERING	L	3512.00	SqFt
57	WEATHERING	M	185.00	SqFt

Sample Number: 110 **Type:** R **Area:** 4389.00 SqFt **PCI:** 86

Sample Comments:

48	L & T CR	L	55.00	Ft
57	WEATHERING	L	4170.00	SqFt
57	WEATHERING	M	219.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW H **Name:** TAXIWAY H **Use:** TAXIWAY **Area:** 122,273 SqFt

Section: 820 of 3 **From:** - **To:** - **Last Const.:** 1/1/2017

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 4,760 SqFt **Length:** 75 Ft **Width:** 64 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1965	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/2/1965	**Work Type:** Surface Treatment - Seal Coat	**Code:** ST-SC	**Is Major M&R:** False
Work Date: 1/1/2017	**Work Type:** Complete Reconstruction - AC	**Code:** CR-AC	**Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 1 **Surveyed:** 1

Conditions: **PCI:** 88

Inspection Comments:

Sample Number: 128 **Type:** R **Area:** 4760.00 SqFt **PCI:** 88

Sample Comments:

48	L & T CR	L	31.00	Ft
50	PATCHING	L	1.00	SqFt
57	WEATHERING	L	4759.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW L **Name:** TAXIWAY L **Use:** TAXIWAY **Area:** 87,808 SqFt

Section: 1205 of 3 **From:** - **To:** - **Last Const.:** 8/1/2016

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 22,175 SqFt **Length:** 164 Ft **Width:** 100 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1986 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Work Date: 8/1/2016 **Work Type:** Complete Reconstruction - AC **Code:** CR-AC **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 5 **Surveyed:** 1

Conditions: **PCI:** 77

Inspection Comments:

Sample Number: 205 **Type:** R **Area:** 5000.00 SqFt **PCI:** 77

Sample Comments:

48	L & T CR	L	191.00	Ft
56	SWELLING	L	6.00	SqFt
57	WEATHERING	L	4473.00	SqFt
57	WEATHERING	M	527.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW L **Name:** TAXIWAY L **Use:** TAXIWAY **Area:** 87,808 SqFt

Section: 1215 of 3 **From:** - **To:** - **Last Const.:** 8/1/2016

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 13,483 SqFt **Length:** 150 Ft **Width:** 80 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1986	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1992	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 8/1/2016	**Work Type:** Complete Reconstruction - AC	**Code:** CR-AC	**Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 3 **Surveyed:** 1

Conditions: **PCI:** 76

Inspection Comments:

Sample Number: 202 **Type:** R **Area:** 3122.00 SqFt **PCI:** 76

Sample Comments:

48	L & T CR	L	140.00	Ft
57	WEATHERING	L	2810.00	SqFt
57	WEATHERING	M	312.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW L **Name:** TAXIWAY L **Use:** TAXIWAY **Area:** 87,808 SqFt

Section: 1245 of 3 **From:** - **To:** - **Last Const.:** 8/1/2016

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 52,150 SqFt **Length:** 1,043 Ft **Width:** 50 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1986 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Work Date: 8/1/2016 **Work Type:** Complete Reconstruction - AC **Code:** CR-AC **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 11 **Surveyed:** 2

Conditions: **PCI:** 79

Inspection Comments:

Sample Number: 215 **Type:** R **Area:** 5000.00 SqFt **PCI:** 76

Sample Comments:

48	L & T CR	L	319.00	Ft
56	SWELLING	L	6.00	SqFt
57	WEATHERING	L	5000.00	SqFt

Sample Number: 221 **Type:** R **Area:** 5000.00 SqFt **PCI:** 82

Sample Comments:

48	L & T CR	L	176.00	Ft
56	SWELLING	L	15.00	SqFt
57	WEATHERING	L	4750.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW P **Name:** TAXIWAY P **Use:** TAXIWAY **Area:** 80,078 SqFt

Section: 1250 of 2 **From:** - **To:** - **Last Const.:** 1/1/2016

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 27,739 SqFt **Length:** 415 Ft **Width:** 50 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1986	**Work Type:** New Construction - Initial	**Code:** NU-IN	**Is Major M&R:** True
Work Date: 1/1/2016	**Work Type:** Complete Reconstruction - AC	**Code:** CR-AC	**Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 6 **Surveyed:** 1

Conditions: **PCI:** 85

Inspection Comments:

Sample Number: 302 **Type:** R **Area:** 4768.00 SqFt **PCI:** 85

Sample Comments:

48	L & T CR	L	82.00	Ft
57	WEATHERING	L	4530.00	SqFt
57	WEATHERING	M	238.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW P **Name:** TAXIWAY P **Use:** TAXIWAY **Area:** 80,078 SqFt

Section: 1255 of 2 **From:** - **To:** - **Last Const.:** 1/1/2016

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 52,339 SqFt **Length:** 1,100 Ft **Width:** 50 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1986 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Work Date: 1/1/2016 **Work Type:** Complete Reconstruction - AC **Code:** CR-AC **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 11 **Surveyed:** 2

Conditions: **PCI:** 91

Inspection Comments:

Sample Number: 309 **Type:** R **Area:** 4990.00 SqFt **PCI:** 91

Sample Comments:

57	WEATHERING	L	4740.00	SqFt
57	WEATHERING	M	250.00	SqFt

Sample Number: 313 **Type:** R **Area:** 5000.00 SqFt **PCI:** 91

Sample Comments:

57	WEATHERING	L	4750.00	SqFt
57	WEATHERING	M	250.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW Q **Name:** TAXIWAY Q **Use:** TAXIWAY **Area:** 8,081 SqFt

Section: 1705 of 2 **From:** - **To:** - **Last Const.:** 6/1/2018

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 4,449 SqFt **Length:** 155 Ft **Width:** 30 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1984 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Work Date: 6/1/2018 **Work Type:** Mill and Overlay **Code:** ML-OVL **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 1 **Surveyed:** 1

Conditions: **PCI:** 94

Inspection Comments:

Sample Number: 101 **Type:** R **Area:** 4449.00 SqFt **PCI:** 94

Sample Comments:

57	WEATHERING	L	4449.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW Q **Name:** TAXIWAY Q **Use:** TAXIWAY **Area:** 8,081 SqFt

Section: 1710 of 2 **From:** - **To:** - **Last Const.:** 6/1/2018

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 3,632 SqFt **Length:** 125 Ft **Width:** 25 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 6/1/2018 **Work Type:** New Construction - AC **Code:** NC-AC **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 1 **Surveyed:** 1

Conditions: **PCI:** 94

Inspection Comments:

Sample Number: 100 **Type:** R **Area:** 3632.00 SqFt **PCI:** 94

Sample Comments:

57	WEATHERING	L	3632.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW T **Name:** TAXIWAY T **Use:** TAXIWAY **Area:** 193,289 SqFt

Section: 2010 of 4 **From:** - **To:** - **Last Const.:** 6/1/2018

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 12,963 SqFt **Length:** 173 Ft **Width:** 75 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1960	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1983	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1992	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 6/1/2018	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 3 **Surveyed:** 1

Conditions: **PCI:** 87

Inspection Comments:

Sample Number: 101 **Type:** R **Area:** 3750.00 SqFt **PCI:** 87

Sample Comments:

48	L & T CR	L	90.00	Ft
57	WEATHERING	L	3750.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW T **Name:** TAXIWAY T **Use:** TAXIWAY **Area:** 193,289 SqFt

Section: 2020 of 4 **From:** - **To:** - **Last Const.:** 6/1/2018

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 14,337 SqFt **Length:** 200 Ft **Width:** 75 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1996 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Work Date: 6/1/2018 **Work Type:** Mill and Overlay **Code:** ML-OVL **Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 4 **Surveyed:** 1

Conditions: **PCI:** 86

Inspection Comments:

Sample Number: 107 **Type:** R **Area:** 3750.00 SqFt **PCI:** 86

Sample Comments:

48	L & T CR	L	62.00	Ft
50	PATCHING	L	5.00	SqFt
57	WEATHERING	L	3745.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW T **Name:** TAXIWAY T **Use:** TAXIWAY **Area:** 193,289 SqFt

Section: 2045 of 4 **From:** - **To:** - **Last Const.:** 8/1/2016

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 16,549 SqFt **Length:** 375 Ft **Width:** 50 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1997	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1997	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1997	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 8/1/2016	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 4 **Surveyed:** 1

Conditions: **PCI:** 74

Inspection Comments:

Sample Number: 300 **Type:** R **Area:** 6234.00 SqFt **PCI:** 74

Sample Comments:

45	DEPRESSION	L	25.00	SqFt
48	L & T CR	L	92.00	Ft
48	L & T CR	M	52.00	Ft
57	WEATHERING	L	5954.00	SqFt
57	WEATHERING	M	280.00	SqFt

Network: PIE **Name:** ST. PETE-CLEARWATER INTERNATIONAL AIRPORT

Branch: TW T **Name:** TAXIWAY T **Use:** TAXIWAY **Area:** 193,289 SqFt

Section: 2050 of 4 **From:** - **To:** - **Last Const.:** 6/1/2018

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 149,440 SqFt **Length:** 1,700 Ft **Width:** 94 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1997	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1997	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1997	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 6/1/2018	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 4/4/2022 **TotalSamples:** 32 **Surveyed:** 5

Conditions: **PCI:** 90

Inspection Comments:

Sample Number: 101 **Type:** R **Area:** 4096.00 SqFt **PCI:** 91

Sample Comments:

48	L & T CR	L	7.00	Ft
57	WEATHERING	L	4096.00	SqFt

Sample Number: 102 **Type:** R **Area:** 3995.00 SqFt **PCI:** 90

Sample Comments:

48	L & T CR	L	19.00	Ft
57	WEATHERING	L	3995.00	SqFt

Sample Number: 111 **Type:** R **Area:** 4700.00 SqFt **PCI:** 94

Sample Comments:

57	WEATHERING	L	4700.00	SqFt

Sample Number: 207 **Type:** R **Area:** 4728.00 SqFt **PCI:** 94

Sample Comments:

57	WEATHERING	L	4728.00	SqFt

Sample Number: 212 **Type:** R **Area:** 4700.00 SqFt **PCI:** 81

Sample Comments:

48	L & T CR	L	3.00	Ft
50	PATCHING	L	296.00	SqFt
57	WEATHERING	L	4404.00	SqFt

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