FLORIDA DEPARTMENT OF TRANSPORTATION | **AVIATION OFFICE**

2022

Statewide Airfield Pavement Management Program

Airport Pavement Evaluation Report

TLH - Tallahassee International Airport | *District 3*

FDOT AVIATION

Florida Department of Transportation

Statewide Airfield Pavement Management Program

Airport Pavement Evaluation Report

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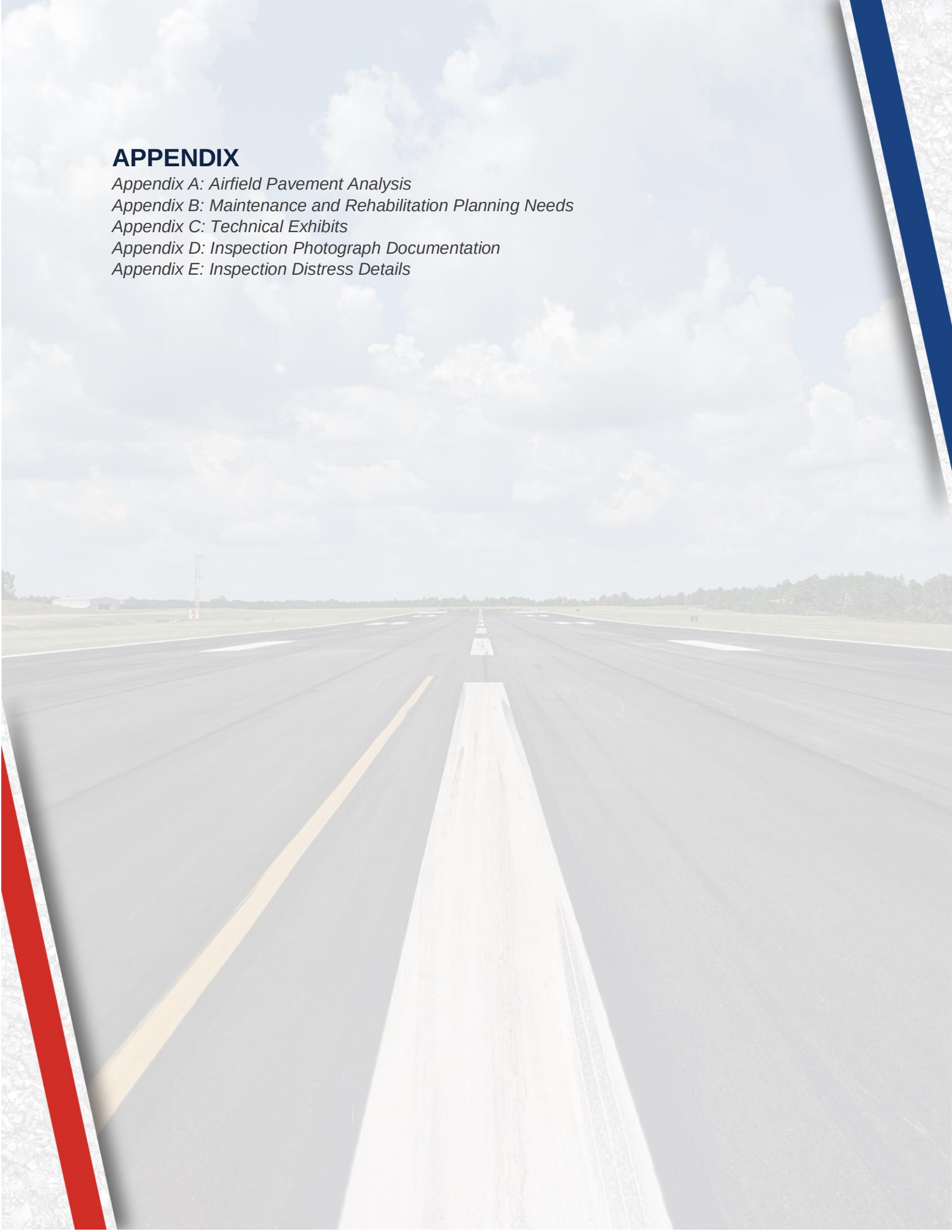

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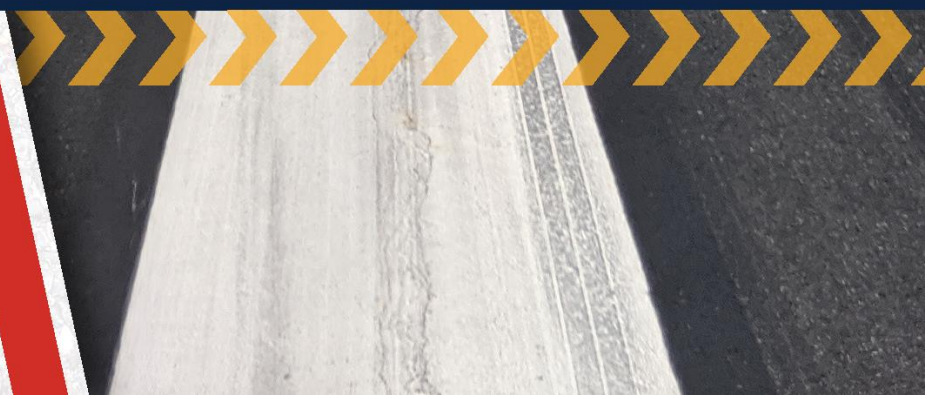


Executive Summary

Program Background

The FDOT Aviation Office (AO) has a mission to provide a safe and secure air transportation system that ensures the mobility of people and goods, enhances economic prosperity, and preserves the quality of our environment and communities. As part of ongoing efforts in fulfilling this mission, the Aviation Office is executing a System Update to the Statewide Airfield Pavement Management Program (SAPMP). The scope of the SAPMP encompasses 95 public-use airport facilities distributed throughout the seven (7) participating FDOT Districts. Tallahassee International Airport's System Update results are presented in this report and can be utilized by FDOT and the Federal Aviation Administration (FAA) to identify, prioritize, and schedule pavement maintenance, repair, and major rehabilitation projects.

Pavement condition was assessed utilizing the pavement condition index (PCI) methodology as defined in FAA Advisory Circular 150/5380-7B "Airport Pavement Management Program (PMP)" using the procedures documented in ASTM D5340-20 **"**Standard Test Method for Airport Pavement Condition Index Surveys".

The PCI methodology provides a means for systematically assessing pavement condition and provides an indication of the degree of maintenance, repair, rehabilitation, or reconstruction efforts required to sustain functional pavement conditions. Pavement deterioration, in accordance with ASTM D5340-20, is characterized in terms of distinct distress types, distress severity levels, and quantity of distress. This information is utilized to calculate a PCI value ranging from 0 to 100, which provides an indication of the overall condition of the pavement, with "100" indicating a pavement in new condition and "0" indicating a failed pavement section. This is graphically depicted in **Figure E.1**.

Figure E.1: PCI Rating

Color	Range	Condition Rating
	86-100	Good
	71-85	Satisfactory
	56-70	Fair
	41-55	Poor
	26-40	Very Poor
	11-25	Serious
	0-10	Failed

Current Pavement Conditions

In December 2021, approximately 8.4 million square feet of pavement was assessed as part of the airside pavement network PCI survey at Tallahassee International Airport (TLH). In general, airfield pavements at TLH are in Satisfactory condition with an area-weighted PCI of 81. The area-weighted average PCI values of the runways, taxiways, and aprons are 94, 71, and 81, respectively. **Figure E.2** and **Table E.1** summarize the current PCI values for TLH.

Figure E.2: Current Condition Summary – Branch-Level

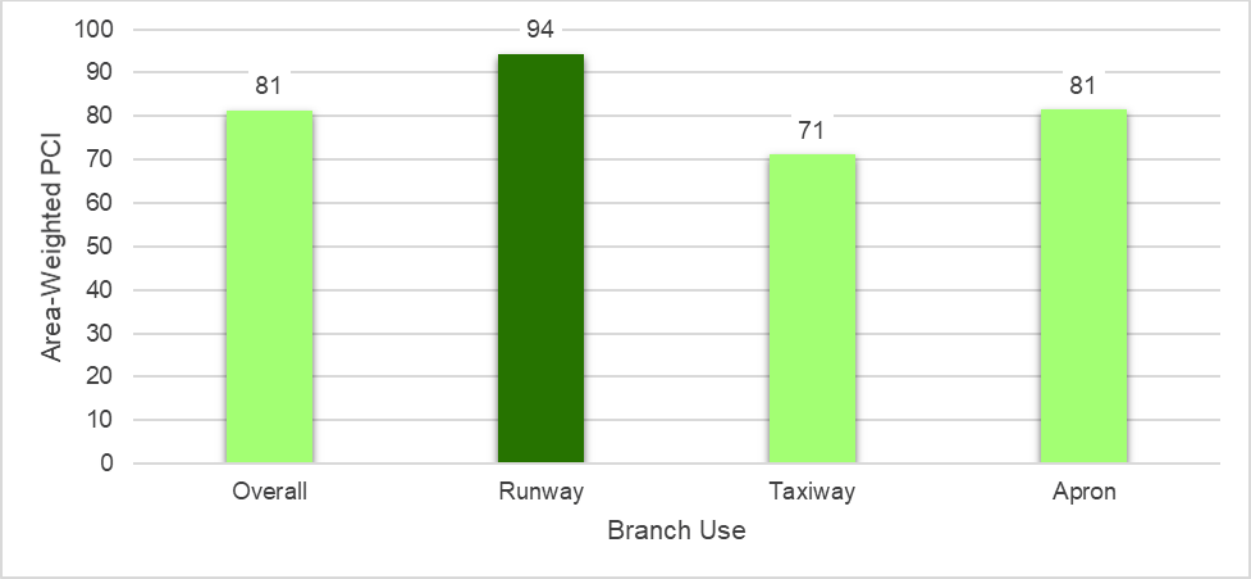


Table E.1: Pavement Condition Index Summary (Current PCI Survey) – Section Level

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	PCI	Condition Rating
TLH	RW 9-27	Runway	6205	400,000	88	Good
TLH	RW 9-27	Runway	6210	800,000	90	Good
TLH	RW 18-36	Runway	6105	607,550	100	Good
TLH	RW 18-36	Runway	6110	303,775	100	Good
TLH	RW 18-36	Runway	6125	63,750	100	Good
TLH	RW 18-36	Runway	6130	31,875	100	Good
TLH	RW 18-36	Runway	6155	28,700	100	Good
TLH	RW 18-36	Runway	6160	14,350	100	Good
TLH	TL AP S	Taxiway	3205	6,963	65	Fair
TLH	TL T-HANG	Taxiway	3105	46,227	62	Fair
TLH	TL T-HANG	Taxiway	3110	16,646	52	Poor
TLH	TL T-HANG	Taxiway	3115	63,002	46	Poor
TLH	TW A	Taxiway	103	79,944	100	Good
TLH	TW A	Taxiway	105	243,781	100	Good
TLH	TW A	Taxiway	106	215,250	61	Fair
TLH	TW A	Taxiway	107	23,925	67	Fair
TLH	TW A1	Taxiway	110	40,291	64	Fair

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	PCI	Condition Rating
TLH	TW A10	Taxiway	170	22,422	100	Good
TLH	TW A10	Taxiway	175	4,954	70	Fair
TLH	TW A11	Taxiway	180	24,154	100	Good
TLH	TW A12	Taxiway	185	43,156	100	Good
TLH	TW A2	Taxiway	115	42,179	70	Fair
TLH	TW A3	Taxiway	125	32,329	61	Fair
TLH	TW A3	Taxiway	130	34,919	67	Fair
TLH	TW A4	Taxiway	140	19,805	54	Poor
TLH	TW A7	Taxiway	150	72,118	100	Good
TLH	TW A8	Taxiway	155	43,518	100	Good
TLH	TW A8	Taxiway	160	11,115	100	Good
TLH	TW A9	Taxiway	165	51,254	100	Good
TLH	TW B	Taxiway	203	50,342	74	Satisfactory
TLH	TW B	Taxiway	205	581,353	50	Poor
TLH	TW B	Taxiway	207	15,151	100	Good
TLH	TW B	Taxiway	209	30,178	100	Good
TLH	TW B1	Taxiway	210	46,292	54	Poor
TLH	TW B1	Taxiway	215	4,782	87	Good
TLH	TW B2	Taxiway	220	49,156	87	Good
TLH	TW B3	Taxiway	230	63,794	90	Good
TLH	TW B3	Taxiway	235	83,567	76	Satisfactory
TLH	TW B4	Taxiway	240	48,156	76	Satisfactory
TLH	TW B5	Taxiway	250	24,545	43	Poor
TLH	TW B6	Taxiway	260	38,862	84	Satisfactory
TLH	TW B6	Taxiway	265	17,002	59	Fair
TLH	TW B6	Taxiway	267	24,158	52	Poor
TLH	TW B7	Taxiway	270	39,535	85	Satisfactory
TLH	TW B7	Taxiway	271	23,946	83	Satisfactory
TLH	TW B7	Taxiway	273	38,359	62	Fair
TLH	TW B7	Taxiway	275	9,455	53	Poor
TLH	TW B7	Taxiway	277	8,669	69	Fair
TLH	TW B8	Taxiway	280	66,948	66	Fair
TLH	TW B8	Taxiway	285	58,220	78	Satisfactory
TLH	TW B9	Taxiway	290	20,199	83	Satisfactory
TLH	TW B9	Taxiway	295	84,260	55	Poor
TLH	TW C	Taxiway	303	37,868	100	Good
TLH	TW C	Taxiway	305	53,314	100	Good
TLH	TW C	Taxiway	307	10,756	65	Fair
TLH	TW C	Taxiway	310	160,476	51	Poor
TLH	TW C	Taxiway	315	55,835	69	Fair
TLH	TW D	Taxiway	405	33,610	69	Fair
TLH	TW D	Taxiway	410	10,157	67	Fair
TLH	TW Z	Taxiway	2605	62,575	73	Satisfactory
TLH	TW Z	Taxiway	2610	2,379	42	Poor
TLH	TW Z	Taxiway	2615	2,615	70	Fair
TLH	AP C	Apron	4505	265,932	74	Satisfactory
TLH	AP CARGO	Apron	4205	65,663	84	Satisfactory

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	PCI	Condition Rating
TLH	AP CARGO	Apron	4210	400,242	74	Satisfactory
TLH	AP CARGO	Apron	4215	18,250	79	Satisfactory
TLH	AP HELI	Apron	4340	17,496	95	Good
TLH	AP HELI	Apron	4345	50,224	98	Good
TLH	AP N	Apron	4405	77,291	80	Satisfactory
TLH	AP N	Apron	4410	215,063	71	Satisfactory
TLH	AP N	Apron	4415	310,550	72	Satisfactory
TLH	AP N	Apron	4420	24,514	79	Satisfactory
TLH	AP N	Apron	4425	9,973	75	Satisfactory
TLH	AP RU 18	Apron	5505	25,207	64	Fair
TLH	AP S	Apron	4305	70,348	91	Good
TLH	AP S	Apron	4310	179,279	95	Good
TLH	AP S	Apron	4313	11,875	98	Good
TLH	AP S	Apron	4315	60,505	96	Good
TLH	AP S	Apron	4320	68,878	97	Good
TLH	AP S	Apron	4325	4,183	98	Good
TLH	AP S	Apron	4332	401,224	96	Good
TLH	AP TERM	Apron	4105	855,384	80	Satisfactory
TLH	AP TERM	Apron	4110	13,317	49	Poor

Forecasted Pavement Conditions

Table E.2 provides section-level details for PCI forecasts. Pavement condition forecasts should be used for planning purposes only, as the actual condition of sections is subject to sensitivities in changes of traffic and maintenance frequency.

The estimation of forecasted PCI values gives no assurance of future pavement conditions as PCI values represent an engineering estimation to be used as a planning tool. Forecasted PCI data should not be the sole metric for determining the year in which a project should be planned. Design-level planning should be undertaken by the responsible engineer prior to the development of airfield design plans.

Table E.2: Forecasted PCI Values 2023-2032 – Section-Level

Network ID	Branch ID	Section ID	Current PCI	2023	2024	2025	2026	2027	2028	2029	2030	2031	2032
TLH	RW 9-27	6205	88	86	84	83	81	80	78	77	75	74	72
TLH	RW 9-27	6210	90	88	86	85	83	82	80	79	77	76	74
TLH	RW 18-36	6105	100	99	98	96	95	93	92	90	89	87	86
TLH	RW 18-36	6110	100	99	98	96	95	93	92	90	89	87	86
TLH	RW 18-36	6125	100	99	97	95	93	91	89	87	86	84	82
TLH	RW 18-36	6130	100	99	97	95	93	91	89	87	86	84	82
TLH	RW 18-36	6155	100	99	97	95	93	91	89	87	86	84	82
TLH	RW 18-36	6160	100	99	97	95	93	91	89	87	86	84	82
TLH	TL AP S	3205	65	63	61	60	59	58	57	56	55	54	54
TLH	TL T-HANG	3105	62	61	60	59	58	57	57	56	55	54	53
TLH	TL T-HANG	3110	52	50	49	48	47	46	45	43	42	41	39

Network ID	Branch ID	Section ID	Current PCI	2023	2024	2025	2026	2027	2028	2029	2030	2031	2032
TLH	TL T-HANG	3115	46	44	43	41	40	38	36	35	33	31	29
TLH	TW A	103	100	99	96	94	91	89	87	84	82	80	78
TLH	TW A	105	100	99	96	94	91	89	87	84	82	80	78
TLH	TW A	106	61	60	59	58	57	56	56	55	54	53	52
TLH	TW A	107	67	65	65	64	63	62	61	60	59	59	58
TLH	TW A1	110	64	63	62	61	60	59	58	58	57	56	55
TLH	TW A10	170	100	99	97	94	92	90	88	86	85	83	81
TLH	TW A10	175	70	68	67	66	65	64	64	63	62	61	60
TLH	TW A11	180	100	99	96	94	91	89	87	84	82	80	78
TLH	TW A12	185	100	99	96	94	91	89	87	84	82	80	78
TLH	TW A2	115	70	68	67	66	65	64	64	63	62	61	60
TLH	TW A3	125	61	60	59	58	57	56	56	55	54	53	52
TLH	TW A3	130	67	65	65	64	63	62	61	60	59	59	58
TLH	TW A4	140	54	53	52	51	50	49	47	46	45	44	42
TLH	TW A7	150	100	99	96	94	91	89	87	84	82	80	78
TLH	TW A8	155	100	99	96	94	91	89	87	84	82	80	78
TLH	TW A8	160	100	99	96	94	91	89	87	84	82	80	78
TLH	TW A9	165	100	99	97	94	92	90	88	86	85	83	81
TLH	TW B	203	74	72	71	70	69	68	67	66	65	64	63
TLH	TW B	205	50	48	47	46	45	43	42	41	39	37	36
TLH	TW B	207	100	99	96	94	91	89	87	84	82	80	78
TLH	TW B	209	100	99	96	94	91	89	87	84	82	80	78
TLH	TW B1	210	54	53	52	51	50	49	47	46	45	44	42
TLH	TW B1	215	87	84	83	81	79	78	76	75	74	73	71
TLH	TW B2	220	87	84	83	81	79	78	76	75	74	73	71
TLH	TW B3	230	90	87	85	83	82	80	79	77	76	75	73
TLH	TW B3	235	76	74	73	72	70	69	68	67	66	65	64
TLH	TW B4	240	76	74	73	72	70	69	68	67	66	65	64
TLH	TW B5	250	43	41	39	38	36	34	32	30	28	26	24
TLH	TW B6	260	84	81	80	78	77	76	74	73	72	71	70
TLH	TW B6	265	59	58	57	56	55	54	53	52	52	51	50
TLH	TW B6	267	52	50	49	48	47	46	45	43	42	41	39
TLH	TW B7	270	85	82	81	79	78	76	75	74	72	71	70
TLH	TW B7	271	83	80	79	77	76	75	74	72	71	70	69
TLH	TW B7	273	62	61	60	59	58	57	57	56	55	54	53
TLH	TW B7	275	53	52	51	51	50	50	49	48	48	47	46
TLH	TW B7	277	69	66	65	63	62	61	60	59	57	57	56
TLH	TW B8	280	66	65	64	63	62	61	60	59	59	58	57
TLH	TW B8	285	78	76	74	73	72	71	70	69	68	67	66
TLH	TW B9	290	83	80	79	77	76	75	74	72	71	70	69
TLH	TW B9	295	55	54	53	52	51	50	49	48	46	45	44
TLH	TW C	303	100	99	96	94	91	89	87	84	82	80	78
TLH	TW C	305	100	99	96	94	91	89	87	84	82	80	78
TLH	TW C	307	65	63	61	60	59	58	57	56	55	54	54
TLH	TW C	310	51	50	49	49	48	48	47	46	45	44	43
TLH	TW C	315	69	66	65	63	62	61	60	59	57	57	56

Network ID	Branch ID	Section ID	Current PCI	2023	2024	2025	2026	2027	2028	2029	2030	2031	2032
TLH	TW D	405	69	67	66	65	65	64	63	62	61	60	59
TLH	TW D	410	67	65	65	64	63	62	61	60	59	59	58
TLH	TW Z	2605	73	71	70	69	68	67	66	65	64	63	62
TLH	TW Z	2610	42	40	38	36	35	33	31	29	27	25	23
TLH	TW Z	2615	70	68	67	66	65	64	64	63	62	61	60
TLH	AP C	4505	74	71	70	68	66	65	63	61	60	58	56
TLH	AP CARGO	4205	84	81	80	78	76	75	73	71	70	68	66
TLH	AP CARGO	4210	74	71	70	68	66	65	63	61	60	58	56
TLH	AP CARGO	4215	79	78	78	77	77	76	76	75	74	74	73
TLH	AP HELI	4340	95	93	92	91	90	89	89	88	87	86	85
TLH	AP HELI	4345	98	95	94	92	90	89	87	85	84	82	80
TLH	AP N	4405	80	77	74	72	71	69	67	65	64	62	61
TLH	AP N	4410	71	68	66	65	63	62	60	59	57	56	55
TLH	AP N	4415	72	69	67	66	64	63	61	60	58	57	55
TLH	AP N	4420	79	76	74	72	70	68	66	65	63	62	60
TLH	AP N	4425	75	72	71	69	67	66	64	62	61	59	57
TLH	AP RU 18	5505	64	61	60	58	56	55	53	51	50	48	46
TLH	AP S	4305	91	87	84	82	79	77	75	73	71	69	68
TLH	AP S	4310	95	90	88	85	83	80	78	76	74	72	70
TLH	AP S	4313	98	96	95	94	93	92	91	90	89	88	87
TLH	AP S	4315	96	91	88	86	83	81	79	76	74	72	71
TLH	AP S	4320	97	92	89	87	84	82	79	77	75	73	71
TLH	AP S	4325	98	96	95	94	93	92	91	90	89	88	87
TLH	AP S	4332	96	93	92	90	88	87	85	83	82	80	78
TLH	AP TERM	4105	80	79	79	78	78	77	77	76	75	75	74
TLH	AP TERM	4110	49	47	45	44	42	40	39	37	35	33	31

Major Rehabilitation Planning 2023-2032

Localized maintenance and repair policies identified within this report are categorized as preventive or stopgap based on FDOT SAPMP and FAA maintenance policies and recommendations. Major rehabilitation is identified within the FDOT SAPMP as a major construction activity that results in a reset of a pavement section's PCI to a value of 100. Major rehabilitation activities can include mill and Asphalt Concrete (AC) overlay, Portland cement concrete (PCC) pavement repair and slab replacement, and full-depth reconstruction. It is recommended that the Airport use this report as a planning tool for future project development and prioritization. Localized maintenance, repair, and major rehabilitation recommendations should be considered as planning-level only. Final localized maintenance, repair, and major rehabilitation recommendations are subject to change based on Airport prioritization and further design-level evaluations.

Due to FAA Order 5100.38D Change 1 Airport Improvement Program (AIP) Handbook (February 26, 2019), a substantial update to the FDOT SAPMP policy on identifying major rehabilitation work has been incorporated in this System Update. In previous System Updates, major rehabilitation had been identified for pavement sections below a PCI Value of 65; however, based on the thresholds identified by the FAA in the AIP Handbook, major rehabilitation will now be identified for pavement sections below a PCI value of 70.

The results of the maintenance, repair, and major rehabilitation analysis identified approximately $74.97M in major rehabilitation needs for the 10-year forecast period. Year 1 major needs are $49.83M and localized maintenance needs for Year 1 are $0.78M.

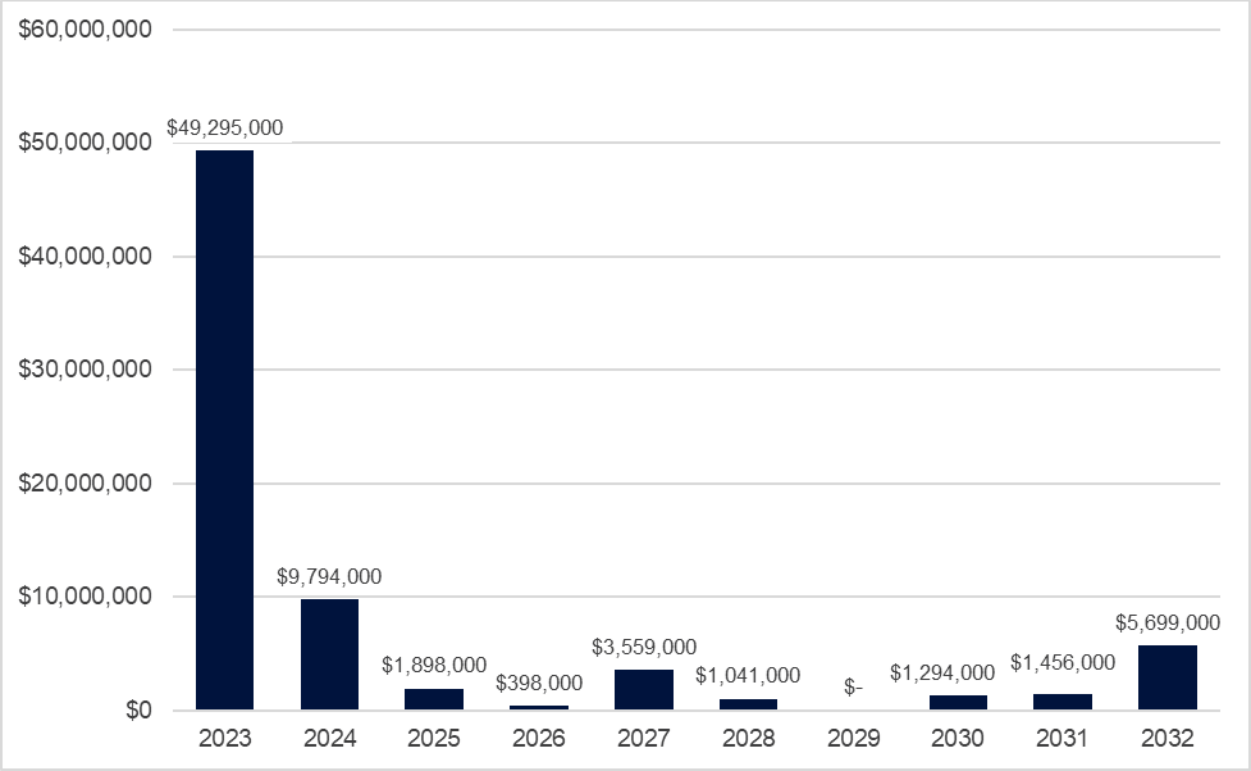
Table E.3: Major Rehabilitation Planning 2023-2032

Program Year	Network ID	Branch ID	Section ID	Surface	Area (SF)	PCI Before	Rehabilitation Type	Planning Cost Estimate
2023	TLH	TL AP S	3205	AAC	6,963	63	AC Rehabilitation	$ 98,000
2023	TLH	TL T-HANG	3105	AC	46,227	61	AC Rehabilitation	$ 648,000
2023	TLH	TL T-HANG	3110	AC	16,646	50	AC Reconstruction	$ 508,000
2023	TLH	TL T-HANG	3115	AC	63,002	44	AC Reconstruction	$ 1,922,000
2023	TLH	TW A	106	AC	215,250	60	AC Rehabilitation	$ 3,014,000
2023	TLH	TW A	107	AC	23,925	65	AC Rehabilitation	$ 335,000
2023	TLH	TW A1	110	AC	40,291	63	AC Rehabilitation	$ 565,000
2023	TLH	TW A10	175	AC	4,954	68	AC Rehabilitation	$ 70,000
2023	TLH	TW A2	115	AC	42,179	68	AC Rehabilitation	$ 591,000
2023	TLH	TW A3	125	AC	32,329	60	AC Rehabilitation	$ 453,000
2023	TLH	TW A3	130	AC	34,919	65	AC Rehabilitation	$ 489,000
2023	TLH	TW A4	140	AC	19,805	53	AC Reconstruction	$ 605,000
2023	TLH	TW B	205	AC	581,353	48	AC Reconstruction	$ 17,732,000
2023	TLH	TW B1	210	AC	46,292	53	AC Reconstruction	$ 1,412,000
2023	TLH	TW B5	250	AC	24,545	41	AC Reconstruction	$ 749,000
2023	TLH	TW B6	265	AC	17,002	58	AC Rehabilitation	$ 239,000
2023	TLH	TW B6	267	AC	24,158	50	AC Reconstruction	$ 737,000
2023	TLH	TW B7	273	AC	38,359	61	AC Rehabilitation	$ 538,000
2023	TLH	TW B7	275	AAC	9,455	52	AC Reconstruction	$ 289,000

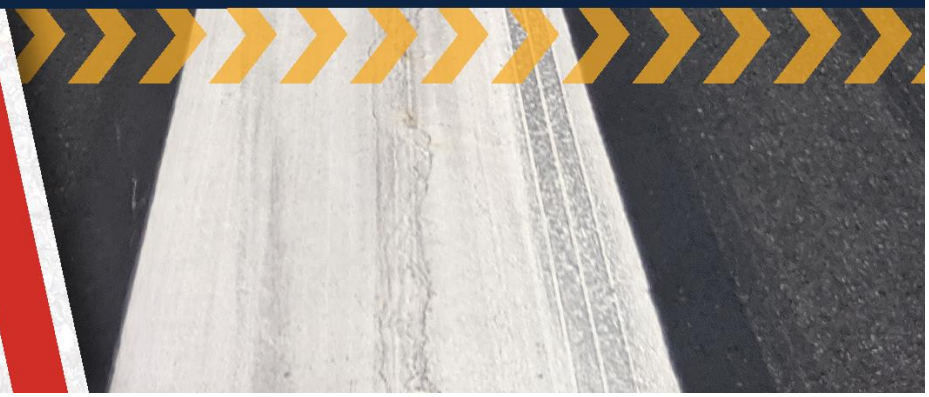
Program Year	Network ID	Branch ID	Section ID	Surface	Area (SF)	PCI Before	Rehabilitation Type	Planning Cost Estimate
2023	**TLH**	TW B7	277	AAC	8,669	66	AC Rehabilitation	$ 122,000
2023	**TLH**	TW B8	280	AC	66,948	65	AC Rehabilitation	$ 938,000
2023	**TLH**	TW B9	295	AC	84,260	54	AC Reconstruction	$ 2,570,000
2023	**TLH**	TW C	307	AAC	10,756	63	AC Rehabilitation	$ 151,000
2023	**TLH**	TW C	310	AAC	160,476	50	AC Reconstruction	$ 4,895,000
2023	**TLH**	TW C	315	AAC	55,835	66	AC Rehabilitation	$ 782,000
2023	**TLH**	TW D	405	AC	33,610	67	AC Rehabilitation	$ 471,000
2023	**TLH**	TW D	410	AC	10,157	65	AC Rehabilitation	$ 143,000
2023	**TLH**	TW Z	2610	AC	2,379	40	AC Reconstruction	$ 73,000
2023	**TLH**	TW Z	2615	AC	2,615	68	AC Rehabilitation	$ 37,000
2023	**TLH**	AP N	4410	AAC	215,063	68	AC Rehabilitation	$ 3,011,000
2023	**TLH**	AP N	4415	APC	310,550	69	AC Rehabilitation	$ 4,348,000
2023	**TLH**	AP RU 18	5505	AC	25,207	61	AC Rehabilitation	$ 353,000
2023	**TLH**	AP TERM	4110	APC	13,317	47	AC Reconstruction	$ 407,000
2024	**TLH**	AP C	4505	AC	265,932	70	AC Rehabilitation	$ 3,910,000
2024	**TLH**	AP CARGO	4210	AC	400,242	70	AC Rehabilitation	$ 5,884,000
2025	**TLH**	TW B	203	AC	50,342	70	AC Rehabilitation	$ 778,000
2025	**TLH**	TW Z	2605	AC	62,575	69	AC Rehabilitation	$ 966,000
2025	**TLH**	AP N	4425	AC	9,973	69	AC Rehabilitation	$ 154,000
2026	**TLH**	AP N	4420	APC	24,514	70	AC Rehabilitation	$ 398,000
2027	**TLH**	TW B3	235	AC	83,567	69	AC Rehabilitation	$ 1,423,000
2027	**TLH**	TW B4	240	AC	48,156	69	AC Rehabilitation	$ 820,000
2027	**TLH**	AP N	4405	AAC	77,291	69	AC Rehabilitation	$ 1,316,000
2028	**TLH**	TW B8	285	AC	58,220	70	AC Rehabilitation	$ 1,041,000
2030	**TLH**	AP CARGO	4205	AC	65,663	70	AC Rehabilitation	$ 1,294,000
2031	**TLH**	AP S	4305	AAC	70,348	69	AC Rehabilitation	$ 1,456,000
2032	**TLH**	TW B6	260	AC	38,862	70	AC Rehabilitation	$ 845,000
2032	**TLH**	TW B7	271	AC	23,946	69	AC Rehabilitation	$ 521,000
2032	**TLH**	TW B9	290	AC	20,199	69	AC Rehabilitation	$ 439,000
2032	**TLH**	AP S	4310	AAC	179,279	70	AC Rehabilitation	$ 3,894,000

**All planning cost values have been rounded up to the nearest thousand dollars.*

Figure E.3: 10-Year Major Rehabilitation Needs by Program Year



Chapter 1: Introduction



Chapter 1 – Introduction

The State of Florida has 128 public airports, 100 of which are recognized as part of the Federal Aviation Administration's (FAA) National Plan of Integrated Airport Systems (NPIAS). These public-use airports are vital to Florida's economy as well as the economy of the United States. The Florida Airport System (FAS) provides opportunities for the State to capitalize on an increasingly global marketplace. Florida's system of commercial service and general aviation airports are important to businesses throughout the State as air travel is essential to tourism, Florida's most prominent industry.

1.1 Background

In 1992, the Florida Department of Transportation (FDOT) established the Statewide Airfield Pavement Management Program (SAPMP) to provide program managers, District Aviation Offices, and Airport operators with a system to proactively manage airfield pavement infrastructure within the FAS. The SAPMP includes network-level Pavement Condition Index (PCI) surveys for Airport facilities that are categorized as General Aviation (GA), Reliever (RL), and Primary/Commercial (PR). Currently, the SAPMP includes 95 participating public-use airports with pavement facilities and provides its users with comprehensive data to better manage their pavement assets.

There are millions of square feet of pavement infrastructure at airports across a network of runways, taxiways, aprons, and other areas. This pavement infrastructure is vital to the support and safety of aircraft operations. Timely maintenance, repair, and major rehabilitation of pavement infrastructure allows the Airport to operate safely, efficiently, and economically without excessive down time.

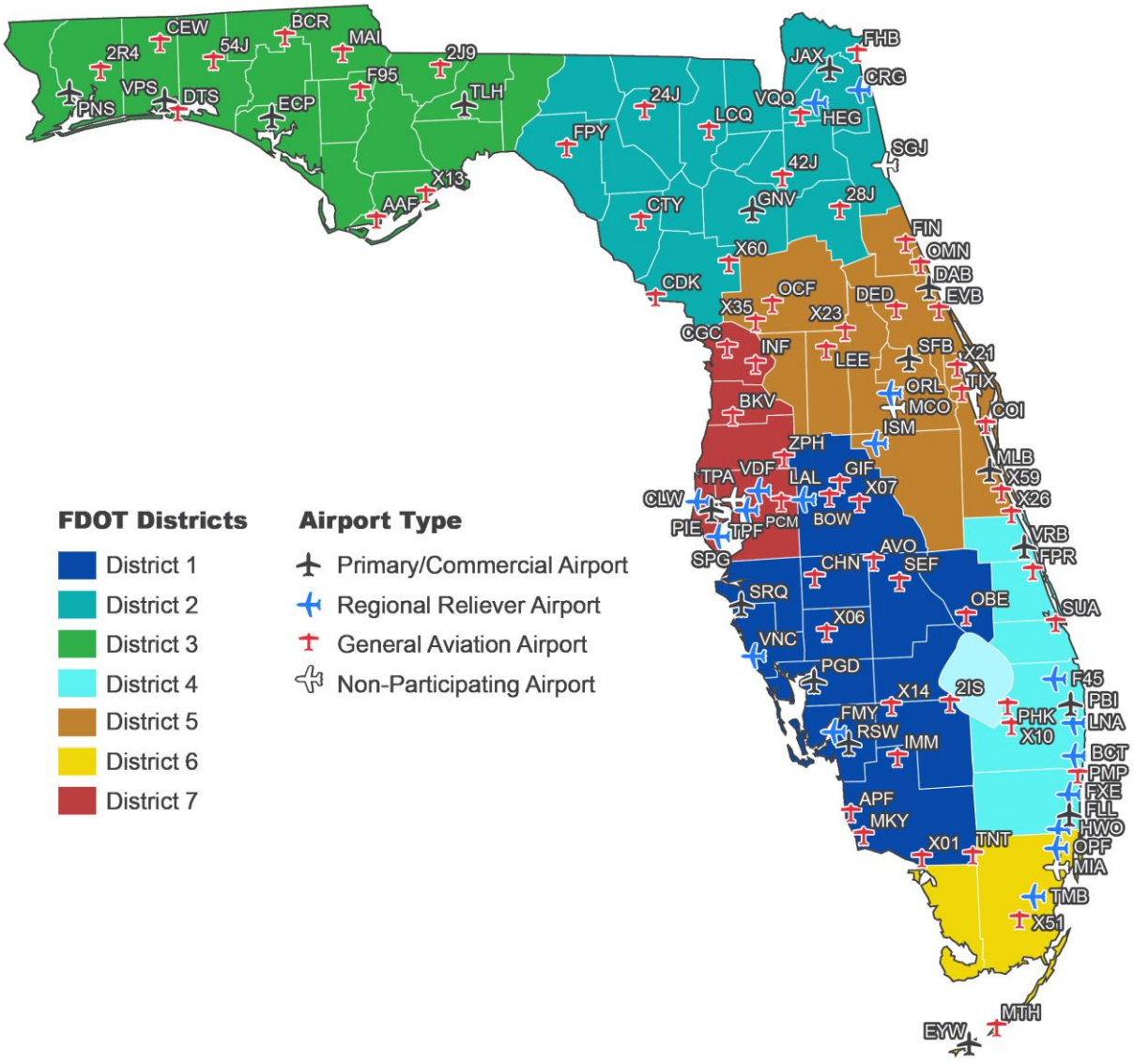
Airports participating in the Airport Improvement Program (AIP) Grant Program are required by the FAA to develop and implement a pavement maintenance program in order to be eligible for funding, per FAA Advisory Circulars 150/5380-6C "Guidelines and Procedures for Maintenance of Airport Pavements" and 150/5380-7B "Airport Pavement Management Program (PMP)". The AIP program requires detailed assessments of airfield pavements at least once a year for a pavement management program. The frequency of the detailed inspections may be extended to every three years if the pavement is assessed according to the PCI survey procedure described in ASTM D5340-20 "Standard Test Method for Airport Pavement Condition Index Surveys".

In general, adherence to the FAA Advisory Circulars is mandatory for projects funded with federal grant monies through the AIP program and with revenue from the Passenger Facilities Charges (PFC) Program. Further information is detailed in FAA Grant Assurance No. 11 "Pavement Maintenance," No. 34 "Policies, Standards, and Specifications," and PFC Assurance No. 9 "Standards and Specifications." The FDOT performs the SAPMP System Updates for the benefit of participating public-use and publicly-owned airports through the Aviation Office (AO).

The SAPMP addresses the requirements of maintaining an effective pavement management program for participating airports at the network level. Network-level management of pavement assets provides insight for short-term and long-term budget needs, understanding of the overall condition of the network (current and future), and knowledge of the pavement facilities that are

under consideration for projects. A network-level evaluation can support the identification of maintenance, repair, and major rehabilitation needs and budgetary planning-level opinions of probable construction costs.

Figure 1.1: Florida Aviation System (Facilities with Pavement) and FDOT Districts



1.2 Stakeholders

The SAPMP is performed for the benefit of the stakeholders. The table below outlines the primary stakeholders of the FDOT SAPMP and their role in the program.

Table 1.2: FDOT SAPMP Stakeholders

Role	Description
FAA Orlando Airports District Office (Orlando ADO)	Key Stakeholder: local ADO Program Manager personnel that oversees the grant administration of AIP grant with Planning Agency Sponsor (Florida Department of Transportation).
Florida Department of Transportation (FDOT)	Key Stakeholder: the FDOT is the "Sponsor" for the AIP grant agreement. Specifically, the Aviation Office (AO) provides development and operations support for the Florida Airport System.
FDOT District Offices	The seven (7) FDOT District Offices, specifically the Aviation representatives, provide essential support to the SAPMP System Update and the AO Program Manager (AO-PM). Each District supports the SAPMP's ongoing efforts by providing local construction cost information throughout the State, which is used as the basis of development for maintenance, repair, and major rehabilitation opinions of probable construction costs for planning purposes.
Participating Public-Use and Publicly-Owned Airports	The airports are the end-user and primary beneficiary of the SAPMP. The SAPMP provides a specific Airport Pavement Evaluation Report that meets the requirements of the FAA AC 150/5380-7B. Individual participating airports are provided a final Airport Pavement Evaluation Report by the Consultant that is specific to each airport's airfield PCI assessment.
Aviation Office Program Manager (AO-PM)	FDOT AO Airport Engineering Manager: oversees and manages the overall Program System Update.

1.3 General Scope of Work

The SAPMP is limited to performing tasks in adherence to the key elements of an effective pavement management program on a statewide level. The primary tasks undertaken to update the FDOT SAPMP include, but are not limited to:

- Research and evaluation of existing record documentation;
- Establishment of a pavement system inventory;
- Development of a pavement network definition map and supplemental GIS model;
- Functional pavement evaluations via the PCI assessment method;
- Customization of PAVER™ software including prioritization, policies, and performance models;
- Analysis of condition data; and
- Maintenance, repair, and rehabilitation planning.

1.4 FDOT SAPMP Objectives

The SAPMP enables the FDOT AO and FAA to monitor pavement conditions at airports in the Florida Airport System. The SAPMP provides objective condition information needed to make informed decisions regarding the significant capital investment that the public-use airport pavement infrastructure represents.

Airport staff are responsible for making decisions regarding the timing and type of maintenance and rehabilitation activities that should be completed in order to maintain an acceptable operational condition and adequate load-carrying capacity. Utilizing the SAPMP will help Airport staff better understand the relative condition of their pavement facilities and when those facilities should be rehabilitated. The data collected from the SAPMP can be used for project programming for the next 10 years. This report summarizes the data collection, analysis, program update, and implementation of the FDOT SAPMP.

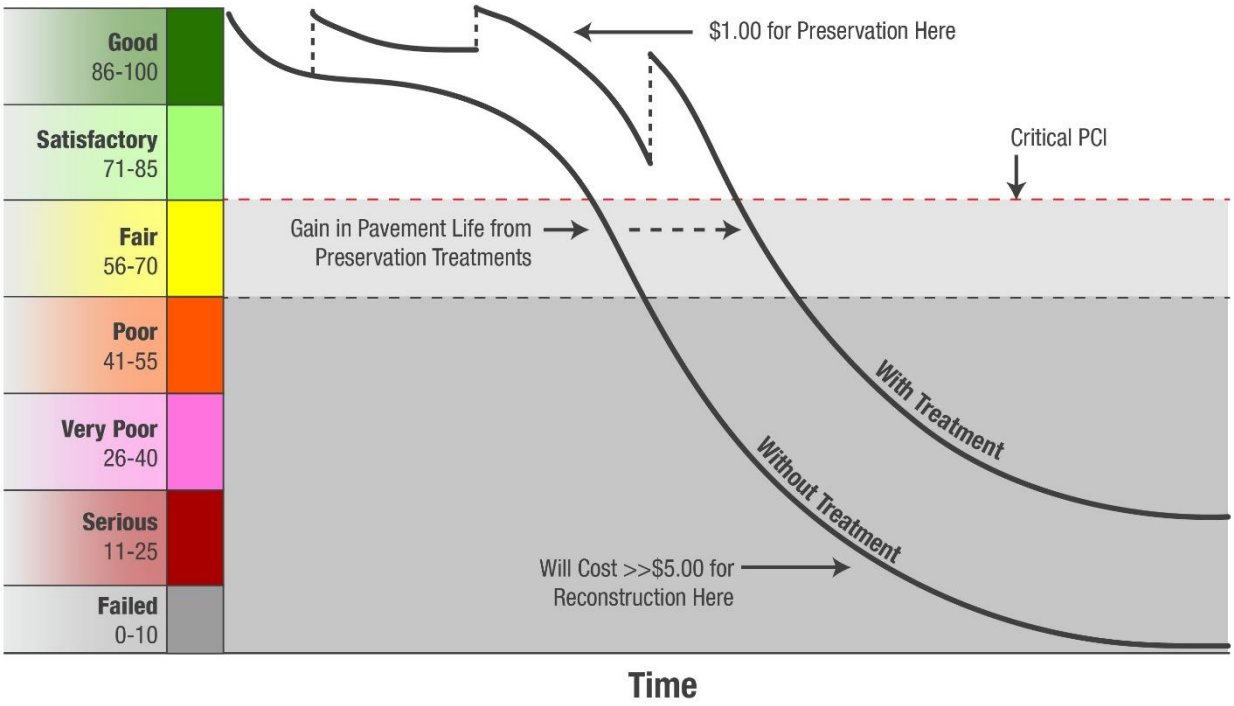
A comprehensive SAPMP provides information that assists with the project programming process. The primary objectives of the FDOT SAPMP consist of the following:

- Assist airports in meeting the requirements of Public Law 103-305;
- Assist airports in complying with FAA Grant Assurances 11 and 19;
- Provide airports with functional pavement condition in accordance with ASTM D5340-20 (current) and with the FAA AC 150/5380-7B (current) based on visual assessment efforts;
- Provide airports with planning-level guidance on maintenance, repair, and rehabilitation in accordance with the FAA AC 150/5380-6C (current) based on pavement conditions and distress data in terms of type, severity, and extent; and
- Provide airports, FDOT Districts, FDOT AO, and the FAA Airports District Office with long-term, planning-level forecasts of pavement performance and rehabilitation budgetary needs (e.g., maintenance, repair, and major reconstruction) through reports.

From a pavement management perspective, one of the most valuable aspects of the PCI methodology is the ability to save money by effectively prioritizing the rehabilitation of pavement assets before they reach critical condition. Critical PCI values are assigned to deterioration models for pavement assets based on their respective use and rank. The concept of critical PCI will be further discussed in **Chapter 5**, but it is used as a benchmark to help identify pavement assets that should receive rehabilitation. In doing so, the PCI methodology can help create a proactive maintenance and rehabilitation (M&R) strategy to effectively address pavement projects before the cost of these projects increases significantly.

With M&R costs escalating over time, the consequences of inadequate maintenance practices can result in an inefficient allocation of funding. If maintenance is conducted before a significant decline in pavement condition occurs, substantial repair and/or rehabilitation costs may be avoided or delayed. **Figure 1.4** illustrates how the cost of pavement repairs can significantly increase if M&R activities are delayed.

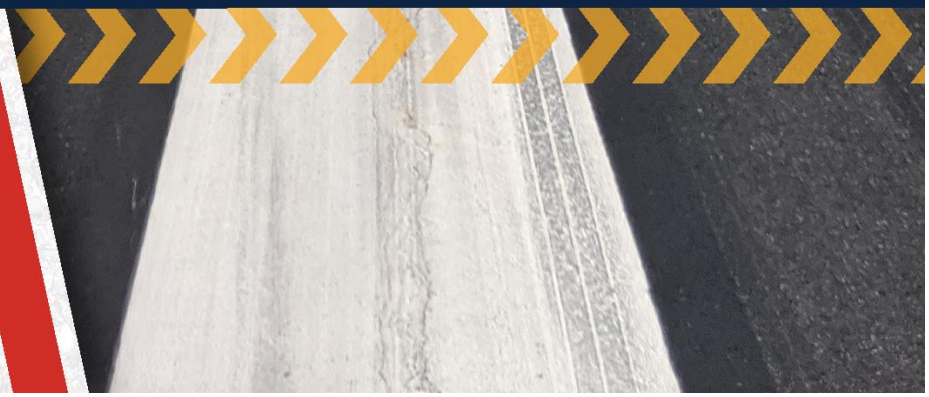
Figure 1.4: Pavement Life and the Effect of Treatments



FAA Eligibilty Thresholds: ☐ **>70:** Routine Maintenance ☐ **55-70:** Rehabilitation Eligible ☐ **<55:** Reconstruction Eligible

**Figure is for conceptual purposes only – unit costs are not specific to airfield pavements*

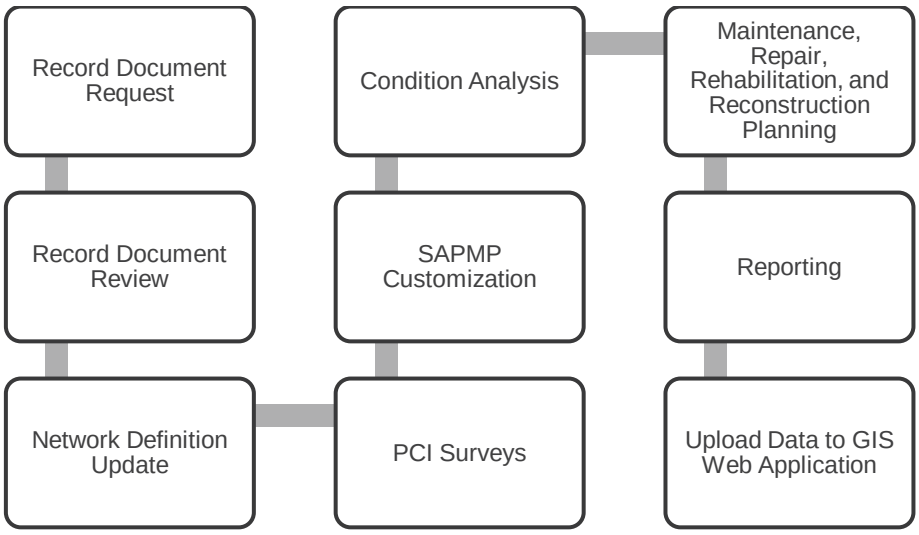
Chapter 2: Methodology



Chapter 2 – Methodology

An effective pavement management program incorporates both the regular collection of pavement condition information and communication of information to appropriate sponsors. This chapter of the report defines the specific methods utilized as part of the SAPMP System Update to meet the requirements of an effective pavement management system as defined by the FAA AC 150/5380-7B. **Figure 2** summarizes the overall process for the FDOT SAPMP.

Figure 2: FDOT SAPMP General Process



2.1 Airfield Pavement Database

This SAPMP utilizes PAVER™ 7.0 software as its airfield pavement database. The PAVER™ software application was developed by the U.S. Army Construction Engineering Research Laboratory and sponsored by the FAA, Federal Highway Administration, U.S. Army, U.S. Air Force, and U.S. Navy to meet the objectives of an effective pavement management system. The PAVER™ database includes a network-level inventory of the participating airport's eligible airfield pavement facilities. PAVER™ can achieve the following pavement management objectives:

- Create a manageable inventory system;
- Analyze the current condition of pavements in accordance with ASTM D5340-20;
- Develop pavement performance models to forecast conditions; and
- Generate maintenance, repair, and major rehabilitation recommendations based on budgetary scenarios.

PAVER™ inventory management is based on a tiered organizational structure consisting of networks, branches, sections, and samples, with the sample being the smallest unit of management. Critical elements of an effective pavement management program are maintained within the network-level PAVER™ database and typically consist of pavement inventory

characteristics, pavement structure, work history, historic condition records, and analytical customization.

2.2 Airfield Pavement Record Keeping (Historical Records Research)

In accordance with the FAA AC 150/5380-7B, it is a best practice that airports maintain records of all airfield construction and maintenance (routine, emergency, and proactive) related to the pavement facilities. These records should consist of:

- Location and limits of work;
- Types and severities of repaired distresses;
- Work type and cost; and
- Supporting documents (e.g., contract documents, construction drawings, specifications, bid tabulations, repair products, and photograph records).

As part of the SAPMP, participating airport's staff was asked to provide documentation regarding the historical work performed at the Airport, including construction drawings and bid tabulations. This information is used to identify location, limits, type of work, pavement cross-sections, and representative material costs.

Updated historical data collected during this task was entered into the PAVER™ database. This database includes the following fields for historical information:

- Date of last construction/rehabilitation
- Work type performed
- Comments for documenting pavement cross-section
- Pavement surface type
- Section area (limits of work)

The SAPMP PAVER™ database accuracy is limited to the record documentation provided by the participating airports. Airport Sponsors should rely on this information as a planning tool and defer to final as-built plans, record drawings, and/or engineer's construction report for pavement construction records.

2.3 Airfield Pavement Structure

A pavement is a prepared surface designed to provide a continuous, smooth ride at a certain speed and to support an estimated amount of traffic for a certain number of years. A pavement structure is composed of constructed layers consisting of subgrade, subbase, base, structural, and surface courses. For the FDOT SAPMP, two (2) predominant pavement types are classified for evaluation and analysis: Asphalt Concrete (AC) and Portland cement concrete (PCC). Composite Structures, known as Whitetopping Pavements consisting of PCC on AC, are also present at limited airports in Florida and are evaluated separately.

2.3.1 Asphalt Concrete

Asphalt concrete is a pavement comprised of aggregate mixture with an asphalt cement binder. The FDOT SAPMP categorizes three (3) Asphalt Concrete surface types: Asphalt Concrete (AC), Asphalt Concrete overlaid on Asphalt Concrete (AAC), and Asphalt Concrete overlaid on Portland cement concrete (APC).

Asphalt Concrete (AC)

A flexible pavement section consisting of aggregate mixture with asphalt cement binder layered on engineered base course material that is layered on subbase and subgrade soil material.

Asphalt Concrete Overlaid on Asphalt Concrete (AAC)

A flexible pavement section consisting of aggregate mixture with asphalt cement binder layered on an existing flexible AC pavement section. Airfield pavement sections are considered to be AAC when a pavement rehabilitation includes a pavement milling and resurfacing operation or a direct overlay of Asphalt Concrete without surface preparation.

Asphalt Concrete Overlaid on Portland Cement Concrete (APC)

A flexible pavement section consisting of aggregate mixture with asphalt cement binder layered on an existing PCC pavement section. This unique pavement composition may result in distinct pavement distress manifestations known as reflective joint cracking.

2.3.2 Portland Cement Concrete

Portland cement concrete is a pavement comprised of aggregate mixture with a Portland cement binder. The FDOT SAPMP categorizes Portland cement concrete (PCC) as the primary rigid pavement section.

Portland Cement Concrete (PCC)

A rigid pavement section composed of Portland cement concrete placed on a granular or treated base course that is supported on a compacted subgrade. The concrete surface provides a texture of nonskid qualities, prevents the infiltration of surface water into the subgrade, and provides structural support for airplane loading. Rigid pavement construction requires the layout of appropriately designed joints. Concrete overlays built in accordance with the FAA Advisory Circular 150/5320-6F “Airport Pavement Design and Evaluation” are recognized as PCC pavement.

2.3.3 Composite Structure – Whitetopping Pavement

Whitetopping pavement is a composite pavement comprised of relatively thin PCC overlaid on an existing AC pavement structure. There are three (3) types of Whitetopping Pavements: Conventional (WT), Thin (TWT), and Ultra-Thin (UWT).

Conventional Whitetopping (WT)

A composite pavement structure consisting of a modified PCC overlaid on an existing AC pavement section. The modified PCC layer is typically greater than 6 inches in thickness.

Thin Whitetopping (TWT)
A composite pavement structure consisting of modified PCC overlaid on an existing AC pavement section. The modified PCC layer is typically between 4 and 6 inches in thickness.

Ultra-Thin Whitetopping (UWT)
A composite pavement structure consisting of a modified PCC overlaid on an existing AC pavement section. The modified PCC layer is typically between 2 and 4 inches in thickness.

2.4 Airfield Pavement Traffic

A pavement section is typically designed to meet the needs of the user (airlines, air cargo, general aviation, and/or military) in providing a safe, smooth, operational surface. Pavement deterioration generally occurs gradually from aircraft loading and environmental conditions.

This System Update does not involve a study or analysis of TLH's aircraft fleet mix or traffic operations. However, it is strongly recommended that the Airport incorporate the requirements of the FAA AC 150/5320-6F when developing design-level rehabilitation activities; this AC provides guidance on incorporation of aircraft traffic fleet mix data.

2.5 Pavement Management Program Network Definition Terminology

To facilitate an effective pavement management program, a pavement network must be established and subdivided into smaller, manageable working units. Sectioning of the pavement network was established in a prior System Update and was revised during this SAPMP to account for work that has been performed on the airfield since the previous Update. Information from historic records is used to help define the limits of the smaller working units. A critical input for a pavement inventory and network definition is the date of last major construction or rehabilitation, as this type of work will reset the section PCI to a value of 100.

The following sections define the common terms used in pavement management systems and cover their application for this SAPMP System Update.

2.5.1 Pavement Network Identification

Establishing the pavement network is the first step in organizing pavements into a structure for pavement management. The network is the starting point of the hierarchy of pavement management organization. A network typically consists of one or more pavement *branches*, which have one or more pavement *sections*. For example, a network can be all the pavements within an Airport's airfield or all the pavements in a statewide program. For the FDOT SAPMP, a network represents an individual Airport's airfield pavement facilities maintained by the Airport.

2.5.2 Pavement Branch Identification

A pavement branch, also known as a facility, is a logical unit of generally identifiable pavement within a network that has a distinct functional classification. For example, within an airfield, each runway, taxiway, or apron is considered a branch. Each branch contains at least one section but may contain more if pavement feature characteristics are distinct throughout the branch.

2.5.3 Pavement Section Identification

A pavement section, or feature, is a subdivision of a branch and has consistent characteristics throughout its length or area. These characteristics include structural composition (pavement layer material type and thickness), construction history, age, traffic type, traffic frequency, and pavement condition. A section is the basic management unit of a pavement network and is the level at which maintenance, repair, or major rehabilitation treatments are considered.

2.5.4 Pavement Sample Unit Identification

A pavement sample unit is an arbitrarily defined subdivision of a pavement section that has a standard size range of 20 contiguous slabs (±8 slabs) for PCC pavement and 5,000 contiguous square feet (±2,000 SF) for AC. A sample unit is the smallest subdivision of a pavement network and is analyzed during field assessments to establish condition ratings.

2.5.5 Terminology Summary

Below is a summary table, **Table 2.5.5**, with definitions and examples of common SAPMP terminology.

Table 2.5.5: SAPMP Terminology

SAPMP Terminology	Common Definition	Airport Example
Network	Totality of pavement assets maintained by the Airport.	"Tallahassee International Airport – Airfield Pavements"
Branch Name	Commonly defined asset name as established by Airport and by use.	"Runway 18-36"
Branch ID	Codified shorthand name for commonly defined asset established for database identification.	"RW 18-36" RW, Branch Use, "Runway" "Runway 18-36", Runway Facility
Section ID	Codified identification for pavement asset that is distinct by pavement composition, work history, aircraft loading, or condition.	"6105"
Sample Unit	A numeric identification of an area of pavement (5,000 ± 2,000 SF of AC or 20 ± 8 slabs of PCC) that has been inspected in accordance with ASTM D5340-20.	"300"

2.6 Airfield PCI Survey Methodology

In adherence to the FAA AC 150/5380-7B, the FDOT SAPMP utilizes the PCI survey method to collect pavement distress data and analyze the condition. The PCI survey procedure is a visual statistical sampling of pavements for recording primary distress types (e.g., cracking and deformation), associated severities, and quantities as defined by the ASTM D5340-20. This effort is the primary means of obtaining and recording pavement distress data. The PCI survey consists primarily of visual assessments of pavement surfaces for signs of distress and deterioration resulting from loading (aircraft) and environmental influences.

Overall, a visual pavement condition survey provides an indication of the cause and rate of deterioration of a pavement section from a functional point of view and can help identify if any underlying structural deficiencies are present. Although a visual PCI survey does not predict the remaining structural life of a pavement section or its ability to support loads, it does assess the rating of the operational surface. Functional condition, determined by the PCI method, can provide a cost-effective means to plan for pavement rehabilitation projects. Timely application of pavement rehabilitation may lead to the extension of functional life of individual pavement sections. This method varies from structural evaluation; functional condition is limited to visually observed distresses and indicative modes of pavement deterioration. A formal structural evaluation analyzes subsurface conditions, material characteristics, and qualitative pavement structure attributes. A structural evaluation may consist of subsurface geotechnical exploration, falling weight deflectometer testing, petrographic testing, material coring, and/or flexural testing.

2.6.1 Pavement Distress Types

For each sample, the severity and quantity of defined distresses are recorded and then analyzed in accordance with the ASTM D5340-20 standard, which identifies 17 AC distress types and 16 PCC distress types. **Tables 2.6.1 (a)** and **2.6.1 (b)** identify these distresses and their common causes or mechanisms.

Table 2.6.1 (a): Pavement Distress Types – Asphalt Concrete

Distress Mechanism	Distress Type
Load	Alligator Cracking Rutting
Climate/Durability	Block Cracking Joint Reflection Cracking Longitudinal and Transverse Cracking (LT) Raveling Shoving Weathering
Construction/Material	Bleeding Corrugation Depression Polished Aggregate Slippage Cracking Swelling
Other	Jet Blast Erosion Oil Spillage Patching and Utility Cut Patching

Table 2.6.1 (b): Pavement Distress Types – Portland Cement Concrete

Distress Mechanism	Distress Type
Load	Corner Break Longitudinal, Transverse, and Diagonal Cracking (LTD) Pumping Shattered Slab/Intersecting Cracks
Climate/Durability	Blowup Durability "D" Cracking Joint Seal Damage Popouts
Construction/Material	Alkali Silica Reaction (ASR) Scaling Shrinkage Cracking
Other	Corner Spalling Joint Spalling Large Patching and Utility Cut Settlement or Faulting Small Patching

2.6.2 PCI Survey Procedures

PCI surveys are conducted on sample units defined in previous System Updates. Sample units are subject to change at the discretion of field personnel and/or to major pavement rehabilitation treatments. Furthermore, access to sample units based on accessibility or operational impacts may affect the overall sampling rate effort at each airport. **Tables 2.6.2 (a)** and **(b)** define the sampling criteria used by the FDOT SAPMP. A higher sampling rate may be utilized to achieve greater statistical confidence, should the Airport have the available resources to perform PCI survey independent of the FDOT SAPMP.

Table 2.6.2 (a): Recommended Sampling Rates for Asphalt Concrete

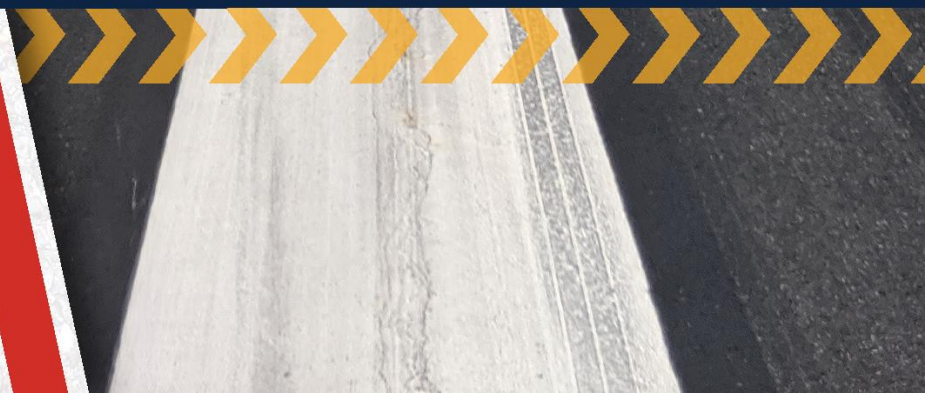
Number of Total Sample Units in Section	Runway Sampling Rate	Taxiways, Aprons, and Others Sampling Rate
1 - 4	1	1
5 - 10	2	1
11 - 15	3	2
16 - 30	5	3
31 - 40	7	4
41 - 50	8	5
51 or more	20% but ≤ 20	10% but ≤ 10

Table 2.6.2 (b): Recommended Sampling Rates for Portland Cement Concrete

Number of Total Sample Units in Section	Runway Sampling Rate	Taxiways, Aprons, and Others Sampling Rate
1 - 3	1	1
4 - 6	2	1
7 - 10	3	2
11 - 15	4	2
16 - 20	5	3
21 - 30	7	3
31 - 40	8	4
41 - 50	10	5
51 or more	20% but ≤ 20	10% but ≤ 10

The FDOT SAPMP is limited to select sample units for each section identified in each airport's Airfield Pavement Network Definition. The intent is to perform a limited amount of sample unit PCI surveys to reasonably reflect the functional condition. Due to the limited sampling criteria, there may be instances of pavement distress and deterioration outside of the inspected sample units that were not observed.

Chapter 3: Airfield Pavement System Inventory



Chapter 3 – Airfield Pavement System Inventory

This chapter discusses the inventory data collected from the Airport and summarizes network-level characteristics of the Airport's airfield pavements. At the start of each FDOT SAPMP System Update, all airports are asked to review the existing Airfield Pavement Network Definition Exhibit for accuracy. Furthermore, participating airports are asked to provide documentation of any recent or anticipated construction related to their airfield pavements.

3.1 Airfield Pavement Network Information

3.1.1 Previous and/or Anticipated Airfield Pavement Construction

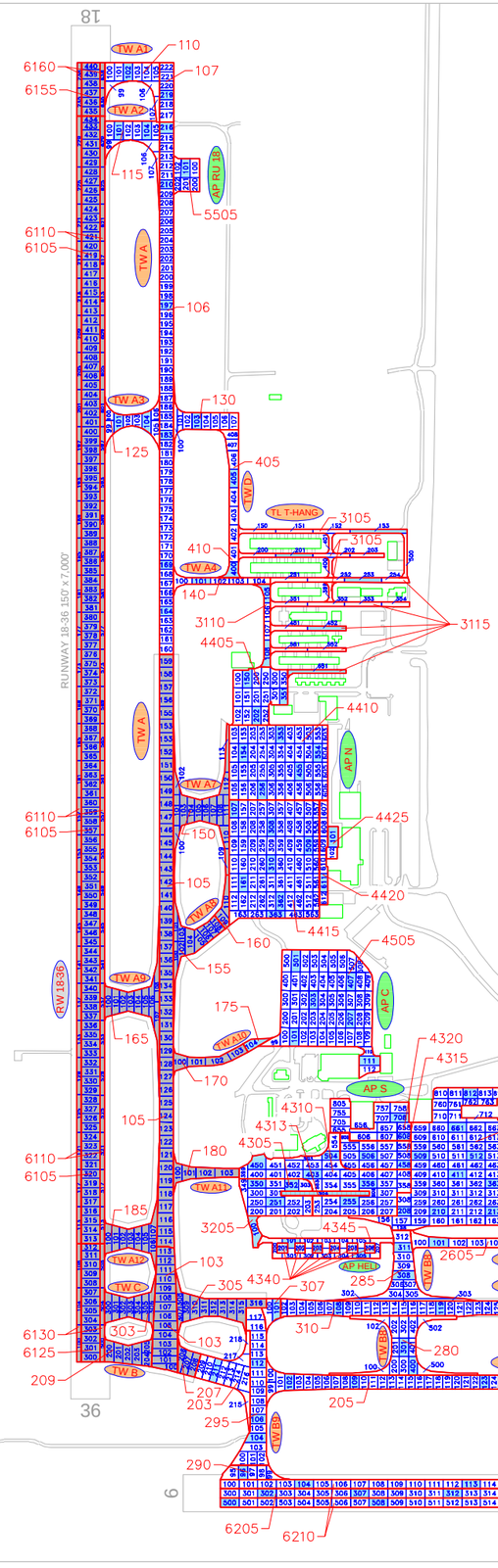
Based on information provided by the Airport, **Table 3.1.1** summarizes recent or anticipated airfield pavement construction projects since 2017.

Table 3.1.1: Summary of Previous and/or Anticipated Airfield Pavement Construction

Construction Year	Location	Work Type / Pavement Section
2018	AP HELI	New Construction - PCC
	AP HELI	New Construction - AC
	AP S	Mill and Overlay | 2" Mill; 2"-4" Variable Overlay P-401
	AP S	Complete Reconstruction - PCC | 8" P-501; 6" P-211; 12" P-152
	AP S	Complete Reconstruction - PCC | 14" P-501; 6" P-211; 12" P-152
	AP S	Complete Reconstruction - AC | 4" P-401; 6" P-211; 12" P-152
2023	RW 18-36	Complete Reconstruction - AC | 4" P-401, 5" P-403, regrade and recompact existing limerock
	RW 18-36	Mill and Overlay | 2" Mill, 2" P-401 Overlay
	TW A, TW A7, TW A8, TW A11, TW A12, TW B, TW C	Mill and Overlay | Variable depth mill, 2" P-401 overlay
	TW A9, TW A10	New Construction - AC
2024	TW B, TW B1, TW B5, TW B6, TW B7, TW B8, TW B9	Rehabilitation

The Airport provided a combination of record drawings, reports, and staff input, which aided in developing the construction history of the Airport's pavements since inception. Major rehabilitation and construction activities performed in the last 24 months, or anticipated in the next 24 months, are assumed to restore the PCI to 100. These activities include pavement overlay, mill and overlay, new construction, and/or complete reconstruction. These pavements were not formally subject to a PCI assessment and actual conditions may vary. Furthermore, any localized maintenance or repair performed in the assessment areas that would improve the PCI are considered in the condition analysis.

Figure 3.1.1 (a), the Airfield Pavement Network Definition Exhibit, provides details of the PCI assessment efforts. The Exhibit identifies pavement facilities, surface types, section definitions, and sample unit delineations. **Figure 3.1.1 (b)**, the Airfield Pavement System Inventory Exhibit, provides details of the work history updates communicated by the Airport. The Exhibit provides the approximate limits of recent and/or anticipated construction on the airfield pavement facilities. The limits are based on documentation provided by the Airport and, if constructed, are confirmed during field surveys.



AAC TW A 103 0 | 14	AAC TW A 105 0 | 46	AC TW A 106 6 | 57	AC TW A 107 1 | 6	AAC TW A1 110 1 | 9	AC TW A2 115 2 | 9	AC TW A3 125 2 | 8	AC TW A3 130 1 | 8	AC TW A4 140 1 | 5	AAC TW A7 150 0 | 14	AAC TW A8 155 0 | 9	AAC TW A8 160 0 | 2	AC TW A9 165 0 | 9	AC TW A10 170 0 | 4
AC TW A10 175 1 | 1	AAC TW A11 180 0 | 4	AAC TW A12 185 0 | 8	AC TW B 203 1 | 10	AC TW B 205 13 | 156	AAC TW B 207 0 | 3	AAC TW B 209 0 | 6	AC TW B1 210 2 | 10	AC TW B1 215 1 | 1	AC TW B2 220 2 | 11	AC TW B3 230 3 | 12	AC TW B3 235 3 | 14	AC TW B4 240 2 | 7	AC TW B5 250 2 | 5
AC TW B6 260 1 | 8	AC TW B6 265 1 | 3	AC TW B6 267 2 | 5	AC TW B7 270 2 | 8	AC TW B7 271 1 | 4	AC TW B7 273 3 | 8	AAC TW B7 275 1 | 3	AAC TW B7 277 1 | 2	AC TW B8 280 2 | 13	AC TW B8 285 2 | 11	AC TW B9 290 1 | 5	AC TW B9 295 3 | 18	AAC TW C 303 0 | 7	AAC TW C 305 0 | 10
AAC TW C 307 1 | 2	AAC TW C 310 4 | 40	AAC TW C 315 2 | 13	AC TW D 405 1 | 7	AC TW D 410 1 | 2	AC TW Z 2605 3 | 12	AC TW Z 2610 1 | 1	AC TW Z 2615 1 | 1	AC TL T-HANG 3105 2 | 12	AC TL T-HANG 3110 2 | 4	AC TL T-HANG 3115 3 | 13	AAC TL AP S 3205 1 | 1	PCC AP TERM 4105 16 | 217	APC AP TERM 4110 1 | 4
AC AP CARGO 4205 2 | 12	AC AP CARGO 4210 9 | 84	PCC AP CARGO 4215 1 | 2	AAC AP S 4305 3 | 14	AAC AP S 4310 5 | 35	PCC AP S 4313 1 | 1	AAC AP S 4315 2 | 13	AAC AP S 4320 2 | 14	PCC AP S 4325 1 | 2	AC AP S 4332 10 | 83	PCC AP HELI 4340 1 | 6	AC AP HELI 4345 2 | 12	AAC AP N 4405 3 | 16	AAC AP N 4410 5 | 44
APC AP N 4415 7 | 65	APC AP N 4420 1 | 6	AC AP N 4425 1 | 2	AC AP C 4505 6 | 53	AAC AP RU 18 5505 1 | 6	AC RW 18-36 6105 0 | 122	AC RW 18-36 6110 0 | 60	AAC RW 18-36 6125 0 | 13	AAC RW 18-36 6130 0 | 6	AAC RW 18-36 6155 0 | 6	AAC RW 18-36 6160 0 | 4	AC RW 9-27 6205 16 | 80	AC RW 9-27 6210 20 | 160

NORTH

LEGEND

- RW 13-31 — TYPICAL RUNWAY BRANCH ID
- TW A — TYPICAL TAXIWAY BRANCH ID
- AP S — TYPICAL APRON BRANCH ID
- AAC / AP MAIN / 4105 / 10 | 100 — PAVEMENT SURFACE TYPE; PAVEMENT BRANCH ID; SECTION NUMBER; NUMBER OF SAMPLE UNITS IN SECTION; NUMBER OF SAMPLE UNITS TO BE INSPECTED
- AAC / AP MAIN / 4105 / 0 | 100 — SECTION NOT INSPECTED DUE TO RECENT CONSTRUCTION. SEE SYSTEM INVENTORY MAP FOR CONSTRUCTION DATES.
- 100 — INSPECTED SAMPLE UNITS.

TOTAL SAMPLES INSPECTED = 202
AC: 182 PCC: 20

RUNWAY LENGTHS DEPICTED IN THIS DRAWING ARE FOR PAVEMENT MANAGEMENT PURPOSES ONLY AND MAY NOT MATCH PUBLISHED RUNWAY LENGTHS. DRAWING NOT TO SCALE.

TLH

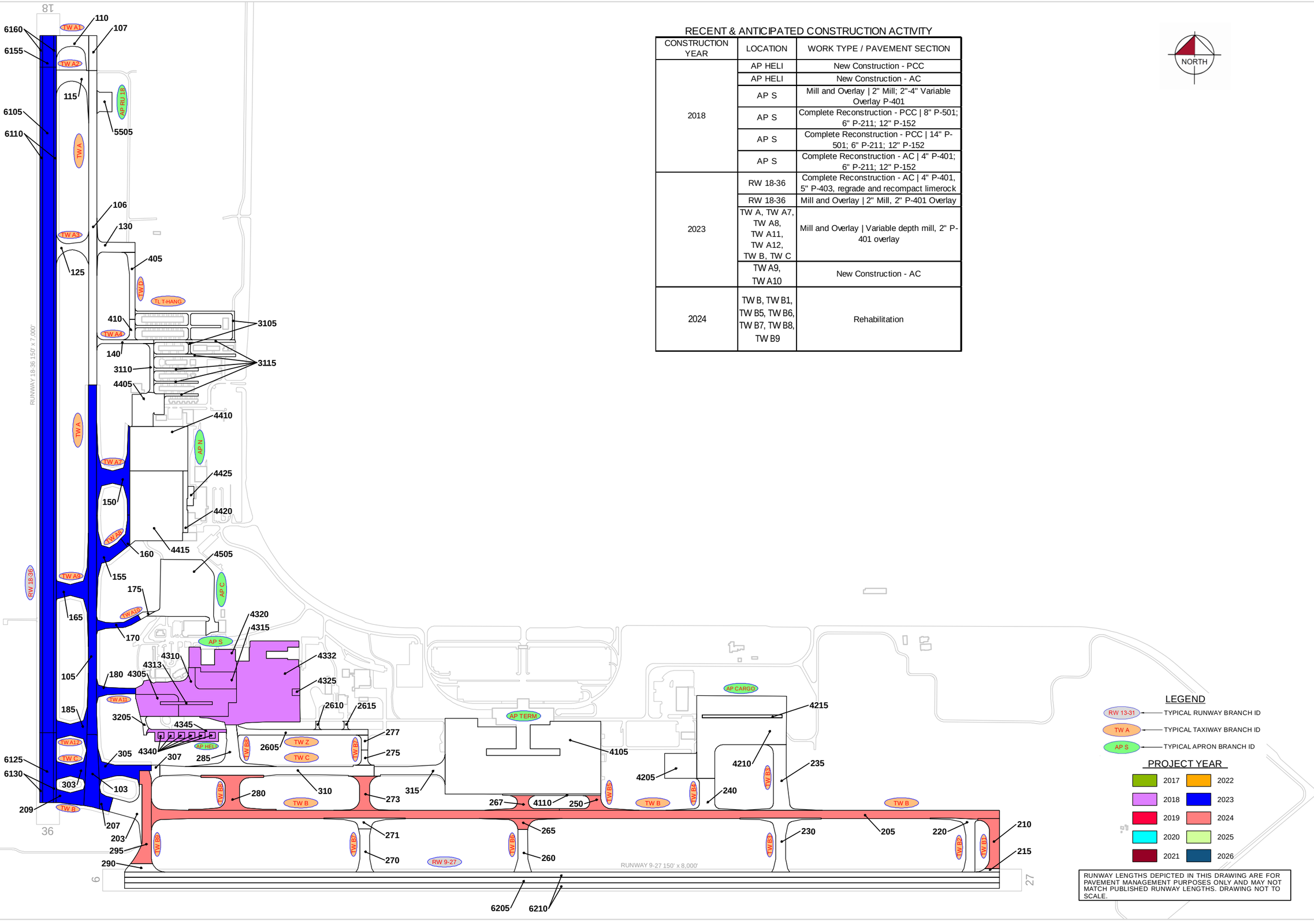
AIRFIELD PAVEMENT NETWORK DEFINITION EXHIBIT

Statewide Airfield Pavement Management Program

TALLAHASSEE INTERNATIONAL AIRPORT

FDOT

2022



RECENT & ANTICIPATED CONSTRUCTION ACTIVITY

CONSTRUCTION YEAR	LOCATION	WORK TYPE / PAVEMENT SECTION
2018	AP HELI	New Construction - PCC
	AP HELI	New Construction - AC
	AP S	Mill and Overlay | 2" Mill; 2"-4" Variable Overlay P-401
	AP S	Complete Reconstruction - PCC | 8" P-501; 6" P-211; 12" P-152
	AP S	Complete Reconstruction - PCC | 14" P-501; 6" P-211; 12" P-152
	AP S	Complete Reconstruction - AC | 4" P-401; 6" P-211; 12" P-152
2023	RW 18-36	Complete Reconstruction - AC | 4" P-401, 5" P-403, regrade and recompact limerock
	RW 18-36	Mill and Overlay | 2" Mill, 2" P-401 Overlay
	TW A, TW A7, TW A8, TW A11, TW A12, TW B, TW C	Mill and Overlay | Variable depth mill, 2" P-401 overlay
	TW A9, TW A10	New Construction - AC
2024	TW B, TW B1, TW B5, TW B6, TW B7, TW B8, TW B9	Rehabilitation

NORTH

LEGEND

- RW 13-31 — TYPICAL RUNWAY BRANCH ID
- TW A — TYPICAL TAXIWAY BRANCH ID
- AP S — TYPICAL APRON BRANCH ID

PROJECT YEAR

2017	2022
2018	2023
2019	2024
2020	2025
2021	2026

RUNWAY LENGTHS DEPICTED IN THIS DRAWING ARE FOR PAVEMENT MANAGEMENT PURPOSES ONLY AND MAY NOT MATCH PUBLISHED RUNWAY LENGTHS. DRAWING NOT TO SCALE.

TLH

AIRFIELD PAVEMENT SYSTEM INVENTORY EXHIBIT

Statewide Airfield Pavement Management Program

TALLAHASSEE INTERNATIONAL AIRPORT

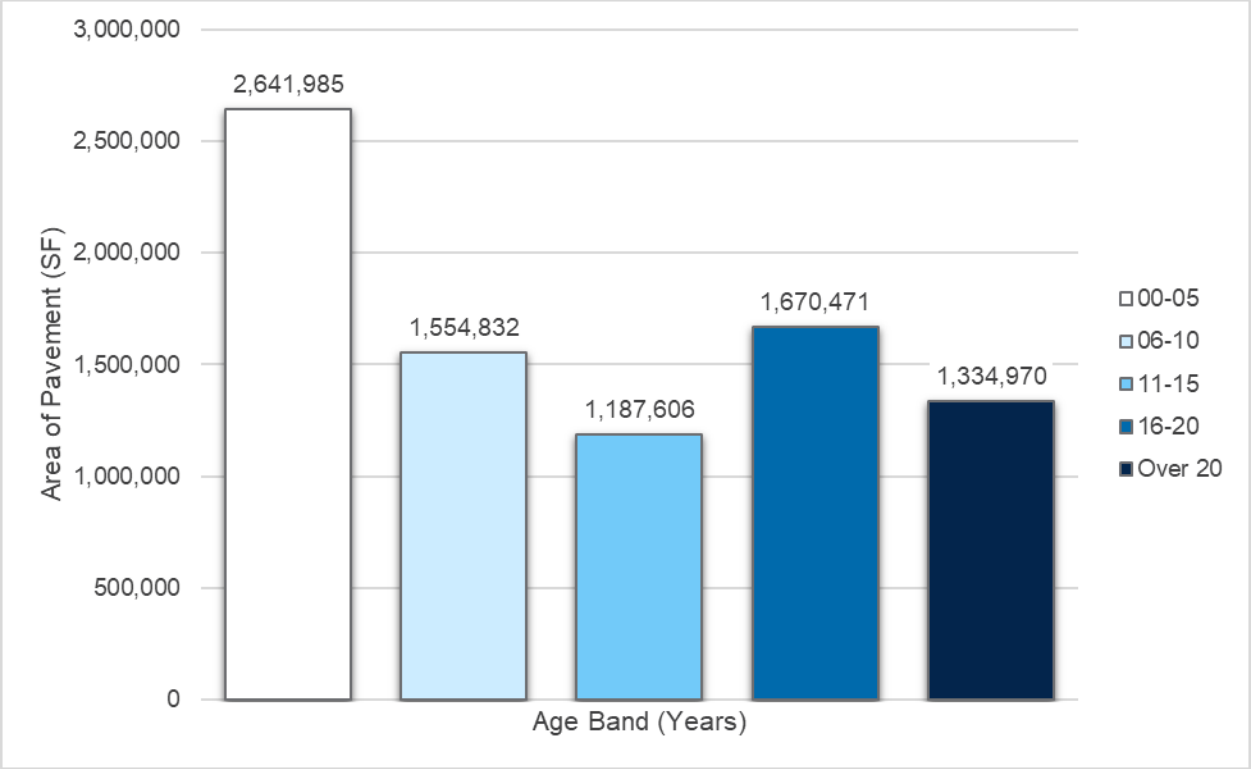
FDOT

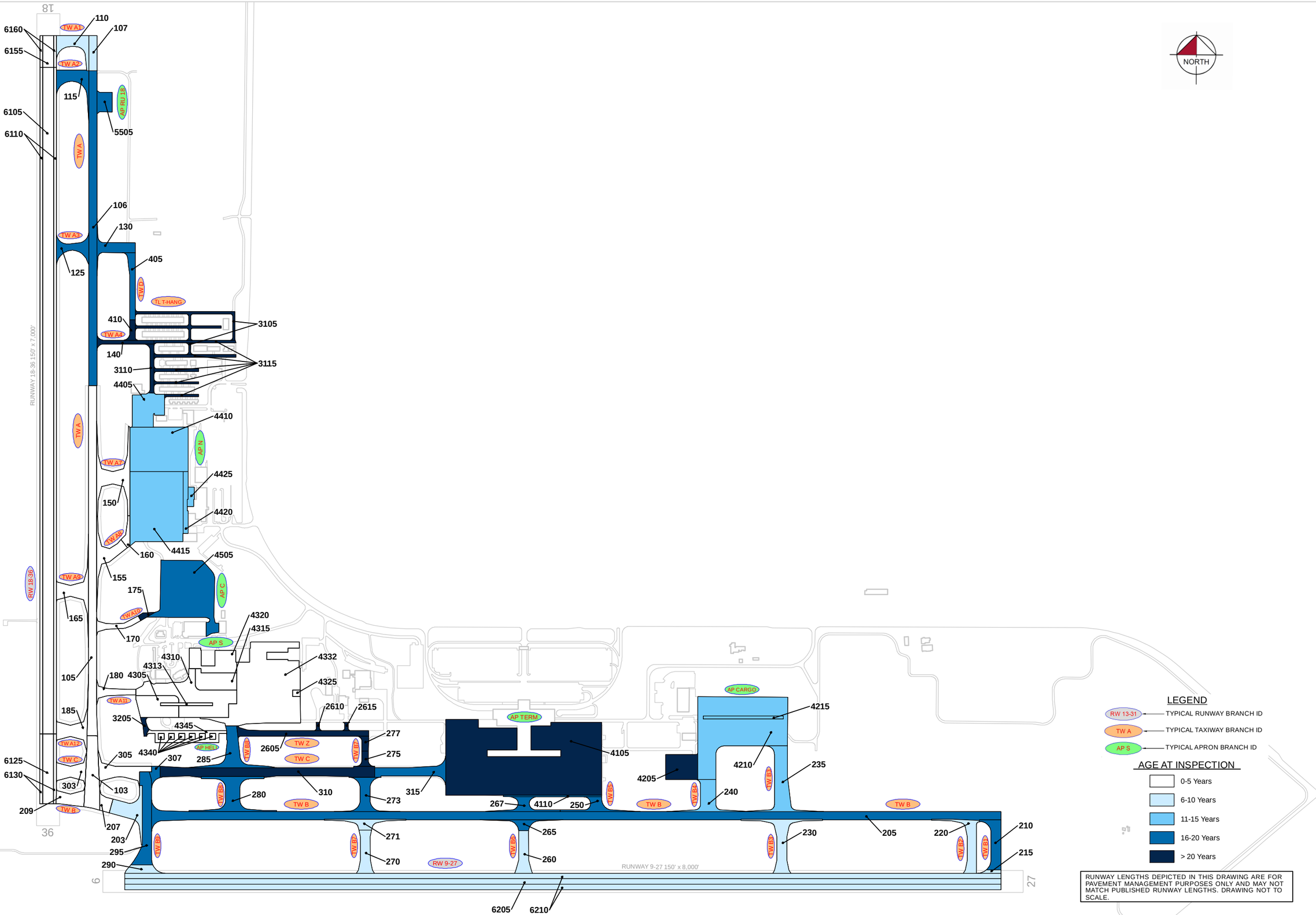
2022

3.1.2 Estimated Pavement Age

Standard pavement design practice considers a design life of 20 years. Design inputs typically require subgrade soil conditions, pavement layer material characteristics, and anticipated loading (aircraft fleet mix) for the design-life period. Based on the review of historic airfield pavement construction activities, **Figure 3.1.2 (a)** summarizes the age of the pavement sections since the last major construction activity has occurred. **Figure 3.1.2 (b)** provides the approximate limits of those age ranges on the airfield pavement facilities. This is intended to be a rough estimate based on interpretation of the limited data available at the time of report. The estimation of pavement age is based on information requested from the Airport.

Figure 3.1.2 (a): Age of Pavements at PCI Survey





RUNWAY LENGTHS DEPICTED IN THIS DRAWING ARE FOR PAVEMENT MANAGEMENT PURPOSES ONLY AND MAY NOT MATCH PUBLISHED RUNWAY LENGTHS. DRAWING NOT TO SCALE.

TLH

AIRFIELD PAVEMENT
ESTIMATED AGE EXHIBIT

Statewide Airfield Pavement
Management Program

TALLAHASSEE INTERNATIONAL AIRPORT

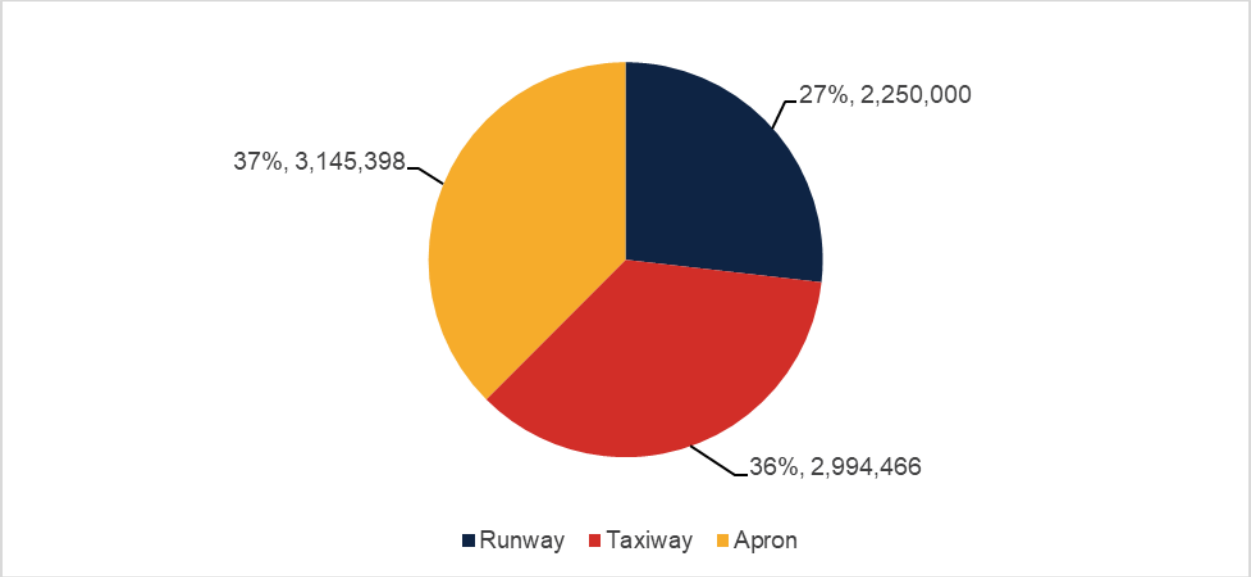
FDOT

2022

3.1.3 Functional Use

Pavements are subject to variations in aircraft loading patterns based on use and overall operations. This is termed "functional use" or "branch use." For this SAPMP System Update, the following categories of pavement functional use are identified: runway, taxiway, taxilane, and apron. **Figure 3.1.3** summarizes pavement functional use by area and excludes paved shoulders.

Figure 3.1.3: Airfield Pavement Branch Use by Area (SF)

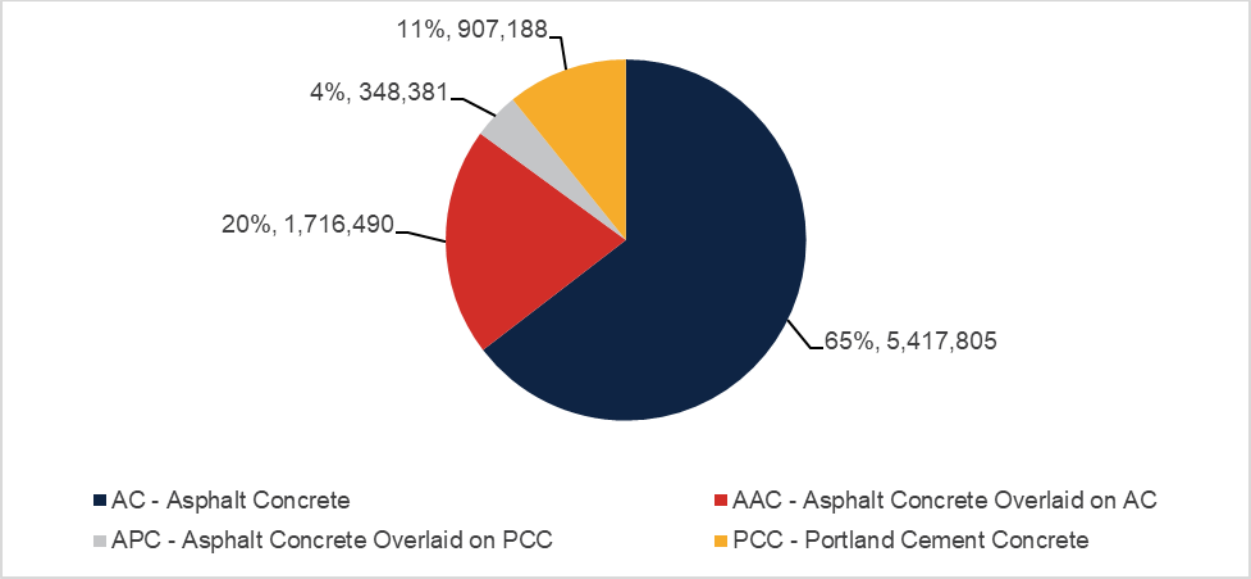


3.1.4 Pavement Surface Type

The airfield pavement facility surface types within the SAPMP include four (4) common types of pavement: Asphalt Concrete (AC), Asphalt Concrete overlaid on Asphalt Concrete (AAC), Asphalt Concrete overlaid on Portland cement concrete (APC), and Portland cement concrete (PCC).

Based on the record documentation incorporated within the SAPMP database and as observed during airfield pavement field assessments, pavement surface types have been assigned to the various pavement sections. **Figure 3.1.4** summarizes the applicable pavement types observed at TLH.

Figure 3.1.4: Airfield Pavement Surface Type by Area (SF)



3.1.5 Pavement System Inventory Details

The pavement inventory scope includes updates to existing pavement geometry and the development of an AutoCAD model with spatial projection for use within GIS. **Appendix C** includes the Airfield Pavement Network Definition Exhibit and the Airfield Pavement System Inventory Exhibit, which visually summarize the results of the airfield pavement system inventory analysis.

Table 3.1.5 displays the section-level pavement inventory data, which is based on record documentation provided by the airports and from previous System Updates. The information presented relies on the accuracy and the adequacy of data provided. In some cases, characteristics such as pavement area may be estimated based on aerial interpretation of spatially-projected imagery. Additionally, if the last construction date is unknown, a date of January 1 of the estimated year was assigned to the section. The accuracy of data is appropriate for this network-level planning document. Should the Airport perform rehabilitation work, it is recommended that project-level investigations be performed to support the data accuracy needed for design and construction.

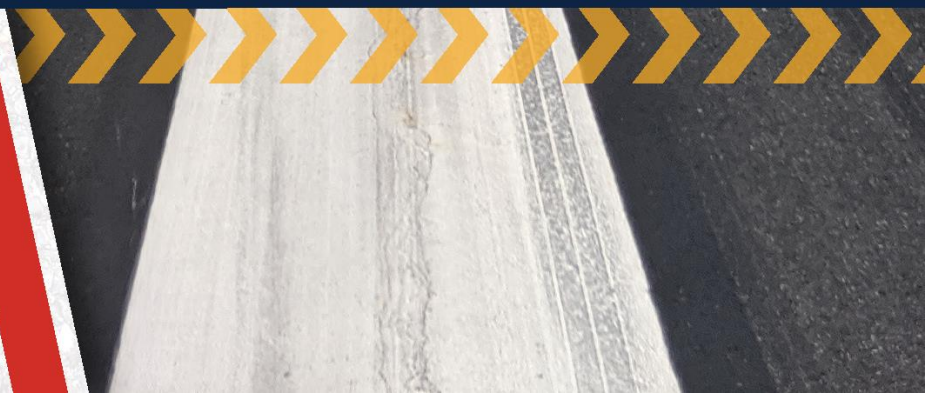
Table 3.1.5: Pavement System Inventory Details

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	Surface Type	Estimate of Last Construction Date
TLH	RW 9-27	Runway	6205	400,000	AC	1/1/2015
TLH	RW 9-27	Runway	6210	800,000	AC	1/1/2015
TLH	RW 18-36	Runway	6105	607,550	AC	1/1/2023
TLH	RW 18-36	Runway	6110	303,775	AC	1/1/2023
TLH	RW 18-36	Runway	6125	63,750	AAC	1/1/2023
TLH	RW 18-36	Runway	6130	31,875	AAC	1/1/2023
TLH	RW 18-36	Runway	6155	28,700	AAC	1/1/2023
TLH	RW 18-36	Runway	6160	14,350	AAC	1/1/2023

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	Surface Type	Estimate of Last Construction Date
TLH	TL AP S	Taxiway	3205	6,963	AAC	1/1/1994
TLH	TL T-HANG	Taxiway	3105	46,227	AC	1/1/1998
TLH	TL T-HANG	Taxiway	3110	16,646	AC	1/1/1985
TLH	TL T-HANG	Taxiway	3115	63,002	AC	1/1/1985
TLH	TW A	Taxiway	103	79,944	AAC	1/1/2023
TLH	TW A	Taxiway	105	243,781	AAC	1/1/2023
TLH	TW A	Taxiway	106	215,250	AC	1/1/2005
TLH	TW A	Taxiway	107	23,925	AC	10/1/2012
TLH	TW A1	Taxiway	110	40,291	AC	10/1/2012
TLH	TW A10	Taxiway	170	22,422	AC	1/1/2023
TLH	TW A10	Taxiway	175	4,954	AC	12/25/1999
TLH	TW A11	Taxiway	180	24,154	AAC	1/1/2023
TLH	TW A12	Taxiway	185	43,156	AAC	1/1/2023
TLH	TW A2	Taxiway	115	42,179	AC	1/1/2005
TLH	TW A3	Taxiway	125	32,329	AC	1/1/2005
TLH	TW A3	Taxiway	130	34,919	AC	7/1/2005
TLH	TW A4	Taxiway	140	19,805	AC	1/1/1985
TLH	TW A7	Taxiway	150	72,118	AAC	1/1/2023
TLH	TW A8	Taxiway	155	43,518	AAC	1/1/2023
TLH	TW A8	Taxiway	160	11,115	AAC	1/1/2023
TLH	TW A9	Taxiway	165	51,254	AC	1/1/2023
TLH	TW B	Taxiway	203	50,342	AC	10/1/2012
TLH	TW B	Taxiway	205	581,353	AC	1/1/2005
TLH	TW B	Taxiway	207	15,151	AAC	1/1/2023
TLH	TW B	Taxiway	209	30,178	AAC	1/1/2023
TLH	TW B1	Taxiway	210	46,292	AC	1/1/2005
TLH	TW B1	Taxiway	215	4,782	AC	1/1/2015
TLH	TW B2	Taxiway	220	49,156	AC	1/1/2015
TLH	TW B3	Taxiway	230	63,794	AC	1/1/2015
TLH	TW B3	Taxiway	235	83,567	AC	1/1/2007
TLH	TW B4	Taxiway	240	48,156	AC	1/1/2007
TLH	TW B5	Taxiway	250	24,545	AC	1/1/2005
TLH	TW B6	Taxiway	260	38,862	AC	1/1/2015
TLH	TW B6	Taxiway	265	17,002	AC	1/1/2005
TLH	TW B6	Taxiway	267	24,158	AC	1/1/2005
TLH	TW B7	Taxiway	270	39,535	AC	1/1/2015
TLH	TW B7	Taxiway	271	23,946	AC	1/1/2015
TLH	TW B7	Taxiway	273	38,359	AC	1/1/2005
TLH	TW B7	Taxiway	275	9,455	AAC	1/2/1992
TLH	TW B7	Taxiway	277	8,669	AAC	1/1/1994
TLH	TW B8	Taxiway	280	66,948	AC	7/1/2003
TLH	TW B8	Taxiway	285	58,220	AC	1/1/2003
TLH	TW B9	Taxiway	290	20,199	AC	1/1/2015
TLH	TW B9	Taxiway	295	84,260	AC	1/1/2005
TLH	TW C	Taxiway	303	37,868	AAC	1/1/2023
TLH	TW C	Taxiway	305	53,314	AAC	1/1/2023

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	Surface Type	Estimate of Last Construction Date
TLH	TW C	Taxiway	307	10,756	AAC	1/1/2005
TLH	TW C	Taxiway	310	160,476	AAC	1/1/1992
TLH	TW C	Taxiway	315	55,835	AAC	1/1/2003
TLH	TW D	Taxiway	405	33,610	AC	7/1/2005
TLH	TW D	Taxiway	410	10,157	AC	1/1/1998
TLH	TW Z	Taxiway	2605	62,575	AC	1/1/1994
TLH	TW Z	Taxiway	2610	2,379	AC	1/1/1994
TLH	TW Z	Taxiway	2615	2,615	AC	1/1/1994
TLH	AP C	Apron	4505	265,932	AC	1/1/2005
TLH	AP CARGO	Apron	4205	65,663	AC	1/1/1990
TLH	AP CARGO	Apron	4210	400,242	AC	1/1/2007
TLH	AP CARGO	Apron	4215	18,250	PCC	1/1/2007
TLH	AP HELI	Apron	4340	17,496	PCC	1/5/2018
TLH	AP HELI	Apron	4345	50,224	AC	1/5/2018
TLH	AP N	Apron	4405	77,291	AAC	1/1/2010
TLH	AP N	Apron	4410	215,063	AAC	1/1/2010
TLH	AP N	Apron	4415	310,550	APC	1/1/2010
TLH	AP N	Apron	4420	24,514	APC	1/1/2010
TLH	AP N	Apron	4425	9,973	AC	1/1/2010
TLH	AP RU 18	Apron	5505	25,207	AC	1/1/2005
TLH	AP S	Apron	4305	70,348	AAC	1/5/2018
TLH	AP S	Apron	4310	179,279	AAC	1/5/2018
TLH	AP S	Apron	4313	11,875	PCC	1/5/2018
TLH	AP S	Apron	4315	60,505	AAC	1/5/2018
TLH	AP S	Apron	4320	68,878	AAC	1/5/2018
TLH	AP S	Apron	4325	4,183	PCC	1/5/2018
TLH	AP S	Apron	4332	401,224	AC	1/5/2018
TLH	AP TERM	Apron	4105	855,384	PCC	1/1/1989
TLH	AP TERM	Apron	4110	13,317	APC	1/1/2005

Chapter 4: Airfield Pavement Condition Analysis



Chapter 4 – Airfield Pavement Condition Analysis

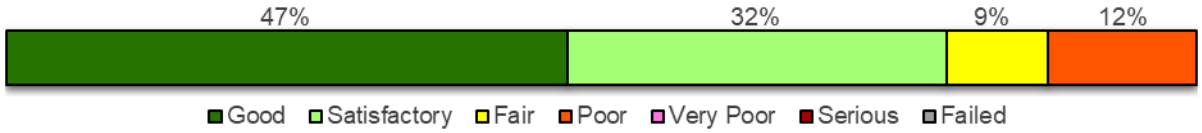
The Pavement Condition Index (PCI) provides insight to possible causes of deterioration to help support pavement maintenance and rehabilitation planning. Distress type, severity, and extent are required in the computation of a PCI value. The PCI method of pavement condition evaluation is strictly a visual review of surface condition, also referred to as a functional evaluation. Further evaluation of pavement conditions may be necessary, such as structural evaluation, for design- and/or project-level determination of pavement rehabilitation needs.

4.1 Airfield Pavement Condition Index

4.1.1 Network-Level Analysis

The following figure, **Figure 4.1.1**, summarizes the network-level pavement condition analysis based on the most recent survey results. On a network level, approximately 79% of inspected pavements are in Good or Satisfactory condition. Presently, roughly 9% of inspected pavements are in Fair condition and the remaining 12% of inspected pavements are in Poor or worse condition.

Figure 4.1.1: Current Condition – Overall Network



4.1.2 Branch-Level Analysis

The following **Figures 4.1.2 (a)-(d)** summarize branch-level pavement conditions according to the most recent PCI assessment results.

Figure 4.1.2 (a): Current Condition Summary – Branch-Level

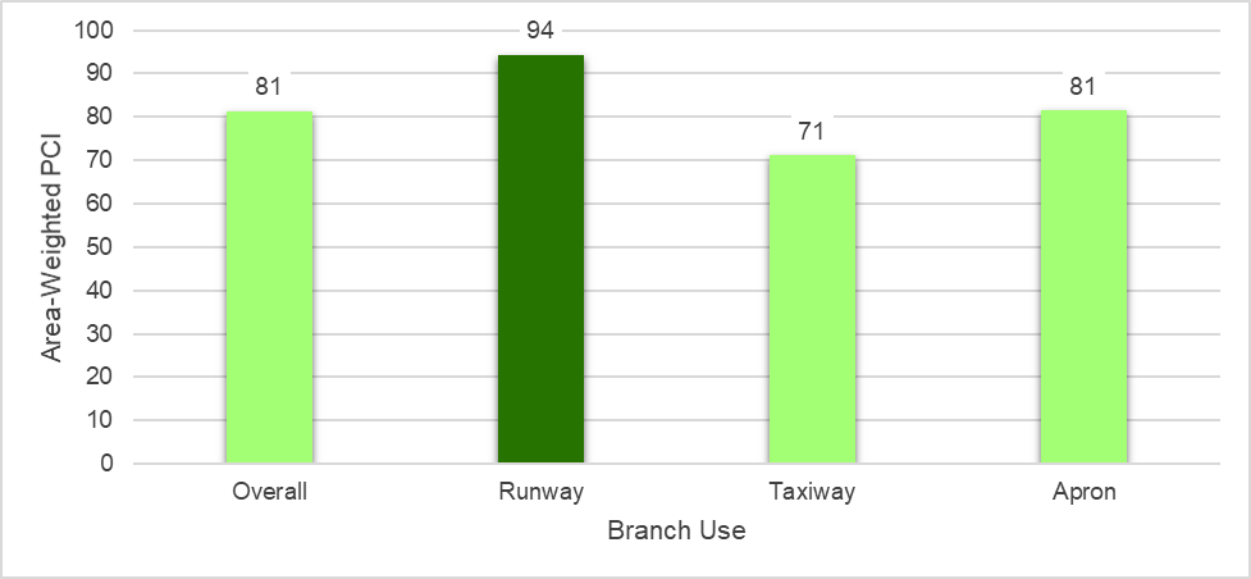


Figure 4.1.2 (b): Current Condition – Runway

Figure 4.1.2 (c): Current Condition – Taxiway

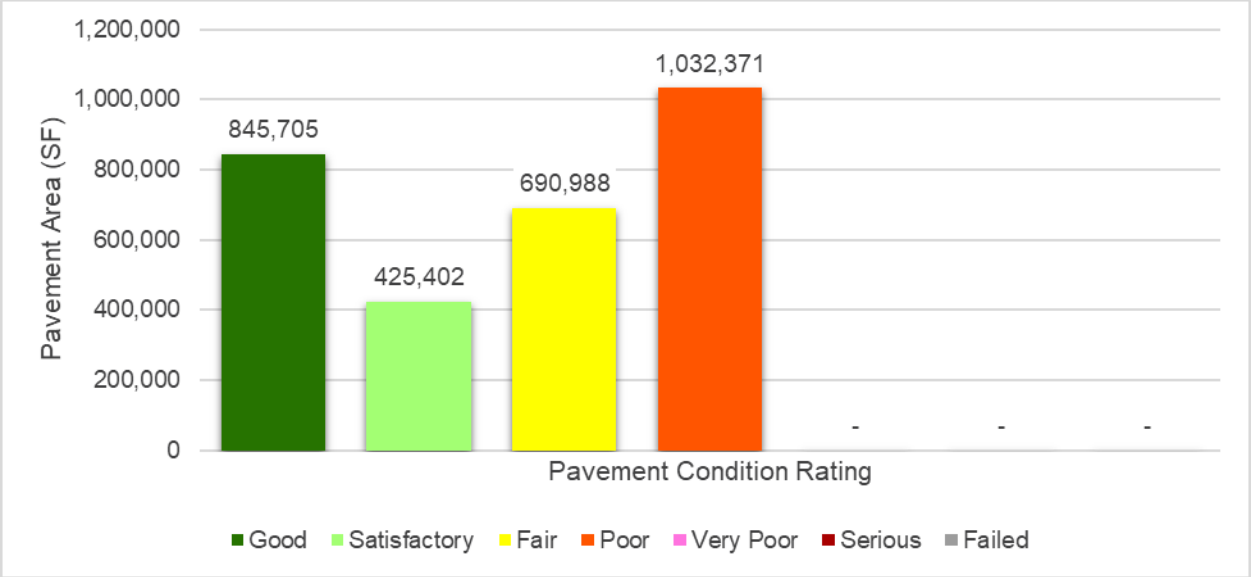


Figure 4.1.2 (d): Current Condition – Apron

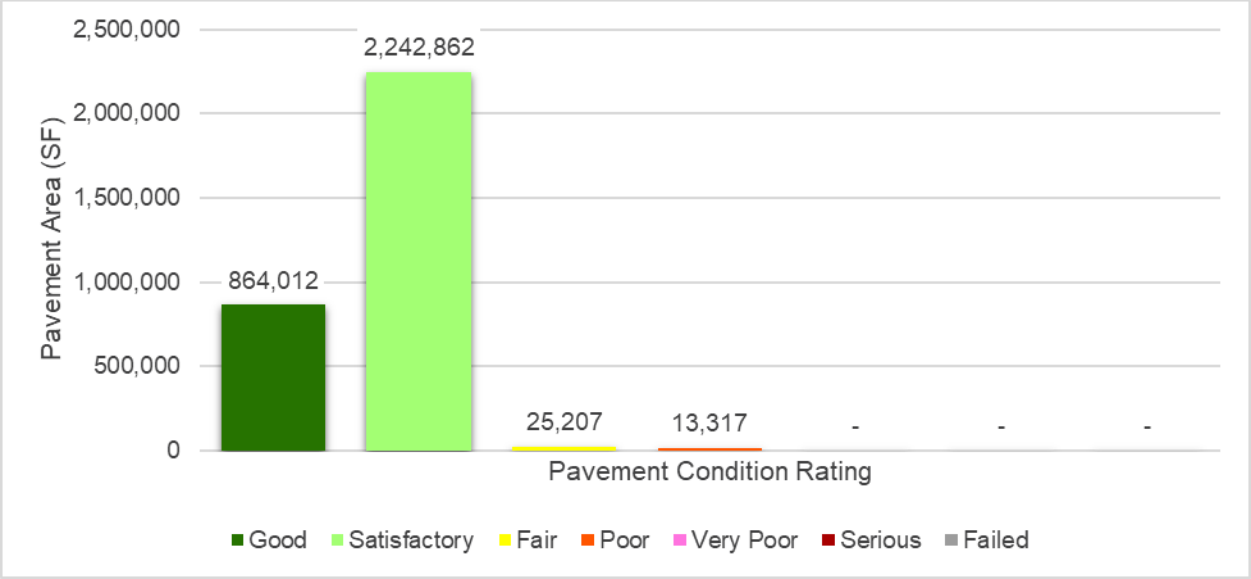


Table 4.1.2 details the branch-level condition for each airfield pavement branch.

Table 4.1.2: Current Condition Summary – Branch-Level

Branch ID	Branch Use	Number of Sections	Branch Area (SF)	Area-Weighted Avg PCI	Condition Rating
RW 9-27	Runway	2	1,200,000	89	Good
RW 18-36	Runway	6	1,050,000	100	Good
TL AP S	Taxiway	1	6,963	65	Fair
TL T-HANG	Taxiway	3	125,875	53	Poor
TW A	Taxiway	4	562,900	84	Satisfactory
TW A1	Taxiway	1	40,291	64	Fair
TW A10	Taxiway	2	27,376	95	Good
TW A11	Taxiway	1	24,154	100	Good
TW A12	Taxiway	1	43,156	100	Good
TW A2	Taxiway	1	42,179	70	Fair
TW A3	Taxiway	2	67,248	64	Fair
TW A4	Taxiway	1	19,805	54	Poor
TW A7	Taxiway	1	72,118	100	Good
TW A8	Taxiway	2	54,633	100	Good
TW A9	Taxiway	1	51,254	100	Good
TW B	Taxiway	4	677,024	55	Poor
TW B1	Taxiway	2	51,074	57	Fair
TW B2	Taxiway	1	49,156	87	Good
TW B3	Taxiway	2	147,361	82	Satisfactory
TW B4	Taxiway	1	48,156	76	Satisfactory
TW B5	Taxiway	1	24,545	43	Poor
TW B6	Taxiway	3	80,022	69	Fair
TW B7	Taxiway	5	119,964	74	Satisfactory

Branch ID	Branch Use	Number of Sections	Branch Area (SF)	Area-Weighted Avg PCI	Condition Rating
TW B8	Taxiway	2	125,168	72	Satisfactory
TW B9	Taxiway	2	104,459	60	Fair
TW C	Taxiway	5	318,249	69	Fair
TW D	Taxiway	2	43,767	69	Fair
TW Z	Taxiway	3	67,569	72	Satisfactory
AP C	Apron	1	265,932	74	Satisfactory
AP CARGO	Apron	3	484,155	76	Satisfactory
AP HELI	Apron	2	67,720	97	Good
AP N	Apron	5	637,391	73	Satisfactory
AP RU 18	Apron	1	25,207	64	Fair
AP S	Apron	7	796,292	95	Good
AP TERM	Apron	2	868,701	80	Satisfactory

4.1.3 Section-Level Analysis

Table 4.1.3 provides each pavement section's area-weighted average PCI and the percent of distress related to load, climate, and other factors. The causes of condition deterioration help inform maintenance, repair, and rehabilitation decisions. For example, load-related distress can indicate that the pavement is reaching the end of its structural design life and the selected rehabilitation treatment should include either strengthening or reconstruction. **Figure 4.1.3** provides a technical exhibit that graphically depicts PCI values and ratings determined from this SAPMP System Update.

Pavement facilities that have been reconstructed within the past 24 months, or are anticipated for reconstruction within the next 24 months, may have been omitted from this assessment. Pavement that has received major rehabilitation will be set to a PCI of 100 for this analysis.

Table 4.1.3: Latest Pavement Condition Index Summary – Section-Level

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	Surface	PCI	Condition Rating	PCI % Climate	PCI % Load	PCI % Other	Sample Units Inspected	Total Sample Units in Section
TLH	RW 9-27	Runway	6205	400,000	AC	88	Good	100	0	0	16	80
TLH	RW 9-27	Runway	6210	800,000	AC	90	Good	100	0	0	20	160
TLH	RW 18-36	Runway	6105	607,550	AC	100	Good	0	0	0	0	0
TLH	RW 18-36	Runway	6110	303,775	AC	100	Good	0	0	0	0	0
TLH	RW 18-36	Runway	6125	63,750	AAC	100	Good	0	0	0	0	0
TLH	RW 18-36	Runway	6130	31,875	AAC	100	Good	0	0	0	0	0
TLH	RW 18-36	Runway	6155	28,700	AAC	100	Good	0	0	0	0	0
TLH	RW 18-36	Runway	6160	14,350	AAC	100	Good	0	0	0	0	0
TLH	TL AP S	Taxiway	3205	6,963	AAC	65	Fair	100	0	0	1	1
TLH	TL T-HANG	Taxiway	3105	46,227	AC	62	Fair	88	12	0	2	12
TLH	TL T-HANG	Taxiway	3110	16,646	AC	52	Poor	100	0	0	2	4
TLH	TL T-HANG	Taxiway	3115	63,002	AC	46	Poor	91	0	9	3	13
TLH	TW A	Taxiway	103	79,944	AAC	100	Good	0	0	0	0	0
TLH	TW A	Taxiway	105	243,781	AAC	100	Good	0	0	0	0	0
TLH	TW A	Taxiway	106	215,250	AC	61	Fair	94	0	6	6	57
TLH	TW A	Taxiway	107	23,925	AC	67	Fair	70	0	30	1	6
TLH	TW A1	Taxiway	110	40,291	AC	64	Fair	71	0	29	1	9
TLH	TW A10	Taxiway	170	22,422	AC	100	Good	0	0	0	0	0
TLH	TW A10	Taxiway	175	4,954	AC	70	Fair	100	0	0	1	1
TLH	TW A11	Taxiway	180	24,154	AAC	100	Good	0	0	0	0	0
TLH	TW A12	Taxiway	185	43,156	AAC	100	Good	0	0	0	0	0
TLH	TW A2	Taxiway	115	42,179	AC	70	Fair	95	0	5	2	9
TLH	TW A3	Taxiway	125	32,329	AC	61	Fair	94	0	6	2	8
TLH	TW A3	Taxiway	130	34,919	AC	67	Fair	100	0	0	1	8
TLH	TW A4	Taxiway	140	19,805	AC	54	Poor	100	0	0	1	5
TLH	TW A7	Taxiway	150	72,118	AAC	100	Good	0	0	0	0	0
TLH	TW A8	Taxiway	155	43,518	AAC	100	Good	0	0	0	0	0
TLH	TW A8	Taxiway	160	11,115	AAC	100	Good	0	0	0	0	0
TLH	TW A9	Taxiway	165	51,254	AC	100	Good	0	0	0	0	0
TLH	TW B	Taxiway	203	50,342	AC	74	Satisfactory	83	0	17	1	10
TLH	TW B	Taxiway	205	581,353	AC	50	Poor	61	33	6	13	156
TLH	TW B	Taxiway	207	15,151	AAC	100	Good	0	0	0	0	0
TLH	TW B	Taxiway	209	30,178	AAC	100	Good	0	0	0	0	0
TLH	TW B1	Taxiway	210	46,292	AC	54	Poor	98	0	2	2	10
TLH	TW B1	Taxiway	215	4,782	AC	87	Good	92	0	8	1	1
TLH	TW B2	Taxiway	220	49,156	AC	87	Good	85	0	15	2	11
TLH	TW B3	Taxiway	230	63,794	AC	90	Good	100	0	0	3	12
TLH	TW B3	Taxiway	235	83,567	AC	76	Satisfactory	100	0	0	3	14
TLH	TW B4	Taxiway	240	48,156	AC	76	Satisfactory	97	0	3	2	7
TLH	TW B5	Taxiway	250	24,545	AC	43	Poor	64	18	18	2	5
TLH	TW B6	Taxiway	260	38,862	AC	84	Satisfactory	100	0	0	1	8
TLH	TW B6	Taxiway	265	17,002	AC	59	Fair	67	20	13	1	3
TLH	TW B6	Taxiway	267	24,158	AC	52	Poor	81	0	19	2	5

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	Surface	PCI	Condition Rating	PCI % Climate	PCI % Load	PCI % Other	Sample Units Inspected	Total Sample Units in Section
TLH	TW B7	Taxiway	270	39,535	AC	85	Satisfactory	100	0	0	2	8
TLH	TW B7	Taxiway	271	23,946	AC	83	Satisfactory	100	0	0	1	4
TLH	TW B7	Taxiway	273	38,359	AC	62	Fair	95	0	5	3	8
TLH	TW B7	Taxiway	275	9,455	AAC	53	Poor	100	0	0	1	3
TLH	TW B7	Taxiway	277	8,669	AAC	69	Fair	100	0	0	1	2
TLH	TW B8	Taxiway	280	66,948	AC	66	Fair	100	0	0	2	13
TLH	TW B8	Taxiway	285	58,220	AC	78	Satisfactory	100	0	0	2	11
TLH	TW B9	Taxiway	290	20,199	AC	83	Satisfactory	100	0	0	1	5
TLH	TW B9	Taxiway	295	84,260	AC	55	Poor	86	0	14	3	18
TLH	TW C	Taxiway	303	37,868	AAC	100	Good	0	0	0	0	0
TLH	TW C	Taxiway	305	53,314	AAC	100	Good	0	0	0	0	0
TLH	TW C	Taxiway	307	10,756	AAC	65	Fair	97	0	3	1	2
TLH	TW C	Taxiway	310	160,476	AAC	51	Poor	98	0	2	4	40
TLH	TW C	Taxiway	315	55,835	AAC	69	Fair	100	0	0	2	13
TLH	TW D	Taxiway	405	33,610	AC	69	Fair	100	0	0	1	7
TLH	TW D	Taxiway	410	10,157	AC	67	Fair	100	0	0	1	2
TLH	TW Z	Taxiway	2605	62,575	AC	73	Satisfactory	100	0	0	3	12
TLH	TW Z	Taxiway	2610	2,379	AC	42	Poor	76	13	11	1	1
TLH	TW Z	Taxiway	2615	2,615	AC	70	Fair	100	0	0	1	1
TLH	AP C	Apron	4505	265,932	AC	74	Satisfactory	96	0	4	6	53
TLH	AP CARGO	Apron	4205	65,663	AC	84	Satisfactory	95	0	5	2	12
TLH	AP CARGO	Apron	4210	400,242	AC	74	Satisfactory	88	0	12	9	84
TLH	AP CARGO	Apron	4215	18,250	PCC	79	Satisfactory	9	0	91	1	2
TLH	AP HELI	Apron	4340	17,496	PCC	95	Good	0	100	0	1	6
TLH	AP HELI	Apron	4345	50,224	AC	98	Good	100	0	0	2	12
TLH	AP N	Apron	4405	77,291	AAC	80	Satisfactory	94	0	6	3	16
TLH	AP N	Apron	4410	215,063	AAC	71	Satisfactory	95	0	5	5	44
TLH	AP N	Apron	4415	310,550	APC	72	Satisfactory	100	0	0	7	65
TLH	AP N	Apron	4420	24,514	APC	79	Satisfactory	100	0	0	1	6
TLH	AP N	Apron	4425	9,973	AC	75	Satisfactory	100	0	0	1	2
TLH	AP RU 18	Apron	5505	25,207	AC	64	Fair	97	0	3	1	6
TLH	AP S	Apron	4305	70,348	AAC	91	Good	83	0	17	3	14
TLH	AP S	Apron	4310	179,279	AAC	95	Good	100	0	0	5	35
TLH	AP S	Apron	4313	11,875	PCC	98	Good	0	0	100	1	1
TLH	AP S	Apron	4315	60,505	AAC	96	Good	100	0	0	2	13
TLH	AP S	Apron	4320	68,878	AAC	97	Good	100	0	0	2	14
TLH	AP S	Apron	4325	4,183	PCC	98	Good	0	0	100	1	2
TLH	AP S	Apron	4332	401,224	AC	96	Good	100	0	0	10	83
TLH	AP TERM	Apron	4105	855,384	PCC	80	Satisfactory	51	7	42	16	217
TLH	AP TERM	Apron	4110	13,317	APC	49	Poor	92	0	8	1	4

** Zero (0) Sample Units Inspected signifies that the pavement section was not inspected during this SAPMP System Update due to recent construction projects. These sections correlate with the gray sections on the Network Definition Exhibit.*

TLH

AIRFIELD PAVEMENT CONDITION INDEX EXHIBIT

Statewide Airfield Pavement Management Program

TALLAHASSEE INTERNATIONAL AIRPORT

FDOT

2022

NORTH

103 PCI = 100	105 PCI = 100	115 PCI = 70	125 PCI = 61	140 PCI = 54	150 PCI = 100	160 PCI = 100	165 PCI = 100	170 PCI = 100
175 PCI = 70	180 PCI = 100	185 PCI = 100	207 PCI = 100	209 PCI = 100	240 PCI = 76	250 PCI = 43	267 PCI = 52	271 PCI = 83
273 PCI = 62	280 PCI = 66	285 PCI = 78	303 PCI = 100	305 PCI = 100	310 PCI = 51	410 PCI = 67	2605 PCI = 73	3205 PCI = 65
4110 PCI = 49	4305 PCI = 91	4313 PCI = 98	4325 PCI = 98	4340 PCI = 95	4345 PCI = 98	4410 PCI = 71	4415 PCI = 72	

RUNWAY 18-36 150' x 7,000'

RUNWAY 9-27 150' x 8,000'

RW 18-36

RW 13-31

RW 9-27

TW A

TW A1

TW A2

TW A3

TW A4

TW A5

TW A6

TW A7

TW A8

TW A9

TW A10

TW A11

TW A12

TW B

TW B1

TW B2

TW B3

TW B4

TW B5

TW B6

TW B7

TW B8

TW B9

TW C

TW D

TW Z

TL T-HANG

AP S

AP N

AP C

AP RU 18

AP HELI

AP TERM

AP CARGO

110 PCI = 64
107 PCI = 67
5505 PCI = 64
115
106 PCI = 61
130 PCI = 67
405 PCI = 69
125
3105 PCI = 62
410
3105
140
3115 PCI = 46
3110 PCI = 52
4405 PCI = 80
4410
4425 PCI = 75
150
4420 PCI = 79
105
4415
160
4505 PCI = 74
155 PCI = 100
175
165
170
4315 PCI = 96
4320 PCI = 97
4332 PCI = 96
4310 PCI = 95
4313
4305
4325
2610 PCI = 42
2615 PCI = 70
277 PCI = 69
275 PCI = 53
185
180
3205
4345
103
305
4340
285
2605
6130 PCI = 100
6125 PCI = 100
303
307 PCI = 65
280
310
273
315 PCI = 69
209
207
203 PCI = 74
295 PCI = 55
290 PCI = 83
271
270 PCI = 85
267
4110
250
265 PCI = 59
260 PCI = 84
6210 PCI = 90
6205 PCI = 88
4105 PCI = 80
4205 PCI = 84
240
4215 PCI = 79
4210 PCI = 74
235 PCI = 76
205 PCI = 50
230 PCI = 90
220 PCI = 87
210 PCI = 54
215 PCI = 87
6160 PCI = 100
6155 PCI = 100
6110 PCI = 100
6105 PCI = 100

18
36
6
27

LEGEND

RW 13-31 — TYPICAL RUNWAY BRANCH ID

TW A — TYPICAL TAXIWAY BRANCH ID

AP S — TYPICAL APRON BRANCH ID

2022 PAVEMENT CONDITION INDEX

- PCI 86-100 Good
- PCI 71-85 Satisfactory
- PCI 56-70 Fair
- PCI 41-55 Poor
- PCI 26-40 Very Poor
- PCI 11-25 Serious
- PCI 0-10 Failed

"SECTION ID"
"PCI VALUE"

RUNWAY LENGTHS DEPICTED IN THIS DRAWING ARE FOR PAVEMENT MANAGEMENT PURPOSES ONLY AND MAY NOT MATCH PUBLISHED RUNWAY LENGTHS. DRAWING NOT TO SCALE.

4.2 Summary of Pavement Condition Evaluation Results

4.2.1 Network-Level Observations

The PCI assessment for Tallahassee International Airport (TLH) was performed in December 2021. The overall area-weighted average PCI value of the network was 81, representing a condition rating of Satisfactory. A large portion of the airfield pavement was not inspected due to the major rehabilitation project anticipated to take place in 2023. These areas include the entirety of Runway 18-36 and a portion of Taxiway A, Taxiway B, and Taxiway C.

Based on the FAA 5010 Report as of 11/04/2022, the Airport has reported 984,087 operations for 12 months ending 03/31/2022.

4.2.2 Branch-Level Observations

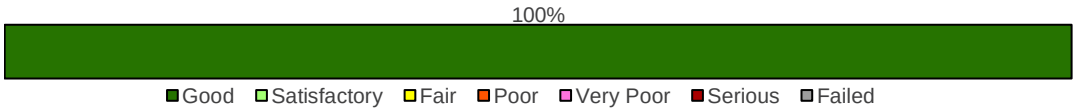
The following branch-level observations are a summary of select pavement facilities identified during the PCI assessment, including a discussion of general conditions and branch characteristics. The summary may not include all branches and/or sections within the Airport's airfield pavement network. Representative distress photographs of airfield pavements are presented in **Appendix D**. "Vicinity" photos refer to the approximate boundaries of an inspected sample unit within the section and provide an overview of the section condition but are not focused on a specific distress. The Re-inspection Report found in **Appendix E** provides listings of each sample unit and distress.

Runways

RW 18-36

Branch ID	Branch Use	Number of Sections	Branch Area (SF)	Branch Area-Weighted Avg PCI	Branch Condition Rating
RW 18-36	RUNWAY	6	1,050,000	100	Good

The following bar graph shows proportional distribution (as % of area within branch) of condition categories among sections within the branch. Given the individual section data shown in the subsequent table, the distribution is as follows: 100% Good (86-100 PCI).



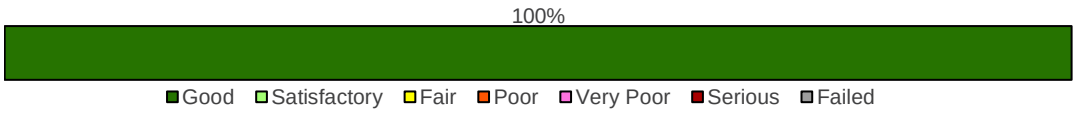
Section ID	Surface Type	Section Area (SF)	PCI	Condition Rating
6105	AC	607,550	100	Good
6110	AC	303,775	100	Good
6125	AAC	63,750	100	Good
6130	AAC	31,875	100	Good
6155	AAC	28,700	100	Good
6160	AAC	14,350	100	Good

RW 18-36 consists of 6 flexible pavement sections, totaling 1,050,000 sf. The last major construction date for the branch was 2023. Overall, RW 18-36 is in Good condition with an area-weighted average PCI of 100.

RW 9-27

Branch ID	Branch Use	Number of Sections	Branch Area (SF)	Branch Area-Weighted Avg PCI	Branch Condition Rating
RW 9-27	RUNWAY	2	1,200,000	89	Good

The following bar graph shows proportional distribution (as % of area within branch) of condition categories among sections within the branch. Given the individual section data shown in the subsequent table, the distribution is as follows: 100% Good (86-100 PCI).



Section ID	Surface Type	Section Area (SF)	PCI	Condition Rating
6205	AC	400,000	88	Good
6210	AC	800,000	90	Good

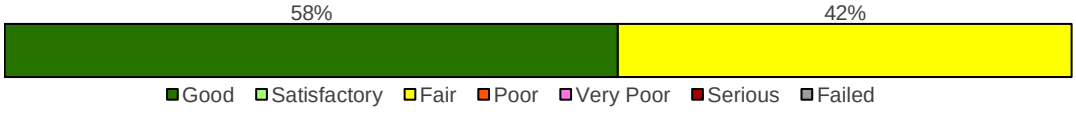
RW 9-27 consists of 2 flexible pavement sections, totaling 1,200,000 sf. The last major construction date for the branch was 2015, resulting in an area-weighted average age at inspection of 7 years old. Overall, RW 9-27 is in Good condition with an area-weighted average PCI of 89.

Taxiways

TW A

Branch ID	Branch Use	Number of Sections	Branch Area (SF)	Branch Area-Weighted Avg PCI	Branch Condition Rating
TW A	TAXIWAY	4	562,900	84	Satisfactory

The following bar graph shows proportional distribution (as % of area within branch) of condition categories among sections within the branch. Given the individual section data shown in the subsequent table, the distribution is as follows: 58% Good (86-100 PCI), 42% Fair (56-70 PCI).



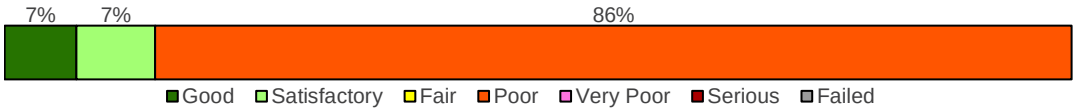
Section ID	Surface Type	Section Area (SF)	PCI	Condition Rating
103	AAC	79,944	100	Good
105	AAC	243,781	100	Good
106	AC	215,250	61	Fair
107	AC	23,925	67	Fair

TW A consists of 4 flexible pavement sections, totaling 562,900 sf. The last major construction dates range from 2005 to 2023, resulting in an area-weighted average age at inspection of 7 years old. Overall, TW A is in Satisfactory condition with an area-weighted average PCI of 84.

TW B

Branch ID	Branch Use	Number of Sections	Branch Area (SF)	Branch Area-Weighted Avg PCI	Branch Condition Rating
TW B	TAXIWAY	4	677,024	55	Poor

The following bar graph shows proportional distribution (as % of area within branch) of condition categories among sections within the branch. Given the individual section data shown in the subsequent table, the distribution is as follows: 7% Good (86-100 PCI), 7% Satisfactory (71-85 PCI), 86% Poor (41-55 PCI).



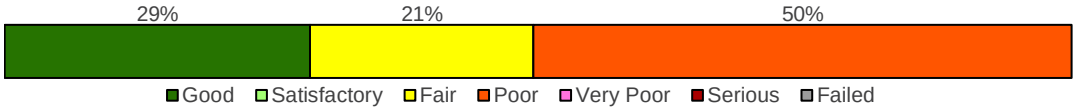
Section ID	Surface Type	Section Area (SF)	PCI	Condition Rating
203	AC	50,342	74	Satisfactory
205	AC	581,353	50	Poor
207	AAC	15,151	100	Good
209	AAC	30,178	100	Good

TW B consists of 4 flexible pavement sections, totaling 677,024 sf. The last major construction dates range from 2005 to 2023, resulting in an area-weighted average age at inspection of 15 years old. Overall, TW B is in Poor condition with an area-weighted average PCI of 55.

TW C

Branch ID	Branch Use	Number of Sections	Branch Area (SF)	Branch Area-Weighted Avg PCI	Branch Condition Rating
TW C	TAXIWAY	5	318,249	69	Fair

The following bar graph shows proportional distribution (as % of area within branch) of condition categories among sections within the branch. Given the individual section data shown in the subsequent table, the distribution is as follows: 29% Good (86-100 PCI), 21% Fair (56-70 PCI), 50% Poor (41-55 PCI).



Section ID	Surface Type	Section Area (SF)	PCI	Condition Rating
303	AAC	37,868	100	Good
305	AAC	53,314	100	Good
307	AAC	10,756	65	Fair
310	AAC	160,476	51	Poor
315	AAC	55,835	69	Fair

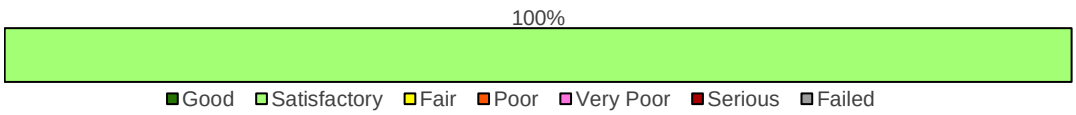
TW C consists of 5 flexible pavement sections, totaling 318,249 sf. The last major construction dates range from 1992 to 2023, resulting in an area-weighted average age at inspection of 19 years old. Overall, TW C is in Fair condition with an area-weighted average PCI of 69.

Aprons

AP CARGO

Branch ID	Branch Use	Number of Sections	Branch Area (SF)	Branch Area-Weighted Avg PCI	Branch Condition Rating
AP CARGO	APRON	3	484,155	76	Satisfactory

The following bar graph shows proportional distribution (as % of area within branch) of condition categories among sections within the branch. Given the individual section data shown in the subsequent table, the distribution is as follows: 100% Satisfactory (71-85 PCI).



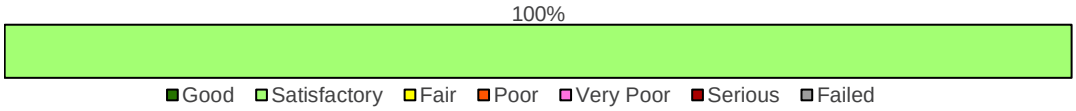
Section ID	Surface Type	Section Area (SF)	PCI	Condition Rating
4205	AC	65,663	84	Satisfactory
4210	AC	400,242	74	Satisfactory
4215	PCC	18,250	79	Satisfactory

AP CARGO consists of 2 flexible and 1 rigid pavement sections, totaling 484,155 sf. The last major construction dates range from 1990 to 2007, resulting in an area-weighted average age at inspection of 17 years old. Overall, AP CARGO is in Satisfactory condition with an area-weighted average PCI of 76.

AP N

Branch ID	Branch Use	Number of Sections	Branch Area (SF)	Branch Area-Weighted Avg PCI	Branch Condition Rating
AP N	APRON	5	637,391	73	Satisfactory

The following bar graph shows proportional distribution (as % of area within branch) of condition categories among sections within the branch. Given the individual section data shown in the subsequent table, the distribution is as follows: 100% Satisfactory (71-85 PCI).



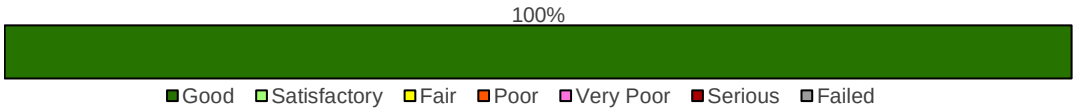
Section ID	Surface Type	Section Area (SF)	PCI	Condition Rating
4405	AAC	77,291	80	Satisfactory
4410	AAC	215,063	71	Satisfactory
4415	APC	310,550	72	Satisfactory
4420	APC	24,514	79	Satisfactory
4425	AC	9,973	75	Satisfactory

AP N consists of 5 flexible pavement sections, totaling 637,391 sf. The last major construction date for the branch was 2010, resulting in an area-weighted average age at inspection of 12 years old. Overall, AP N is in Satisfactory condition with an area-weighted average PCI of 73.

AP S

Branch ID	Branch Use	Number of Sections	Branch Area (SF)	Branch Area-Weighted Avg PCI	Branch Condition Rating
AP S	APRON	7	796,292	95	Good

The following bar graph shows proportional distribution (as % of area within branch) of condition categories among sections within the branch. Given the individual section data shown in the subsequent table, the distribution is as follows: 100% Good (86-100 PCI).



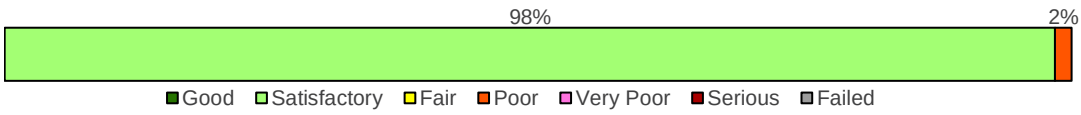
Section ID	Surface Type	Section Area (SF)	PCI	Condition Rating
4305	AAC	70,348	91	Good
4310	AAC	179,279	95	Good
4313	PCC	11,875	98	Good
4315	AAC	60,505	96	Good
4320	AAC	68,878	97	Good
4325	PCC	4,183	98	Good
4332	AC	401,224	96	Good

AP S consists of 5 flexible and 2 rigid pavement sections, totaling 796,292 sf. The last major construction date for the branch was 2018, resulting in an area-weighted average age at inspection of 4 years old. Overall, AP S is in Good condition with an area-weighted average PCI of 95.

AP TERM

Branch ID	Branch Use	Number of Sections	Branch Area (SF)	Branch Area-Weighted Avg PCI	Branch Condition Rating
AP TERM	APRON	2	868,701	80	Satisfactory

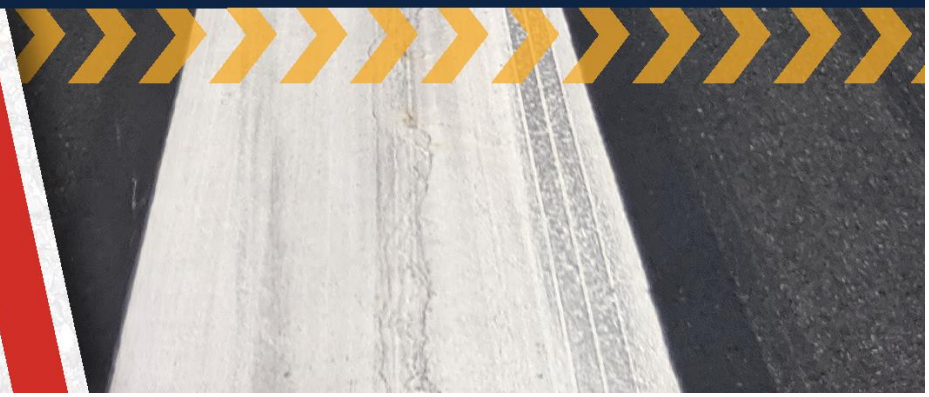
The following bar graph shows proportional distribution (as % of area within branch) of condition categories among sections within the branch. Given the individual section data shown in the subsequent table, the distribution is as follows: 98% Satisfactory (71-85 PCI), 2% Poor (41-55 PCI).



Section ID	Surface Type	Section Area (SF)	PCI	Condition Rating
4105	PCC	855,384	80	Satisfactory
4110	APC	13,317	49	Poor

AP TERM consists of 1 flexible and 1 rigid pavement sections, totaling 868,701 sf. The last major construction dates range from 1989 to 2005, resulting in an area-weighted average age at inspection of 33 years old. Overall, AP TERM is in Satisfactory condition with an area-weighted average PCI of 80.

Chapter 5: SAPMP Customization



Chapter 5 – SAPMP Customization

Once the PAVER™ database is populated with inventory and condition data (including PCI and rank), it is further customized with key elements such as network-level attributes, performance models, critical PCI, maintenance policies, and unit costs that are specific to the FDOT SAPMP. Each of these factors play a role in the development of rehabilitation strategies as they help to identify maintenance and rehabilitation needs for long-term management.

The FDOT SAPMP is organized to provide airports with planning-level data and does not intend to preclude the responsible engineer from performing the appropriate level of investigation and analysis in determining the appropriate design details of a pavement rehabilitation. It would not be advisable to solely base design-level rehabilitation without the appropriate level of investigation and determination of pavement deterioration beyond that of a visual functional condition assessment.

5.1 Network-Level Customization

The network-level attribute fields used in the FDOT SAPMP PAVER™ database consist of the Network, Airport Classification, District, FAA ADO Area, Inspection Phase, and Continuing Florida Aviation System Planning Process (CFASPP) Center. Each of these elements are briefly defined below.

- The “Network” field identifies the airport being analyzed;
- The “Airport Classification” field classifies the Airport according to the type and volume of aircraft traffic;
  - “GA” for General Aviation, community airports
  - “RL” for Regional Relievers
  - “PR” for Primary/Commercial airports
- The “District” field identifies the FDOT District to which the Airport belongs;
- The “FAA ADO Area” is an area used by the Orlando ADO to assign airports within those areas to the responsible FAA ADO personnel (planners, engineers, and environmentalists);
- The “Inspection Phase” denotes which phase of the SAPMP the Airport is surveyed (Phase 1 or Phase 2); and
- The “CFASPP Center” identifies which Region or Metropolitan Area of the Continuing Florida Aviation Systems Planning Process an Airport falls within.

5.2 Pavement Condition Forecasts

Pavement performance models, alternatively known as forecast models, prediction curves, or family curves, are developed from past and current distress data, as well as age data. These prediction curves are used to develop forecasts of PCI values that then help determine optimum timing for pavement maintenance and rehabilitation.

5.2.1 Forecasting PCI Considerations

Performance models will continue to be refined as the FDOT updates the SAPMP with subsequent PCI surveys. With the refinement of additional PCI and age data points, the forecasting of pavement conditions will continue to better reflect the performance trends of airfield pavements in the FAS. As a reminder, forecasting of pavement condition for the Airport is intended for planning purposes only. **The estimation of forecasted PCI values gives no assurance of future pavement conditions as PCI values represent an engineering estimation to be used as a planning tool. Forecasted PCI data should not be the sole metric for determining the year in which a project should be planned. Design-level planning should be undertaken by the responsible engineer prior to the development of airfield design plans.** Design-level recommendations for pavement rehabilitation and/or reconstruction will require the appropriate application of the procedures defined in the FAA AC 150/5320-6F.

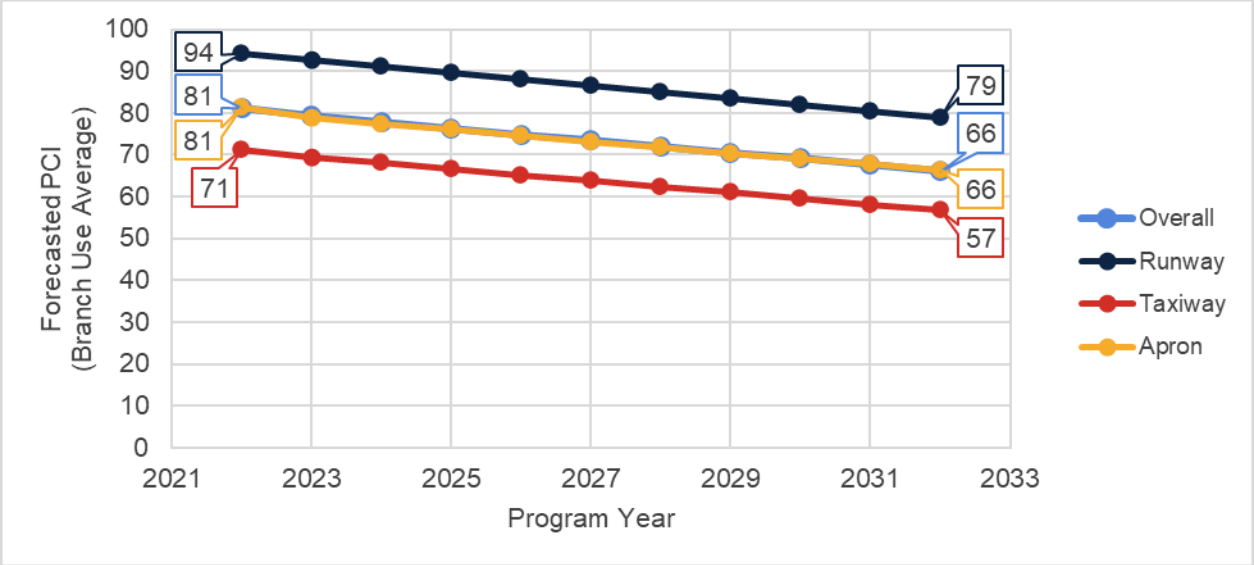
5.2.2 Performance Models

To develop pavement performance models, data for each section is combined into "groups" or "families" according to pavement type, traffic, and functional use. For the FDOT SAPMP, the models were defined for both PCC- and AC-surfaced pavements and further divided according to functional use. Based on average deterioration rates for different pavement types, each pavement section is assigned to a specific deterioration family to forecast the condition over a 10-year period.

5.2.3 Branch-Level Pavement Condition Forecast

Figure 5.2.3 depicts the branch-level pavement condition forecast for each branch use (Runway, Taxiway, Taxilane, and/or Apron) as well as the overall network. The condition forecasts are for a 10-year duration, starting in 2023 through 2032.

Figure 5.2.3: Forecasted Branch-Level Pavement Performance



5.2.4 Section-Level Pavement Condition Forecast

Table 5.2.4 provides section-level details for PCI forecasts. Pavement condition forecasts should be used for planning purposes only, as actual condition of sections is subject to the sensitivities in changes of traffic and maintenance frequency.

Table 5.2.4: Forecasted PCI Values 2023-2032 – Section-Level

Network ID	Branch ID	Section ID	Current PCI	2023	2024	2025	2026	2027	2028	2029	2030	2031	2032
TLH	RW 9-27	6205	88	86	84	83	81	80	78	77	75	74	72
TLH	RW 9-27	6210	90	88	86	85	83	82	80	79	77	76	74
TLH	RW 18-36	6105	100	99	98	96	95	93	92	90	89	87	86
TLH	RW 18-36	6110	100	99	98	96	95	93	92	90	89	87	86
TLH	RW 18-36	6125	100	99	97	95	93	91	89	87	86	84	82
TLH	RW 18-36	6130	100	99	97	95	93	91	89	87	86	84	82
TLH	RW 18-36	6155	100	99	97	95	93	91	89	87	86	84	82
TLH	RW 18-36	6160	100	99	97	95	93	91	89	87	86	84	82
TLH	TL AP S	3205	65	63	61	60	59	58	57	56	55	54	54
TLH	TL T-HANG	3105	62	61	60	59	58	57	57	56	55	54	53
TLH	TL T-HANG	3110	52	50	49	48	47	46	45	43	42	41	39
TLH	TL T-HANG	3115	46	44	43	41	40	38	36	35	33	31	29
TLH	TW A	103	100	99	96	94	91	89	87	84	82	80	78
TLH	TW A	105	100	99	96	94	91	89	87	84	82	80	78
TLH	TW A	106	61	60	59	58	57	56	56	55	54	53	52
TLH	TW A	107	67	65	65	64	63	62	61	60	59	59	58
TLH	TW A1	110	64	63	62	61	60	59	58	58	57	56	55
TLH	TW A10	170	100	99	97	94	92	90	88	86	85	83	81
TLH	TW A10	175	70	68	67	66	65	64	64	63	62	61	60
TLH	TW A11	180	100	99	96	94	91	89	87	84	82	80	78
TLH	TW A12	185	100	99	96	94	91	89	87	84	82	80	78
TLH	TW A2	115	70	68	67	66	65	64	64	63	62	61	60
TLH	TW A3	125	61	60	59	58	57	56	56	55	54	53	52
TLH	TW A3	130	67	65	65	64	63	62	61	60	59	59	58
TLH	TW A4	140	54	53	52	51	50	49	47	46	45	44	42
TLH	TW A7	150	100	99	96	94	91	89	87	84	82	80	78
TLH	TW A8	155	100	99	96	94	91	89	87	84	82	80	78
TLH	TW A8	160	100	99	96	94	91	89	87	84	82	80	78
TLH	TW A9	165	100	99	97	94	92	90	88	86	85	83	81
TLH	TW B	203	74	72	71	70	69	68	67	66	65	64	63
TLH	TW B	205	50	48	47	46	45	43	42	41	39	37	36
TLH	TW B	207	100	99	96	94	91	89	87	84	82	80	78
TLH	TW B	209	100	99	96	94	91	89	87	84	82	80	78
TLH	TW B1	210	54	53	52	51	50	49	47	46	45	44	42
TLH	TW B1	215	87	84	83	81	79	78	76	75	74	73	71
TLH	TW B2	220	87	84	83	81	79	78	76	75	74	73	71
TLH	TW B3	230	90	87	85	83	82	80	79	77	76	75	73
TLH	TW B3	235	76	74	73	72	70	69	68	67	66	65	64
TLH	TW B4	240	76	74	73	72	70	69	68	67	66	65	64

Network ID	Branch ID	Section ID	Current PCI	2023	2024	2025	2026	2027	2028	2029	2030	2031	2032
TLH	TW B5	250	43	41	39	38	36	34	32	30	28	26	24
TLH	TW B6	260	84	81	80	78	77	76	74	73	72	71	70
TLH	TW B6	265	59	58	57	56	55	54	53	52	52	51	50
TLH	TW B6	267	52	50	49	48	47	46	45	43	42	41	39
TLH	TW B7	270	85	82	81	79	78	76	75	74	72	71	70
TLH	TW B7	271	83	80	79	77	76	75	74	72	71	70	69
TLH	TW B7	273	62	61	60	59	58	57	57	56	55	54	53
TLH	TW B7	275	53	52	51	51	50	50	49	48	48	47	46
TLH	TW B7	277	69	66	65	63	62	61	60	59	57	57	56
TLH	TW B8	280	66	65	64	63	62	61	60	59	59	58	57
TLH	TW B8	285	78	76	74	73	72	71	70	69	68	67	66
TLH	TW B9	290	83	80	79	77	76	75	74	72	71	70	69
TLH	TW B9	295	55	54	53	52	51	50	49	48	46	45	44
TLH	TW C	303	100	99	96	94	91	89	87	84	82	80	78
TLH	TW C	305	100	99	96	94	91	89	87	84	82	80	78
TLH	TW C	307	65	63	61	60	59	58	57	56	55	54	54
TLH	TW C	310	51	50	49	49	48	48	47	46	45	44	43
TLH	TW C	315	69	66	65	63	62	61	60	59	57	57	56
TLH	TW D	405	69	67	66	65	65	64	63	62	61	60	59
TLH	TW D	410	67	65	65	64	63	62	61	60	59	59	58
TLH	TW Z	2605	73	71	70	69	68	67	66	65	64	63	62
TLH	TW Z	2610	42	40	38	36	35	33	31	29	27	25	23
TLH	TW Z	2615	70	68	67	66	65	64	64	63	62	61	60
TLH	AP C	4505	74	71	70	68	66	65	63	61	60	58	56
TLH	AP CARGO	4205	84	81	80	78	76	75	73	71	70	68	66
TLH	AP CARGO	4210	74	71	70	68	66	65	63	61	60	58	56
TLH	AP CARGO	4215	79	78	78	77	77	76	76	75	74	74	73
TLH	AP HELI	4340	95	93	92	91	90	89	89	88	87	86	85
TLH	AP HELI	4345	98	95	94	92	90	89	87	85	84	82	80
TLH	AP N	4405	80	77	74	72	71	69	67	65	64	62	61
TLH	AP N	4410	71	68	66	65	63	62	60	59	57	56	55
TLH	AP N	4415	72	69	67	66	64	63	61	60	58	57	55
TLH	AP N	4420	79	76	74	72	70	68	66	65	63	62	60
TLH	AP N	4425	75	72	71	69	67	66	64	62	61	59	57
TLH	AP RU 18	5505	64	61	60	58	56	55	53	51	50	48	46
TLH	AP S	4305	91	87	84	82	79	77	75	73	71	69	68
TLH	AP S	4310	95	90	88	85	83	80	78	76	74	72	70
TLH	AP S	4313	98	96	95	94	93	92	91	90	89	88	87
TLH	AP S	4315	96	91	88	86	83	81	79	76	74	72	71
TLH	AP S	4320	97	92	89	87	84	82	79	77	75	73	71
TLH	AP S	4325	98	96	95	94	93	92	91	90	89	88	87
TLH	AP S	4332	96	93	92	90	88	87	85	83	82	80	78
TLH	AP TERM	4105	80	79	79	78	78	77	77	76	75	75	74
TLH	AP TERM	4110	49	47	45	44	42	40	39	37	35	33	31

5.3 Critical PCI Value

An important concept in pavement management is the critical PCI value, a value that prompts major rehabilitation activities. It serves as a condition threshold that helps determine a section's suitability to receive major work. As soon as a section's PCI reaches the critical PCI value, the rate of PCI loss (deterioration) is expected to increase. The critical PCI concept assumes that once a pavement section deteriorates to this critical level, it is more cost-effective to complete a major rehabilitation project rather than continuing to apply preventive maintenance or deferring major work until more costly reconstruction activities are required. **Figure 5.3 (a)** illustrates the benefit of applying lower cost preventive maintenance to extend the life of the pavement.

Figure 5.3 (a): Pavement Life and the Effect of Treatments

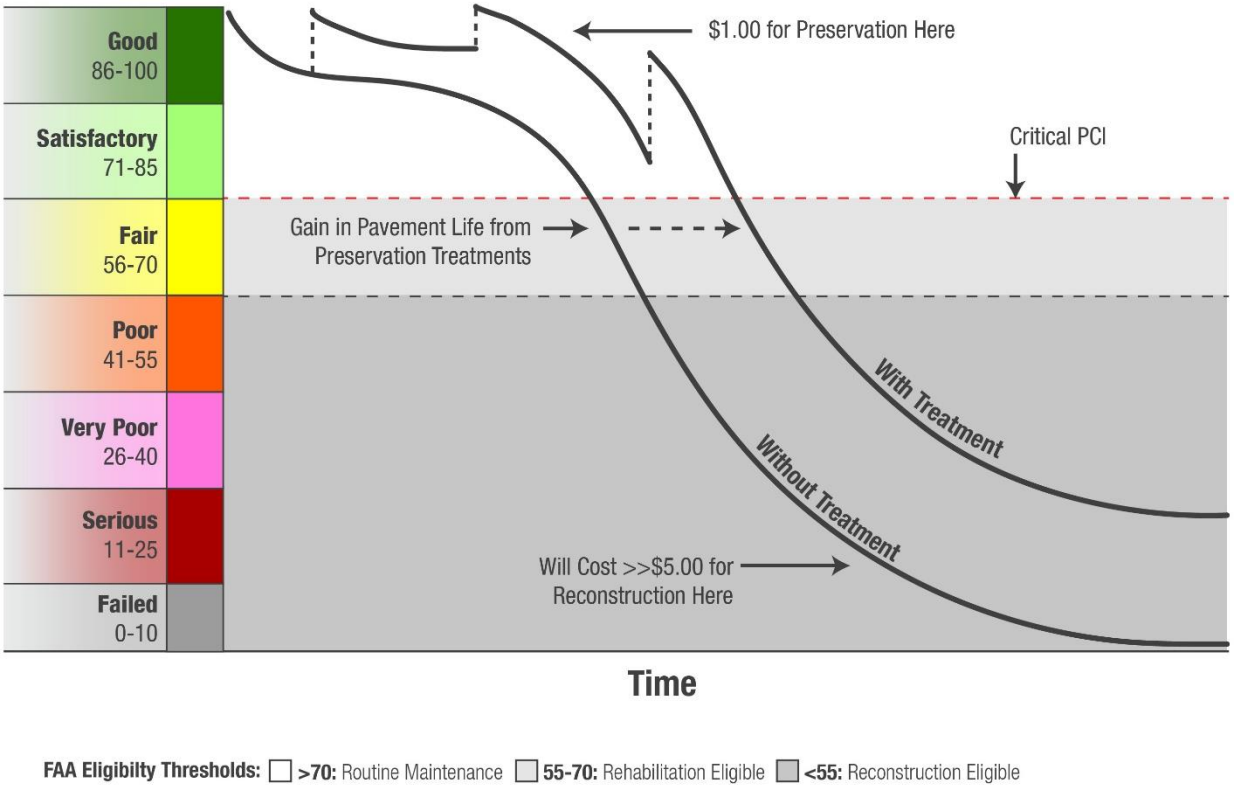


**Figure is for conceptual purposes only – unit costs are not specific to airfield pavements.*

Critical PCI values vary and are typically based on a pavement's surface type, functional use, and importance, or priority, in daily operations. Pavement priority is generally assigned based on the branch use of a pavement section. In previous System Updates, the critical PCI value was set to 65 for all functional uses. Now, based on FAA Order 5100.38D Change 1 Airport Improvement Handbook, issued February 26, 2019, the FAA has established pavement construction based on thresholds that distinguish Rehabilitation and Reconstruction. Pavement sections between PCI Values 55 and 70 will be considered for Rehabilitation and sections less than 55 will be considered for Reconstruction at the planning-level, as shown in **Table 5.3 (a)**. The FDOT SAPMP will

integrate the PCI thresholds for airfield pavement projects to maintain alignment with the FAA AIP and/or PFC eligibility for project planning. Moving forward, the critical PCI value will be defined at 70 for the FDOT SAPMP. Critical PCI values for this SAPMP System Update are shown in **Table 5.3 (b)**.

Table 5.3 (a): AIP Handbook PCI Requirements for Airfield Pavement Projects

Airfield Pavement Project Type	PCI Requirement
Reconstruction	PCI < 55 (Poor)
Rehabilitation	PCI < 70 (Fair)
Maintenance	N/A

**Source:* AIP Handbook, in reference to Runways, Taxiways, and Aprons as seen in table G-2, H-1, and I-1 respectively

Table 5.3 (b): Critical PCI Values by Branch Use

Runway	Taxiway	Apron
70	70	70

Figures 5.3 (b) and **5.3 (c)** depict the decision process for major rehabilitation project identification with the assumption of available funds (Shahin). Should funding be unavailable for pavement sections in need of major rehabilitation, the Airport may elect to apply appropriate localized stopgap repair strategies. As the figures show, once major rehabilitation has been applied, the PCI of the section is reset to 100.

Figure 5.3 (b): Major Rehabilitation Planning Decision Diagram, PCI < Critical PCI

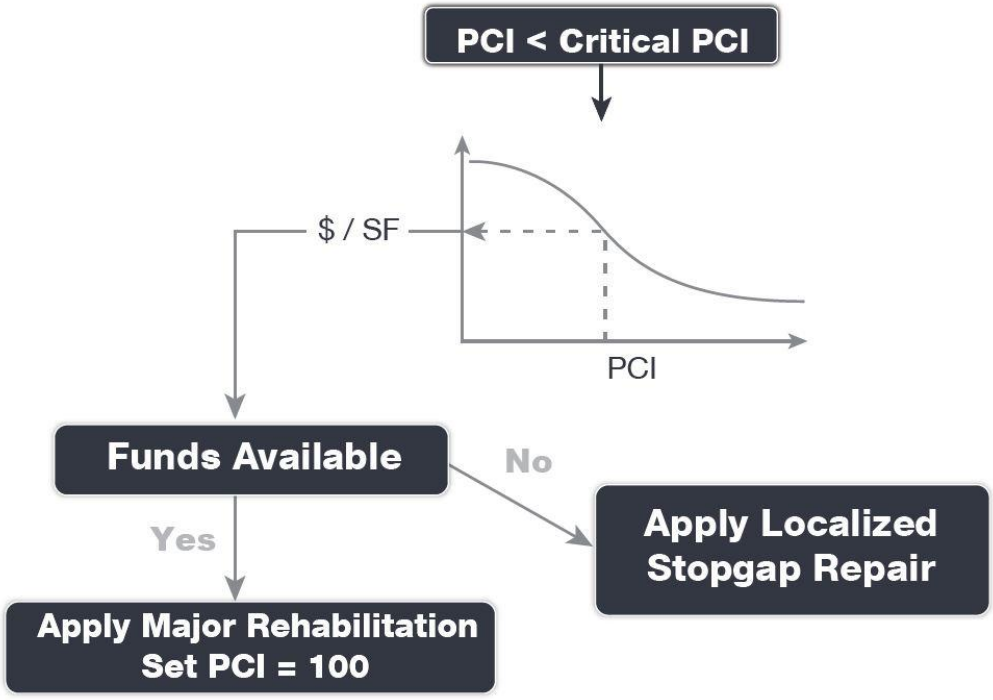
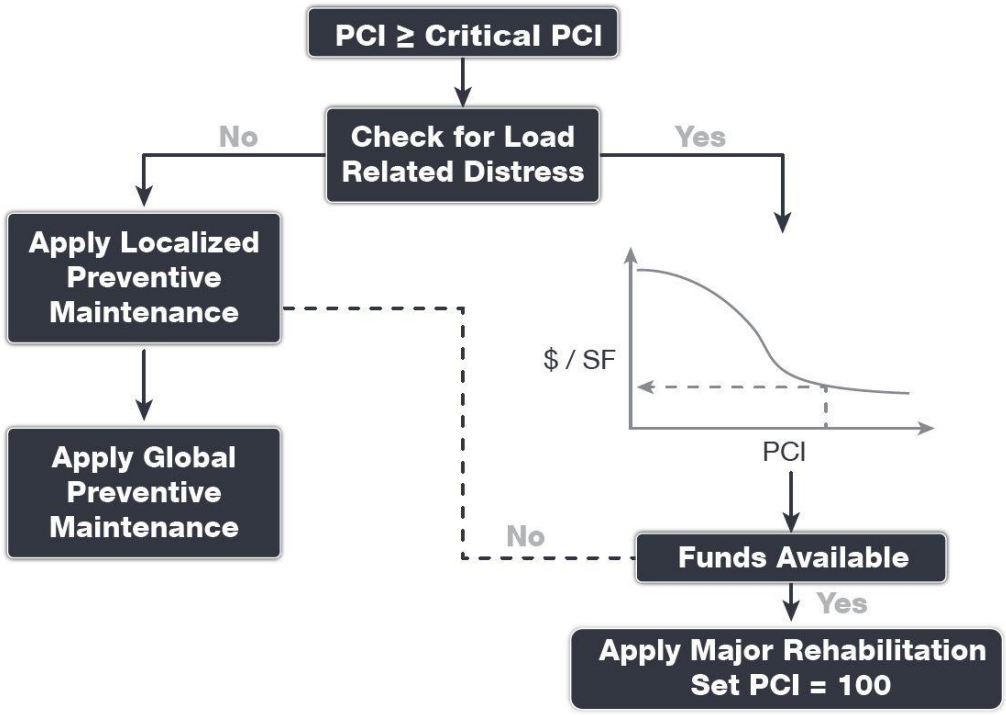


Figure 5.3 (c): Major Rehabilitation Planning Decision Diagram, PCI ≥ Critical PCI



5.4 Localized Maintenance and Repair

This section discusses both localized maintenance and major rehabilitation methods and how they may be most effectively applied to extend the life of the pavement network. General maintenance and rehabilitation (M&R) methods are characterized under two (2) broad categories: localized maintenance and major rehabilitation.

Localized maintenance is best applied as a conservation measure and is applied to slow the rate of pavement deterioration. It may, however, be applied as a temporary corrective measure in isolated areas. Proactive localized maintenance, and specifically preservation, is highly recommended to the Airport. However, it is recognized that once pavements have deteriorated below a certain condition threshold (the critical PCI value), the pavement benefits from more substantial rehabilitation in lieu of localized repairs.

Major rehabilitation is recommended when a pavement section falls below the critical PCI value or if a pavement section has a significant presence of load-related distress. Major rehabilitation efforts can correct or improve structural deficiencies and/or functional deterioration for pavement sections within a network.

M&R planning combines methods of repair to address the cause of the problem rather than just treating the symptom. For example, a PCC corner break may require slab under-sealing, full-depth patching, and joint sealing. While these repair methods apply to specific distress and pavement types, they also consider the impact of Foreign Object Debris (FOD) on aircraft operations. Untidy or improperly constructed repair activities may disintegrate and potentially create FOD at or near the repair site. Therefore, maintenance activities must include quality control monitoring to ensure that repairs are conducted properly and clean-up activities are undertaken to address this potential. The current version of the FAA Advisory Circular 150/5210-24 “Airport Foreign Object Debris (FOD) Management” provides additional guidance for developing and managing an airport FOD program.

5.4.1 Localized Maintenance and Repair Approach

Localized maintenance differs from major rehabilitation in that localized maintenance is applied based on the distresses observed and not an averaged or forecasted PCI value. Treatments are selected based on the appropriate corrective measure for a given distress type and severity level. Localized maintenance can be applied either as a preventive measure or a safety (“stopgap”) measure. The two (2) types of localized maintenance are described below in further detail.

- Localized Preventive Maintenance and Repair
  - Distress maintenance activities performed with the primary objective of slowing the rate of deterioration. These activities typically include crack sealing and patching.
- Localized Stopgap/Safety Maintenance and Repair
  - Defined as the localized distress repair needed to keep a pavement in a safe and operational condition. These activities are typically applied to high-severity distresses or distresses impacting operations.

5.4.2 Localized Work Types

The following sections provide detailed descriptions of the maintenance policy work types identified in the Localized Maintenance Policy.

AC Crack Sealing

Crack sealing is the process of cleaning and sealing (or resealing) cracks in AC pavements. This repair is used to fill longitudinal and transverse cracks, including reflective cracks and block cracks that are wider than 1/8-inch. The purpose of this treatment is to prevent water and incompressible materials from entering cracks and causing further deterioration of the pavement structure. Accumulation of incompressible materials in cracks may lead to spalling and is a source of FOD. Crack sealing is cost-effective when used as a preventive measure. Depending on the size of the crack, routing and cleaning the crack may be necessary to remove the loose material within the crack for better adherence of the crack sealant to the crack face. Measurement of this work type is typically in linear feet.

AC Full-Depth Patching

This technique involves replacing the full thickness of the AC layer and may include replacement of the base and subbase layers. Full-depth patching is used to repair structural and material-related distresses, such as alligator cracking, corrugation, depressions, rutting, slippage cracking, and swelling in AC pavements. This repair may be limited to the top AC layer (partial-depth patch) if the base and subbase layers exhibit no signs of deterioration. Measurement of this work type is typically in square feet or square yards.

AC Partial-Depth AC Patching

This technique involves the removal of a given thickness of the surface layer using a milling machine and adding back a layer of AC pavement. This technique removes the deteriorated layer and provides a good bond for an overlay. It can correct or improve the structural capacity or functional requirement, such as skid resistance and ride quality. This repair is used for surface distresses that can occur over a large area, such as raveling, shoving, and bleeding. While mill and replace can be a major rehabilitation M&R method when applied at a large scale, its application in a localized capacity to treat specific distress types also classifies it under localized maintenance for the purpose of this study. After milling operations are completed, any cracks still present should be cleaned and sealed prior to the placement of a tack coat and AC overlay layer(s). Measurement of this work type is typically in square feet or square yards.

Grinding

Grinding is the process of removing a thin layer of the existing concrete by grinding it with a series of closely spaced, rotating saw blades. This method is used to re-profile jointed concrete pavements with poor ride quality due to faulting or warping. Grinding is also used to restore transverse drainage and to provide a textured pavement surface. The concern with this type of maintenance is that if too much material is removed, the overall structural composition of the pavement section may change, potentially reducing the overall life of the pavement. Measurement of this work type is typically in square feet or square yards.

Monitor Pavement

Monitor pavement is recommended when the distresses do not interfere with ride quality, do not have FOD potential, and do not pose an immediate safety concern.

PCC Crack Sealing

Crack sealing is the process of routing, cleaning, and sealing (or resealing) cracks in PCC pavement to prevent water from infiltrating into the pavement foundation and to stop the accumulation of incompressible materials in the cracks. Water entering cracks can weaken the subgrade, potentially leading to pumping, corner breaks, and/or shattered slabs. Accumulation of incompressible materials in cracks may lead to spalling and is a source of FOD. Routing and cleaning of the crack is often necessary to adhere the crack sealant to both sides of the crack. Measurement of this work type is typically in linear feet.

PCC Full-Depth Patching

This type of M&R activity involves full-depth replacement of a portion of a PCC slab. This repair is used for medium- and high-severity corner breaks, medium-severity durability cracking, medium-severity blowups and buckling, and high-severity large patches. This repair requires restoring load transfer if near a joint or crack. Measurement of this work type is typically in square feet or square yards.

PCC Joint Seal

Joint sealing is the process of cleaning and sealing (or resealing) joints in PCC pavement to prevent water from infiltrating into the pavement foundation and to stop the accumulation of incompressible materials in the joints. Water entering joints can weaken the subgrade, potentially leading to pumping, corner breaks, and/or shattered slabs. Accumulation of incompressible materials in joints leads to spalling of the concrete and is a source of FOD. In some cases, it may be necessary to re-saw the pavement joints to remove old material prior to resealing. Measurement of this work type is typically in linear feet.

PCC Partial-Depth Patching

Partial-depth patching involves removing shallow, localized areas of deteriorated or spalled PCC pavement and replacing them with a suitable patch-like cement concrete or epoxy concrete. This method is used to repair distresses that are confined to the top few inches of the slab, such as joint and corner spalling. This repair would require restoring the joint sealant if near a joint. Measurement of this work type is typically in square feet or square yards.

PCC Slab Replacement

This type of M&R activity involves full-depth replacement of an entire PCC slab. This repair is used to repair high-severity blowups and buckling, high-severity durability cracking, medium- and high-severity shattered slabs, and medium- and high-severity ASR. This repair requires restoring load transfer with adjacent slabs through dowels or similar means. Measurement of this work type is typically in square feet or square yards.

Surface Seal

Application of a surface treatment provides AC-surfaced pavements with an unoxidized layer of bituminous material that can help extend the life of a pavement that is experiencing climate-related distresses such as weathering and raveling. The surface treatment can also serve as a repair that re-establishes a bond between aggregates, slowing pavement deterioration and reducing FOD potential. Measurement of this work type is typically in square feet or square yards.

5.4.3 Localized Maintenance Planning-Level Unit Costs

The activities identified here are based on research of practical pavement treatments in consideration of the FAA AC 150/5380-6C. The Localized Maintenance Policies and associated planning-level unit costs are developed in consideration of a network-level analysis.

The Localized Maintenance and Repair Policies and associated planning-level unit costs are based on a statewide consideration of pavement treatments and construction costs from both airfield pavements and the FDOT Historical Cost Information archives. Furthermore, a consideration of limited repair quantities is factored into the determination of conservative planning-level unit costs. Neither the FDOT nor the Consultant team have control over the cost of labor, materials, equipment, the Contractor's methods of determining prices, or over competitive bidding or market conditions. Opinions of probable construction costs provided herein are based on the information known to the FDOT at this time and represent only the Consultant team's judgment as a design professional familiar with the construction industry. This Report cannot and does not guarantee that proposals, bids, or actual construction costs will not vary from its opinions of probable construction costs.

Tables 5.4.3 (a) and **(b)** display the cost by maintenance activity for AC and PCC pavement types, respectively. Because the localized maintenance activities identified for both preventive and stopgap work types are based on a statewide network approach, project-specific evaluations and maintenance quantities should be developed prior to construction.

Table 5.4.3 (a): Localized M&R Planning-Level Unit Costs – Asphalt Concrete

Localized Work Type	Primary/Commercial Costs		Work Type Unit
AC Crack Sealing	$	4.00	LF
AC Full-Depth Patching	$	18.75	SF
AC Partial-Depth Patching	$	6.50	SF
Surface Seal	$	0.75	SF

Table 5.4.3 (b): Localized M&R Planning-Level Unit Costs – Portland Cement Concrete

Localized Work Type	Primary/Commercial Costs		Work Type Unit
Grinding	$	2.00	SF
PCC Crack Sealing	$	7.00	LF
PCC Joint Seal	$	4.25	LF
PCC Full-Depth Patching	$	75.00	SF
PCC Partial-Depth Patching	$	169.00	SF
PCC Slab Replacement	$	51.50	SF

**PCC Partial-Depth Patching considers high-early-strength and high-performing repair material.*

5.4.4 Localized Maintenance and Repair Policy

Table 5.4.4 and **Table 5.4.5** depicts the Localized Preventive Maintenance Policy and the Localized Stopgap Maintenance Policy for AC and PCC pavements. The resulting Localized Maintenance recommendations for this program are identified based on this policy.

Table 5.4.4: AC Pavement Localized Preventive& Stopgap Maintenance & Repair Policy

Distress	Severity	Description	AC Preventive Work Type	AC Stopgap Work Type
41	Low	Alligator Cracking	Monitor Pavement	Monitor Pavement
41	Medium	Alligator Cracking	AC Full Depth Patching	AC Full Depth Patching
41	High	Alligator Cracking	AC Full Depth Patching	AC Full Depth Patching
42	N/A	Bleeding	Monitor Pavement	Monitor Pavement
43	Low	Block Cracking	Monitor Pavement	Monitor Pavement
43	Medium	Block Cracking	AC Crack Sealing	Monitor Pavement
43	High	Block Cracking	AC Crack Sealing	AC Crack Sealing
44	Low	Corrugation	Monitor Pavement	Monitor Pavement
44	Medium	Corrugation	AC Full Depth Patching	Monitor Pavement
44	High	Corrugation	AC Full Depth Patching	AC Full Depth Patching
45	Low	Depression	Monitor Pavement	Monitor Pavement
45	Medium	Depression	AC Full Depth Patching	Monitor Pavement
45	High	Depression	AC Full Depth Patching	AC Full Depth Patching
46	N/A	Jet Blast	Monitor Pavement	Monitor Pavement
47	Low	Jt. Reflective Cracking	Monitor Pavement	Monitor Pavement
47	Medium	Jt. Reflective Cracking	AC Crack Sealing	Monitor Pavement
47	High	Jt. Reflective Cracking	AC Full Depth Patching	AC Full Depth Patching
48	Low	L&T Cracking	Monitor Pavement	Monitor Pavement
48	Medium	L&T Cracking	AC Crack Sealing	Monitor Pavement
48	High	L&T Cracking	AC Full Depth Patching	AC Full Depth Patching
49	N/A	Oil Spillage	Monitor Pavement	Monitor Pavement
50	Low	Patching	Monitor Pavement	Monitor Pavement
50	Medium	Patching	AC Full Depth Patching	Monitor Pavement
50	High	Patching	AC Full Depth Patching	AC Full Depth Patching
51	N/A	Polished Aggregate	Monitor Pavement	Monitor Pavement
52	Low	Raveling	Surface Seal	Monitor Pavement
52	Medium	Raveling	Surface Seal	Monitor Pavement
52	High	Raveling	AC Partial Depth Patching	AC Partial Depth Patching
53	Low	Rutting	Monitor Pavement	Monitor Pavement
53	Medium	Rutting	AC Full Depth Patching	Monitor Pavement
53	High	Rutting	AC Full Depth Patching	AC Full Depth Patching
54	Low	Shoving	Monitor Pavement	Monitor Pavement
54	Medium	Shoving	AC Partial Depth Patching	Monitor Pavement
54	High	Shoving	AC Full Depth Patching	AC Full Depth Patching
55	N/A	Slippage Cracking	AC Full Depth Patching	AC Full Depth Patching
56	Low	Swelling	Monitor Pavement	Monitor Pavement
56	Medium	Swelling	AC Full Depth Patching	Monitor Pavement
56	High	Swelling	AC Full Depth Patching	AC Full Depth Patching

Distress	Severity	Description	AC Preventive Work Type	AC Stopgap Work Type
57	Low	Weathering	Monitor Pavement	Monitor Pavement
57	Medium	Weathering	Surface Seal	Monitor Pavement
57	High	Weathering	AC Partial Depth Patching	Surface Seal

Table 5.4.5: PCC Pavement Localized Preventive& Stopgap Maintenance & Repair Policy

Distress	Severity	Description	PCC Preventive Work Type	PCC Stopgap Work Type
61	Low	Blow-up	PCC Full Depth Patching	Monitor Pavement
61	Medium	Blow-up	PCC Full Depth Patching	PCC Full Depth Patching
61	High	Blow-up	PCC Slab Replacement	PCC Slab Replacement
62	Low	Corner Break	Monitor Pavement	Monitor Pavement
62	Medium	Corner Break	PCC Full Depth Patching	PCC Full Depth Patching
62	High	Corner Break	PCC Full Depth Patching	PCC Full Depth Patching
63	Low	Linear Cracking	Monitor Pavement	Monitor Pavement
63	Medium	Linear Cracking	PCC Crack Sealing	PCC Crack Sealing
63	High	Linear Cracking	PCC Full Depth Patching	PCC Crack Sealing
64	Low	Durability Cracking	Monitor Pavement	Monitor Pavement
64	Medium	Durability Cracking	PCC Full Depth Patching	PCC Full Depth Patching
64	High	Durability Cracking	PCC Slab Replacement	PCC Slab Replacement
65	Low	Jt. Seal Damage	PCC Joint Seal	Monitor Pavement
65	Medium	Jt. Seal Damage	PCC Joint Seal	Monitor Pavement
65	High	Jt. Seal Damage	PCC Joint Seal	PCC Joint Seal
66	Low	Small Patch	Monitor Pavement	Monitor Pavement
66	Medium	Small Patch	PCC Partial Depth Patching	Monitor Pavement
66	High	Small Patch	PCC Partial Depth Patching	PCC Partial Depth Patching
67	Low	Large Patch	Monitor Pavement	Monitor Pavement
67	Medium	Large Patch	PCC Full Depth Patching	Monitor Pavement
67	High	Large Patch	PCC Full Depth Patching	PCC Full Depth Patching
68	N/A	Popouts	Monitor Pavement	Monitor Pavement
69	N/A	Pumping	Monitor Pavement	Monitor Pavement
70	Low	Scaling	Monitor Pavement	Monitor Pavement
70	Medium	Scaling	PCC Slab Replacement	Monitor Pavement
70	High	Scaling	PCC Slab Replacement	PCC Slab Replacement
71	Low	Faulting	Monitor Pavement	Monitor Pavement
71	Medium	Faulting	Grinding	Monitor Pavement
71	High	Faulting	PCC Slab Replacement	PCC Slab Replacement
72	Low	Shattered Slab	PCC Crack Sealing	Monitor Pavement
72	Medium	Shattered Slab	PCC Slab Replacement	PCC Crack Sealing
72	High	Shattered Slab	PCC Slab Replacement	PCC Slab Replacement
73	N/A	Shrinkage Cracking	Monitor Pavement	Monitor Pavement

Distress	Severity	Description	PCC Preventive Work Type	PCC Stopgap Work Type
74	Low	Joint Spall	Monitor Pavement	Monitor Pavement
74	Medium	Joint Spall	PCC Partial Depth Patching	PCC Partial Depth Patching
74	High	Joint Spall	PCC Partial Depth Patching	PCC Partial Depth Patching
75	Low	Corner Spall	Monitor Pavement	Monitor Pavement
75	Medium	Corner Spall	PCC Partial Depth Patching	PCC Partial Depth Patching
75	High	Corner Spall	PCC Partial Depth Patching	PCC Partial Depth Patching
76	Low	ASR	Monitor Pavement	Monitor Pavement
76	Medium	ASR	PCC Slab Replacement	PCC Slab Replacement
76	High	ASR	PCC Slab Replacement	PCC Slab Replacement

5.5 Major Rehabilitation

Major rehabilitation is recommended to correct or improve structural deficiencies and/or functional deterioration. Often, when pavements are subject to significant changes in the aircraft fleet mix (frequency and type), major rehabilitation is required to provide a pavement section that can meet the structural demands of traffic loading. Major rehabilitation is generally described as a pavement construction that removes and replaces the pavement surface, thus resetting the PCI value to 100 and the pavement age to zero. Typical policies include full- and partial-depth reconstruction and mill and overlay.

5.5.1 Major Rehabilitation Pavement Section Development

Once the timing of the major rehabilitation activity is determined based on the PCI value, existing as-built record documentation is used to determine typical rehabilitation processes and pavement sections. Refinement of the pavement section layers is performed in consideration of the FAA AC 150/5320-6F. It should be noted that no subsurface geotechnical investigation, American Land Title Association (ALTA)/American Congress on Surveying and Mapping (ACSM) Survey, topographic survey, utilities survey, environmental, or site-specific air traffic study(s) have been utilized in the development of the design criteria. No warranty or assurance is implied in this document for final design nor construction for any airfield pavements discussed within this Report.

Major rehabilitation is divided into two (2) policy categories as part of this System Update: Full-Depth Reconstruction (Reconstruction) and Intermediate Major Rehabilitation (Rehabilitation). Based on the pavement type, the general categories are defined as AC Reconstruction and AC Rehabilitation for AC, AAC, and APC pavement types, and PCC Reconstruction and PCC Rehabilitation for PCC pavement types. The pavement sections are based on the average Primary/Commercial Airport Type requirements; no pavement design has been performed in accordance with the FAA AC 150/5320-6F for the determined conceptual sections. **Table 5.5.1** provide details on the conceptual pavement sections developed for this study.

Table 5.5.1: Conceptual Pavement Sections for Major Rehabilitation

Rehabilitation Type	Primary/Commercial Pavement Section
AC Reconstruction	
Full-depth asphalt pavement section reconstruction. Removal of existing pavement section and construction of a new section. ***PCI < 55***	Pavement Removal
	Unclassified Excavation
	Subgrade Stabilization (12")
	Limerock Base Course (8")
	Prime Coat
	Tack Coat
	P-403 Stabilized Base Course (5")
	P-401 Surface Course (4")
	Excludes any paved shoulder features
AC Rehabilitation	
Combination of asphalt pavement milling and replacement overlay with 15% of the areas subject to full-depth reconstruction. ***PCI = 55 to 70***	**15% AC Reconstruction**
	Mill and Overlay
	AC Milling (4")
	Tack Coat
	P-401 Surface Course (4")
	Excludes any paved shoulder features
PCC Reconstruction	
Full-depth rigid pavement section reconstruction. ***PCI < 55***	Pavement Removal
	Unclassified Excavation
	Subgrade Stabilization (12")
	Limerock Base Course (6")
	Prime Coat
	Tack Coat
	P-403 Stabilized Base Course (5")
	P-501 PCC Pavement (17")
	PCC Joint Seal
PCC Rehabilitation	
Rehabilitation of PCC pavement with a combination of crack sealing, joint seal replacement, limited patching, and replacement of 15% of slab panels. ***PCI = 55 to 70***	**15% Slab Replacement**
	Joint and Crack Seal
	Limited Patching

The identification of rehabilitation needs and conceptual pavement sections have been determined at the planning level. Design-level investigation is recommended prior to developing construction-level design documents and budgets. **This type of construction typically warrants consideration for non-pavement efforts that may include drainage, turfing, electrical lighting, pavement marking, construction contingency, mobilization costs, and project soft costs.**

Reconstruction (AC or PCC)

Reconstruction is the removal and replacement of the existing AC or PCC pavement and base layer and includes preparation of the existing subgrade material. This technique is utilized when the pavement is badly deteriorated or a structural improvement is required. Reconstruction is used when the pavements are structurally deficient and an overlay is not possible due to adjacent pavement grades.

AC Rehabilitation

AC Rehabilitation, for the purposes of this SAPMP, is a removal of all or a portion of the asphalt surface through milling and replacing the milled depth with an overlay of asphalt. This rehabilitation activity is typically applied to pavement that does not require a structural improvement and does not display an extensive amount of load-related distresses. However, this work type conservatively accounts for 15% of the planned area to receive a full-depth replacement of the pavement structure. This is meant to capture any deficiencies that may not be apparent from a visual evaluation of the surface of the pavement. This work type occurs on pavement sections with a PCI value between 55 and 70. As a general rule of thumb, intermediate rehabilitation activities have a shorter pavement life compared to a full-depth reconstruction, but AC Rehabilitation will still reset the pavement to a PCI of 100.

PCC Rehabilitation

PCC Rehabilitation, for the purposes of this SAPMP, is a planning-level estimate of several concurrent PCC maintenance activities intended to raise the PCI above Critical without reconstructing the entire area. This work type accounts for the replacement of 15% of the slabs as well as a PCC patching, crack sealing, and joint sealing for areas outside of the panel replacement. This work type occurs on pavement sections with a PCI value between 55 and 70.

5.5.2 Major Rehabilitation Planning-Level Unit Costs

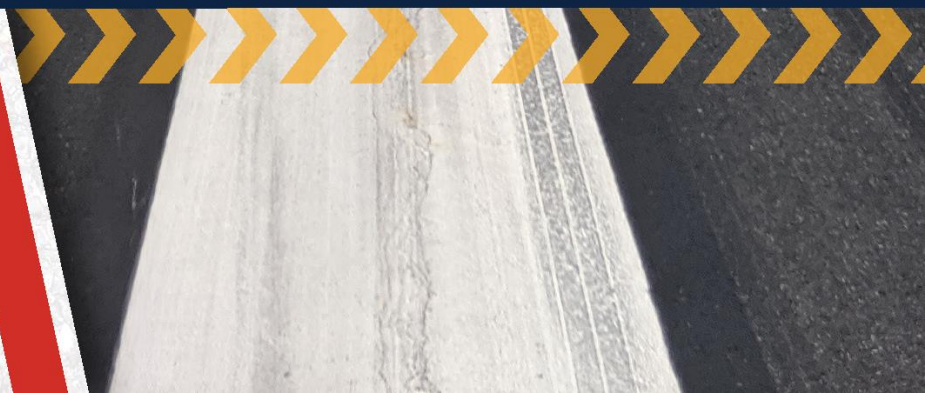
Planning-level opinions of probable construction cost developed for this System Update are based on archived bid tabulations and records from airfield pavement projects provided by participating airports. A review of cost trends and cost factors have been incorporated to assist airports in planning for project budgets.

Neither the FDOT nor the Consultant team have control over the cost of labor, materials, equipment, Contractor's methods of determining prices, or over competitive bidding or market conditions. Opinions of probable construction costs provided herein are based on the information known to the FDOT at this time and represent only the Consultant team's judgment as a design professional familiar with the construction industry. This Report cannot and does not guarantee that proposals, bids, or actual construction costs will not vary from its opinions of probable construction costs. **Table 5.5.2** depicts the associated work type planning-level unit costs for Major Rehabilitation for each pavement type.

Table 5.5.2: PR Major Rehabilitation Planning-Level Unit Cost by Pavement Type

Rehabilitation Type	PCI Range	Asphalt Concrete Cost per SF	Portland Cement Concrete Cost Per SF
Rehabilitation	55 to 70	$14.00	$30.50
Reconstruction	0 to 55	$30.50	$60.00

Chapter 6: M&R Planning and Budget Scenario Analysis



Chapter 6 – M&R Planning and Budget Scenario Analysis

6.1 Localized Maintenance and Repair Analysis and Recommendations

This FDOT SAPMP System Update provides a planning-level estimation of Localized Maintenance and Repair costs based on the results of the latest PCI assessment performed at the Airport. Due to the limited sample units inspected in certain pavement sections, a statistical extrapolation of distresses is used to estimate the quantities of recommended repair activities at the section level, based the policies defined in **5.4.4 Localized Maintenance and Repair Policy**. These work quantities are limited to a near-term application since they were determined directly from the PCI assessment efforts. As pavements continue to deteriorate year-to-year, quantities and/or distress severities may increase, which will affect the amount and type of localized maintenance required. This analysis can be utilized as a planning tool to assist Airport staff in determining an annual budget allocation for maintenance activities that will help maintain Airport pavements above the critical PCI value and extend the life of the pavement.

Table 6.1 (a) provides a summary of the anticipated planning-level costs for Year 1 Localized Preventive Maintenance and Localized Stopgap Maintenance. The following table depicts planning-level costs rounded up to the next 10-dollar increment.

Table 6.1 (a): Year 1 Summary of Localized Maintenance

Work Category	Cost	
Preventive	$	717,520
Stopgap	$	57,590
Planning-Level Localized M&R Needs =	***$***	***775,110***

Localized Preventive Maintenance is typically applied to pavements that are in a condition above the critical PCI value of the pavement section. Localized Stopgap Maintenance is typically applied to pavement sections that are at or below the critical PCI value. Application of localized maintenance and repair should be coordinated with the planning of major rehabilitation efforts identified through the Major Rehabilitation analysis. Pavements with stopgap recommendations that are subject to near-term major rehabilitation efforts may remove the need to perform localized (stopgap) maintenance efforts in subsequent years.

Table 6.1 (b) summarizes the anticipated Year 1 Localized Maintenance recommendations by work type, based on the PCI assessment efforts performed as part of this SAPMP System Update. The following table depicts planning-level costs rounded up to the next 10-dollar increment.

Table 6.1 (b): Year 1 Localized Maintenance by Work Type Summary

Localized Maintenance Category	Localized Work Type	Rough Estimate of Work Quantity	Work Units	Planning Material Cost
Localized Preventive Maintenance	AC Crack Sealing	13,548	LF	$ 54,260
	Surface Seal	155,828	SF	$ 116,950
	PCC Joint Seal	102,273	LF	$ 434,670
	PCC Partial-Depth Patching	624	SF	$ 105,610
	PCC Full-Depth Patching	32	SF	$ 2,430
	PCC Slab Replacement	70	SF	$ 3,600
Localized Stopgap Maintenance	AC Partial-Depth Patching	18	SF	$ 120
	AC Full-Depth Patching	3,063	SF	$ 57,470

Table 6.1 (c) provides a breakdown of the anticipated planning-level costs by section for those areas exhibiting distresses that would benefit from Year 1 Localized M&R. The table shows the approximate improved "End Condition" PCI value of the section after the application of Localized M&R. This approximation is intended to depict a planning-level estimate of the effect of the localized M&R on the section-level PCI; the performance of the work does not guarantee the pavement will not deteriorate in other ways outside of the described treatment. The following table depicts planning-level costs rounded up to the next 10-dollar increment.

Table 6.1 (c): Section-Level Year 1 Localized M&R Planning Cost Summary

Network ID	Branch ID	Section ID	Area (SF)	Start PCI	End PCI	Cost
TLH	RW 9-27	6205	400,000	88	90	$ 10,590
TLH	RW 9-27	6210	800,000	90	90	$ 450
TLH	RW 18-36	6105	607,550	100	100	$ -
TLH	RW 18-36	6110	303,775	100	100	$ -
TLH	RW 18-36	6125	63,750	100	100	$ -
TLH	RW 18-36	6130	31,875	100	100	$ -
TLH	RW 18-36	6155	28,700	100	100	$ -
TLH	RW 18-36	6160	14,350	100	100	$ -
TLH	TL AP S	3205	6,963	65	65	$ -
TLH	TL T-HANG	3105	46,227	62	62	$ -
TLH	TL T-HANG	3110	16,646	52	55	$ 3,710
TLH	TL T-HANG	3115	63,002	46	46	$ -
TLH	TW A	103	79,944	100	100	$ -
TLH	TW A	105	243,781	100	100	$ -
TLH	TW A	106	215,250	61	61	$ -
TLH	TW A	107	23,925	67	67	$ -
TLH	TW A1	110	40,291	64	64	$ -
TLH	TW A10	170	22,422	100	100	$ -
TLH	TW A10	175	4,954	70	70	$ -
TLH	TW A11	180	24,154	100	100	$ -
TLH	TW A12	185	43,156	100	100	$ -
TLH	TW A2	115	42,179	70	70	$ -
TLH	TW A3	125	32,329	61	61	$ -
TLH	TW A3	130	34,919	67	67	$ -

Network ID	Branch ID	Section ID	Area (SF)	Start PCI	End PCI	Cost
TLH	TW A4	140	19,805	54	54	$ -
TLH	TW A7	150	72,118	100	100	$ -
TLH	TW A8	155	43,518	100	100	$ -
TLH	TW A8	160	11,115	100	100	$ -
TLH	TW A9	165	51,254	100	100	$ -
TLH	TW B	203	50,342	74	77	$ 1,550
TLH	TW B	205	581,353	50	51	$ 53,760
TLH	TW B	207	15,151	100	100	$ -
TLH	TW B	209	30,178	100	100	$ -
TLH	TW B1	210	46,292	54	54	$ -
TLH	TW B1	215	4,782	87	87	$ -
TLH	TW B2	220	49,156	87	87	$ -
TLH	TW B3	230	63,794	90	92	$ 1,490
TLH	TW B3	235	83,567	76	86	$ 6,560
TLH	TW B4	240	48,156	76	88	$ 5,870
TLH	TW B5	250	24,545	43	43	$ -
TLH	TW B6	260	38,862	84	89	$ 380
TLH	TW B6	265	17,002	59	59	$ -
TLH	TW B6	267	24,158	52	52	$ -
TLH	TW B7	270	39,535	85	85	$ -
TLH	TW B7	271	23,946	83	83	$ -
TLH	TW B7	273	38,359	62	63	$ 120
TLH	TW B7	275	9,455	53	53	$ -
TLH	TW B7	277	8,669	69	69	$ -
TLH	TW B8	280	66,948	66	66	$ -
TLH	TW B8	285	58,220	78	84	$ 5,410
TLH	TW B9	290	20,199	83	85	$ 200
TLH	TW B9	295	84,260	55	55	$ -
TLH	TW C	303	37,868	100	100	$ -
TLH	TW C	305	53,314	100	100	$ -
TLH	TW C	307	10,756	65	65	$ -
TLH	TW C	310	160,476	51	51	$ -
TLH	TW C	315	55,835	69	69	$ -
TLH	TW D	405	33,610	69	69	$ -
TLH	TW D	410	10,157	67	67	$ -
TLH	TW Z	2605	62,575	73	87	$ 10,670
TLH	TW Z	2610	2,379	42	42	$ -
TLH	TW Z	2615	2,615	70	70	$ -
TLH	AP C	4505	265,932	74	84	$ 47,860
TLH	AP CARGO	4205	65,663	84	89	$ 740
TLH	AP CARGO	4210	400,242	74	83	$ 36,010
TLH	AP CARGO	4215	18,250	79	81	$ 3,280
TLH	AP HELI	4340	17,496	95	95	$ -
TLH	AP HELI	4345	50,224	98	98	$ -
TLH	AP N	4405	77,291	80	85	$ 3,490
TLH	AP N	4410	215,063	71	75	$ 10,230
TLH	AP N	4415	310,550	72	79	$ 28,350

Network ID	Branch ID	Section ID	Area (SF)	Start PCI	End PCI	Cost
TLH	AP N	4420	24,514	79	83	$ 920
TLH	AP N	4425	9,973	75	78	$ 380
TLH	AP RU 18	5505	25,207	64	64	$ -
TLH	AP S	4305	70,348	91	91	$ -
TLH	AP S	4310	179,279	95	95	$ -
TLH	AP S	4313	11,875	98	98	$ -
TLH	AP S	4315	60,505	96	96	$ -
TLH	AP S	4320	68,878	97	97	$ -
TLH	AP S	4325	4,183	98	98	$ -
TLH	AP S	4332	401,224	96	96	$ -
TLH	AP TERM	4105	855,384	80	87	$ 542,990
TLH	AP TERM	4110	13,317	49	49	$ -

6.2 Major Rehabilitation Needs

Major rehabilitation is identified within the FDOT SAPMP as a major construction activity that results in a substantial improvement to the pavement condition and resets the pavement section's PCI value to 100. Major rehabilitation recommendations (AC Rehabilitation, AC Reconstruction, PCC Rehabilitation, and PCC Reconstruction) should be considered as planning-level only. Additional design-level investigation in accordance with FAA Advisory Circulars is required. Recommendations identified within this planning document do not imply final design.

The objective of the Major Pavement Rehabilitation Needs analysis is to develop planning-level projects within an Airport's airfield pavement network. As depicted in **Figures 5.3 (b)** and **(c)** in **Chapter 5**, major rehabilitation activities are recommended when a pavement section has deteriorated below the critical PCI value, a point at which localized maintenance and repair activities may not be a cost-effective solution. In addition, major rehabilitation is also recommended when the section's PCI value is above the critical PCI value with the section exhibiting a significant amount of load-related distresses. Identification of rehabilitation needs is done at the section-level. This, however, does not limit the Airport from further refining limits of project planning areas.

6.2.1 10-Year Unconstrained Budget Major Rehabilitation Needs

Major rehabilitation needs are identified by analyzing the Airport's pavement condition in relationship to critical PCI values, major rehabilitation policies, and unit costs, assuming there are no budget constraints. This is done over a 10-year analysis period. While this is financially impractical, it does yield the unbiased pavement needs over a 10-year time frame at the Airport given current and forecasted pavement conditions. The FDOT recognizes that airports are constrained by budgets and does not intend to convey an unrealistic approach of addressing pavement rehabilitation. Each airport has a unique set of challenges and FDOT's goals are to provide it with the data needed to formulate a practical Capital Improvement Program and identify needs in the Joint Automated Capital Improvement Program (JACIP). This includes:

- An estimation of current pavement condition;
- Major pavement rehabilitation needs based on condition and policies; and

- Planning-level cost estimates for the major rehabilitation needs.

Table 6.2.1 (a) summarizes section-level major rehabilitation needs forecasted for a 10-year period. It should be noted that the following table depicts planning-level costs and has been rounded up to the nearest $1,000 for planning purposes.

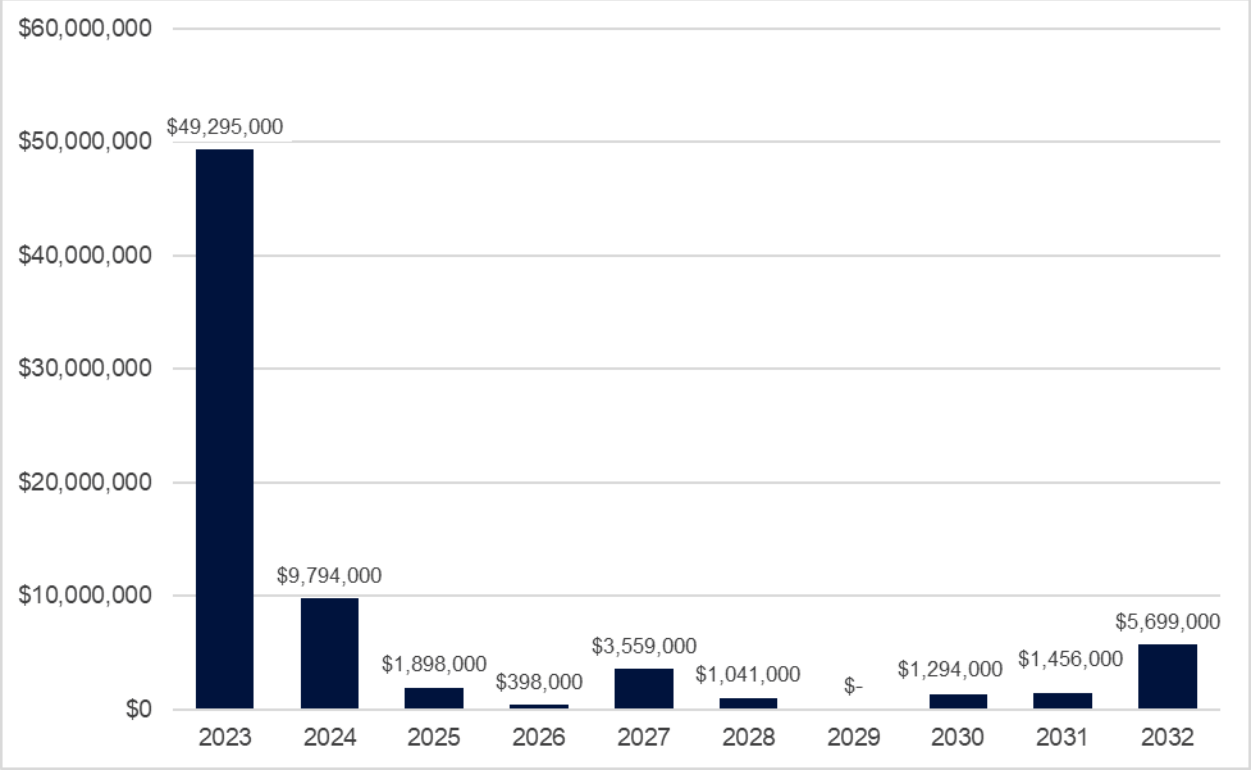
Table 6.2.1 (a): Section-Level 10-Year Major Rehabilitation Needs

Program Year	Network ID	Branch ID	Section ID	Surface	Area (SF)	PCI Before	Rehabilitation Type	Planning Cost Estimate
2023	TLH	TL AP S	3205	AAC	6,963	63	AC Rehabilitation	$ 98,000
2023	TLH	TL T-HANG	3105	AC	46,227	61	AC Rehabilitation	$ 648,000
2023	TLH	TL T-HANG	3110	AC	16,646	50	AC Reconstruction	$ 508,000
2023	TLH	TL T-HANG	3115	AC	63,002	44	AC Reconstruction	$ 1,922,000
2023	TLH	TW A	106	AC	215,250	60	AC Rehabilitation	$ 3,014,000
2023	TLH	TW A	107	AC	23,925	65	AC Rehabilitation	$ 335,000
2023	TLH	TW A1	110	AC	40,291	63	AC Rehabilitation	$ 565,000
2023	TLH	TW A10	175	AC	4,954	68	AC Rehabilitation	$ 70,000
2023	TLH	TW A2	115	AC	42,179	68	AC Rehabilitation	$ 591,000
2023	TLH	TW A3	125	AC	32,329	60	AC Rehabilitation	$ 453,000
2023	TLH	TW A3	130	AC	34,919	65	AC Rehabilitation	$ 489,000
2023	TLH	TW A4	140	AC	19,805	53	AC Reconstruction	$ 605,000
2023	TLH	TW B	205	AC	581,353	48	AC Reconstruction	$ 17,732,000
2023	TLH	TW B1	210	AC	46,292	53	AC Reconstruction	$ 1,412,000
2023	TLH	TW B5	250	AC	24,545	41	AC Reconstruction	$ 749,000
2023	TLH	TW B6	265	AC	17,002	58	AC Rehabilitation	$ 239,000
2023	TLH	TW B6	267	AC	24,158	50	AC Reconstruction	$ 737,000
2023	TLH	TW B7	273	AC	38,359	61	AC Rehabilitation	$ 538,000
2023	TLH	TW B7	275	AAC	9,455	52	AC Reconstruction	$ 289,000
2023	TLH	TW B7	277	AAC	8,669	66	AC Rehabilitation	$ 122,000
2023	TLH	TW B8	280	AC	66,948	65	AC Rehabilitation	$ 938,000
2023	TLH	TW B9	295	AC	84,260	54	AC Reconstruction	$ 2,570,000
2023	TLH	TW C	307	AAC	10,756	63	AC Rehabilitation	$ 151,000
2023	TLH	TW C	310	AAC	160,476	50	AC Reconstruction	$ 4,895,000
2023	TLH	TW C	315	AAC	55,835	66	AC Rehabilitation	$ 782,000
2023	TLH	TW D	405	AC	33,610	67	AC Rehabilitation	$ 471,000
2023	TLH	TW D	410	AC	10,157	65	AC Rehabilitation	$ 143,000
2023	TLH	TW Z	2610	AC	2,379	40	AC Reconstruction	$ 73,000
2023	TLH	TW Z	2615	AC	2,615	68	AC Rehabilitation	$ 37,000
2023	TLH	AP N	4410	AAC	215,063	68	AC Rehabilitation	$ 3,011,000
2023	TLH	AP N	4415	APC	310,550	69	AC Rehabilitation	$ 4,348,000
2023	TLH	AP RU 18	5505	AC	25,207	61	AC Rehabilitation	$ 353,000
2023	TLH	AP TERM	4110	APC	13,317	47	AC Reconstruction	$ 407,000
2024	TLH	AP C	4505	AC	265,932	70	AC Rehabilitation	$ 3,910,000
2024	TLH	AP CARGO	4210	AC	400,242	70	AC Rehabilitation	$ 5,884,000
2025	TLH	TW B	203	AC	50,342	70	AC Rehabilitation	$ 778,000
2025	TLH	TW Z	2605	AC	62,575	69	AC Rehabilitation	$ 966,000
2025	TLH	AP N	4425	AC	9,973	69	AC Rehabilitation	$ 154,000

Program Year	Network ID	Branch ID	Section ID	Surface	Area (SF)	PCI Before	Rehabilitation Type	Planning Cost Estimate
2026	TLH	AP N	4420	APC	24,514	70	AC Rehabilitation	$ 398,000
2027	TLH	TW B3	235	AC	83,567	69	AC Rehabilitation	$ 1,423,000
2027	TLH	TW B4	240	AC	48,156	69	AC Rehabilitation	$ 820,000
2027	TLH	AP N	4405	AAC	77,291	69	AC Rehabilitation	$ 1,316,000
2028	TLH	TW B8	285	AC	58,220	70	AC Rehabilitation	$ 1,041,000
2030	TLH	AP CARGO	4205	AC	65,663	70	AC Rehabilitation	$ 1,294,000
2031	TLH	AP S	4305	AAC	70,348	69	AC Rehabilitation	$ 1,456,000
2032	TLH	TW B6	260	AC	38,862	70	AC Rehabilitation	$ 845,000
2032	TLH	TW B7	271	AC	23,946	69	AC Rehabilitation	$ 521,000
2032	TLH	TW B9	290	AC	20,199	69	AC Rehabilitation	$ 439,000
2032	TLH	AP S	4310	AAC	179,279	70	AC Rehabilitation	$ 3,894,000

Figure 6.2.1 (a) summarizes the section-level major rehabilitation needs for a 10-year period between 2023 and 2032. **Figure 6.2.1 (b)**, the Airfield Pavement Major Rehabilitation Exhibit, graphically depicts the major rehabilitation needs with rounded costs. As suggested previously, this is planning-level data that can be used by the Airport to support developing a practical CIP.

Figure 6.2.1 (a): 10-Year Major Rehabilitation Needs by Program Year



TLH

AIRFIELD PAVEMENT MAJOR REHABILITATION EXHIBIT

Statewide Airfield Pavement Management Program

TALLAHASSEE INTERNATIONAL AIRPORT

FDOT

2022

NORTH

TW A2:115 2023 | AC REHAB $0.59 M	TW A3:125 2023 | AC REHAB $0.45 M	TW A4:140 2023 | AC RECON $0.61 M	TW A10:175 2023 | AC REHAB $0.07 M	TW B5:250 2023 | AC RECON $0.75 M	TW B6:265 2023 | AC REHAB $0.24 M	TW B6:267 2023 | AC RECON $0.74 M	TW B7:273 2023 | AC REHAB $0.54 M	TW B7:275 2023 | AC RECON $0.29 M
TW B7:277 2023 | AC REHAB $0.12 M	TW B8:280 2023 | AC REHAB $0.94 M	TW B9:295 2023 | AC RECON $2.57 M	TW C:307 2023 | AC REHAB $0.15 M	TW C:310 2023 | AC RECON $4.90 M	TW C:315 2023 | AC REHAB $0.78 M	TW D:410 2023 | AC REHAB $0.14 M	TW Z:2610 2023 | AC RECON $0.07 M	TW Z:2615 2023 | AC REHAB $0.04 M
TL T-HANG:3105 2023 | AC REHAB $0.65 M	TL T-HANG:3110 2023 | AC RECON $0.51 M	TL AP S:3205 2023 | AC REHAB $0.10 M	AP TERM:4110 2023 | AC RECON $0.41 M	AP N:4410 2023 | AC REHAB $3.01 M	AP N:4415 2023 | AC REHAB $4.35 M	TW B:203 2025 | AC REHAB $0.78 M	TW B4:240 2027 | AC REHAB $0.82 M	TW B8:285 2028 | AC REHAB $1.04 M
AP S:4305 2031 | AC REHAB $1.46 M	TW B6:260 2032 | AC REHAB $0.85 M	TW B7:271 2032 | AC REHAB $0.52 M	AP S:4310 2032 | AC REHAB $3.89 M					

TW A1:110
2023 | AC REHAB
$0.57 M

TW A:107
2023 | AC REHAB
$0.34 M

AP RU 18:5505
2023 | AC REHAB
$0.35 M

TW A:106
2023 | AC REHAB
$3.01 M

TW A3:130
2023 | AC REHAB
$0.49 M

TW D:405
2023 | AC REHAB
$0.47 M

TL T-HANG:3115
2023 | AC RECON
$1.92 M

AP N:4405
2027 | AC REHAB
$1.32 M

AP N:4425
2025 | AC REHAB
$0.15 M

AP N:4420
2026 | AC REHAB
$0.40 M

AP C:4505
2024 | AC REHAB
$3.91 M

TW Z:2605
2025 | AC REHAB
$0.97 M

AP CARGO:4205
2030 | AC REHAB
$1.29 M

AP CARGO:4210
2024 | AC REHAB
$5.88 M

TW B3:235
2027 | AC REHAB
$1.42 M

TW B1:210
2023 | AC RECON
$1.41 M

TW B:205
2023 | AC RECON
$17.73 M

TW B9:290
2032 | AC REHAB
$0.44 M

RUNWAY 18-36 150' x 7,000'

RUNWAY 9-27 150' x 8,000'

RW 9-27

RW 18-36

TW A1 TW A2 TW A3 TW A4 TW A5 TW A6 TW A7 TW A8 TW A9 TW A10 TW A11 TW A12 TW B TW C TW D TL T-HANG TW Z TW B1 TW B2 TW B3 TW B4 TW B5 TW B6 TW B7 TW B8 TW B9

AP RU 18 AP S AP TERM AP CARGO AP HOT AP N AP C

LEGEND

RW 13-31 — TYPICAL RUNWAY BRANCH ID
TW A — TYPICAL TAXIWAY BRANCH ID
AP S — TYPICAL APRON BRANCH ID

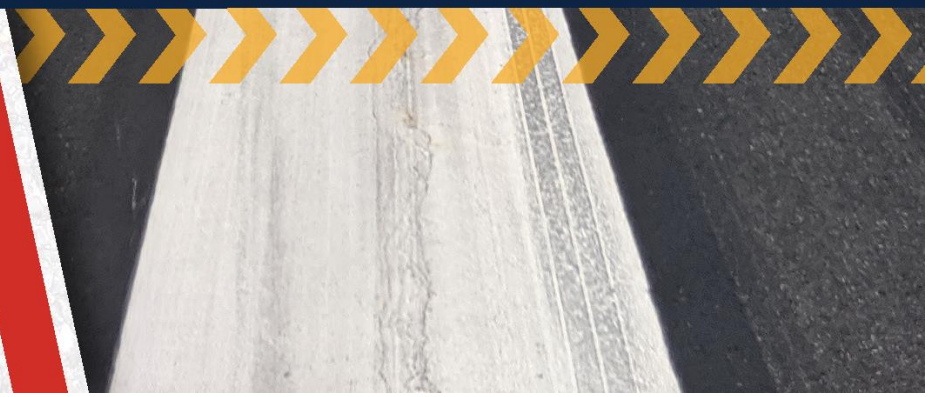
PROGRAM YEAR

2023	2028
2024	2029
2025	2030
2026	2031
2027	2032

"BRANCH":"SECTION"
"YEAR"|"REHAB ACTIVITY"
"EST. COST"

RUNWAY LENGTHS DEPICTED IN THIS DRAWING ARE FOR PAVEMENT MANAGEMENT PURPOSES ONLY AND MAY NOT MATCH PUBLISHED RUNWAY LENGTHS. DRAWING NOT TO SCALE.

Chapter 7: Conclusion



Chapter 7 – Conclusion

7.1 Recommendations

7.1.1 Continued PCI Surveys

It is recommended that the Airport continue to perform regularly scheduled PCI surveys in accordance with the ASTM D5340-20 (or latest edition) to monitor the condition of airfield pavement facilities.

A high priority should be placed on maintaining good record keeping and re-inspecting the Airport's maintained pavement facilities to ensure continued safe aircraft operations. Per the FAA AC 150/5380-7B, a series of scheduled periodic inspections must be carried out for an effective maintenance program. Re-inspection of pavements should be scheduled in a timely manner to ensure that all areas, particularly those that may not come under day-to-day observation, are thoroughly evaluated and reported.

7.1.2 Localized Maintenance and Repair

While deterioration of the pavements due to usage and exposure to the environment cannot be prevented, applying timely and effective maintenance efforts can slow the anticipated rate of deterioration. Lack of adequate and timely maintenance is a significant factor in pavement deterioration. **Chapter 6** identified localized maintenance and repair needs. It is recommended that Airport sponsors coordinate with their respective Airport maintenance staff and Airport engineer when developing project-level maintenance and repair efforts.

7.1.3 Major Rehabilitation

Chapter 6 also identified major pavement rehabilitation project needs from 2023-2032. Identification of these rehabilitation needs are performed at the section level for manageable project areas and assume an unconstrained budget scenario. Given the uncertainty in Airport-specific budget information and prioritization goals, the unconstrained budget scenario represents a conservative scenario and identifies pavement needs over a 10-year period. Certainly, it is understood that most airports are faced with constrained budgets, thus further evaluation of projects based on prioritization, operational criticality, funding availability, and practicality is recommended.

7.1.4 Pavement Management System

The following recommendations are made to fully implement an effective pavement management program for the Airport:

- Develop a detailed preventive maintenance program for the Airport based on the recommendations provided in **Section 6.1**;
- Further refine and implement the identified 10-year major rehabilitation needs provided in **Section 6.2**;
- Maintain detailed records on pavement maintenance, construction, and inspection; and
- Maintain records on major pavement construction projects (year, scope, cost, and construction documents).

7.2 Supporting Documents

Airfield Pavement Network Definition Exhibit

The Airfield Pavement Network Definition Exhibit is located in **Chapter 3** and **Appendix C**. The Exhibit depicts the airfield layout in a manner that defines the airfield pavement infrastructure as branches, sections, and sample units in accordance with the ASTM D5340-20. The Exhibit is intended for planning purposes only. Further details can be found on the Airport's adopted Airport Layout Plan. Detailed characteristics are tabulated in **Appendix A.**

Airfield Pavement System Inventory Exhibit

The Airfield Pavement System Inventory Exhibit is located in **Chapter 3** and **Appendix C**. The Exhibit depicts recent and/or anticipated construction activity within the airfield pavement facilities reported by Airport staff. The Exhibit is intended to schematically identify the pavement limits of work and general work description. The information reported on the Airport Response Form provided by each participating airport was used as the basis of the changes. Furthermore, changes are confirmed at the Airport with Airport staff during the in-brief and debrief meeting.

Airfield Pavement Estimated Age Exhibit

The Airfield Pavement Estimated Age Exhibit is located in **Chapter 3** and **Appendix C**. Based on the review of historic airfield pavement construction activities, the Exhibit provides the approximate limits of the age of the pavement sections since the last major construction activity has occurred. This is intended to be a rough estimate based on interpretation of the limited data available at the time of report.

Airfield Pavement Condition Index Exhibit

The Airfield Pavement Condition Index Exhibit is located in **Chapter 4** and **Appendix C**. The Exhibit is a visual summary of the latest conditions reported from the PCI assessment performed at the Airport. Distress analysis occurred in accordance with ASTM D5340-20 (referenced in **Appendix E**), with results being analyzed using PAVER™ software to determine PCI values. The PCI values are identified in the Exhibit and graphically represented using the standard ASTM D5340-20 condition rating categories.

Airfield Pavement Major Rehabilitation Exhibit

The Airfield Pavement Major Rehabilitation Exhibit is located in **Chapter 6** and **Appendix C**. The Exhibit has been prepared based on the section condition analysis, pavement condition forecasts, and major rehabilitation needs analysis. The Exhibit graphically depicts the inventory with the associated rehabilitation type activity, program year, and the planning-level costs. Area limits, rehabilitation type, and planning-level costs should not be considered a design-level recommendation. A tabulation of the 10-Year Major Rehabilitation is located in **Appendix B**.

Inspection Photograph Documentation

Representative field conditions from the PCI assessment are documented with digital photographs located in **Appendix D**. Select photographs are provided with a limited caption on the distress(es) observed. "Vicinity" photos refer to the approximate boundaries of an inspected sample unit within the section and provide an overview of the section condition but are not focused on a specific distress. The Appendix does not contain photographs for every section and sample unit.

7.3 Conclusion

The FDOT SAPMP System Update Phase 2 2021-2023 was completed for the Airport on behalf of the FDOT AO in accordance with the FAA AC 150/5380-7B and 150/5380-6C. FDOT's implementation of the SAPMP has assisted public airports with this requirement in performing PCI survey inspections and analysis in accordance with the ASTM D5340-20.

7.4 References

The following documents are referenced as specific guidelines and procedures for maintaining Airport pavements, establishing an effective pavement maintenance program, and identifying specific pavement distresses, probable causes of distresses, survey guidelines, and recommended methods of repair.

- ASTM D5340-20, Standard Test Method for Airport Pavement Condition Index Surveys, American Society for Testing and Materials, West Conshohocken, PA, 2018.
- AC 150/5210-24 Airport Foreign Object Debris (FOD) Management, Federal Aviation Administration, Washington, D.C., 2010.
- AC 150/5320-6F, Airport Pavement Design and Evaluation, Federal Aviation Administration, Washington, D.C., 2016.
- AC 150/5380-7B, Airport Pavement Management Program (PMP), Federal Aviation Administration, Washington, D.C., 2014.
- AC 150/5380-6C, Guidelines and Procedures for Maintenance of Airport Pavements, Federal Aviation Administration, Washington, D.C., 2014.
- AC 150/5370-10H, Standard Specifications for Construction of Airports, Federal Aviation Administration, Washington, D.C., 2018.
- Airport Improvement Program Handbook, Order 5100.38D, Change 1, Federal Aviation Administration, Washington, D.C., 2019.
- Tri-Service Pavements Working Group (TSPWG) Manual 3-270-08. 14-03, Preventive Maintenance Plan (PMP) for Airfield Pavements, Department of Defense, Washington, D.C., 2019.
- Unified Facilities Criteria (UFC) 3-260-16, O&M Manual: Standard Practice for Airfield Pavement Condition Surveys, Department of Defense, Washington, D.C., 2019.
- Unified Facilities Criteria (UFC) 3-260-03, Airfield Pavement Evaluation, Department of Defense, Washington, D.C., 2001.
- Shahin, Mohamed Y., Pavement Management for Airports, Roads, and Parking Lots, Springer, 2005.

Appendix A: Airfield Pavement Analysis

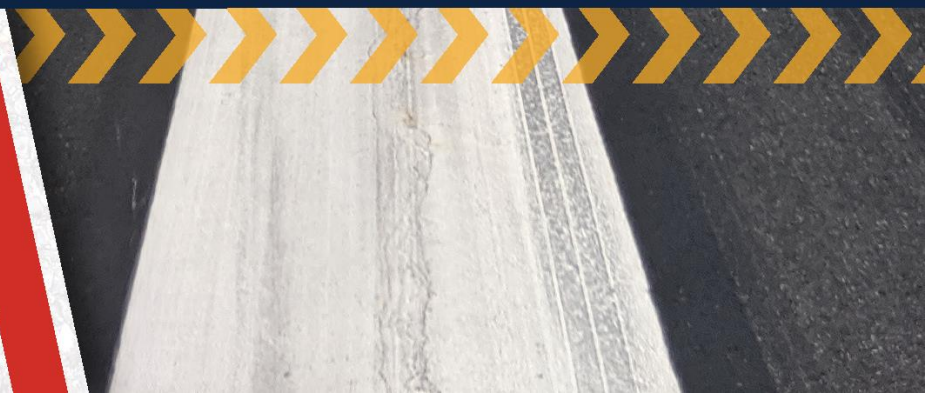


Table A.1: Pavement System Inventory Details

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	Surface Type	Estimate of Last Construction Date
TLH	RW 9-27	Runway	6205	400,000	AC	1/1/2015
TLH	RW 9-27	Runway	6210	800,000	AC	1/1/2015
TLH	RW 18-36	Runway	6105	607,550	AC	1/1/2023
TLH	RW 18-36	Runway	6110	303,775	AC	1/1/2023
TLH	RW 18-36	Runway	6125	63,750	AAC	1/1/2023
TLH	RW 18-36	Runway	6130	31,875	AAC	1/1/2023
TLH	RW 18-36	Runway	6155	28,700	AAC	1/1/2023
TLH	RW 18-36	Runway	6160	14,350	AAC	1/1/2023
TLH	TL AP S	Taxiway	3205	6,963	AAC	1/1/1994
TLH	TL T-HANG	Taxiway	3105	46,227	AC	1/1/1998
TLH	TL T-HANG	Taxiway	3110	16,646	AC	1/1/1985
TLH	TL T-HANG	Taxiway	3115	63,002	AC	1/1/1985
TLH	TW A	Taxiway	103	79,944	AAC	1/1/2023
TLH	TW A	Taxiway	105	243,781	AAC	1/1/2023
TLH	TW A	Taxiway	106	215,250	AC	1/1/2005
TLH	TW A	Taxiway	107	23,925	AC	10/1/2012
TLH	TW A1	Taxiway	110	40,291	AC	10/1/2012
TLH	TW A10	Taxiway	170	22,422	AC	1/1/2023
TLH	TW A10	Taxiway	175	4,954	AC	12/25/1999
TLH	TW A11	Taxiway	180	24,154	AAC	1/1/2023
TLH	TW A12	Taxiway	185	43,156	AAC	1/1/2023
TLH	TW A2	Taxiway	115	42,179	AC	1/1/2005
TLH	TW A3	Taxiway	125	32,329	AC	1/1/2005
TLH	TW A3	Taxiway	130	34,919	AC	7/1/2005
TLH	TW A4	Taxiway	140	19,805	AC	1/1/1985
TLH	TW A7	Taxiway	150	72,118	AAC	1/1/2023
TLH	TW A8	Taxiway	155	43,518	AAC	1/1/2023
TLH	TW A8	Taxiway	160	11,115	AAC	1/1/2023
TLH	TW A9	Taxiway	165	51,254	AC	1/1/2023
TLH	TW B	Taxiway	203	50,342	AC	10/1/2012
TLH	TW B	Taxiway	205	581,353	AC	1/1/2005
TLH	TW B	Taxiway	207	15,151	AAC	1/1/2023
TLH	TW B	Taxiway	209	30,178	AAC	1/1/2023
TLH	TW B1	Taxiway	210	46,292	AC	1/1/2005
TLH	TW B1	Taxiway	215	4,782	AC	1/1/2015
TLH	TW B2	Taxiway	220	49,156	AC	1/1/2015
TLH	TW B3	Taxiway	230	63,794	AC	1/1/2015
TLH	TW B3	Taxiway	235	83,567	AC	1/1/2007
TLH	TW B4	Taxiway	240	48,156	AC	1/1/2007
TLH	TW B5	Taxiway	250	24,545	AC	1/1/2005
TLH	TW B6	Taxiway	260	38,862	AC	1/1/2015
TLH	TW B6	Taxiway	265	17,002	AC	1/1/2005
TLH	TW B6	Taxiway	267	24,158	AC	1/1/2005
TLH	TW B7	Taxiway	270	39,535	AC	1/1/2015

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	Surface Type	Estimate of Last Construction Date
TLH	TW B7	Taxiway	271	23,946	AC	1/1/2015
TLH	TW B7	Taxiway	273	38,359	AC	1/1/2005
TLH	TW B7	Taxiway	275	9,455	AAC	1/2/1992
TLH	TW B7	Taxiway	277	8,669	AAC	1/1/1994
TLH	TW B8	Taxiway	280	66,948	AC	7/1/2003
TLH	TW B8	Taxiway	285	58,220	AC	1/1/2003
TLH	TW B9	Taxiway	290	20,199	AC	1/1/2015
TLH	TW B9	Taxiway	295	84,260	AC	1/1/2005
TLH	TW C	Taxiway	303	37,868	AAC	1/1/2023
TLH	TW C	Taxiway	305	53,314	AAC	1/1/2023
TLH	TW C	Taxiway	307	10,756	AAC	1/1/2005
TLH	TW C	Taxiway	310	160,476	AAC	1/1/1992
TLH	TW C	Taxiway	315	55,835	AAC	1/1/2003
TLH	TW D	Taxiway	405	33,610	AC	7/1/2005
TLH	TW D	Taxiway	410	10,157	AC	1/1/1998
TLH	TW Z	Taxiway	2605	62,575	AC	1/1/1994
TLH	TW Z	Taxiway	2610	2,379	AC	1/1/1994
TLH	TW Z	Taxiway	2615	2,615	AC	1/1/1994
TLH	AP C	Apron	4505	265,932	AC	1/1/2005
TLH	AP CARGO	Apron	4205	65,663	AC	1/1/1990
TLH	AP CARGO	Apron	4210	400,242	AC	1/1/2007
TLH	AP CARGO	Apron	4215	18,250	PCC	1/1/2007
TLH	AP HELI	Apron	4340	17,496	PCC	1/5/2018
TLH	AP HELI	Apron	4345	50,224	AC	1/5/2018
TLH	AP N	Apron	4405	77,291	AAC	1/1/2010
TLH	AP N	Apron	4410	215,063	AAC	1/1/2010
TLH	AP N	Apron	4415	310,550	APC	1/1/2010
TLH	AP N	Apron	4420	24,514	APC	1/1/2010
TLH	AP N	Apron	4425	9,973	AC	1/1/2010
TLH	AP RU 18	Apron	5505	25,207	AC	1/1/2005
TLH	AP S	Apron	4305	70,348	AAC	1/5/2018
TLH	AP S	Apron	4310	179,279	AAC	1/5/2018
TLH	AP S	Apron	4313	11,875	PCC	1/5/2018
TLH	AP S	Apron	4315	60,505	AAC	1/5/2018
TLH	AP S	Apron	4320	68,878	AAC	1/5/2018
TLH	AP S	Apron	4325	4,183	PCC	1/5/2018
TLH	AP S	Apron	4332	401,224	AC	1/5/2018
TLH	AP TERM	Apron	4105	855,384	PCC	1/1/1989
TLH	AP TERM	Apron	4110	13,317	APC	1/1/2005

Table A.2: Pavement Condition Index Summary (Current PCI Survey) – Section Level

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	PCI	Condition Rating
TLH	RW 9-27	Runway	6205	400,000	88	Good
TLH	RW 9-27	Runway	6210	800,000	90	Good
TLH	RW 18-36	Runway	6105	607,550	100	Good
TLH	RW 18-36	Runway	6110	303,775	100	Good
TLH	RW 18-36	Runway	6125	63,750	100	Good
TLH	RW 18-36	Runway	6130	31,875	100	Good
TLH	RW 18-36	Runway	6155	28,700	100	Good
TLH	RW 18-36	Runway	6160	14,350	100	Good
TLH	TL AP S	Taxiway	3205	6,963	65	Fair
TLH	TL T-HANG	Taxiway	3105	46,227	62	Fair
TLH	TL T-HANG	Taxiway	3110	16,646	52	Poor
TLH	TL T-HANG	Taxiway	3115	63,002	46	Poor
TLH	TW A	Taxiway	103	79,944	100	Good
TLH	TW A	Taxiway	105	243,781	100	Good
TLH	TW A	Taxiway	106	215,250	61	Fair
TLH	TW A	Taxiway	107	23,925	67	Fair
TLH	TW A1	Taxiway	110	40,291	64	Fair
TLH	TW A10	Taxiway	170	22,422	100	Good
TLH	TW A10	Taxiway	175	4,954	70	Fair
TLH	TW A11	Taxiway	180	24,154	100	Good
TLH	TW A12	Taxiway	185	43,156	100	Good
TLH	TW A2	Taxiway	115	42,179	70	Fair
TLH	TW A3	Taxiway	125	32,329	61	Fair
TLH	TW A3	Taxiway	130	34,919	67	Fair
TLH	TW A4	Taxiway	140	19,805	54	Poor
TLH	TW A7	Taxiway	150	72,118	100	Good
TLH	TW A8	Taxiway	155	43,518	100	Good
TLH	TW A8	Taxiway	160	11,115	100	Good
TLH	TW A9	Taxiway	165	51,254	100	Good
TLH	TW B	Taxiway	203	50,342	74	Satisfactory
TLH	TW B	Taxiway	205	581,353	50	Poor
TLH	TW B	Taxiway	207	15,151	100	Good
TLH	TW B	Taxiway	209	30,178	100	Good
TLH	TW B1	Taxiway	210	46,292	54	Poor
TLH	TW B1	Taxiway	215	4,782	87	Good
TLH	TW B2	Taxiway	220	49,156	87	Good
TLH	TW B3	Taxiway	230	63,794	90	Good
TLH	TW B3	Taxiway	235	83,567	76	Satisfactory
TLH	TW B4	Taxiway	240	48,156	76	Satisfactory
TLH	TW B5	Taxiway	250	24,545	43	Poor
TLH	TW B6	Taxiway	260	38,862	84	Satisfactory
TLH	TW B6	Taxiway	265	17,002	59	Fair
TLH	TW B6	Taxiway	267	24,158	52	Poor
TLH	TW B7	Taxiway	270	39,535	85	Satisfactory
TLH	TW B7	Taxiway	271	23,946	83	Satisfactory

Network ID	Branch ID	Branch Use	Section ID	Area (SF)	PCI	Condition Rating
TLH	TW B7	Taxiway	273	38,359	62	Fair
TLH	TW B7	Taxiway	275	9,455	53	Poor
TLH	TW B7	Taxiway	277	8,669	69	Fair
TLH	TW B8	Taxiway	280	66,948	66	Fair
TLH	TW B8	Taxiway	285	58,220	78	Satisfactory
TLH	TW B9	Taxiway	290	20,199	83	Satisfactory
TLH	TW B9	Taxiway	295	84,260	55	Poor
TLH	TW C	Taxiway	303	37,868	100	Good
TLH	TW C	Taxiway	305	53,314	100	Good
TLH	TW C	Taxiway	307	10,756	65	Fair
TLH	TW C	Taxiway	310	160,476	51	Poor
TLH	TW C	Taxiway	315	55,835	69	Fair
TLH	TW D	Taxiway	405	33,610	69	Fair
TLH	TW D	Taxiway	410	10,157	67	Fair
TLH	TW Z	Taxiway	2605	62,575	73	Satisfactory
TLH	TW Z	Taxiway	2610	2,379	42	Poor
TLH	TW Z	Taxiway	2615	2,615	70	Fair
TLH	AP C	Apron	4505	265,932	74	Satisfactory
TLH	AP CARGO	Apron	4205	65,663	84	Satisfactory
TLH	AP CARGO	Apron	4210	400,242	74	Satisfactory
TLH	AP CARGO	Apron	4215	18,250	79	Satisfactory
TLH	AP HELI	Apron	4340	17,496	95	Good
TLH	AP HELI	Apron	4345	50,224	98	Good
TLH	AP N	Apron	4405	77,291	80	Satisfactory
TLH	AP N	Apron	4410	215,063	71	Satisfactory
TLH	AP N	Apron	4415	310,550	72	Satisfactory
TLH	AP N	Apron	4420	24,514	79	Satisfactory
TLH	AP N	Apron	4425	9,973	75	Satisfactory
TLH	AP RU 18	Apron	5505	25,207	64	Fair
TLH	AP S	Apron	4305	70,348	91	Good
TLH	AP S	Apron	4310	179,279	95	Good
TLH	AP S	Apron	4313	11,875	98	Good
TLH	AP S	Apron	4315	60,505	96	Good
TLH	AP S	Apron	4320	68,878	97	Good
TLH	AP S	Apron	4325	4,183	98	Good
TLH	AP S	Apron	4332	401,224	96	Good
TLH	AP TERM	Apron	4105	855,384	80	Satisfactory
TLH	AP TERM	Apron	4110	13,317	49	Poor

Table A.3: Forecasted PCI Values 2023-2032 – Section-Level

Network ID	Branch ID	Section ID	Current PCI	2023	2024	2025	2026	2027	2028	2029	2030	2031	2032
TLH	RW 9-27	6205	88	86	84	83	81	80	78	77	75	74	72
TLH	RW 9-27	6210	90	88	86	85	83	82	80	79	77	76	74
TLH	RW 18-36	6105	100	99	98	96	95	93	92	90	89	87	86
TLH	RW 18-36	6110	100	99	98	96	95	93	92	90	89	87	86
TLH	RW 18-36	6125	100	99	97	95	93	91	89	87	86	84	82
TLH	RW 18-36	6130	100	99	97	95	93	91	89	87	86	84	82
TLH	RW 18-36	6155	100	99	97	95	93	91	89	87	86	84	82
TLH	RW 18-36	6160	100	99	97	95	93	91	89	87	86	84	82
TLH	TL AP S	3205	65	63	61	60	59	58	57	56	55	54	54
TLH	TL T-HANG	3105	62	61	60	59	58	57	57	56	55	54	53
TLH	TL T-HANG	3110	52	50	49	48	47	46	45	43	42	41	39
TLH	TL T-HANG	3115	46	44	43	41	40	38	36	35	33	31	29
TLH	TW A	103	100	99	96	94	91	89	87	84	82	80	78
TLH	TW A	105	100	99	96	94	91	89	87	84	82	80	78
TLH	TW A	106	61	60	59	58	57	56	56	55	54	53	52
TLH	TW A	107	67	65	65	64	63	62	61	60	59	59	58
TLH	TW A1	110	64	63	62	61	60	59	58	58	57	56	55
TLH	TW A10	170	100	99	97	94	92	90	88	86	85	83	81
TLH	TW A10	175	70	68	67	66	65	64	64	63	62	61	60
TLH	TW A11	180	100	99	96	94	91	89	87	84	82	80	78
TLH	TW A12	185	100	99	96	94	91	89	87	84	82	80	78
TLH	TW A2	115	70	68	67	66	65	64	64	63	62	61	60
TLH	TW A3	125	61	60	59	58	57	56	56	55	54	53	52
TLH	TW A3	130	67	65	65	64	63	62	61	60	59	59	58
TLH	TW A4	140	54	53	52	51	50	49	47	46	45	44	42
TLH	TW A7	150	100	99	96	94	91	89	87	84	82	80	78
TLH	TW A8	155	100	99	96	94	91	89	87	84	82	80	78
TLH	TW A8	160	100	99	96	94	91	89	87	84	82	80	78
TLH	TW A9	165	100	99	97	94	92	90	88	86	85	83	81
TLH	TW B	203	74	72	71	70	69	68	67	66	65	64	63
TLH	TW B	205	50	48	47	46	45	43	42	41	39	37	36
TLH	TW B	207	100	99	96	94	91	89	87	84	82	80	78
TLH	TW B	209	100	99	96	94	91	89	87	84	82	80	78
TLH	TW B1	210	54	53	52	51	50	49	47	46	45	44	42
TLH	TW B1	215	87	84	83	81	79	78	76	75	74	73	71
TLH	TW B2	220	87	84	83	81	79	78	76	75	74	73	71
TLH	TW B3	230	90	87	85	83	82	80	79	77	76	75	73
TLH	TW B3	235	76	74	73	72	70	69	68	67	66	65	64
TLH	TW B4	240	76	74	73	72	70	69	68	67	66	65	64
TLH	TW B5	250	43	41	39	38	36	34	32	30	28	26	24
TLH	TW B6	260	84	81	80	78	77	76	74	73	72	71	70
TLH	TW B6	265	59	58	57	56	55	54	53	52	52	51	50
TLH	TW B6	267	52	50	49	48	47	46	45	43	42	41	39
TLH	TW B7	270	85	82	81	79	78	76	75	74	72	71	70

Network ID	Branch ID	Section ID	Current PCI	2023	2024	2025	2026	2027	2028	2029	2030	2031	2032
TLH	TW B7	271	83	80	79	77	76	75	74	72	71	70	69
TLH	TW B7	273	62	61	60	59	58	57	57	56	55	54	53
TLH	TW B7	275	53	52	51	51	50	50	49	48	48	47	46
TLH	TW B7	277	69	66	65	63	62	61	60	59	57	57	56
TLH	TW B8	280	66	65	64	63	62	61	60	59	59	58	57
TLH	TW B8	285	78	76	74	73	72	71	70	69	68	67	66
TLH	TW B9	290	83	80	79	77	76	75	74	72	71	70	69
TLH	TW B9	295	55	54	53	52	51	50	49	48	46	45	44
TLH	TW C	303	100	99	96	94	91	89	87	84	82	80	78
TLH	TW C	305	100	99	96	94	91	89	87	84	82	80	78
TLH	TW C	307	65	63	61	60	59	58	57	56	55	54	54
TLH	TW C	310	51	50	49	49	48	48	47	46	45	44	43
TLH	TW C	315	69	66	65	63	62	61	60	59	57	57	56
TLH	TW D	405	69	67	66	65	65	64	63	62	61	60	59
TLH	TW D	410	67	65	65	64	63	62	61	60	59	59	58
TLH	TW Z	2605	73	71	70	69	68	67	66	65	64	63	62
TLH	TW Z	2610	42	40	38	36	35	33	31	29	27	25	23
TLH	TW Z	2615	70	68	67	66	65	64	64	63	62	61	60
TLH	AP C	4505	74	71	70	68	66	65	63	61	60	58	56
TLH	AP CARGO	4205	84	81	80	78	76	75	73	71	70	68	66
TLH	AP CARGO	4210	74	71	70	68	66	65	63	61	60	58	56
TLH	AP CARGO	4215	79	78	78	77	77	76	76	75	74	74	73
TLH	AP HELI	4340	95	93	92	91	90	89	89	88	87	86	85
TLH	AP HELI	4345	98	95	94	92	90	89	87	85	84	82	80
TLH	AP N	4405	80	77	74	72	71	69	67	65	64	62	61
TLH	AP N	4410	71	68	66	65	63	62	60	59	57	56	55
TLH	AP N	4415	72	69	67	66	64	63	61	60	58	57	55
TLH	AP N	4420	79	76	74	72	70	68	66	65	63	62	60
TLH	AP N	4425	75	72	71	69	67	66	64	62	61	59	57
TLH	AP RU 18	5505	64	61	60	58	56	55	53	51	50	48	46
TLH	AP S	4305	91	87	84	82	79	77	75	73	71	69	68
TLH	AP S	4310	95	90	88	85	83	80	78	76	74	72	70
TLH	AP S	4313	98	96	95	94	93	92	91	90	89	88	87
TLH	AP S	4315	96	91	88	86	83	81	79	76	74	72	71
TLH	AP S	4320	97	92	89	87	84	82	79	77	75	73	71
TLH	AP S	4325	98	96	95	94	93	92	91	90	89	88	87
TLH	AP S	4332	96	93	92	90	88	87	85	83	82	80	78
TLH	AP TERM	4105	80	79	79	78	78	77	77	76	75	75	74
TLH	AP TERM	4110	49	47	45	44	42	40	39	37	35	33	31

Pavement Database: FDOT

Network: TALLAHASSEE INT **Branch:** AP C CENTRAL RAMP **Section:** 4505 **Surface:**AC
L.C.D. 1/1/2005 **Use:** APRON **Rank:** P **Length:** 500.00 (Ft) **Width:** 500.00 (Ft) **True Area:** 265932.0000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2005	SR-AC	Surface Reconstruction - AC	0.00	0.00	☑	1.5-2" P-401, 1" S-180, P-603
12/25/1999	NU-IN	New Construction - Initial	0.00	0.00	☑	

Network: TALLAHASSEE INT **Branch:** AP CARGO CARGO APRON **Section:** 4205 **Surface:**AC
L.C.D. 1/1/1990 **Use:** APRON **Rank:** P **Length:** 280.00 (Ft) **Width:** 220.00 (Ft) **True Area:** 65663.00002 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/2/1990	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	
1/1/1990	IMPORTED	BUILT	0.00	4.00	☑	1990: 4" P-401 ON 14" P-211 ON 6" P-160

Network: TALLAHASSEE INT **Branch:** AP CARGO CARGO APRON **Section:** 4210 **Surface:**AC
L.C.D. 1/1/2007 **Use:** APRON **Rank:** P **Length:** 1,042.00 (Ft) **Width:** 820.00 (Ft) **True Area:** 400242.0001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2007	NU-IN	New Construction - Initial	0.00	0.00	☑	

Network: TALLAHASSEE INT **Branch:** AP CARGO CARGO APRON **Section:** 4215 **Surface:**PCC
L.C.D. 1/1/2007 **Use:** APRON **Rank:** P **Length:** 738.00 (Ft) **Width:** 26.00 (Ft) **True Area:** 18250.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2007	NU-IN	New Construction - Initial	0.00	0.00	☑	

Network: TALLAHASSEE INT **Branch:** AP HELI HELICOPTER PA **Section:** 4340 **Surface:**PCC
L.C.D. 1/5/2018 **Use:** APRON **Rank:** P **Length:** 54.00 (Ft) **Width:** 324.00 (Ft) **True Area:** 17496.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/5/2018	NC-PC	New Construction - PCC	0.00	0.00	☑	

Network: TALLAHASSEE INT **Branch:** AP HELI HELICOPTER PA **Section:** 4345 **Surface:**AC
L.C.D. 1/5/2018 **Use:** APRON **Rank:** P **Length:** 110.00 (Ft) **Width:** 580.00 (Ft) **True Area:** 50224.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/5/2018	NC-AC	New Construction - AC	0.00	0.00	☑	

Network: TALLAHASSEE INT **Branch:** AP N NORTH RAMP **Section:** 4405 **Surface:**AAC
L.C.D. 1/1/2010 **Use:** APRON **Rank:** P **Length:** 300.00 (Ft) **Width:** 200.00 (Ft) **True Area:** 77291.00002 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2010	OL-AS	Overlay - AC Structural	0.00	0.00	☑	
1/2/1985	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	PART OF THIS FEATURE IS SEAL
1/1/1985	IMPORTED	BUILT	0.00	3.00	☑	1985: 3" P-401 ON 7" P-211

Network: TALLAHASSEE INT **Branch:** AP N NORTH RAMP **Section:** 4410 **Surface:**AAC

L.C.D. 1/1/2010 **Use:** APRON **Rank:** P **Length:** 405.00 (Ft) **Width:** 530.00 (Ft) **True Area:** 215063.0000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2010	OL-AS	Overlay - AC Structural	0.00	0.00	☑	
1/2/1985	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	EMULSION SEAL ON THIS PAVE
1/1/1985	IMPORTED	OVERLAY	0.00	2.00	☑	1985: 2" P-401 OVERLAY
1/1/1971	IMPORTED	BUILT	0.00	3.00	☑	1971: 3" P-401 ON 11" P-211

Network: TALLAHASSEE INT **Branch:** AP N NORTH RAMP **Section:** 4415 **Surface:**APC

L.C.D. 1/1/2010 **Use:** APRON **Rank:** P **Length:** 635.00 (Ft) **Width:** 485.00 (Ft) **True Area:** 310550.0000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2010	OL-AS	Overlay - AC Structural	0.00	0.00	☑	
1/2/1971	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	EMULSION SEAL ON THIS PAVE
1/1/1971	IMPORTED	OVERLAY	0.00	3.00	☑	1971: 3" P-401
1/1/1960	IMPORTED	BUILT	0.00	11.00	☑	1960: 11" P-501

Network: TALLAHASSEE INT **Branch:** AP N NORTH RAMP **Section:** 4420 **Surface:**APC

L.C.D. 1/1/2010 **Use:** APRON **Rank:** P **Length:** 564.00 (Ft) **Width:** 45.00 (Ft) **True Area:** 24514.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2010	OL-AS	Overlay - AC Structural	0.00	0.00	☑	
1/2/1971	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	EMULSION SEAL ON THIS PAVE
1/1/1971	IMPORTED	OVERLAY	0.00	3.00	☑	1971: 3" P-401 OVERLAY
1/1/1960	IMPORTED	BUILT	0.00	6.00	☑	1960: 6" P-501

Network: TALLAHASSEE INT **Branch:** AP N NORTH RAMP **Section:** 4425 **Surface:**AC

L.C.D. 1/1/2010 **Use:** APRON **Rank:** P **Length:** 175.00 (Ft) **Width:** 45.00 (Ft) **True Area:** 9973.000003 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2010	NU-IN	New Construction - Initial	0.00	0.00	☑	

Network: TALLAHASSEE INT **Branch:** AP RU 18 RUN-UP APRON **Section:** 5505 **Surface:**AC

L.C.D. 1/1/2005 **Use:** APRON **Rank:** P **Length:** 140.00 (Ft) **Width:** 180.00 (Ft) **True Area:** 25207.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2005	SR-AC	Surface Reconstruction - AC	0.00	0.00	☑	1.5-2" P-401, 1" S-180, P-603
1/1/1993	IMPORTED	BUILT	0.00	3.00	☑	1993: 3 INCH P-401 OVERLAY ON EXISTING FLEX. PAVEMENT

Network: TALLAHASSEE INT **Branch:** AP S SOUTH RAMP **Section:** 4305 **Surface:**AAC

L.C.D. 1/5/2018 **Use:** APRON **Rank:** P **Length:** 350.00 (Ft) **Width:** 200.00 (Ft) **True Area:** 70348.00002 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/5/2018	ML-OVL	Mill and Overlay	0.00	0.00	☑	2" Mill; 2"-4" Variable Overlay P-401
1/1/1993	IMPORTED	BUILT	0.00	3.00	☑	1993: 3 INCH P-401 OVERLAY
1/1/1993	IMPORTED	OVERLAY	0.00	0.00	☑	EXISTING ASPHALT ON EXISTING SAND-ASPHALT BASE

Network: TALLAHASSEE INT **Branch:** AP S SOUTH RAMP **Section:** 4310 **Surface:**AAC

L.C.D. 1/5/2018 **Use:** APRON **Rank:** P **Length:** 250.00 (Ft) **Width:** 680.00 (Ft) **True Area:** 179279.0000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/5/2018	ML-OVL	Mill and Overlay	0.00	0.00	☑	2" Mill; 2"-4" Variable Overlay P-401
1/1/1994	IMPORTED	OVERLAY	0.00	3.00	☑	1994: 3 INCH P-401 OVERLAY
1/1/1960	IMPORTED	BUILT	0.00	0.50	☑	1960: 1-1/2 INCH P-401 ON 7-1/2 INCH P-211

Network: TALLAHASSEE INT **Branch:** AP S SOUTH RAMP **Section:** 4313 **Surface:**PCC

L.C.D. 1/5/2018 **Use:** APRON **Rank:** P **Length:** 25.00 (Ft) **Width:** 475.00 (Ft) **True Area:** 11875.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/5/2018	CR-PC	Complete Reconstruction - PCC	0.00	0.00	☑	8" P-501; 6" P-211; 12" P-152
1/1/1994	IMPORTED	OVERLAY	0.00	3.00	☑	1994: 3 INCH P-401 OVERLAY
1/1/1960	IMPORTED	BUILT	0.00	0.50	☑	1960: 1-1/2 INCH P-401 ON 7-1/2 INCH P-211

Network: TALLAHASSEE INT **Branch:** AP S SOUTH RAMP **Section:** 4315 **Surface:**AAC

L.C.D. 1/5/2018 **Use:** APRON **Rank:** P **Length:** 400.00 (Ft) **Width:** 150.00 (Ft) **True Area:** 60505.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/5/2018	ML-OVL	Mill and Overlay	0.00	0.00	☑	2" Mill; 2"-4" Variable Overlay P-401
1/1/1994	IMPORTED	BUILT	0.00	3.00	☑	1994: 3 INCH P-401 OVERLAY
1/1/1994	IMPORTED	OVERLAY	0.00	0.00	☑	EXISTING ASPHALT ON EXISTING SAND-ASPHALT BASE

Network: TALLAHASSEE INT **Branch:** AP S SOUTH RAMP **Section:** 4320 **Surface:**AAC

L.C.D. 1/5/2018 **Use:** APRON **Rank:** P **Length:** 350.00 (Ft) **Width:** 80.00 (Ft) **True Area:** 68878.00002 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/5/2018	ML-OVL	Mill and Overlay	0.00	0.00	☑	2" Mill; 2"-4" Variable Overlay P-401
1/1/1994	IMPORTED	BUILT	0.00	0.00	☑	1994: EB-35 COAL TAR PITCH EMULSION SEALCOAT
1/1/1994	IMPORTED	OVERLAY	0.00	0.00	☑	EXISTING ASPHALT ON EXISTING SAND-ASPHALT BASE
1/1/1975	IMPORTED	OVERLAY	0.00	0.00	☑	ESTIMATE 1975 CONSTRUCTION DATE

Network: TALLAHASSEE INT **Branch:** AP S SOUTH RAMP **Section:** 4325 **Surface:**PCC

L.C.D. 1/5/2018 **Use:** APRON **Rank:** P **Length:** 60.00 (Ft) **Width:** 72.00 (Ft) **True Area:** 4183.000001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/5/2018	CR-PC	Complete Reconstruction - PCC	0.00	0.00	☑	14" P-501; 6" P-211; 12" P-152
1/1/1994	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	1994: EB-35 COAL TAR PITCH EM
1/1/1971	IMPORTED	BUILT	0.00	0.50	☑	1971: 1-1/2 INCH P-401 ON 8 INCH P-211

Network: TALLAHASSEE INT **Branch:** AP S SOUTH RAMP **Section:** 4332 **Surface:**AC

L.C.D. 1/5/2018 **Use:** APRON **Rank:** P **Length:** 554.00 (Ft) **Width:** 580.00 (Ft) **True Area:** 401224.0001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/5/2018	CR-AC	Complete Reconstruction - AC	0.00	0.00	☑	4" P-401; 6" P-211; 12" P-152
1/1/1994	IMPORTED	BUILT	0.00	0.00	☑	1994 EB-35 COAL TAR PITCH EMULSION SEAL
1/1/1975	IMPORTED	OVERLAY	0.00	0.00	☑	EST 1975 AC PAVEMENT SECTION UNKNOWN

Network: TALLAHASSEE INT **Branch:** AP TERM TERMINAL APR **Section:** 4105 **Surface:**PCC

L.C.D. 1/1/1989 **Use:** APRON **Rank:** P **Length:** 1,480.00 (Ft) **Width:** 500.00 (Ft) **True Area:** 855384.0002 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/1989	IMPORTED	BUILT	0.00	14.00	☑	1989: 14" P-501 ON 6" P-301 (SOIL-CEMENT)

Network: TALLAHASSEE INT **Branch:** AP TERM TERMINAL APR **Section:** 4110 **Surface:**APC

L.C.D. 1/1/2005 **Use:** APRON **Rank:** P **Length:** 930.00 (Ft) **Width:** 15.00 (Ft) **True Area:** 13317.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2005	OL-AS	Overlay - AC Structural	0.00	0.00	☑	P-401 UNKOWN DEPTH
1/1/1989	NC-PC	New Construction - PCC	0.00	0.00	☑	1989: 14" P-501 ON 6" P-301 (SOIL-

Network: TALLAHASSEE INT **Branch:** RW 18-36 RUNWAY 18-36 **Section:** 6105 **Surface:**AC

L.C.D. 1/1/2023 **Use:** RUNWAY **Rank:** P **Length:** 6,076.00 (Ft) **Width:** 100.00 (Ft) **True Area:** 607550.0001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2023	CR-AC	Complete Reconstruction - AC	0.00	0.00	☑	4" P-401, 5" P-403, regrade and recom
10/1/2012	PA-AC	Patching - AC	0.00	0.00	☐	2012: 2" MILL AND OVERLAY 15'
1/1/1993	IMPORTED	OVERLAY	0.00	3.00	☑	1993: 3 INCH P-401 OVERLAY
1/1/1976	IMPORTED	OVERLAY	0.00	3.00	☑	1976: 3 INCH P-401 OVERLAY
1/1/1960	IMPORTED	BUILT	0.00	0.50	☑	1960: 1-1/2 INCH P-401 ON 10 INCH P-211

Network: TALLAHASSEE INT **Branch:** RW 18-36 RUNWAY 18-36 **Section:** 6110 **Surface:**AC

L.C.D. 1/1/2023 **Use:** RUNWAY **Rank:** P **Length:** 12,151.00 (Ft) **Width:** 25.00 (Ft) **True Area:** 303775.0000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2023	CR-AC	Complete Reconstruction - AC	0.00	0.00	☑	4" P-401, 5" P-403, regrade and recom
10/1/2012	ST-SC	Surface Treatment - Seal Coat	0.00	0.00	☐	2012: SEAL COAT
1/1/1993	IMPORTED	OVERLAY	0.00	3.00	☑	1993: 3 INCH P-401 OVERLAY
1/1/1976	IMPORTED	OVERLAY	0.00	2.00	☑	1976: 2 INCH TO 3 INCH P-401 OVERLAY
1/1/1960	IMPORTED	BUILT	0.00	0.50	☑	1960: 1-1/2 INCH P-401 ON 10 INCH P-211

Network: TALLAHASSEE INT **Branch:** RW 18-36 RUNWAY 18-36 **Section:** 6125 **Surface:**AAC

L.C.D. 1/1/2023 **Use:** RUNWAY **Rank:** P **Length:** 638.00 (Ft) **Width:** 100.00 (Ft) **True Area:** 63750.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2023	ML-OVL	Mill and Overlay	0.00	0.00	☑	2" Mill, 2" P-401 Overlay
10/1/2012	NU-IN	New Construction - Initial	0.00	0.00	☑	5" P-401, 10" P-211 LIMEROCK BA

Network: TALLAHASSEE INT **Branch:** RW 18-36 RUNWAY 18-36 **Section:** 6130 **Surface:**AAC

L.C.D. 1/1/2023 **Use:** RUNWAY **Rank:** P **Length:** 1,275.00 (Ft) **Width:** 25.00 (Ft) **True Area:** 31875.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2023	ML-OVL	Mill and Overlay	0.00	0.00	☑	2" Mill, 2" P-401 Overlay
10/1/2012	NU-IN	New Construction - Initial	0.00	0.00	☑	5" P-401, 10" P-211 LIMEROCK BA

Network: TALLAHASSEE INT **Branch:** RW 18-36 RUNWAY 18-36 **Section:** 6155 **Surface:**AAC

L.C.D. 1/1/2023 **Use:** RUNWAY **Rank:** P **Length:** 287.00 (Ft) **Width:** 100.00 (Ft) **True Area:** 28700.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2023	ML-OVL	Mill and Overlay	0.00	0.00	☑	2" Mill, 2" P-401 Overlay
10/1/2012	NU-IN	New Construction - Initial	0.00	0.00	☑	5" P-401, 10" P-211 LIMEROCK BA

Network: TALLAHASSEE INT **Branch:** RW 18-36 RUNWAY 18-36 **Section:** 6160 **Surface:**AAC

L.C.D. 1/1/2023 **Use:** RUNWAY **Rank:** P **Length:** 574.00 (Ft) **Width:** 25.00 (Ft) **True Area:** 14350.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2023	ML-OVL	Mill and Overlay	0.00	0.00	☑	2" Mill, 2" P-401 Overlay
10/1/2012	NU-IN	New Construction - Initial	0.00	0.00	☑	5" P-401, 10" P-211 LIMEROCK BA

Network: TALLAHASSEE INT **Branch:** RW 9-27 RUNWAY 9-27 **Section:** 6205 **Surface:**AC

L.C.D. 1/1/2015 **Use:** RUNWAY **Rank:** P **Length:** 8,050.00 (Ft) **Width:** 100.00 (Ft) **True Area:** 400000.0001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2015	CR-AC	Complete Reconstruction - AC	0.00	0.00	☑	5" P-401 BITUMINOUS, 10" P-211 L
1/1/1992	IMPORTED	OVERLAY	0.00	3.00	☑	1992: 3" P-401 OVERLAY
1/1/1980	IMPORTED	BUILT	0.00	3.00	☑	1980: 3" P-401 ON 13" P-211 ON 4" P-160

Network: TALLAHASSEE INT **Branch:** RW 9-27 RUNWAY 9-27 **Section:** 6210 **Surface:**AC

L.C.D. 1/1/2015 **Use:** RUNWAY **Rank:** P **Length:** 16,100.00 (Ft) **Width:** 25.00 (Ft) **True Area:** 800000.0002 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2015	CR-AC	Complete Reconstruction - AC	0.00	0.00	☑	5" P-401 BITUMINOUS, 10" P-211 L
1/1/1992	IMPORTED	OVERLAY	0.00	3.00	☑	1992: 3" P-401 OVERLAY
1/1/1980	IMPORTED	BUILT	0.00	3.00	☑	1980: 3" P-401 ON 13" P-211 ON 4" P-160

Network: TALLAHASSEE INT **Branch:** TL AP S TAXILANE SOU **Section:** 3205 **Surface:**AAC

L.C.D. 1/1/1994 **Use:** TAXIWAY **Rank:** P **Length:** 150.00 (Ft) **Width:** 38.00 (Ft) **True Area:** 6963.000002 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/1994	IMPORTED	BUILT	0.00	3.00	☑	1994: 3 INCH P-401 OVERLAY ON EXISTING FLEX. PAVEMENT

Network: TALLAHASSEE INT **Branch:** TL T-HANG TAXILANE T-HA **Section:** 3105 **Surface:**AC

L.C.D. 1/1/1998 **Use:** TAXIWAY **Rank:** P **Length:** 2,330.00 (Ft) **Width:** 20.00 (Ft) **True Area:** 46227.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/1998	NU-IN	New Construction - Initial	0.00	0.00	☑	

Network: TALLAHASSEE INT **Branch:** TL T-HANG TAXILANE T-HA **Section:** 3110 **Surface:**AC

L.C.D. 1/1/1985 **Use:** TAXIWAY **Rank:** P **Length:** 485.00 (Ft) **Width:** 35.00 (Ft) **True Area:** 16646.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/1985	IMPORTED	BUILT	0.00	3.00	☑	1985: 3" P-401 ON 7" P-211

Network: TALLAHASSEE INT **Branch:** TL T-HANG TAXILANE T-HA **Section:** 3115 **Surface:**AC

L.C.D. 1/1/1985 **Use:** TAXIWAY **Rank:** P **Length:** 750.00 (Ft) **Width:** 25.00 (Ft) **True Area:** 63002.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/1985	IMPORTED	BUILT	0.00	2.00	☑	1985: 2" P-401 ON 6" P-211

Network: TALLAHASSEE INT **Branch:** TW A10 TAXIWAY A10 **Section:** 170 **Surface:**AC

L.C.D. 1/1/2023 **Use:** TAXIWAY **Rank:** P **Length:** 445.00 (Ft) **Width:** 50.00 (Ft) **True Area:** 22422.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2023	NC-AC	New Construction - AC	0.00	0.00	☑	

Network: TALLAHASSEE INT **Branch:** TW A10 TAXIWAY A10 **Section:** 175 **Surface:**AC

L.C.D. 12/25/199 **Use:** TAXIWAY **Rank:** P **Length:** 100.00 (Ft) **Width:** 50.00 (Ft) **True Area:** 4954.000001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
12/25/1999	NU-IN	New Construction - Initial	0.00	0.00	☑	

Network: TALLAHASSEE INT **Branch:** TW A TAXIWAY A **Section:** 103 **Surface:**AAC

L.C.D. 1/1/2023 **Use:** TAXIWAY **Rank:** P **Length:** 660.00 (Ft) **Width:** 125.00 (Ft) **True Area:** 79944.00002 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2023	ML-OVL	Mill and Overlay	0.00	0.00	☑	Variable depth mill, 2" P-401 overlay
10/1/2012	NU-IN	New Construction - Initial	0.00	0.00	☑	5" P-401, 10" P-211 LIMEROCK BA

Network: TALLAHASSEE INT **Branch:** TW A TAXIWAY A **Section:** 105 **Surface:**AAC

L.C.D. 1/1/2023 **Use:** TAXIWAY **Rank:** P **Length:** 3,190.00 (Ft) **Width:** 75.00 (Ft) **True Area:** 243781.0000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2023	ML-OVL	Mill and Overlay	0.00	0.00	☑	Variable depth mill, 2" P-401 overlay
1/1/2005	SR-AC	Surface Reconstruction - AC	0.00	0.00	☑	1.5-2" P-401, 1" S-180, P-603
1/1/1993	IMPORTED	OVERLAY	0.00	3.00	☑	1993: 3 INCH P-401 OVERLAY
1/1/1971	IMPORTED	OVERLAY	0.00	0.50	☑	1971: 1-1/2 INCH P-401 OVERLAY
1/1/1961	IMPORTED	BUILT	0.00	0.50	☑	1961: 1-1/2 INCH P-401 ON 10 INCH P-211

Network: TALLAHASSEE INT **Branch:** TW A TAXIWAY A **Section:** 106 **Surface:**AC

L.C.D. 1/1/2005 **Use:** TAXIWAY **Rank:** P **Length:** 2,870.00 (Ft) **Width:** 75.00 (Ft) **True Area:** 215250.0000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2005	SR-AC	Surface Reconstruction - AC	0.00	0.00	☑	1.5-2" P-401, 1" S-180, P-603
1/1/1993	IMPORTED	OVERLAY	0.00	3.00	☑	1993: 3 INCH P-401 OVERLAY
1/1/1971	IMPORTED	OVERLAY	0.00	0.50	☑	1971: 1-1/2 INCH P-401 OVERLAY
1/1/1961	IMPORTED	BUILT	0.00	0.50	☑	1961: 1-1/2 INCH P-401 ON 10 INCH P-211

Network: TALLAHASSEE INT **Branch:** TW A TAXIWAY A **Section:** 107 **Surface:**AC

L.C.D. 10/1/2012 **Use:** TAXIWAY **Rank:** P **Length:** 320.00 (Ft) **Width:** 75.00 (Ft) **True Area:** 23925.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
10/1/2012	NU-IN	New Construction - Initial	0.00	0.00	☑	5" P-401, 10" P-211 LIMEROCK BA

Network: TALLAHASSEE INT **Branch:** TW A1 TAXIWAY A1 **Section:** 110 **Surface:**AC

L.C.D. 10/1/2012 **Use:** TAXIWAY **Rank:** P **Length:** 295.00 (Ft) **Width:** 100.00 (Ft) **True Area:** 40291.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
10/1/2012	NU-IN	New Construction - Initial	0.00	0.00	☑	5" P-401, 10" P-211 LIMEROCK BA

Network: TALLAHASSEE INT **Branch:** TW A11 TAXIWAY A11 **Section:** 180 **Surface:**AAC

L.C.D. 1/1/2023 **Use:** TAXIWAY **Rank:** P **Length:** 356.00 (Ft) **Width:** 55.00 (Ft) **True Area:** 24154.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2023	ML-OVL	Mill and Overlay	0.00	0.00	☑	Variable depth mill, 2" P-401 overlay
1/1/2005	SR-AC	Surface Reconstruction - AC	0.00	0.00	☑	1.5-2" P-401, 1" S-180, P-603
1/1/1993	IMPORTED	OVERLAY	0.00	3.00	☑	1993: 3 INCH P-401 OVERLAY
1/1/1961	IMPORTED	BUILT	0.00	0.50	☑	1961: 1-1/2 INCH P-401 ON 7-1/2 INCH P-211

Network: TALLAHASSEE INT **Branch:** TW A12 TAXIWAY A12 **Section:** 185 **Surface:**AAC

L.C.D. 1/1/2023 **Use:** TAXIWAY **Rank:** P **Length:** 295.00 (Ft) **Width:** 100.00 (Ft) **True Area:** 43156.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2023	ML-OVL	Mill and Overlay	0.00	0.00	☑	Variable depth mill, 2" P-401 overlay
1/1/2005	SR-AC	Surface Reconstruction - AC	0.00	0.00	☑	1.5-2" P-401, 1" S-180, P-603
1/1/1992	IMPORTED	OVERLAY	0.00	3.00	☑	1992: 3" P-401
1/1/1980	IMPORTED	BUILT	0.00	3.00	☑	1980: 3" P-401 ON 13" P-211 ON 4" P-160

Network: TALLAHASSEE INT **Branch:** TW A2 TAXIWAY A2 **Section:** 115 **Surface:**AC

L.C.D. 1/1/2005 **Use:** TAXIWAY **Rank:** P **Length:** 295.00 (Ft) **Width:** 100.00 (Ft) **True Area:** 42179.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2005	SR-AC	Surface Reconstruction - AC	0.00	0.00	☑	1.5-2" P-401, 1" S-180, P-603
1/1/1993	IMPORTED	OVERLAY	0.00	3.00	☑	1993: 3 INCH P-401 OVERLAY
1/1/1971	IMPORTED	BUILT	0.00	2.00	☑	1971: 2 INCH MINIMUM P-401 ON 10 INCH P-211

Network: TALLAHASSEE INT **Branch:** TW A3 TAXIWAY A3 **Section:** 125 **Surface:**AC

L.C.D. 1/1/2005 **Use:** TAXIWAY **Rank:** P **Length:** 295.00 (Ft) **Width:** 60.00 (Ft) **True Area:** 32329.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2005	SR-AC	Surface Reconstruction - AC	0.00	0.00	☑	1.5-2" P-401, 1" S-180, P-603
1/1/1993	IMPORTED	OVERLAY	0.00	3.00	☑	1993: 3 INCH P-401 OVERLAY
1/1/1971	IMPORTED	BUILT	0.00	2.00	☑	1971: 2 INCH MINIMUM P-401 ON 10 INCH P-211

Network: TALLAHASSEE INT **Branch:** TW A3 TAXIWAY A3 **Section:** 130 **Surface:**AC

L.C.D. 7/1/2005 **Use:** TAXIWAY **Rank:** P **Length:** 350.00 (Ft) **Width:** 90.00 (Ft) **True Area:** 34919.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
7/1/2005	NU-IN	New Construction - Initial	0.00	0.00	☑	

Network: TALLAHASSEE INT **Branch:** TW A4 TAXIWAY A4 **Section:** 140 **Surface:**AC

L.C.D. 1/1/1985 **Use:** TAXIWAY **Rank:** P **Length:** 520.00 (Ft) **Width:** 35.00 (Ft) **True Area:** 19805.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/1985	IMPORTED	BUILT	0.00	3.00	☑	1985: 3" P-401 ON 7" P-211

Network: TALLAHASSEE INT **Branch:** TW A7 TAXIWAY A7 **Section:** 150 **Surface:**AAC

L.C.D. 1/1/2023 **Use:** TAXIWAY **Rank:** P **Length:** 300.00 (Ft) **Width:** 110.00 (Ft) **True Area:** 72118.00002 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2023	ML-OVL	Mill and Overlay	0.00	0.00	☑	Variable depth mill, 2" P-401 overlay
1/1/2005	SR-AC	Surface Reconstruction - AC	0.00	0.00	☑	1.5-2" P-401, 1" S-180, P-603
1/1/1993	IMPORTED	OVERLAY	0.00	3.00	☑	1993: 3 INCH P-401 OVERLAY
1/1/1971	IMPORTED	OVERLAY	0.00	0.50	☑	1971: 1-1/2 INCH P-401 OVERLAY
1/1/1961	IMPORTED	BUILT	0.00	0.50	☑	1961: 1-1/2 INCH P-401 ON 10 INCH P-211

Network: TALLAHASSEE INT **Branch:** TW A8 TAXIWAY A8 **Section:** 155 **Surface:**AAC

L.C.D. 1/1/2023 **Use:** TAXIWAY **Rank:** P **Length:** 330.00 (Ft) **Width:** 90.00 (Ft) **True Area:** 43518.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2023	ML-OVL	Mill and Overlay	0.00	0.00	☑	Variable depth mill, 2" P-401 overlay
1/1/2005	SR-AC	Surface Reconstruction - AC	0.00	0.00	☑	1.5-2" P-401, 1" S-180, P-603
1/1/1993	IMPORTED	OVERLAY	0.00	3.00	☑	1993: 3 INCH P-401 OVERLAY
1/1/1971	IMPORTED	OVERLAY	0.00	0.50	☑	1971: 1-1/2 INCH P-401 OVERLAY
1/1/1961	IMPORTED	BUILT	0.00	0.50	☑	1961: 1-1/2 INCH P-401 ON 10 INCH P-211

Network: TALLAHASSEE INT **Branch:** TW A8 TAXIWAY A8 **Section:** 160 **Surface:**AAC

L.C.D. 1/1/2023 **Use:** TAXIWAY **Rank:** P **Length:** 70.00 (Ft) **Width:** 105.00 (Ft) **True Area:** 11115.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2023	ML-OVL	Mill and Overlay	0.00	0.00	☑	Variable depth mill, 2" P-401 overlay
1/1/2010	OL-AS	Overlay - AC Structural	0.00	0.00	☑	
1/1/2005	SR-AC	Surface Reconstruction - AC	0.00	0.00	☑	1.5-2" P-401, 1" S-180, P-603
1/1/1993	IMPORTED	OVERLAY	0.00	3.00	☑	1993: 3 INCH P-401 OVERLAY
1/1/1971	IMPORTED	OVERLAY	0.00	0.50	☑	1971: 1-1/2 INCH P-401 OVERLAY
1/1/1961	IMPORTED	BUILT	0.00	0.50	☑	1961: 1-1/2 INCH P-401 ON 10 INCH P-211

Network: TALLAHASSEE INT **Branch:** TW A9 TAXIWAY A9 **Section:** 165 **Surface:**AC

L.C.D. 1/1/2023 **Use:** TAXIWAY **Rank:** P **Length:** 295.00 (Ft) **Width:** 100.00 (Ft) **True Area:** 51254.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2023	NC-AC	New Construction - AC	0.00	0.00	☑	

Network: TALLAHASSEE INT **Branch:** TW B1 TAXIWAY B1 **Section:** 210 **Surface:**AC

L.C.D. 1/1/2005 **Use:** TAXIWAY **Rank:** P **Length:** 470.00 (Ft) **Width:** 90.00 (Ft) **True Area:** 46292.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2005	SR-AC	Surface Reconstruction - AC	0.00	0.00	☑	1.5-2" P-401, 1" S-180, P-603
1/1/1992	IMPORTED	OVERLAY	0.00	3.00	☑	1992: 3" P-401 OVERLAY
1/1/1980	IMPORTED	BUILT	0.00	3.00	☑	1980: 3" P-401 ON 13" P-211 ON 4" P-160

Network: TALLAHASSEE INT **Branch:** TW B1 TAXIWAY B1 **Section:** 215 **Surface:**AC

L.C.D. 1/1/2015 **Use:** TAXIWAY **Rank:** P **Length:** 135.00 (Ft) **Width:** 30.00 (Ft) **True Area:** 4782.000001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2015	CR-AC	Complete Reconstruction - AC	0.00	0.00	☑	5" P-401; 10" P-211; 12" P-152
1/1/2005	SR-AC	Surface Reconstruction - AC	0.00	0.00	☑	1.5-2" P-401, 1" S-180, P-603
1/1/1992	IMPORTED	OVERLAY	0.00	3.00	☑	1992: 3" P-401 OVERLAY
1/1/1980	IMPORTED	BUILT	0.00	3.00	☑	1980: 3" P-401 ON 13" P-211 ON 4" P-160

Network: TALLAHASSEE INT **Branch:** TW B TAXIWAY B **Section:** 203 **Surface:**AC

L.C.D. 10/1/2012 **Use:** TAXIWAY **Rank:** P **Length:** 290.00 (Ft) **Width:** 130.00 (Ft) **True Area:** 50342.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
10/1/2012	NC-AC	New Construction - AC	0.00	0.00	☑	5" P-401, 10" P-211 LIMEROCK BA

Network: TALLAHASSEE INT **Branch:** TW B TAXIWAY B **Section:** 205 **Surface:**AC

L.C.D. 1/1/2005 **Use:** TAXIWAY **Rank:** P **Length:** 7,865.00 (Ft) **Width:** 75.00 (Ft) **True Area:** 581353.0001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2005	SR-AC	Surface Reconstruction - AC	0.00	0.00	☑	1.5-2" P-401, 1" S-180, P-603
1/1/1992	IMPORTED	OVERLAY	0.00	2.00	☑	1992: 2" P-401 OVERLAY
1/1/1980	IMPORTED	BUILT	0.00	3.00	☑	1980: 3" P-401 ON 13" P-211 ON 4" P-160

Network: TALLAHASSEE INT **Branch:** TW B TAXIWAY B **Section:** 207 **Surface:**AAC

L.C.D. 1/1/2023 **Use:** TAXIWAY **Rank:** P **Length:** 110.00 (Ft) **Width:** 130.00 (Ft) **True Area:** 15151.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2023	ML-OVL	Mill and Overlay	0.00	0.00	☑	Variable depth mill, 2" P-401 overlay
10/1/2012	NC-AC	New Construction - AC	0.00	0.00	☑	5" P-401, 10" P-211 LIMEROCK BA

Network: TALLAHASSEE INT **Branch:** TW B TAXIWAY B **Section:** 209 **Surface:**AAC

L.C.D. 1/1/2023 **Use:** TAXIWAY **Rank:** P **Length:** 255.00 (Ft) **Width:** 100.00 (Ft) **True Area:** 30178.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2023	ML-OVL	Mill and Overlay	0.00	0.00	☑	Variable depth mill, 2" P-401 overlay
10/1/2012	NC-AC	New Construction - AC	0.00	0.00	☑	5" P-401, 10" P-211 LIMEROCK BA

Network: TALLAHASSEE INT **Branch:** TW B2 TAXIWAY B2 **Section:** 220 **Surface:**AC

L.C.D. 1/1/2015 **Use:** TAXIWAY **Rank:** P **Length:** 500.00 (Ft) **Width:** 90.00 (Ft) **True Area:** 49156.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2015	CR-AC	Complete Reconstruction - AC	0.00	0.00	☑	5" P-401, 10" P-211 LIMEROCK BA
1/1/2005	NU-IN	New Construction - Initial	0.00	0.00	☑	

Network: TALLAHASSEE INT **Branch:** TW B3 TAXIWAY B3 **Section:** 230 **Surface:**AC

L.C.D. 1/1/2015 **Use:** TAXIWAY **Rank:** P **Length:** 500.00 (Ft) **Width:** 90.00 (Ft) **True Area:** 63794.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2015	CR-AC	Complete Reconstruction - AC	0.00	0.00	☑	5" P-401, 10" P-211 LIMEROCK BA
1/1/2005	SR-AC	Surface Reconstruction - AC	0.00	0.00	☑	1.5-2" P-401, 1" S-180, P-603
1/1/1992	IMPORTED	OVERLAY	0.00	3.00	☑	1992: 3" P-401 OVERLAY
1/1/1980	IMPORTED	BUILT	0.00	3.00	☑	1980: 3" P-401 ON 13" P-211 ON 4" P-160

Network: TALLAHASSEE INT **Branch:** TW B3 TAXIWAY B3 **Section:** 235 **Surface:**AC

L.C.D. 1/1/2007 **Use:** TAXIWAY **Rank:** P **Length:** 600.00 (Ft) **Width:** 125.00 (Ft) **True Area:** 83567.00002 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2007	NU-IN	New Construction - Initial	0.00	0.00	☑	

Network: TALLAHASSEE INT **Branch:** TW B4 TAXIWAY B4 **Section:** 240 **Surface:**AC

L.C.D. 1/1/2007 **Use:** TAXIWAY **Rank:** P **Length:** 400.00 (Ft) **Width:** 125.00 (Ft) **True Area:** 48156.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2007	NU-IN	New Construction - Initial	0.00	0.00	☑	

Network: TALLAHASSEE INT **Branch:** TW B5 TAXIWAY B5 **Section:** 250 **Surface:**AC

L.C.D. 1/1/2005 **Use:** TAXIWAY **Rank:** P **Length:** 100.00 (Ft) **Width:** 100.00 (Ft) **True Area:** 24545.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2005	SR-AC	Surface Reconstruction - AC	0.00	0.00	☑	1.5"-2" P-401, 1" S-180, P-603
1/1/1989	IMPORTED	BUILT	0.00	4.00	☑	1989: 4" P-401 ON 14" P-211

Network: TALLAHASSEE INT **Branch:** TW B6 TAXIWAY B6 **Section:** 260 **Surface:**AC

L.C.D. 1/1/2015 **Use:** TAXIWAY **Rank:** P **Length:** 390.00 (Ft) **Width:** 90.00 (Ft) **True Area:** 38862.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2015	CR-AC	Complete Reconstruction - AC	0.00	0.00	☑	5" P-401, 10" P-211 LIMEROCK BA
1/1/2005	NU-IN	New Construction - Initial	0.00	0.00	☑	

Network: TALLAHASSEE INT **Branch:** TW B6 TAXIWAY B6 **Section:** 265 **Surface:**AC

L.C.D. 1/1/2005 **Use:** TAXIWAY **Rank:** P **Length:** 100.00 (Ft) **Width:** 150.00 (Ft) **True Area:** 17002.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2005	SR-AC	Surface Reconstruction - AC	0.00	0.00	☑	1.5-2" P-401, 1" S-180, P-603
1/1/1992	IMPORTED	OVERLAY	0.00	3.00	☑	1992: 3" P-401 OVERLAY
1/1/1980	IMPORTED	BUILT	0.00	3.00	☑	1980: 3" P-401 ON 13"P-211 ON 4" P-160

Network: TALLAHASSEE INT **Branch:** TW B6 TAXIWAY B6 **Section:** 267 **Surface:**AC

L.C.D. 1/1/2005 **Use:** TAXIWAY **Rank:** P **Length:** 100.00 (Ft) **Width:** 75.00 (Ft) **True Area:** 24158.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2005	SR-AC	Surface Reconstruction - AC	0.00	0.00	☑	1.5-2" P-401, 1" S-180, P-603
1/1/1989	IMPORTED	BUILT	0.00	4.00	☑	1989: 4" P-401 ON 14" P-211

Network: TALLAHASSEE INT **Branch:** TW B7 TAXIWAY B7 **Section:** 270 **Surface:**AC

L.C.D. 1/1/2015 **Use:** TAXIWAY **Rank:** P **Length:** 500.00 (Ft) **Width:** 90.00 (Ft) **True Area:** 39535.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2015	CR-AC	Complete Reconstruction - AC	0.00	0.00	☑	5" P-401, 10" P-211 Limerock Base, 1
1/1/2005	NC-AC	New Construction - AC	0.00	0.00	☑	

Network: TALLAHASSEE INT **Branch:** TW B7 TAXIWAY B7 **Section:** 271 **Surface:**AC

L.C.D. 1/1/2015 **Use:** TAXIWAY **Rank:** P **Length:** 500.00 (Ft) **Width:** 90.00 (Ft) **True Area:** 23946.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2015	CR-AC	Complete Reconstruction - AC	0.00	0.00	☑	5" P-401, 10" P-211 Limerock Base, 1
1/1/2005	SR-AC	Surface Reconstruction - AC	0.00	0.00	☑	1.5-2" P-401, 1" S-180, P-603
1/1/1992	IMPORTED	OVERLAY	0.00	3.00	☑	1992: 3" P-401 OVERLAY
1/1/1980	IMPORTED	BUILT	0.00	3.00	☑	1980: 3" P-401 ON 13" P-211 ON 4" P-160

Network: TALLAHASSEE INT **Branch:** TW B7 TAXIWAY B7 **Section:** 273 **Surface:**AC

L.C.D. 1/1/2005 **Use:** TAXIWAY **Rank:** P **Length:** 312.00 (Ft) **Width:** 90.00 (Ft) **True Area:** 38359.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2005	SR-AC	Surface Reconstruction - AC	0.00	0.00	☑	1.5-2" P-401, 1" S-180, P-603
1/1/1992	IMPORTED	OVERLAY	0.00	3.00	☑	1992: 3" P-401 OVERLAY
1/1/1980	IMPORTED	BUILT	0.00	3.00	☑	1980: 3" P-401 ON 11" P-211 ON 7" P-160

Network: TALLAHASSEE INT **Branch:** TW B7 TAXIWAY B7 **Section:** 275 **Surface:**AAC

L.C.D. 1/2/1992 **Use:** TAXIWAY **Rank:** P **Length:** 150.00 (Ft) **Width:** 60.00 (Ft) **True Area:** 9455.000002 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/2/1992	OL-AS	Overlay - AC Structural	0.00	0.00	☑	1992: P-401 FEATHERED FROM A
1/1/1961	IMPORTED	BUILT	0.00	0.00	☑	1961: 1.5" P-401 ON 7.5" P-211

Network: TALLAHASSEE INT **Branch:** TW B7 TAXIWAY B7 **Section:** 277 **Surface:**AAC

L.C.D. 1/1/1994 **Use:** TAXIWAY **Rank:** P **Length:** 150.00 (Ft) **Width:** 60.00 (Ft) **True Area:** 8669.000002 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/1994	IMPORTED	OVERLAY	0.00	3.00	☑	1994: 3 INCH P-401 OVERLAY
1/1/1961	IMPORTED	BUILT	0.00	0.50	☑	1961: 1-1/2 INCH P-401 ON 7-1/2 INCH P-211

Network: TALLAHASSEE INT **Branch:** TW B8 TAXIWAY B8 **Section:** 280 **Surface:**AC

L.C.D. 7/1/2003 **Use:** TAXIWAY **Rank:** P **Length:** 320.00 (Ft) **Width:** 130.00 (Ft) **True Area:** 66948.00002 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
7/1/2003	NU-IN	New Construction - Initial	0.00	0.00	☑	

Network: TALLAHASSEE INT **Branch:** TW B8 TAXIWAY B8 **Section:** 285 **Surface:**AC

L.C.D. 1/1/2003 **Use:** TAXIWAY **Rank:** P **Length:** 380.00 (Ft) **Width:** 100.00 (Ft) **True Area:** 58220.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2003	CR-AC	Complete Reconstruction - AC	0.00	0.00	☑	4" P-401, P-602, 8" P-211, 6" P-160, P
1/1/1992	IMPORTED	OVERLAY	0.00	0.00	☑	1992: P-401 FEATHERED OVERLAY
1/1/1960	IMPORTED	BUILT	0.00	1.50	☑	1960: 1.5" P-401 ON 7.5" P-211

Network: TALLAHASSEE INT **Branch:** TW B9 TAXIWAY B9 **Section:** 290 **Surface:**AC

L.C.D. 1/1/2015 **Use:** TAXIWAY **Rank:** P **Length:** 77.00 (Ft) **Width:** 90.00 (Ft) **True Area:** 20199.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2015	CR-AC	Complete Reconstruction - AC	0.00	0.00	☑	5" P-401, 10" P-211 Limerock Base, 1
1/1/2005	SR-AC	Surface Reconstruction - AC	0.00	0.00	☑	
1/1/1992	OL-AS	Overlay - AC Structural	0.00	0.00	☑	
1/1/1980	NC-AC	New Construction - AC	0.00	0.00	☑	

Network: TALLAHASSEE INT **Branch:** TW B9 TAXIWAY B9 **Section:** 295 **Surface:**AC

L.C.D. 1/1/2005 **Use:** TAXIWAY **Rank:** P **Length:** 850.00 (Ft) **Width:** 90.00 (Ft) **True Area:** 84260.00002 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2005	SR-AC	Surface Reconstruction - AC	0.00	0.00	☑	1.5-2" P-401, 1" S-180, P-603
1/1/1992	IMPORTED	OVERLAY	0.00	3.00	☑	1992: 3" P-401
1/1/1980	IMPORTED	BUILT	0.00	3.00	☑	1980: 3" P-401 ON 13" P-211 ON 4" P-160

Network: TALLAHASSEE INT **Branch:** TW C TAXIWAY C **Section:** 303 **Surface:**AAC

L.C.D. 1/1/2023 **Use:** TAXIWAY **Rank:** P **Length:** 270.00 (Ft) **Width:** 100.00 (Ft) **True Area:** 37868.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2023	ML-OVL	Mill and Overlay	0.00	0.00	☑	Variable depth mill, 2" P-401 overlay
10/1/2012	NC-AC	New Construction - AC	0.00	0.00	☑	5" P-401, 10" P-211 LIMEROCK BA

Network: TALLAHASSEE INT **Branch:** TW C TAXIWAY C **Section:** 305 **Surface:**AAC

L.C.D. 1/1/2023 **Use:** TAXIWAY **Rank:** P **Length:** 415.00 (Ft) **Width:** 100.00 (Ft) **True Area:** 53314.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2023	ML-OVL	Mill and Overlay	0.00	0.00	☑	Variable depth mill, 2" P-401 overlay
10/1/2012	NC-AC	New Construction - AC	0.00	0.00	☑	5" P-401, 10" P-211 LIMEROCK BA

Network: TALLAHASSEE INT **Branch:** TW C TAXIWAY C **Section:** 307 **Surface:**AAC

L.C.D. 1/1/2005 **Use:** TAXIWAY **Rank:** P **Length:** 100.00 (Ft) **Width:** 125.00 (Ft) **True Area:** 10756.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2005	OL-AS	Overlay - AC Structural	0.00	0.00	☑	1.5" P-401; 1" S-180; P-603
1/1/1992	IMPORTED	OVERLAY	0.00	3.00	☑	1992: 3" P-401 OVERLAY
1/1/1985	IMPORTED	OVERLAY	0.00	2.50	☑	1985: 2.5" P-401 OVERLAY
1/1/1961	IMPORTED	BUILT	0.00	1.50	☑	1961: 1.5" P-401 ON 7.5" P-211

Network: TALLAHASSEE INT **Branch:** TW C TAXIWAY C **Section:** 310 **Surface:**AAC

L.C.D. 1/1/1992 **Use:** TAXIWAY **Rank:** P **Length:** 1,960.00 (Ft) **Width:** 75.00 (Ft) **True Area:** 160476.0000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/1992	IMPORTED	OVERLAY	0.00	3.00	☑	1992: 3" P-401 OVERLAY
1/1/1985	IMPORTED	OVERLAY	0.00	2.50	☑	1985: 2.5" P-401 OVERLAY
1/1/1961	IMPORTED	BUILT	0.00	1.50	☑	1961: 1.5" P-401 ON 7.5" P-211

Network: TALLAHASSEE INT **Branch:** TW C TAXIWAY C **Section:** 315 **Surface:**AAC

L.C.D. 1/1/2003 **Use:** TAXIWAY **Rank:** P **Length:** 650.00 (Ft) **Width:** 75.00 (Ft) **True Area:** 55835.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/2003	ML-OVL	Mill and Overlay	0.00	0.00	☑	1.75 Mill and Overlay (Due to Grout)
7/24/1991	OL-AS	Overlay - AC Structural	0.00	0.00	☑	6-AC over existing
3/1/1985	OL-AS	Overlay - AC Structural	0.00	0.00	☑	2.5-AC over existing
1/15/1960	NU-IN	New Construction - Initial	0.00	0.00	☑	1.5-AC, 7.5-LR, 6-SG

Network: TALLAHASSEE INT **Branch:** TW D TAXIWAY D **Section:** 405 **Surface:**AC

L.C.D. 7/1/2005 **Use:** TAXIWAY **Rank:** P **Length:** 612.00 (Ft) **Width:** 50.00 (Ft) **True Area:** 33610.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
7/1/2005	NU-IN	New Construction - Initial	0.00	0.00	☑	

Network: TALLAHASSEE INT **Branch:** TW D TAXIWAY D **Section:** 410 **Surface:**AC

L.C.D. 1/1/1998 **Use:** TAXIWAY **Rank:** P **Length:** 185.00 (Ft) **Width:** 50.00 (Ft) **True Area:** 10157.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/1998	NU-IN	New Construction - Initial	0.00	0.00	☑	

Network: TALLAHASSEE INT **Branch:** TW Z TAXIWAY Z **Section:** 2605 **Surface:**AC

L.C.D. 1/1/1994 **Use:** TAXIWAY **Rank:** P **Length:** 1,200.00 (Ft) **Width:** 50.00 (Ft) **True Area:** 62575.00001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/1994	IMPORTED	BUILT	0.00	3.00	☑	1994 - 3 INCH P-401 ON 1960 - 7-1/2 INCH P-211
1/1/1994	IMPORTED	OVERLAY	0.00	0.00	☑	EX. SURFACE COURSE MILLED OFF IN 1994 OVERLAY

Network: TALLAHASSEE INT **Branch:** TW Z TAXIWAY Z **Section:** 2610 **Surface:**AC

L.C.D. 1/1/1994 **Use:** TAXIWAY **Rank:** P **Length:** 90.00 (Ft) **Width:** 20.00 (Ft) **True Area:** 2379.000000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/1994	IMPORTED	BUILT	0.00	3.00	☑	1994 - 3 INCH P-401 ON EX. BASE
1/1/1994	IMPORTED	OVERLAY	0.00	0.00	☑	EX. ASPHALT WAS MILLED OFF DURING 1994 JOB

Network: TALLAHASSEE INT **Branch:** TW Z TAXIWAY Z **Section:** 2615 **Surface:**AC

L.C.D. 1/1/1994 **Use:** TAXIWAY **Rank:** P **Length:** 90.00 (Ft) **Width:** 40.00 (Ft) **True Area:** 2615.000000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/1994	IMPORTED	BUILT	0.00	3.00	☑	1994 - 3 INCH P-401 ON EX. BASE
1/1/1994	IMPORTED	OVERLAY	0.00	0.00	☑	EXISTING SURFACE MILLED OFF PRIOR TO 1994 P-401

Summary:

Work Description	Section Count	Area Total (SqFt)	Thickness Avg (in)	Thickness STD (in)
BUILT	49	6,538,941.00	2.56	2.47
Complete Reconstruction - AC	13	2,811,043.00	0.00	0.00
Complete Reconstruction - PCC	2	16,058.00	0.00	0.00
Mill and Overlay	20	1,227,817.00	0.00	0.00
New Construction - AC	10	370,487.00	0.00	0.00
New Construction - Initial	22	1,449,623.00	0.00	0.00
New Construction - PCC	2	30,813.00	0.00	0.00
OVERLAY	47	7,083,856.00	2.07	1.27
Overlay - AC Structural	11	803,930.00	0.00	0.00
Patching - AC	1	607,550.00	0.00	0.00
Surface Reconstruction - AC	22	1,947,429.00	0.00	0.00
Surface Treatment - Seal Coat	7	1,001,039.00	0.00	0.00

Branch Condition Report

Pavement Database: FDOT

Branch ID	Number of Sections	Sum Section Length (Ft)	Avg Section Width (Ft)	True Area (SqFt)	Use	Average PCI	Standard Deviation PCI	Weighted Average PCI
AP C	1	500.00	500.00	265,932.00	APRON	74.00	0.00	74.00
AP CARGO	3	2,060.00	355.33	484,155.00	APRON	79.00	4.08	75.54
AP HELI	2	164.00	452.00	67,720.00	APRON	96.50	1.50	97.22
AP N	5	2,079.00	261.00	637,391.00	APRON	75.40	3.61	72.95
AP RU 18	1	140.00	180.00	25,207.00	APRON	64.00	0.00	64.00
AP S	7	1,989.00	319.57	796,292.00	APRON	95.86	2.23	95.46
AP TERM	2	2,410.00	257.50	868,701.00	APRON	64.50	15.50	79.52
RW 18-36	6	21,001.00	62.50	1,050,000.00	RUNWAY	100.00	0.00	100.00
RW 9-27	2	24,150.00	62.50	1,200,000.00	RUNWAY	89.00	1.00	89.33
TL AP S	1	150.00	38.00	6,963.00	TAXIWAY	65.00	0.00	65.00
TL T-HANG	3	3,565.00	26.67	125,875.00	TAXIWAY	53.33	6.60	52.67
TW A	4	7,040.00	87.50	562,900.00	TAXIWAY	82.00	18.12	83.68
TW A1	1	295.00	100.00	40,291.00	TAXIWAY	64.00	0.00	64.00
TW A10	2	545.00	50.00	27,376.00	TAXIWAY	85.00	15.00	94.57
TW A11	1	356.00	55.00	24,154.00	TAXIWAY	100.00	0.00	100.00
TW A12	1	295.00	100.00	43,156.00	TAXIWAY	100.00	0.00	100.00
TW A2	1	295.00	100.00	42,179.00	TAXIWAY	70.00	0.00	70.00
TW A3	2	645.00	75.00	67,248.00	TAXIWAY	64.00	3.00	64.12
TW A4	1	520.00	35.00	19,805.00	TAXIWAY	54.00	0.00	54.00
TW A7	1	300.00	110.00	72,118.00	TAXIWAY	100.00	0.00	100.00
TW A8	2	400.00	97.50	54,633.00	TAXIWAY	100.00	0.00	100.00
TW A9	1	295.00	100.00	51,254.00	TAXIWAY	100.00	0.00	100.00
TW B	4	8,520.00	108.75	677,024.00	TAXIWAY	81.00	20.81	55.13
TW B1	2	605.00	60.00	51,074.00	TAXIWAY	70.50	16.50	57.09
TW B2	1	500.00	90.00	49,156.00	TAXIWAY	87.00	0.00	87.00
TW B3	2	1,100.00	107.50	147,361.00	TAXIWAY	83.00	7.00	82.06
TW B4	1	400.00	125.00	48,156.00	TAXIWAY	76.00	0.00	76.00
TW B5	1	100.00	100.00	24,545.00	TAXIWAY	43.00	0.00	43.00
TW B6	3	590.00	105.00	80,022.00	TAXIWAY	65.00	13.74	69.03
TW B7	5	1,612.00	78.00	119,964.00	TAXIWAY	70.40	12.22	73.57
TW B8	2	700.00	115.00	125,168.00	TAXIWAY	72.00	6.00	71.58
TW B9	2	927.00	90.00	104,459.00	TAXIWAY	69.00	14.00	60.41
TW C	5	3,395.00	95.00	318,249.00	TAXIWAY	77.00	19.71	68.67
TW D	2	797.00	50.00	43,767.00	TAXIWAY	68.00	1.00	68.54
TW Z	3	1,380.00	36.67	67,569.00	TAXIWAY	61.67	13.96	71.79

Pavement Database: FDOT

Use Category	Number of Sections	Total Area (SqFt)	Arithmetic Average PCI	Average STD PCI	Weighted Average PCI
APRON	21	3,145,398.00	83.10	13.13	81.40
RUNWAY	8	2,250,000.00	97.25	4.79	94.31
TAXIWAY	54	2,994,466.00	74.28	18.09	71.09
ALL	83	8,389,864.00	78.72	17.59	81.18

Pavement Database: FDOT *NetworkId: TLH*

Branch ID	Section ID	Last Const. Date	Surface	Use	Rank	Lanes	True Area (SqFt)	Last Inspection Date	Age At Inspection	PCI
AP C	4505	1/1/2005	AC	APRON	P	0	265,932.00	11/30/2021	16	74
AP CARGO	4205	1/1/1990	AC	APRON	P	0	65,663.00	11/30/2021	31	84
AP CARGO	4210	1/1/2007	AC	APRON	P	0	400,242.00	11/30/2021	14	74
AP CARGO	4215	1/1/2007	PCC	APRON	P	0	18,250.00	11/30/2021	14	79
AP HELI	4340	1/5/2018	PCC	APRON	P	0	17,496.00	11/30/2021	3	95
AP HELI	4345	1/5/2018	AC	APRON	P	0	50,224.00	11/30/2021	3	98
AP N	4405	1/1/2010	AAC	APRON	P	0	77,291.00	11/30/2021	11	80
AP N	4410	1/1/2010	AAC	APRON	P	0	215,063.00	11/30/2021	11	71
AP N	4415	1/1/2010	APC	APRON	P	0	310,550.00	11/30/2021	11	72
AP N	4420	1/1/2010	APC	APRON	P	0	24,514.00	11/30/2021	11	79
AP N	4425	1/1/2010	AC	APRON	P	0	9,973.00	11/30/2021	11	75
AP RU 18	5505	1/1/2005	AC	APRON	P	0	25,207.00	11/30/2021	16	64
AP S	4305	1/5/2018	AAC	APRON	P	0	70,348.00	11/30/2021	3	91
AP S	4310	1/5/2018	AAC	APRON	P	0	179,279.00	11/30/2021	3	95
AP S	4313	1/5/2018	PCC	APRON	P	0	11,875.00	11/30/2021	3	98
AP S	4315	1/5/2018	AAC	APRON	P	0	60,505.00	11/30/2021	3	96
AP S	4320	1/5/2018	AAC	APRON	P	0	68,878.00	11/30/2021	3	97
AP S	4325	1/5/2018	PCC	APRON	P	0	4,183.00	11/30/2021	3	98
AP S	4332	1/5/2018	AC	APRON	P	0	401,224.00	11/30/2021	3	96
AP TERM	4105	1/1/1989	PCC	APRON	P	0	855,384.00	11/30/2021	32	80
AP TERM	4110	1/1/2005	APC	APRON	P	0	13,317.00	11/30/2021	16	49
RW 18-36	6105	1/1/2023	AC	RUNWAY	P	0	607,550.00	1/1/2023	0	100
RW 18-36	6110	1/1/2023	AC	RUNWAY	P	0	303,775.00	1/1/2023	0	100
RW 18-36	6125	1/1/2023	AAC	RUNWAY	P	0	63,750.00	1/1/2023	0	100
RW 18-36	6130	1/1/2023	AAC	RUNWAY	P	0	31,875.00	1/1/2023	0	100
RW 18-36	6155	1/1/2023	AAC	RUNWAY	P	0	28,700.00	1/1/2023	0	100
RW 18-36	6160	1/1/2023	AAC	RUNWAY	P	0	14,350.00	1/1/2023	0	100
RW 9-27	6205	1/1/2015	AC	RUNWAY	P	0	400,000.00	11/30/2021	6	88
RW 9-27	6210	1/1/2015	AC	RUNWAY	P	0	800,000.00	11/30/2021	6	90

TL AP S	3205	1/1/1994	AAC	TAXIWAY	P	0	6,963.00	11/30/2021	27	65
TL T-HANG	3105	1/1/1998	AC	TAXIWAY	P	0	46,227.00	11/30/2021	23	62
TL T-HANG	3110	1/1/1985	AC	TAXIWAY	P	0	16,646.00	11/30/2021	36	52
TL T-HANG	3115	1/1/1985	AC	TAXIWAY	P	0	63,002.00	11/30/2021	36	46
TW A	103	1/1/2023	AAC	TAXIWAY	P	0	79,944.00	1/1/2023	0	100
TW A	105	1/1/2023	AAC	TAXIWAY	P	0	243,781.00	1/1/2023	0	100
TW A	106	1/1/2005	AC	TAXIWAY	P	0	215,250.00	11/30/2021	16	61
TW A	107	10/1/2012	AC	TAXIWAY	P	0	23,925.00	11/30/2021	9	67
TW A1	110	10/1/2012	AC	TAXIWAY	P	0	40,291.00	11/30/2021	9	64
TW A10	170	1/1/2023	AC	TAXIWAY	P	0	22,422.00	1/1/2023	0	100
TW A10	175	12/25/1999	AC	TAXIWAY	P	0	4,954.00	11/30/2021	22	70
TW A11	180	1/1/2023	AAC	TAXIWAY	P	0	24,154.00	1/1/2023	0	100
TW A12	185	1/1/2023	AAC	TAXIWAY	P	0	43,156.00	1/1/2023	0	100
TW A2	115	1/1/2005	AC	TAXIWAY	P	0	42,179.00	11/30/2021	16	70
TW A3	125	1/1/2005	AC	TAXIWAY	P	0	32,329.00	11/30/2021	16	61
TW A3	130	7/1/2005	AC	TAXIWAY	P	0	34,919.00	11/30/2021	16	67
TW A4	140	1/1/1985	AC	TAXIWAY	P	0	19,805.00	11/30/2021	36	54
TW A7	150	1/1/2023	AAC	TAXIWAY	P	0	72,118.00	1/1/2023	0	100
TW A8	155	1/1/2023	AAC	TAXIWAY	P	0	43,518.00	1/1/2023	0	100
TW A8	160	1/1/2023	AAC	TAXIWAY	P	0	11,115.00	1/1/2023	0	100
TW A9	165	1/1/2023	AC	TAXIWAY	P	0	51,254.00	1/1/2023	0	100
TW B	203	10/1/2012	AC	TAXIWAY	P	0	50,342.00	11/30/2021	9	74
TW B	205	1/1/2005	AC	TAXIWAY	P	0	581,353.00	11/30/2021	16	50
TW B	207	1/1/2023	AAC	TAXIWAY	P	0	15,151.00	1/1/2023	0	100
TW B	209	1/1/2023	AAC	TAXIWAY	P	0	30,178.00	1/1/2023	0	100
TW B1	210	1/1/2005	AC	TAXIWAY	P	0	46,292.00	11/30/2021	16	54
TW B1	215	1/1/2015	AC	TAXIWAY	P	0	4,782.00	11/30/2021	6	87
TW B2	220	1/1/2015	AC	TAXIWAY	P	0	49,156.00	11/30/2021	6	87
TW B3	230	1/1/2015	AC	TAXIWAY	P	0	63,794.00	11/30/2021	6	90
TW B3	235	1/1/2007	AC	TAXIWAY	P	0	83,567.00	11/30/2021	14	76
TW B4	240	1/1/2007	AC	TAXIWAY	P	0	48,156.00	11/30/2021	14	76
TW B5	250	1/1/2005	AC	TAXIWAY	P	0	24,545.00	11/30/2021	16	43

TW B6	260	1/1/2015	AC	TAXIWAY	P	0	38,862.00	11/30/2021	6	84
TW B6	265	1/1/2005	AC	TAXIWAY	P	0	17,002.00	11/30/2021	16	59
TW B6	267	1/1/2005	AC	TAXIWAY	P	0	24,158.00	11/30/2021	16	52
TW B7	270	1/1/2015	AC	TAXIWAY	P	0	39,535.00	11/30/2021	6	85
TW B7	271	1/1/2015	AC	TAXIWAY	P	0	23,946.00	11/30/2021	6	83
TW B7	273	1/1/2005	AC	TAXIWAY	P	0	38,359.00	11/30/2021	16	62
TW B7	275	1/2/1992	AAC	TAXIWAY	P	0	9,455.00	11/30/2021	29	53
TW B7	277	1/1/1994	AAC	TAXIWAY	P	0	8,669.00	11/30/2021	27	69
TW B8	280	7/1/2003	AC	TAXIWAY	P	0	66,948.00	11/30/2021	18	66
TW B8	285	1/1/2003	AC	TAXIWAY	P	0	58,220.00	11/30/2021	18	78
TW B9	290	1/1/2015	AC	TAXIWAY	P	0	20,199.00	11/30/2021	6	83
TW B9	295	1/1/2005	AC	TAXIWAY	P	0	84,260.00	11/30/2021	16	55
TW C	303	1/1/2023	AAC	TAXIWAY	P	0	37,868.00	1/1/2023	0	100
TW C	305	1/1/2023	AAC	TAXIWAY	P	0	53,314.00	1/1/2023	0	100
TW C	307	1/1/2005	AAC	TAXIWAY	P	0	10,756.00	11/30/2021	16	65
TW C	310	1/1/1992	AAC	TAXIWAY	P	0	160,476.00	11/30/2021	29	51
TW C	315	1/1/2003	AAC	TAXIWAY	P	0	55,835.00	11/30/2021	18	69
TW D	405	7/1/2005	AC	TAXIWAY	P	0	33,610.00	11/30/2021	16	69
TW D	410	1/1/1998	AC	TAXIWAY	P	0	10,157.00	11/30/2021	23	67
TW Z	2605	1/1/1994	AC	TAXIWAY	P	0	62,575.00	11/30/2021	27	73
TW Z	2610	1/1/1994	AC	TAXIWAY	P	0	2,379.00	11/30/2021	27	42
TW Z	2615	1/1/1994	AC	TAXIWAY	P	0	2,615.00	11/30/2021	27	70

Pavement Database: FDOT

Age Category	Average Age at Inspection	Total Area (SqFt)	Number of Sections	Arithmetic Average PCI	Standard Deviation PCI	Weighted Average PCI
00-02		1,777,973.00	19	100.00	0.00	100.00
03-05	3	864,012.00	9	96.00	2.11	95.60
06-10	7	1,554,832.00	12	81.83	8.37	87.36
11-15	12	1,187,606.00	9	75.78	2.97	73.73
16-20	16	1,670,471.00	19	61.47	8.91	59.90
21-25	23	61,338.00	3	66.33	3.30	63.47
26-30	28	253,132.00	7	60.43	10.87	57.63
31-35	32	921,047.00	2	82.00	2.00	80.29
36-40	36	99,453.00	3	50.67	3.40	48.60
ALL	12	8,389,864.00	83	78.72	17.59	81.18

Appendix B: Maintenance and Rehabilitation Planning Needs

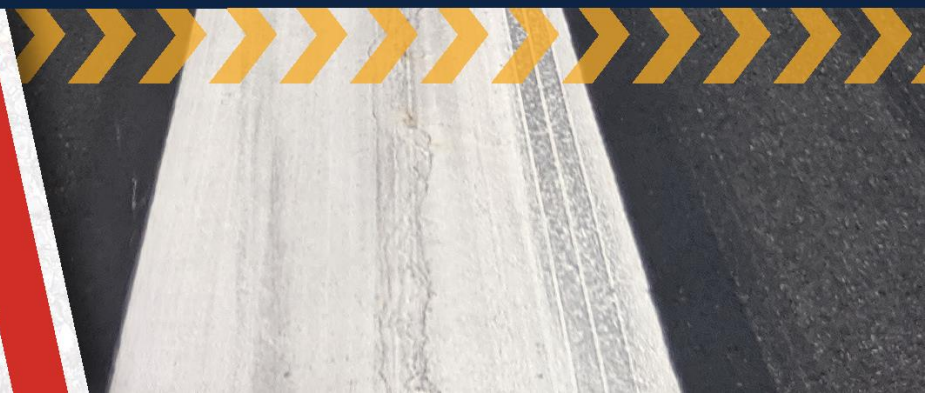


Table B.1: Localized Maintenance and Repair Needs Based on Current Distresses

Network ID	Branch ID	Section ID	Description	Severity	Distress Qty	Distress Unit	Distress Density	Policy Type	Localized Work Type	Work Qty	Work Unit	Unit Cost	Work Cost
TLH	RW 9-27	6205	L & T CR	Medium	25	LF	0.0%	Preventive	AC Crack Sealing	25	LF	$ 4.00	$ 100
TLH	RW 9-27	6205	RAVELING	Low	250	SF	0.1%	Preventive	Surface Seal	250	SF	$ 0.75	$ 190
TLH	RW 9-27	6205	WEATHERING	Medium	13,725	SF	3.4%	Preventive	Surface Seal	13,725	SF	$ 0.75	$ 10,300
TLH	RW 9-27	6210	L & T CR	Medium	112	LF	0.0%	Preventive	AC Crack Sealing	112	LF	$ 4.00	$ 450
TLH	TW B	203	L & T CR	Medium	387	LF	0.8%	Preventive	AC Crack Sealing	387	LF	$ 4.00	$ 1,550
TLH	TW B3	230	WEATHERING	Medium	1,982	SF	3.1%	Preventive	Surface Seal	1,982	SF	$ 0.75	$ 1,490
TLH	TW B3	235	L & T CR	Medium	855	LF	1.0%	Preventive	AC Crack Sealing	855	LF	$ 4.00	$ 3,430
TLH	TW B3	235	WEATHERING	Medium	4,179	SF	5.0%	Preventive	Surface Seal	4,179	SF	$ 0.75	$ 3,140
TLH	TW B4	240	L & T CR	Medium	336	LF	0.7%	Preventive	AC Crack Sealing	337	LF	$ 4.00	$ 1,350
TLH	TW B4	240	RAVELING	Low	1,202	SF	2.5%	Preventive	Surface Seal	1,201	SF	$ 0.75	$ 910
TLH	TW B4	240	WEATHERING	Medium	4,824	SF	10.0%	Preventive	Surface Seal	4,824	SF	$ 0.75	$ 3,620
TLH	TW B6	260	L & T CR	Medium	93	LF	0.2%	Preventive	AC Crack Sealing	93	LF	$ 4.00	$ 380
TLH	TW B8	285	L & T CR	Medium	259	LF	0.5%	Preventive	AC Crack Sealing	259	LF	$ 4.00	$ 1,040
TLH	TW B8	285	WEATHERING	Medium	5,823	SF	10.0%	Preventive	Surface Seal	5,823	SF	$ 0.75	$ 4,370
TLH	TW B9	290	WEATHERING	Medium	262	SF	1.3%	Preventive	Surface Seal	263	SF	$ 0.75	$ 200
TLH	TW Z	2605	L & T CR	Medium	320	LF	0.5%	Preventive	AC Crack Sealing	320	LF	$ 4.00	$ 1,280
TLH	TW Z	2605	RAVELING	Low	12,514	SF	20.0%	Preventive	Surface Seal	12,514	SF	$ 0.75	$ 9,390
TLH	AP C	4505	L & T CR	Medium	1,366	LF	0.5%	Preventive	AC Crack Sealing	1,366	LF	$ 4.00	$ 5,470
TLH	AP C	4505	RAVELING	Low	6,277	SF	2.4%	Preventive	Surface Seal	6,276	SF	$ 0.75	$ 4,710
TLH	AP C	4505	WEATHERING	Medium	50,248	SF	18.9%	Preventive	Surface Seal	50,248	SF	$ 0.75	$ 37,690
TLH	AP CARGO	4205	L & T CR	Medium	64	LF	0.1%	Preventive	AC Crack Sealing	64	LF	$ 4.00	$ 260
TLH	AP CARGO	4205	RAVELING	Low	637	SF	1.0%	Preventive	Surface Seal	637	SF	$ 0.75	$ 480
TLH	AP CARGO	4210	L & T CR	Medium	2,853	LF	0.7%	Preventive	AC Crack Sealing	2,853	LF	$ 4.00	$ 11,420
TLH	AP CARGO	4210	RAVELING	Low	29,960	SF	7.5%	Preventive	Surface Seal	29,959	SF	$ 0.75	$ 22,470
TLH	AP CARGO	4210	WEATHERING	Medium	2,835	SF	0.7%	Preventive	Surface Seal	2,835	SF	$ 0.75	$ 2,130
TLH	AP CARGO	4215	JT SEAL DMG	Low	29	Slabs	100.0%	Preventive	PCC Joint Seal	771	LF	$ 4.25	$ 3,280
TLH	AP N	4405	L & T CR	Medium	145	LF	0.2%	Preventive	AC Crack Sealing	145	LF	$ 4.00	$ 590
TLH	AP N	4405	WEATHERING	Medium	3,868	SF	5.0%	Preventive	Surface Seal	3,868	SF	$ 0.75	$ 2,910
TLH	AP N	4410	L & T CR	Medium	2,555	LF	1.2%	Preventive	AC Crack Sealing	2,555	LF	$ 4.00	$ 10,230
TLH	AP N	4415	L & T CR	Medium	4,177	LF	1.3%	Preventive	AC Crack Sealing	4,177	LF	$ 4.00	$ 16,710
TLH	AP N	4415	WEATHERING	Medium	15,521	SF	5.0%	Preventive	Surface Seal	15,522	SF	$ 0.75	$ 11,650
TLH	AP N	4420	WEATHERING	Medium	1,226	SF	5.0%	Preventive	Surface Seal	1,226	SF	$ 0.75	$ 920
TLH	AP N	4425	WEATHERING	Medium	496	SF	5.0%	Preventive	Surface Seal	496	SF	$ 0.75	$ 380
TLH	AP TERM	4105	CORNER BREAK	High	1	Slabs	0.0%	Preventive	PCC Full-Depth Patching	32	SF	$ 75.00	$ 2,430
TLH	AP TERM	4105	JT SEAL DMG	Low	557	Slabs	13.3%	Preventive	PCC Joint Seal	13,713	LF	$ 4.25	$ 58,280
TLH	AP TERM	4105	JT SEAL DMG	Medium	3,273	Slabs	78.1%	Preventive	PCC Joint Seal	80,563	LF	$ 4.25	$ 342,400
TLH	AP TERM	4105	JT SEAL DMG	High	294	Slabs	7.0%	Preventive	PCC Joint Seal	7,226	LF	$ 4.25	$ 30,710
TLH	AP TERM	4105	SMALL PATCH	Medium	57	Slabs	1.4%	Preventive	PCC Partial-Depth Patching	153	SF	$ 169.00	$ 25,790
TLH	AP TERM	4105	SCALING	Medium	1	Slabs	0.0%	Preventive	PCC Slab Replacement	70	SF	$ 51.50	$ 3,600
TLH	AP TERM	4105	JOINT SPALL	Medium	56	Slabs	1.3%	Preventive	PCC Partial-Depth Patching	360	SF	$ 169.00	$ 60,810
TLH	AP TERM	4105	CORNER SPALL	Medium	42	Slabs	1.0%	Preventive	PCC Partial-Depth Patching	112	SF	$ 169.00	$ 19,010
TLH	TL T-HANG	3110	PATCHING	High	145	SF	0.9%	Stopgap	AC Full-Depth Patching	197	SF	$ 18.75	$ 3,710
TLH	TW B	205	ALLIGATOR CR	Medium	2,655	SF	0.5%	Stopgap	AC Full-Depth Patching	2,866	SF	$ 18.75	$ 53,760
TLH	TW B7	273	RAVELING	High	18	SF	0.1%	Stopgap	AC Partial-Depth Patching	18	SF	$ 6.50	$ 120

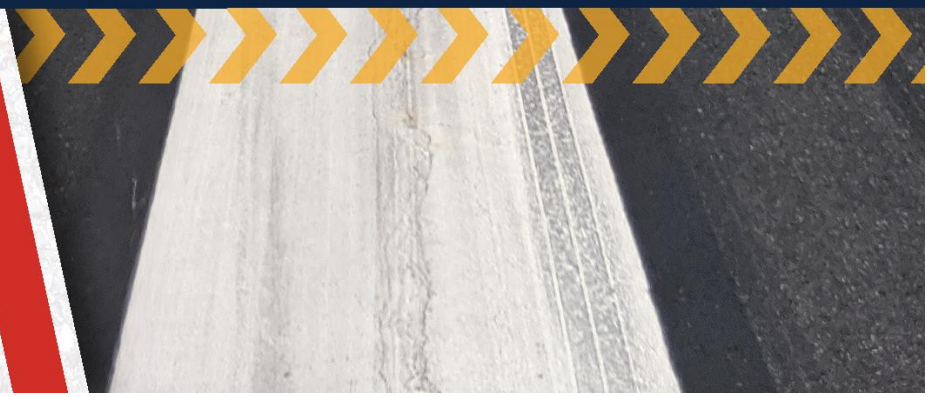
Table B.2: Section-Level 10-Year Major Rehabilitation Needs

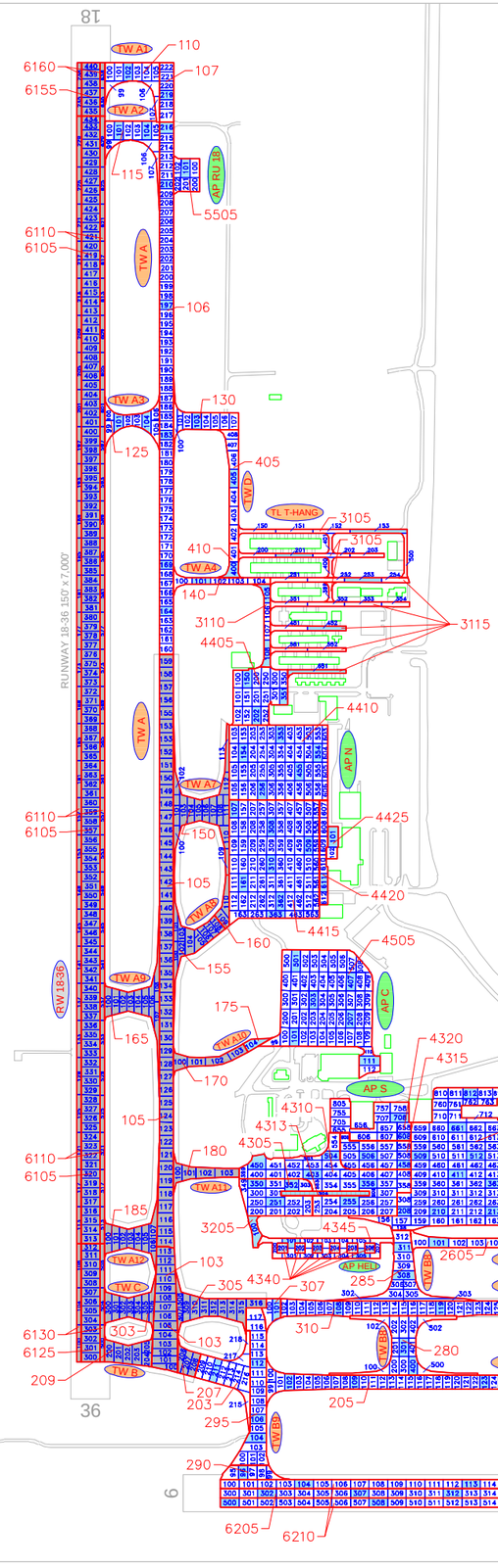
Program Year	Network ID	Branch ID	Section ID	Surface	Area (SF)	PCI Before	Rehabilitation Type	Planning Cost Estimate
2023	TLH	TL AP S	3205	AAC	6,963	63	AC Rehabilitation	$ 98,000
2023	TLH	TL T-HANG	3105	AC	46,227	61	AC Rehabilitation	$ 648,000
2023	TLH	TL T-HANG	3110	AC	16,646	50	AC Reconstruction	$ 508,000
2023	TLH	TL T-HANG	3115	AC	63,002	44	AC Reconstruction	$ 1,922,000
2023	TLH	TW A	106	AC	215,250	60	AC Rehabilitation	$ 3,014,000
2023	TLH	TW A	107	AC	23,925	65	AC Rehabilitation	$ 335,000
2023	TLH	TW A1	110	AC	40,291	63	AC Rehabilitation	$ 565,000
2023	TLH	TW A10	175	AC	4,954	68	AC Rehabilitation	$ 70,000
2023	TLH	TW A2	115	AC	42,179	68	AC Rehabilitation	$ 591,000
2023	TLH	TW A3	125	AC	32,329	60	AC Rehabilitation	$ 453,000
2023	TLH	TW A3	130	AC	34,919	65	AC Rehabilitation	$ 489,000
2023	TLH	TW A4	140	AC	19,805	53	AC Reconstruction	$ 605,000
2023	TLH	TW B	205	AC	581,353	48	AC Reconstruction	$ 17,732,000
2023	TLH	TW B1	210	AC	46,292	53	AC Reconstruction	$ 1,412,000
2023	TLH	TW B5	250	AC	24,545	41	AC Reconstruction	$ 749,000
2023	TLH	TW B6	265	AC	17,002	58	AC Rehabilitation	$ 239,000
2023	TLH	TW B6	267	AC	24,158	50	AC Reconstruction	$ 737,000
2023	TLH	TW B7	273	AC	38,359	61	AC Rehabilitation	$ 538,000
2023	TLH	TW B7	275	AAC	9,455	52	AC Reconstruction	$ 289,000
2023	TLH	TW B7	277	AAC	8,669	66	AC Rehabilitation	$ 122,000
2023	TLH	TW B8	280	AC	66,948	65	AC Rehabilitation	$ 938,000
2023	TLH	TW B9	295	AC	84,260	54	AC Reconstruction	$ 2,570,000
2023	TLH	TW C	307	AAC	10,756	63	AC Rehabilitation	$ 151,000
2023	TLH	TW C	310	AAC	160,476	50	AC Reconstruction	$ 4,895,000
2023	TLH	TW C	315	AAC	55,835	66	AC Rehabilitation	$ 782,000
2023	TLH	TW D	405	AC	33,610	67	AC Rehabilitation	$ 471,000
2023	TLH	TW D	410	AC	10,157	65	AC Rehabilitation	$ 143,000
2023	TLH	TW Z	2610	AC	2,379	40	AC Reconstruction	$ 73,000
2023	TLH	TW Z	2615	AC	2,615	68	AC Rehabilitation	$ 37,000
2023	TLH	AP N	4410	AAC	215,063	68	AC Rehabilitation	$ 3,011,000

Program Year	Network ID	Branch ID	Section ID	Surface	Area (SF)	PCI Before	Rehabilitation Type	Planning Cost Estimate
2023	**TLH**	AP N	4415	APC	310,550	69	AC Rehabilitation	$ 4,348,000
2023	**TLH**	AP RU 18	5505	AC	25,207	61	AC Rehabilitation	$ 353,000
2023	**TLH**	AP TERM	4110	APC	13,317	47	AC Reconstruction	$ 407,000
2024	**TLH**	AP C	4505	AC	265,932	70	AC Rehabilitation	$ 3,910,000
2024	**TLH**	AP CARGO	4210	AC	400,242	70	AC Rehabilitation	$ 5,884,000
2025	**TLH**	TW B	203	AC	50,342	70	AC Rehabilitation	$ 778,000
2025	**TLH**	TW Z	2605	AC	62,575	69	AC Rehabilitation	$ 966,000
2025	**TLH**	AP N	4425	AC	9,973	69	AC Rehabilitation	$ 154,000
2026	**TLH**	AP N	4420	APC	24,514	70	AC Rehabilitation	$ 398,000
2027	**TLH**	TW B3	235	AC	83,567	69	AC Rehabilitation	$ 1,423,000
2027	**TLH**	TW B4	240	AC	48,156	69	AC Rehabilitation	$ 820,000
2027	**TLH**	AP N	4405	AAC	77,291	69	AC Rehabilitation	$ 1,316,000
2028	**TLH**	TW B8	285	AC	58,220	70	AC Rehabilitation	$ 1,041,000
2030	**TLH**	AP CARGO	4205	AC	65,663	70	AC Rehabilitation	$ 1,294,000
2031	**TLH**	AP S	4305	AAC	70,348	69	AC Rehabilitation	$ 1,456,000
2032	**TLH**	TW B6	260	AC	38,862	70	AC Rehabilitation	$ 845,000
2032	**TLH**	TW B7	271	AC	23,946	69	AC Rehabilitation	$ 521,000
2032	**TLH**	TW B9	290	AC	20,199	69	AC Rehabilitation	$ 439,000
2032	**TLH**	AP S	4310	AAC	179,279	70	AC Rehabilitation	$ 3,894,000

**All planning cost values have been rounded up to the nearest thousand dollars.*

Appendix C: Technical Exhibits





AAC TW A 103 0 | 14	AAC TW A 105 0 | 46	AC TW A 106 6 | 57	AC TW A 107 1 | 6	AAC TW A1 110 1 | 9	AC TW A2 115 2 | 9	AC TW A3 125 2 | 8	AC TW A3 130 1 | 8	AC TW A4 140 1 | 5	AAC TW A7 150 0 | 14	AAC TW A8 155 0 | 9	AAC TW A8 160 0 | 2	AC TW A9 165 0 | 9	AC TW A10 170 0 | 4
AC TW A10 175 1 | 1	AAC TW A11 180 0 | 4	AAC TW A12 185 0 | 8	AC TW B 203 1 | 10	AC TW B 205 13 | 156	AAC TW B 207 0 | 3	AAC TW B 209 0 | 6	AC TW B1 210 2 | 10	AC TW B1 215 1 | 1	AC TW B2 220 2 | 11	AC TW B3 230 3 | 12	AC TW B3 235 3 | 14	AC TW B4 240 2 | 7	AC TW B5 250 2 | 5
AC TW B6 260 1 | 8	AC TW B6 265 1 | 3	AC TW B6 267 2 | 5	AC TW B7 270 2 | 8	AC TW B7 271 1 | 4	AC TW B7 273 3 | 8	AAC TW B7 275 1 | 3	AAC TW B7 277 1 | 2	AC TW B8 280 2 | 13	AC TW B8 285 2 | 11	AC TW B9 290 1 | 5	AC TW B9 295 3 | 18	AAC TW C 303 0 | 7	AAC TW C 305 0 | 10
AAC TW C 307 1 | 2	AAC TW C 310 4 | 40	AAC TW C 315 2 | 13	AC TW D 405 1 | 7	AC TW D 410 1 | 2	AC TW Z 2605 3 | 12	AC TW Z 2610 1 | 1	AC TW Z 2615 1 | 1	AC TL T-HANG 3105 2 | 12	AC TL T-HANG 3110 2 | 4	AC TL T-HANG 3115 3 | 13	AAC TL AP S 3205 1 | 1	PCC AP TERM 4105 16 | 217	APC AP TERM 4110 1 | 4
AC AP CARGO 4205 2 | 12	AC AP CARGO 4210 9 | 84	PCC AP CARGO 4215 1 | 2	AAC AP S 4305 3 | 14	AAC AP S 4310 5 | 35	PCC AP S 4313 1 | 1	AAC AP S 4315 2 | 13	AAC AP S 4320 2 | 14	PCC AP S 4325 1 | 2	AC AP S 4332 10 | 83	PCC AP HELI 4340 1 | 6	AC AP HELI 4345 2 | 12	AAC AP N 4405 3 | 16	AAC AP N 4410 5 | 44
APC AP N 4415 7 | 65	APC AP N 4420 1 | 6	AC AP N 4425 1 | 2	AC AP C 4505 6 | 53	AAC AP RU 18 5505 1 | 6	AC RW 18-36 6105 0 | 122	AC RW 18-36 6110 0 | 60	AAC RW 18-36 6125 0 | 13	AAC RW 18-36 6130 0 | 6	AAC RW 18-36 6155 0 | 6	AAC RW 18-36 6160 0 | 4	AC RW 9-27 6205 16 | 80	AC RW 9-27 6210 20 | 160

NORTH

LEGEND

- RW 13-31 — TYPICAL RUNWAY BRANCH ID
- TW A — TYPICAL TAXIWAY BRANCH ID
- AP S — TYPICAL APRON BRANCH ID
- AAC / AP MAIN / 4105 / 10 | 100 — PAVEMENT SURFACE TYPE; PAVEMENT BRANCH ID; SECTION NUMBER; NUMBER OF SAMPLE UNITS IN SECTION; NUMBER OF SAMPLE UNITS TO BE INSPECTED
- AAC / AP MAIN / 4105 / 0 | 100 — SECTION NOT INSPECTED DUE TO RECENT CONSTRUCTION. SEE SYSTEM INVENTORY MAP FOR CONSTRUCTION DATES.
- 100 — INSPECTED SAMPLE UNITS.

TOTAL SAMPLES INSPECTED = 202
AC: 182 PCC: 20

RUNWAY LENGTHS DEPICTED IN THIS DRAWING ARE FOR PAVEMENT MANAGEMENT PURPOSES ONLY AND MAY NOT MATCH PUBLISHED RUNWAY LENGTHS. DRAWING NOT TO SCALE.

TLH

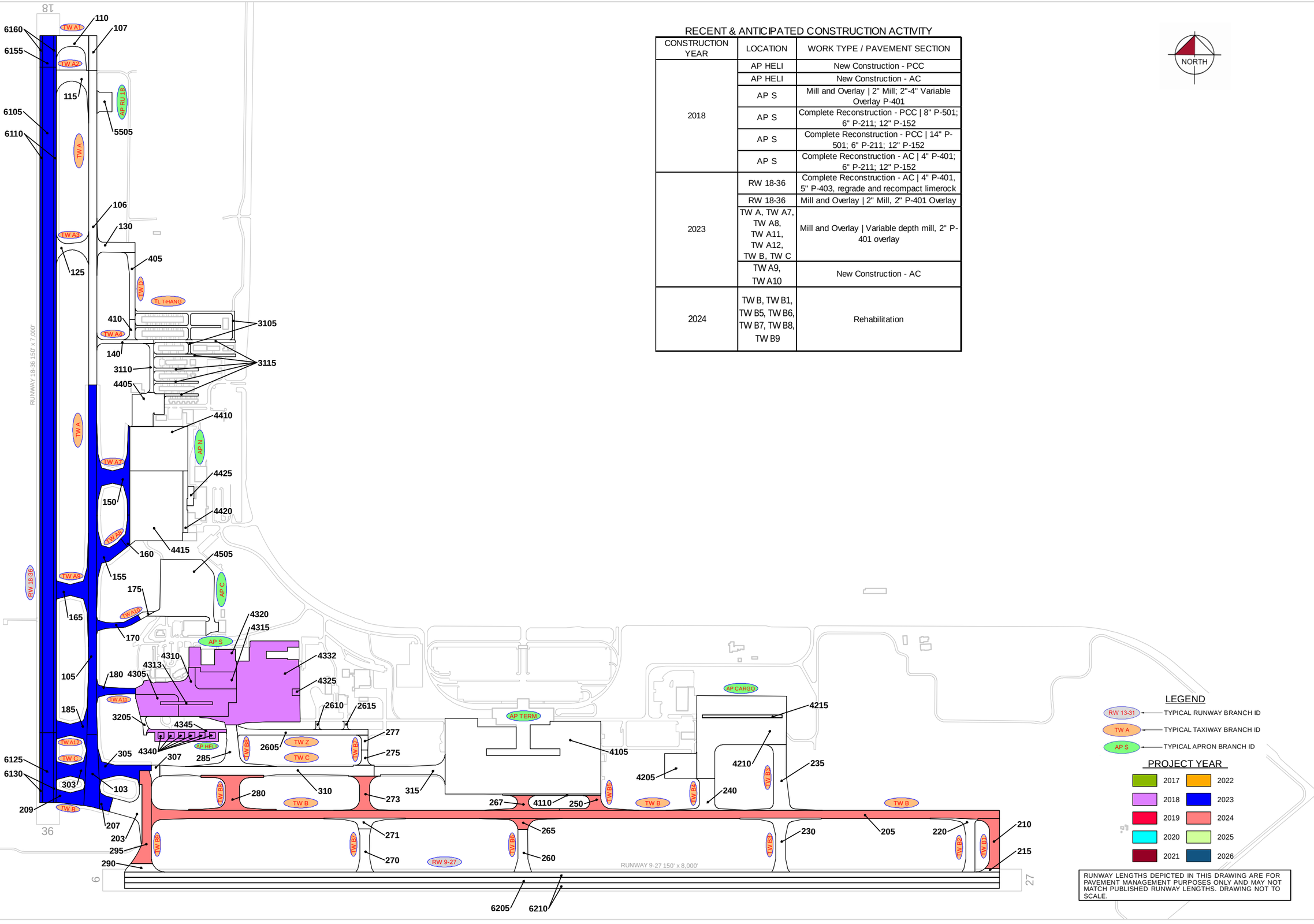
AIRFIELD PAVEMENT NETWORK DEFINITION EXHIBIT

Statewide Airfield Pavement Management Program

TALLAHASSEE INTERNATIONAL AIRPORT

FDOT

2022



RECENT & ANTICIPATED CONSTRUCTION ACTIVITY

CONSTRUCTION YEAR	LOCATION	WORK TYPE / PAVEMENT SECTION
2018	AP HELI	New Construction - PCC
	AP HELI	New Construction - AC
	AP S	Mill and Overlay | 2" Mill; 2"-4" Variable Overlay P-401
	AP S	Complete Reconstruction - PCC | 8" P-501; 6" P-211; 12" P-152
	AP S	Complete Reconstruction - PCC | 14" P-501; 6" P-211; 12" P-152
	AP S	Complete Reconstruction - AC | 4" P-401; 6" P-211; 12" P-152
2023	RW 18-36	Complete Reconstruction - AC | 4" P-401, 5" P-403, regrade and recompact limerock
	RW 18-36	Mill and Overlay | 2" Mill, 2" P-401 Overlay
	TW A, TW A7, TW A8, TW A11, TW A12, TW B, TW C	Mill and Overlay | Variable depth mill, 2" P-401 overlay
	TW A9, TW A10	New Construction - AC
2024	TW B, TW B1, TW B5, TW B6, TW B7, TW B8, TW B9	Rehabilitation

NORTH

LEGEND

- RW 13-31 — TYPICAL RUNWAY BRANCH ID
- TW A — TYPICAL TAXIWAY BRANCH ID
- AP S — TYPICAL APRON BRANCH ID

PROJECT YEAR

2017	2022
2018	2023
2019	2024
2020	2025
2021	2026

RUNWAY LENGTHS DEPICTED IN THIS DRAWING ARE FOR PAVEMENT MANAGEMENT PURPOSES ONLY AND MAY NOT MATCH PUBLISHED RUNWAY LENGTHS. DRAWING NOT TO SCALE.

TLH

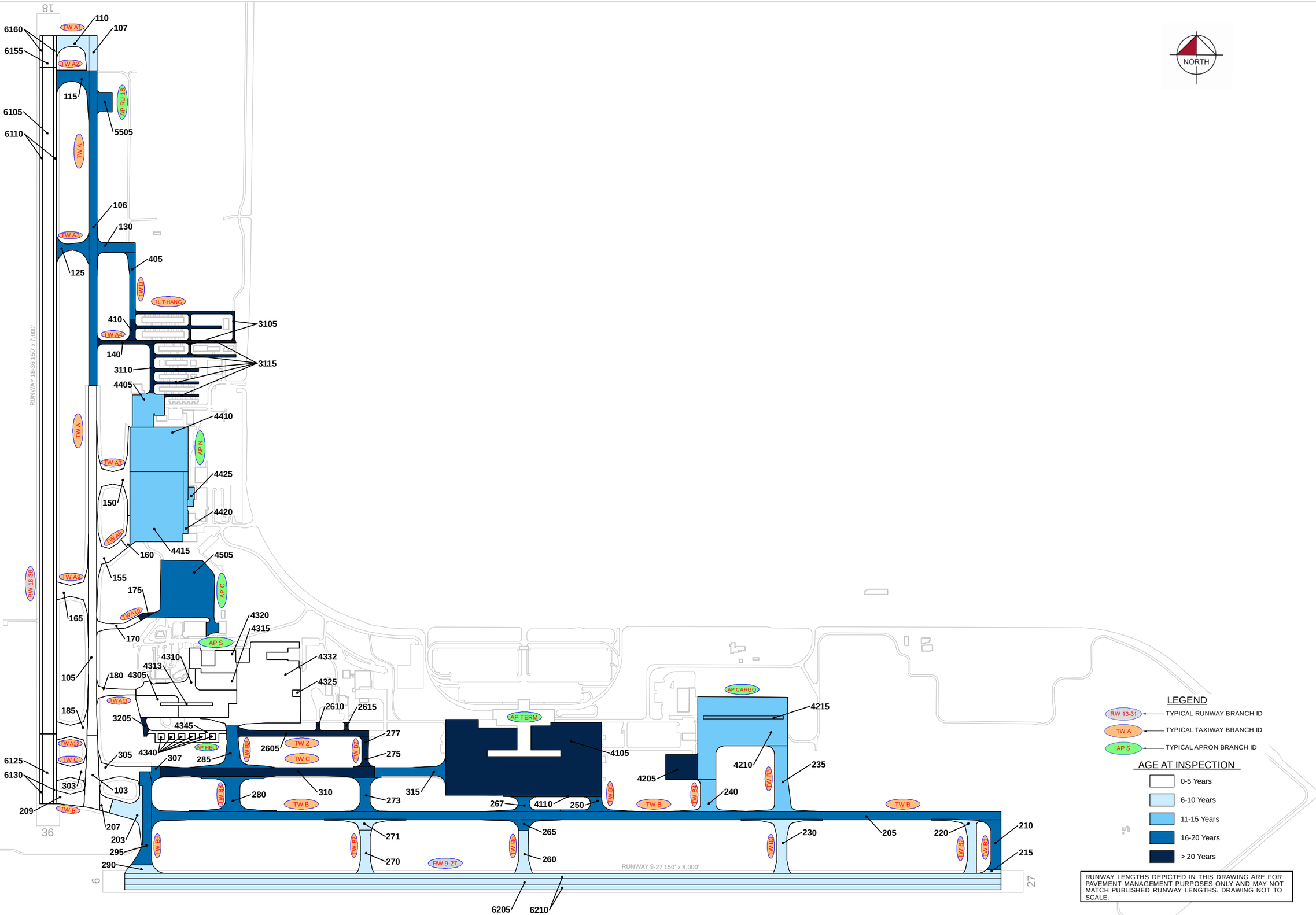
AIRFIELD PAVEMENT SYSTEM INVENTORY EXHIBIT

Statewide Airfield Pavement Management Program

TALLAHASSEE INTERNATIONAL AIRPORT

FDOT

2022



RUNWAY LENGTHS DEPICTED IN THIS DRAWING ARE FOR PAVEMENT MANAGEMENT PURPOSES ONLY AND MAY NOT MATCH PUBLISHED RUNWAY LENGTHS. DRAWING NOT TO SCALE.

TLH

AIRFIELD PAVEMENT
ESTIMATED AGE EXHIBIT

Statewide Airfield Pavement
Management Program

TALLAHASSEE INTERNATIONAL AIRPORT

FDOT

2022

TLH

AIRFIELD PAVEMENT CONDITION INDEX EXHIBIT

Statewide Airfield Pavement Management Program

TALLAHASSEE INTERNATIONAL AIRPORT

FDOT

2022

NORTH

103 PCI = 100	105 PCI = 100	115 PCI = 70	125 PCI = 61	140 PCI = 54	150 PCI = 100	160 PCI = 100	165 PCI = 100	170 PCI = 100
175 PCI = 70	180 PCI = 100	185 PCI = 100	207 PCI = 100	209 PCI = 100	240 PCI = 76	250 PCI = 43	267 PCI = 52	271 PCI = 83
273 PCI = 62	280 PCI = 66	285 PCI = 78	303 PCI = 100	305 PCI = 100	310 PCI = 51	410 PCI = 67	2605 PCI = 73	3205 PCI = 65
4110 PCI = 49	4305 PCI = 91	4313 PCI = 98	4325 PCI = 98	4340 PCI = 95	4345 PCI = 98	4410 PCI = 71	4415 PCI = 72	

RUNWAY 18-36 150' x 7,000'

RUNWAY 9-27 150' x 8,000'

RW 18-36

RW 13-31

RW 9-27

TW A

TW A1

TW A2

TW A3

TW A4

TW A5

TW A6

TW A7

TW A8

TW A9

TW A10

TW A11

TW A12

TW B

TW B1

TW B2

TW B3

TW B4

TW B5

TW B6

TW B7

TW B8

TW B9

TW C

TW D

TW Z

TL T-HANG

AP S

AP N

AP C

AP RU 18

AP HELI

AP TERM

AP CARGO

110 PCI = 64
107 PCI = 67
5505 PCI = 64
115
106 PCI = 61
130 PCI = 67
405 PCI = 69
125
3105 PCI = 62
410
3105
140
3115 PCI = 46
3110 PCI = 52
4405 PCI = 80
4410
4425 PCI = 75
150
4420 PCI = 79
105
4415
160
4505 PCI = 74
155 PCI = 100
175
165
170
4315 PCI = 96
4320 PCI = 97
4332 PCI = 96
4310 PCI = 95
4313
4305
4325
2610 PCI = 42
2615 PCI = 70
277 PCI = 69
275 PCI = 53
185
180
3205
4345
103
305
4340
285
2605
6130 PCI = 100
6125 PCI = 100
303
307 PCI = 65
280
310
273
315 PCI = 69
209
207
203 PCI = 74
295 PCI = 55
290 PCI = 83
271
270 PCI = 85
267
4110
250
265 PCI = 59
260 PCI = 84
6210 PCI = 90
6205 PCI = 88
4105 PCI = 80
4205 PCI = 84
240
4215 PCI = 79
4210 PCI = 74
235 PCI = 76
205 PCI = 50
230 PCI = 90
220 PCI = 87
210 PCI = 54
215 PCI = 87
6160 PCI = 100
6155 PCI = 100
6110 PCI = 100
6105 PCI = 100

18
36
6
27

LEGEND

RW 13-31 — TYPICAL RUNWAY BRANCH ID

TW A — TYPICAL TAXIWAY BRANCH ID

AP S — TYPICAL APRON BRANCH ID

2022 PAVEMENT CONDITION INDEX

- PCI 86-100 Good
- PCI 71-85 Satisfactory
- PCI 56-70 Fair
- PCI 41-55 Poor
- PCI 26-40 Very Poor
- PCI 11-25 Serious
- PCI 0-10 Failed

"SECTION ID"
"PCI VALUE"

RUNWAY LENGTHS DEPICTED IN THIS DRAWING ARE FOR PAVEMENT MANAGEMENT PURPOSES ONLY AND MAY NOT MATCH PUBLISHED RUNWAY LENGTHS. DRAWING NOT TO SCALE.

TLH

AIRFIELD PAVEMENT MAJOR REHABILITATION EXHIBIT

Statewide Airfield Pavement Management Program

TALLAHASSEE INTERNATIONAL AIRPORT

FDOT

2022

NORTH

TW A2:115 2023 | AC REHAB $0.59 M	TW A3:125 2023 | AC REHAB $0.45 M	TW A4:140 2023 | AC RECON $0.61 M	TW A10:175 2023 | AC REHAB $0.07 M	TW B5:250 2023 | AC RECON $0.75 M	TW B6:265 2023 | AC REHAB $0.24 M	TW B6:267 2023 | AC RECON $0.74 M	TW B7:273 2023 | AC REHAB $0.54 M	TW B7:275 2023 | AC RECON $0.29 M
TW B7:277 2023 | AC REHAB $0.12 M	TW B8:280 2023 | AC REHAB $0.94 M	TW B9:295 2023 | AC RECON $2.57 M	TW C:307 2023 | AC REHAB $0.15 M	TW C:310 2023 | AC RECON $4.90 M	TW C:315 2023 | AC REHAB $0.78 M	TW D:410 2023 | AC REHAB $0.14 M	TW Z:2610 2023 | AC RECON $0.07 M	TW Z:2615 2023 | AC REHAB $0.04 M
TL T-HANG:3105 2023 | AC REHAB $0.65 M	TL T-HANG:3110 2023 | AC RECON $0.51 M	TL AP S:3205 2023 | AC REHAB $0.10 M	AP TERM:4110 2023 | AC RECON $0.41 M	AP N:4410 2023 | AC REHAB $3.01 M	AP N:4415 2023 | AC REHAB $4.35 M	TW B:203 2025 | AC REHAB $0.78 M	TW B4:240 2027 | AC REHAB $0.82 M	TW B8:285 2028 | AC REHAB $1.04 M
AP S:4305 2031 | AC REHAB $1.46 M	TW B6:260 2032 | AC REHAB $0.85 M	TW B7:271 2032 | AC REHAB $0.52 M	AP S:4310 2032 | AC REHAB $3.89 M					

TW A1:110
2023 | AC REHAB
$0.57 M

TW A:107
2023 | AC REHAB
$0.34 M

AP RU 18:5505
2023 | AC REHAB
$0.35 M

TW A:106
2023 | AC REHAB
$3.01 M

TW A3:130
2023 | AC REHAB
$0.49 M

TW D:405
2023 | AC REHAB
$0.47 M

TL T-HANG:3115
2023 | AC RECON
$1.92 M

AP N:4405
2027 | AC REHAB
$1.32 M

AP N:4425
2025 | AC REHAB
$0.15 M

AP N:4420
2026 | AC REHAB
$0.40 M

AP C:4505
2024 | AC REHAB
$3.91 M

TW Z:2605
2025 | AC REHAB
$0.97 M

AP CARGO:4205
2030 | AC REHAB
$1.29 M

AP CARGO:4210
2024 | AC REHAB
$5.88 M

TW B3:235
2027 | AC REHAB
$1.42 M

TW B1:210
2023 | AC RECON
$1.41 M

TW B:205
2023 | AC RECON
$17.73 M

TW B9:290
2032 | AC REHAB
$0.44 M

RUNWAY 18-36 150' x 7,000'

RUNWAY 9-27 150' x 8,000'

RW 9-27

RW 18-36

TW A1 TW A2 TW A3 TW A4 TW A5 TW A6 TW A7 TW A8 TW A9 TW A10 TW A11 TW A12 TW B TW C TW D TL T-HANG TW Z TW B1 TW B2 TW B3 TW B4 TW B5 TW B6 TW B7 TW B8 TW B9

AP RU 18 AP S AP TERM AP CARGO AP HOT AP N AP C

LEGEND

RW 13-31 — TYPICAL RUNWAY BRANCH ID
TW A — TYPICAL TAXIWAY BRANCH ID
AP S — TYPICAL APRON BRANCH ID

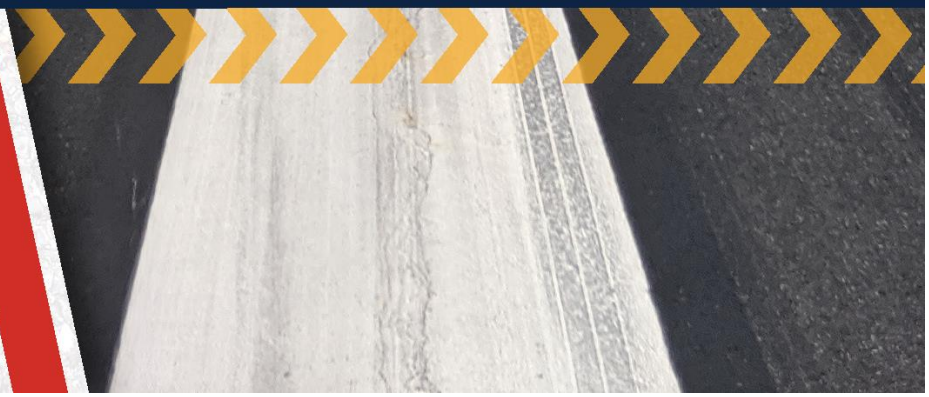
PROGRAM YEAR

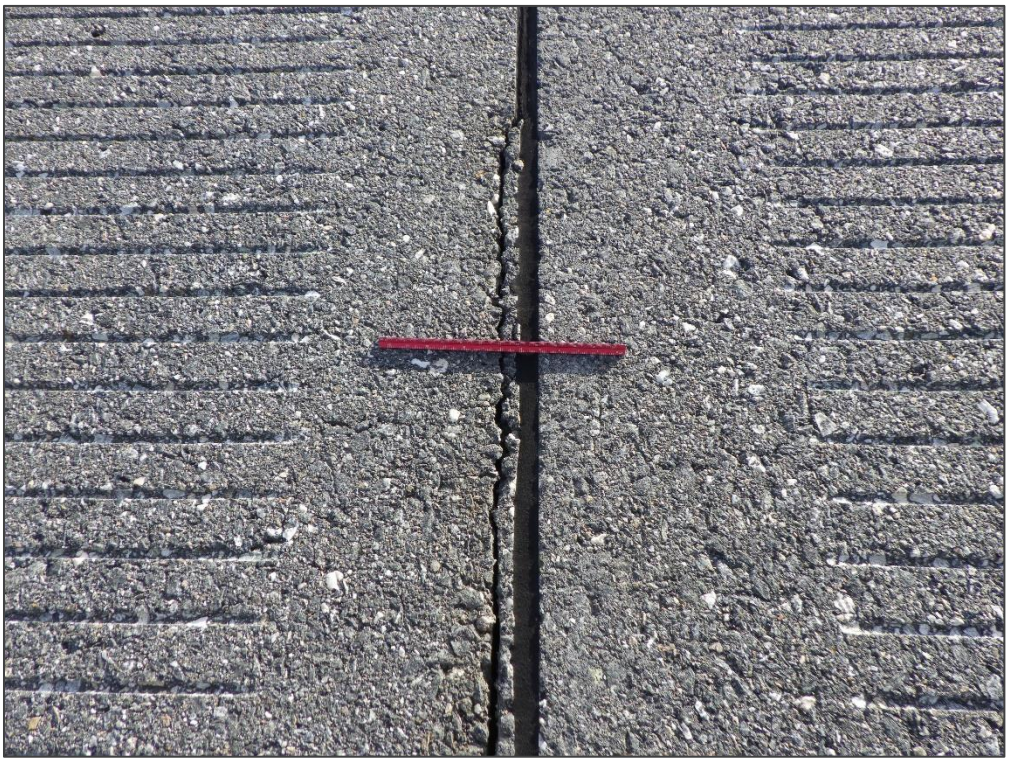
2023	2028
2024	2029
2025	2030
2026	2031
2027	2032

"BRANCH":"SECTION"
"YEAR"|"REHAB ACTIVITY"
"EST. COST"

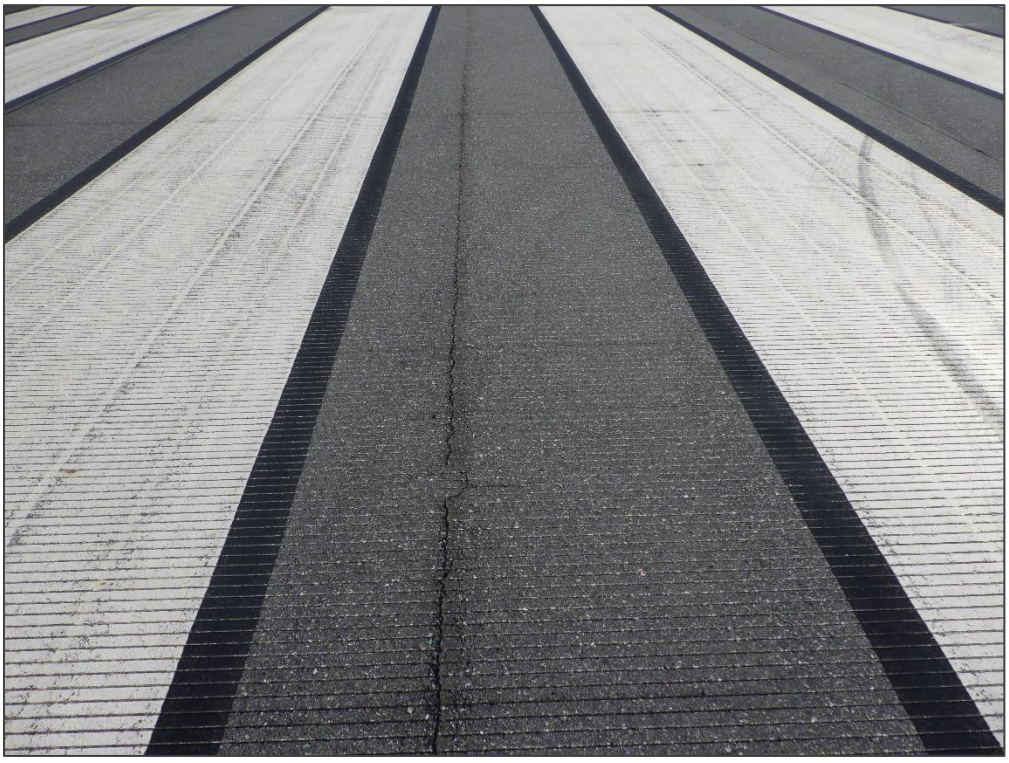
RUNWAY LENGTHS DEPICTED IN THIS DRAWING ARE FOR PAVEMENT MANAGEMENT PURPOSES ONLY AND MAY NOT MATCH PUBLISHED RUNWAY LENGTHS. DRAWING NOT TO SCALE.

Appendix D: Inspection Photograph Documentation





RW 9-27, Section 6210, Sample Unit 128 - Longitudinal & Transverse Cracking



RW 9-27, Section 6210, Sample Unit 500 - Longitudinal & Transverse Cracking

TW A, Section 106, Sample Unit 169 – Longitudinal & Transverse Cracking

TW A, Section 106, Sample Unit 216 - Raveling

TW B, Section 205, Sample Unit 228 – Longitudinal and Transverse Cracking

TW C, Section 310, Sample Unit 119 - Vicinity

TW B5, Section 250, Sample Unit 101 - Vicinity

AP C, Section 4505, Sample Unit 101 - Longitudinal and Transverse Cracking

AP CARGO, Section 4210, Sample Unit 213 - Longitudinal & Transverse Cracking

AP N, Section 4410, Sample Unit 554 - Longitudinal & Transverse Cracking and Swelling

AP N, Section 4415, Sample Unit 362 - Longitudinal & Transverse Cracking

AP RU 18, Section 5505, Sample Unit 101 - Longitudinal & Transverse Cracking

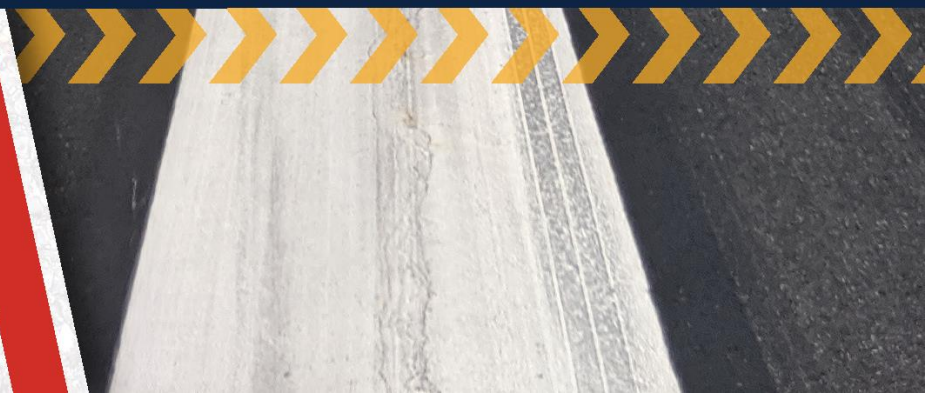
AP S, Section 4310, Sample Unit 450 - Longitudinal & Transverse Cracking

AP TERM, Section 4105, Sample Unit 110 - Corner Break

AP TERM, Section 4105, Sample Unit 456 - Joint Spall

AP TERM, Section 4110, Sample Unit 97 - Joint Reflection Cracking

Appendix E: Inspection Distress Details



Re-Inspection Report

FDOT

Generated Date 11/18/2022

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: AP C **Name:** CENTRAL RAMP **Use:** APRON **Area:** 265,932 SqFt

Section: 4505 of 1 **From:** - **To:** - **Last Const.:** 1/1/2005

Surface: AC **Family:** CA653-PR-AP-AC **Zone:** **Category:** **Rank:** P

Area: 265,932 SqFt **Length:** 500 Ft **Width:** 500 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 12/25/1999 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Work Date: 1/1/2005 **Work Type:** Surface Reconstruction - AC **Code:** SR-AC **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 53 **Surveyed:** 6

Conditions: **PCI:** 74

Inspection Comments:

Sample Number: 101 **Type:** R **Area:** 5001.00 SqFt **PCI:** 75

Sample Comments:

48	L & T CR	L	151.00	Ft
48	L & T CR	M	25.00	Ft
52	RAVELING	L	500.00	SqFt
57	WEATHERING	L	4501.00	SqFt

Sample Number: 111 **Type:** R **Area:** 5751.00 SqFt **PCI:** 74

Sample Comments:

45	DEPRESSION	L	20.00	SqFt
48	L & T CR	L	117.00	Ft
48	L & T CR	M	15.00	Ft
57	WEATHERING	L	4601.00	SqFt
57	WEATHERING	M	1150.00	SqFt

Sample Number: 207 **Type:** R **Area:** 5001.00 SqFt **PCI:** 65

Sample Comments:

42	BLEEDING	N	9.00	SqFt
48	L & T CR	L	105.00	Ft
48	L & T CR	M	55.00	Ft
50	PATCHING	L	13.00	SqFt
52	RAVELING	L	249.00	SqFt
57	WEATHERING	L	3741.00	SqFt
57	WEATHERING	M	998.00	SqFt

Sample Number: 303 **Type:** R **Area:** 5001.00 SqFt **PCI:** 70

Sample Comments:

42	BLEEDING	N	6.00	SqFt
45	DEPRESSION	L	70.00	SqFt
48	L & T CR	L	37.00	Ft
48	L & T CR	M	29.00	Ft
57	WEATHERING	L	2501.00	SqFt
57	WEATHERING	M	2500.00	SqFt

Sample Number: 407 **Type:** R **Area:** 5001.00 SqFt **PCI:** 76

Sample Comments:

48	L & T CR	L	124.00	Ft
48	L & T CR	M	24.00	Ft
57	WEATHERING	L	4251.00	SqFt
57	WEATHERING	M	750.00	SqFt

Sample Number: 501 **Type:** R **Area:** 5978.00 SqFt **PCI:** 80

Sample Comments:

48	L & T CR	L	40.00	Ft

Branch:	AP C	**Name:**	CENTRAL RAMP	**Use:**	APRON	**Area:**	265,932 SqFt

48	L & T CR	M	15.00	Ft
57	WEATHERING	L	5380.00	SqFt
57	WEATHERING	M	598.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: AP CARGO **Name:** CARGO APRON **Use:** APRON **Area:** 484,155 SqFt

Section: 4205 of 3 **From:** - **To:** - **Last Const.:** 1/1/1990

Surface: AC **Family:** CA653-PR-AP-AC **Zone:** **Category:** **Rank:** P

Area: 65,663 SqFt **Length:** 280 Ft **Width:** 220 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1990 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Work Date: 1/2/1990 **Work Type:** Surface Treatment - Seal Coat **Code:** ST-SC **Is Major M&R:** False

Last Insp. Date: 11/30/2021 **TotalSamples:** 12 **Surveyed:** 2

Conditions: **PCI:** 84

Inspection Comments:

Sample Number: 201 **Type:** R **Area:** 5000.00 SqFt **PCI:** 85

Sample Comments:

48	L & T CR	L	22.00	Ft
52	RAVELING	L	100.00	SqFt
56	SWELLING	L	4.00	SqFt
57	WEATHERING	L	4900.00	SqFt

Sample Number: 402 **Type:** R **Area:** 6336.00 SqFt **PCI:** 83

Sample Comments:

48	L & T CR	L	99.00	Ft
48	L & T CR	M	11.00	Ft
52	RAVELING	L	10.00	SqFt
57	WEATHERING	L	6326.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: AP CARGO **Name:** CARGO APRON **Use:** APRON **Area:** 484,155 SqFt

Section: 4210 of 3 **From:** - **To:** - **Last Const.:** 1/1/2007

Surface: AC **Family:** CA653-PR-AP-AC **Zone:** **Category:** **Rank:** P

Area: 400,242 SqFt **Length:** 1,042 Ft **Width:** 820 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/2007 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 84 **Surveyed:** 9

Conditions: **PCI:** 74

Inspection Comments:

Sample Number: 108 **Type:** R **Area:** 5000.00 SqFt **PCI:** 73

Sample Comments:

48	L & T CR	L	86.00	Ft
48	L & T CR	M	50.00	Ft
52	RAVELING	L	750.00	SqFt
57	WEATHERING	L	4250.00	SqFt

Sample Number: 213 **Type:** R **Area:** 5000.00 SqFt **PCI:** 65

Sample Comments:

48	L & T CR	L	311.00	Ft
48	L & T CR	M	100.00	Ft
52	RAVELING	L	750.00	SqFt
56	SWELLING	L	35.00	SqFt
57	WEATHERING	L	4250.00	SqFt

Sample Number: 319 **Type:** R **Area:** 5000.00 SqFt **PCI:** 72

Sample Comments:

45	DEPRESSION	L	75.00	SqFt
48	L & T CR	L	174.00	Ft
49	OIL SPILLAGE	N	2.00	SqFt
52	RAVELING	L	500.00	SqFt
57	WEATHERING	L	4500.00	SqFt

Sample Number: 416 **Type:** R **Area:** 4500.00 SqFt **PCI:** 80

Sample Comments:

48	L & T CR	L	92.00	Ft
52	RAVELING	L	225.00	SqFt
56	SWELLING	L	30.00	SqFt
57	WEATHERING	L	4275.00	SqFt

Sample Number: 517 **Type:** R **Area:** 3000.00 SqFt **PCI:** 77

Sample Comments:

48	L & T CR	L	93.00	Ft
52	RAVELING	L	300.00	SqFt
56	SWELLING	L	20.00	SqFt
57	WEATHERING	L	2700.00	SqFt

Sample Number: 612 **Type:** R **Area:** 5200.00 SqFt **PCI:** 77

Sample Comments:

45	DEPRESSION	L	12.00	SqFt
48	L & T CR	L	104.00	Ft
48	L & T CR	M	16.00	Ft
52	RAVELING	L	260.00	SqFt
57	WEATHERING	L	4940.00	SqFt

Sample Number: 719 **Type:** R **Area:** 5000.00 SqFt **PCI:** 83

Sample Comments:

45	DEPRESSION	L	25.00	SqFt
48	L & T CR	L	119.00	Ft

57	WEATHERING	L	5000.00	SqFt

Sample Number: 812 **Type:** R **Area:** 6263.00 SqFt **PCI:** 71

Sample Comments:

48	L & T CR	L	92.00	Ft
48	L & T CR	M	129.00	Ft
57	WEATHERING	L	5950.00	SqFt
57	WEATHERING	M	313.00	SqFt

Sample Number: 820 **Type:** R **Area:** 5230.00 SqFt **PCI:** 74

Sample Comments:

48	L & T CR	L	38.00	Ft
48	L & T CR	M	20.00	Ft
52	RAVELING	L	523.00	SqFt
56	SWELLING	L	10.00	SqFt
57	WEATHERING	L	4707.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: AP CARGO **Name:** CARGO APRON **Use:** APRON **Area:** 484,155 SqFt

Section: 4215 of 3 **From:** - **To:** - **Last Const.:** 1/1/2007

Surface: PCC **Family:** CA653-PR-AP-PCC **Zone:** **Category:** **Rank:** P

Area: 18,250 SqFt **Length:** 738 Ft **Width:** 26 Ft

Slabs: 29 **Slab Length:** 25 Ft **Slab Width:** 25 Ft **Joint Length:** 771 Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/2007 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 2 **Surveyed:** 1

Conditions: **PCI:** 79

Inspection Comments:

Sample Number: 151 **Type:** R **Area:** 15.00 Slabs **PCI:** 79

Sample Comments:

65	JT SEAL DMG	L	15.00	Slabs
73	SHRINKAGE CR	N	15.00	Slabs
74	JOINT SPALL	L	4.00	Slabs

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: AP HELI **Name:** HELICOPTER PARKING APRON **Use:** APRON **Area:** 67,720 SqFt

Section: 4340 of 2 **From:** - **To:** - **Last Const.:** 1/5/2018

Surface: PCC **Family:** CA653-PR-AP-PCC **Zone:** **Category:** **Rank:** P

Area: 17,496 SqFt **Length:** 54 Ft **Width:** 324 Ft

Slabs: 96 **Slab Length:** 13 Ft **Slab Width:** 13 Ft **Joint Length:** 2,214 Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/5/2018 **Work Type:** New Construction - PCC **Code:** NC-PC **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 6 **Surveyed:** 1

Conditions: **PCI:** 95

Inspection Comments:

Sample Number: 203 **Type:** R **Area:** 16.00 Slabs **PCI:** 95

Sample Comments:

62	CORNER BREAK	L	1.00	Slabs

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: AP HELI **Name:** HELICOPTER PARKING APRON **Use:** APRON **Area:** 67,720 SqFt

Section: 4345 of 2 **From:** - **To:** - **Last Const.:** 1/5/2018

Surface: AC **Family:** CA653-PR-AP-AC **Zone:** **Category:** **Rank:** P

Area: 50,224 SqFt **Length:** 110 Ft **Width:** 580 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/5/2018 **Work Type:** New Construction - AC **Code:** NC-AC **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 12 **Surveyed:** 2

Conditions: **PCI:** 98

Inspection Comments:

Sample Number: 101 **Type:** R **Area:** 3524.00 SqFt **PCI:** 97

Sample Comments:

57	WEATHERING	L	881.00	SqFt

Sample Number: 305 **Type:** R **Area:** 3900.00 SqFt **PCI:** 100

Sample Comments:

57	WEATHERING	L	25.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: AP N **Name:** NORTH RAMP **Use:** APRON **Area:** 637,391 SqFt

Section: 4405 of 5 **From:** - **To:** - **Last Const.:** 1/1/2010

Surface: AAC **Family:** CA653-PR-AP-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 77,291 SqFt **Length:** 300 Ft **Width:** 200 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1985 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Work Date: 1/2/1985 **Work Type:** Surface Treatment - Seal Coat **Code:** ST-SC **Is Major M&R:** False

Work Date: 1/1/2010 **Work Type:** Overlay - AC Structural **Code:** OL-AS **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 16 **Surveyed:** 3

Conditions: **PCI:** 80

Inspection Comments:

Sample Number: 150 **Type:** R **Area:** 4837.00 SqFt **PCI:** 78

Sample Comments:

48	L & T CR	L	160.00	Ft
56	SWELLING	L	36.00	SqFt
57	WEATHERING	L	4595.00	SqFt
57	WEATHERING	M	242.00	SqFt

Sample Number: 202 **Type:** R **Area:** 5000.00 SqFt **PCI:** 78

Sample Comments:

48	L & T CR	L	100.00	Ft
48	L & T CR	M	26.00	Ft
57	WEATHERING	L	4750.00	SqFt
57	WEATHERING	M	250.00	SqFt

Sample Number: 351 **Type:** R **Area:** 3991.00 SqFt **PCI:** 84

Sample Comments:

48	L & T CR	L	82.00	Ft
57	WEATHERING	L	3791.00	SqFt
57	WEATHERING	M	200.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: AP N **Name:** NORTH RAMP **Use:** APRON **Area:** 637,391 SqFt

Section: 4410 of 5 **From:** - **To:** - **Last Const.:** 1/1/2010

Surface: AAC **Family:** CA653-PR-AP-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 215,063 SqFt **Length:** 405 Ft **Width:** 530 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1971	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1985	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/2/1985	**Work Type:** Surface Treatment - Seal Coat	**Code:** ST-SC	**Is Major M&R:** False
Work Date: 1/1/2010	**Work Type:** Overlay - AC Structural	**Code:** OL-AS	**Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 44 **Surveyed:** 5

Conditions: **PCI:** 71

Inspection Comments:

Sample Number: 154 **Type:** R **Area:** 5000.00 SqFt **PCI:** 72

Sample Comments:

48	L & T CR	L	294.00	Ft
48	L & T CR	M	50.00	Ft
56	SWELLING	L	24.00	SqFt
57	WEATHERING	L	5000.00	SqFt

Sample Number: 256 **Type:** R **Area:** 5250.00 SqFt **PCI:** 66

Sample Comments:

48	L & T CR	L	546.00	Ft
48	L & T CR	M	100.00	Ft
57	WEATHERING	L	5250.00	SqFt

Sample Number: 353 **Type:** R **Area:** 5000.00 SqFt **PCI:** 79

Sample Comments:

48	L & T CR	L	147.00	Ft
48	L & T CR	M	50.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 455 **Type:** R **Area:** 5000.00 SqFt **PCI:** 70

Sample Comments:

48	L & T CR	L	319.00	Ft
48	L & T CR	M	50.00	Ft
56	SWELLING	L	35.00	SqFt
57	WEATHERING	L	5000.00	SqFt

Sample Number: 554 **Type:** R **Area:** 5000.00 SqFt **PCI:** 70

Sample Comments:

48	L & T CR	L	325.00	Ft
48	L & T CR	M	50.00	Ft
56	SWELLING	L	32.00	SqFt
57	WEATHERING	L	5000.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: AP N **Name:** NORTH RAMP **Use:** APRON **Area:** 637,391 SqFt

Section: 4415 of 5 **From:** - **To:** - **Last Const.:** 1/1/2010

Surface: APC **Family:** CA653-PR-AP-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 310,550 SqFt **Length:** 635 Ft **Width:** 485 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1960	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1971	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/2/1971	**Work Type:** Surface Treatment - Seal Coat	**Code:** ST-SC	**Is Major M&R:** False
Work Date: 1/1/2010	**Work Type:** Overlay - AC Structural	**Code:** OL-AS	**Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 65 **Surveyed:** 7

Conditions: **PCI:** 72

Inspection Comments:

Sample Number: 107 **Type:** R **Area:** 4845.00 SqFt **PCI:** 66

Sample Comments:

48	L & T CR	L	57.00	Ft
48	L & T CR	M	183.00	Ft
57	WEATHERING	L	4603.00	SqFt
57	WEATHERING	M	242.00	SqFt

Sample Number: 161 **Type:** R **Area:** 5000.00 SqFt **PCI:** 76

Sample Comments:

48	L & T CR	L	275.00	Ft
57	WEATHERING	L	4750.00	SqFt
57	WEATHERING	M	250.00	SqFt

Sample Number: 308 **Type:** R **Area:** 5000.00 SqFt **PCI:** 77

Sample Comments:

48	L & T CR	L	248.00	Ft
57	WEATHERING	L	4750.00	SqFt
57	WEATHERING	M	250.00	SqFt

Sample Number: 310 **Type:** R **Area:** 5000.00 SqFt **PCI:** 71

Sample Comments:

48	L & T CR	L	200.00	Ft
48	L & T CR	M	100.00	Ft
57	WEATHERING	L	4750.00	SqFt
57	WEATHERING	M	250.00	SqFt

Sample Number: 362 **Type:** R **Area:** 5000.00 SqFt **PCI:** 71

Sample Comments:

48	L & T CR	L	200.00	Ft
48	L & T CR	M	100.00	Ft
57	WEATHERING	L	4750.00	SqFt
57	WEATHERING	M	250.00	SqFt

Sample Number: 363 **Type:** R **Area:** 4209.00 SqFt **PCI:** 76

Sample Comments:

48	L & T CR	L	132.00	Ft
48	L & T CR	M	25.00	Ft
57	WEATHERING	L	3999.00	SqFt
57	WEATHERING	M	210.00	SqFt

Sample Number: 509 **Type:** R **Area:** 5000.00 SqFt **PCI:** 70

Sample Comments:

48	L & T CR	L	285.00	Ft

48	L & T CR	M	50.00	Ft
57	WEATHERING	L	4750.00	SqFt
57	WEATHERING	M	250.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: AP N **Name:** NORTH RAMP **Use:** APRON **Area:** 637,391 SqFt

Section: 4420 of 5 **From:** - **To:** - **Last Const.:** 1/1/2010

Surface: APC **Family:** CA653-PR-AP-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 24,514 SqFt **Length:** 564 Ft **Width:** 45 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1960	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1971	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/2/1971	**Work Type:** Surface Treatment - Seal Coat	**Code:** ST-SC	**Is Major M&R:** False
Work Date: 1/1/2010	**Work Type:** Overlay - AC Structural	**Code:** OL-AS	**Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 6 **Surveyed:** 1

Conditions: **PCI:** 79

Inspection Comments:

Sample Number: 611 **Type:** R **Area:** 4500.00 SqFt **PCI:** 79

Sample Comments:

48	L & T CR	L	179.00	Ft
57	WEATHERING	L	4275.00	SqFt
57	WEATHERING	M	225.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: AP N **Name:** NORTH RAMP **Use:** APRON **Area:** 637,391 SqFt

Section: 4425 of 5 **From:** - **To:** - **Last Const.:** 1/1/2010

Surface: AC **Family:** CA653-PR-AP-AC **Zone:** **Category:** **Rank:** P

Area: 9,973 SqFt **Length:** 175 Ft **Width:** 45 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/2010 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 2 **Surveyed:** 1

Conditions: **PCI:** 75

Inspection Comments:

Sample Number: 101 **Type:** R **Area:** 6272.00 SqFt **PCI:** 75

Sample Comments:

48	L & T CR	L	301.00	Ft
50	PATCHING	L	26.00	SqFt
57	WEATHERING	L	5934.00	SqFt
57	WEATHERING	M	312.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: AP RU 18 **Name:** RUN-UP APRON AT RW 18 **Use:** APRON **Area:** 25,207 SqFt

Section: 5505 of 1 **From:** - **To:** - **Last Const.:** 1/1/2005

Surface: AC **Family:** CA653-PR-AP-AC **Zone:** **Category:** **Rank:** P

Area: 25,207 SqFt **Length:** 140 Ft **Width:** 180 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1993 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Work Date: 1/1/2005 **Work Type:** Surface Reconstruction - AC **Code:** SR-AC **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 6 **Surveyed:** 1

Conditions: **PCI:** 64

Inspection Comments:

Sample Number: 101 **Type:** R **Area:** 5000.00 SqFt **PCI:** 64

Sample Comments:

48	L & T CR	L	342.00	Ft
48	L & T CR	M	79.00	Ft
52	RAVELING	L	1000.00	SqFt
56	SWELLING	L	35.00	SqFt
57	WEATHERING	M	4000.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: AP S **Name:** SOUTH RAMP **Use:** APRON **Area:** 796,292 SqFt

Section: 4305 of 7 **From:** - **To:** - **Last Const.:** 1/5/2018

Surface: AAC **Family:** CA653-PR-AP-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 70,348 SqFt **Length:** 350 Ft **Width:** 200 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1993	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1993	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/5/2018	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 14 **Surveyed:** 3

Conditions: **PCI:** 91

Inspection Comments:

Sample Number: 251 **Type:** R **Area:** 5000.00 SqFt **PCI:** 92

Sample Comments:

49	OIL SPILLAGE	N	1.00	SqFt
57	WEATHERING	L	5000.00	SqFt

Sample Number: 350 **Type:** R **Area:** 5000.00 SqFt **PCI:** 90

Sample Comments:

48	L & T CR	L	22.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 352 **Type:** R **Area:** 5025.00 SqFt **PCI:** 92

Sample Comments:

48	L & T CR	L	51.00	Ft
57	WEATHERING	L	1256.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: AP S **Name:** SOUTH RAMP **Use:** APRON **Area:** 796,292 SqFt

Section: 4310 of 7 **From:** - **To:** - **Last Const.:** 1/5/2018

Surface: AAC **Family:** CA653-PR-AP-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 179,279 SqFt **Length:** 250 Ft **Width:** 680 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1960 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Work Date: 1/1/1994 **Work Type:** OVERLAY **Code:** IMPORTED **Is Major M&R:** True

Work Date: 1/5/2018 **Work Type:** Mill and Overlay **Code:** ML-OVL **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 35 **Surveyed:** 5

Conditions: **PCI:** 95

Inspection Comments:

Sample Number: 255 **Type:** R **Area:** 5800.00 SqFt **PCI:** 94

Sample Comments:

57	WEATHERING	L	5800.00	SqFt

Sample Number: 356 **Type:** R **Area:** 5000.00 SqFt **PCI:** 93

Sample Comments:

48	L & T CR	L	22.00	Ft
57	WEATHERING	L	1250.00	SqFt

Sample Number: 403 **Type:** R **Area:** 5000.00 SqFt **PCI:** 97

Sample Comments:

57	WEATHERING	L	1250.00	SqFt

Sample Number: 450 **Type:** R **Area:** 6618.00 SqFt **PCI:** 93

Sample Comments:

48	L & T CR	L	15.00	Ft
57	WEATHERING	L	1654.00	SqFt

Sample Number: 504 **Type:** R **Area:** 4754.00 SqFt **PCI:** 97

Sample Comments:

57	WEATHERING	L	1188.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: AP S **Name:** SOUTH RAMP **Use:** APRON **Area:** 796,292 SqFt

Section: 4313 of 7 **From:** - **To:** - **Last Const.:** 1/5/2018

Surface: PCC **Family:** CA653-PR-AP-PCC **Zone:** **Category:** **Rank:** P

Area: 11,875 SqFt **Length:** 25 Ft **Width:** 475 Ft

Slabs: 19 **Slab Length:** 25 Ft **Slab Width:** 25 Ft **Joint Length:** 450 Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1960	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1994	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/5/2018	**Work Type:** Complete Reconstruction - PCC	**Code:** CR-PC	**Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 1 **Surveyed:** 1

Conditions: **PCI:** 98

Inspection Comments:

Sample Number: 304 **Type:** R **Area:** 19.00 Slabs **PCI:** 98

Sample Comments:

73	SHRINKAGE CR	N	2.00	Slabs

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: AP S **Name:** SOUTH RAMP **Use:** APRON **Area:** 796,292 SqFt

Section: 4315 of 7 **From:** - **To:** - **Last Const.:** 1/5/2018

Surface: AAC **Family:** CA653-PR-AP-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 60,505 SqFt **Length:** 400 Ft **Width:** 150 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1994	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1994	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/5/2018	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 13 **Surveyed:** 2

Conditions: **PCI:** 96

Inspection Comments:

Sample Number: 458 **Type:** R **Area:** 3750.00 SqFt **PCI:** 94

Sample Comments:

48	L & T CR	L	2.00	Ft
57	WEATHERING	L	938.00	SqFt

Sample Number: 506 **Type:** R **Area:** 5000.00 SqFt **PCI:** 97

Sample Comments:

57	WEATHERING	L	1250.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: AP S **Name:** SOUTH RAMP **Use:** APRON **Area:** 796,292 SqFt

Section: 4320 of 7 **From:** - **To:** - **Last Const.:** 1/5/2018

Surface: AAC **Family:** CA653-PR-AP-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 68,878 SqFt **Length:** 350 Ft **Width:** 80 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1975	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1994	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1994	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/5/2018	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 14 **Surveyed:** 2

Conditions: **PCI:** 97

Inspection Comments:

Sample Number: 608 **Type:** R **Area:** 3375.00 SqFt **PCI:** 97

Sample Comments:

57	WEATHERING	L	844.00	SqFt

Sample Number: 708 **Type:** R **Area:** 5000.00 SqFt **PCI:** 97

Sample Comments:

57	WEATHERING	L	1250.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: AP S **Name:** SOUTH RAMP **Use:** APRON **Area:** 796,292 SqFt

Section: 4325 of 7 **From:** - **To:** - **Last Const.:** 1/5/2018

Surface: PCC **Family:** CA653-PR-AP-PCC **Zone:** **Category:** **Rank:** P

Area: 4,183 SqFt **Length:** 60 Ft **Width:** 72 Ft

Slabs: 29 **Slab Length:** 12 Ft **Slab Width:** 12 Ft **Joint Length:** 588 Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1971	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1994	**Work Type:** Surface Treatment - Seal Coat	**Code:** ST-SC	**Is Major M&R:** False
Work Date: 1/5/2018	**Work Type:** Complete Reconstruction - PCC	**Code:** CR-PC	**Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 2 **Surveyed:** 1

Conditions: **PCI:** 98

Inspection Comments:

Sample Number: 414 **Type:** R **Area:** 15.00 Slabs **PCI:** 98

Sample Comments:

74	JOINT SPALL	L	1.00	Slabs

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: AP S **Name:** SOUTH RAMP **Use:** APRON **Area:** 796,292 SqFt

Section: 4332 of 7 **From:** - **To:** - **Last Const.:** 1/5/2018

Surface: AC **Family:** CA653-PR-AP-AC **Zone:** **Category:** **Rank:** P

Area: 401,224 SqFt **Length:** 554 Ft **Width:** 580 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1975 **Work Type:** OVERLAY **Code:** IMPORTED **Is Major M&R:** True

Work Date: 1/1/1994 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Work Date: 1/5/2018 **Work Type:** Complete Reconstruction - AC **Code:** CR-AC **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 83 **Surveyed:** 10

Conditions: **PCI:** 96

Inspection Comments:

Sample Number: 208 **Type:** R **Area:** 3750.00 SqFt **PCI:** 94

Sample Comments:

57	WEATHERING	L	3750.00	SqFt

Sample Number: 210 **Type:** R **Area:** 5000.00 SqFt **PCI:** 92

Sample Comments:

48	L & T CR	L	1.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 213 **Type:** R **Area:** 5000.00 SqFt **PCI:** 94

Sample Comments:

57	WEATHERING	L	5000.00	SqFt

Sample Number: 363 **Type:** R **Area:** 5000.00 SqFt **PCI:** 97

Sample Comments:

57	WEATHERING	L	1250.00	SqFt

Sample Number: 411 **Type:** R **Area:** 5000.00 SqFt **PCI:** 97

Sample Comments:

57	WEATHERING	L	1250.00	SqFt

Sample Number: 509 **Type:** R **Area:** 5000.00 SqFt **PCI:** 97

Sample Comments:

57	WEATHERING	L	1250.00	SqFt

Sample Number: 512 **Type:** R **Area:** 5000.00 SqFt **PCI:** 97

Sample Comments:

57	WEATHERING	L	1250.00	SqFt

Sample Number: 564 **Type:** R **Area:** 3881.00 SqFt **PCI:** 97

Sample Comments:

57	WEATHERING	L	970.00	SqFt

Sample Number: 661 **Type:** R **Area:** 5400.00 SqFt **PCI:** 97

Sample Comments:

57	WEATHERING	L	1350.00	SqFt

Sample Number: 812 **Type:** R **Area:** 4560.00 SqFt **PCI:** 97

Sample Comments:

57	WEATHERING	L	1140.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: AP TERM **Name:** TERMINAL APRON **Use:** APRON **Area:** 868,701 SqFt

Section: 4105 of 2 **From:** - **To:** - **Last Const.:** 1/1/1989

Surface: PCC **Family:** CA653-PR-AP-PCC **Zone:** **Category:** **Rank:** P

Area: 855,384 SqFt **Length:** 1,480 Ft **Width:** 500 Ft

Slabs: 4,193 **Slab Length:** 12 Ft **Slab Width:** 17 Ft **Joint Length:** 103,216 Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1989 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 217 **Surveyed:** 16

Conditions: **PCI:** 80

Inspection Comments:

Sample Number: 101 **Type:** R **Area:** 20.00 Slabs **PCI:** 79

Sample Comments:

65	JT SEAL DMG	H	20.00	Slabs
73	SHRINKAGE CR	N	7.00	Slabs
74	JOINT SPALL	L	1.00	Slabs
75	CORNER SPALL	L	1.00	Slabs

Sample Number: 110 **Type:** A **Area:** 15.00 Slabs **PCI:** 64

Sample Comments:

62	CORNER BREAK	H	1.00	Slabs
65	JT SEAL DMG	H	15.00	Slabs
66	SMALL PATCH	M	1.00	Slabs
70	SCALING	M	1.00	Slabs
73	SHRINKAGE CR	N	8.00	Slabs

Sample Number: 157 **Type:** R **Area:** 20.00 Slabs **PCI:** 87

Sample Comments:

65	JT SEAL DMG	M	20.00	Slabs
73	SHRINKAGE CR	N	11.00	Slabs

Sample Number: 163 **Type:** R **Area:** 20.00 Slabs **PCI:** 88

Sample Comments:

65	JT SEAL DMG	M	20.00	Slabs
73	SHRINKAGE CR	N	10.00	Slabs

Sample Number: 167 **Type:** R **Area:** 20.00 Slabs **PCI:** 84

Sample Comments:

65	JT SEAL DMG	M	20.00	Slabs
73	SHRINKAGE CR	N	14.00	Slabs

Sample Number: 220 **Type:** R **Area:** 20.00 Slabs **PCI:** 77

Sample Comments:

65	JT SEAL DMG	M	20.00	Slabs
66	SMALL PATCH	L	4.00	Slabs
66	SMALL PATCH	M	2.00	Slabs
71	FAULTING	L	1.00	Slabs
73	SHRINKAGE CR	N	5.00	Slabs

Sample Number: 251 **Type:** R **Area:** 20.00 Slabs **PCI:** 71

Sample Comments:

65	JT SEAL DMG	M	20.00	Slabs
66	SMALL PATCH	L	1.00	Slabs
66	SMALL PATCH	M	1.00	Slabs
71	FAULTING	L	3.00	Slabs
73	SHRINKAGE CR	N	9.00	Slabs
74	JOINT SPALL	L	2.00	Slabs

Sample Number: 254 **Type:** R **Area:** 20.00 Slabs **PCI:** 87

Sample Comments:

65	JT SEAL DMG	L	20.00	Slabs
73	SHRINKAGE CR	N	15.00	Slabs

Sample Number: 256 **Type:** R **Area:** 20.00 Slabs **PCI:** 87

Sample Comments:

65	JT SEAL DMG	L	20.00	Slabs
66	SMALL PATCH	L	2.00	Slabs
73	SHRINKAGE CR	N	12.00	Slabs

Sample Number: 310 **Type:** R **Area:** 15.00 Slabs **PCI:** 88

Sample Comments:

65	JT SEAL DMG	M	15.00	Slabs
73	SHRINKAGE CR	N	6.00	Slabs

Sample Number: 371 **Type:** R **Area:** 20.00 Slabs **PCI:** 76

Sample Comments:

65	JT SEAL DMG	M	20.00	Slabs
66	SMALL PATCH	L	2.00	Slabs
66	SMALL PATCH	M	1.00	Slabs
73	SHRINKAGE CR	N	9.00	Slabs
74	JOINT SPALL	L	2.00	Slabs
75	CORNER SPALL	L	2.00	Slabs

Sample Number: 402 **Type:** R **Area:** 20.00 Slabs **PCI:** 78

Sample Comments:

65	JT SEAL DMG	M	20.00	Slabs
66	SMALL PATCH	L	4.00	Slabs
73	SHRINKAGE CR	N	20.00	Slabs

Sample Number: 456 **Type:** R **Area:** 20.00 Slabs **PCI:** 66

Sample Comments:

65	JT SEAL DMG	M	20.00	Slabs
66	SMALL PATCH	L	2.00	Slabs
73	SHRINKAGE CR	N	5.00	Slabs
74	JOINT SPALL	L	4.00	Slabs
74	JOINT SPALL	M	4.00	Slabs
75	CORNER SPALL	M	2.00	Slabs

Sample Number: 505 **Type:** R **Area:** 25.00 Slabs **PCI:** 81

Sample Comments:

65	JT SEAL DMG	M	20.00	Slabs
66	SMALL PATCH	L	2.00	Slabs
73	SHRINKAGE CR	N	3.00	Slabs
74	JOINT SPALL	L	3.00	Slabs
75	CORNER SPALL	L	1.00	Slabs
75	CORNER SPALL	M	1.00	Slabs

Sample Number: 551 **Type:** R **Area:** 20.00 Slabs **PCI:** 74

Sample Comments:

65	JT SEAL DMG	M	20.00	Slabs
66	SMALL PATCH	L	2.00	Slabs
73	SHRINKAGE CR	N	20.00	Slabs
74	JOINT SPALL	L	5.00	Slabs

Sample Number: 568 **Type:** R **Area:** 20.00 Slabs **PCI:** 80

Sample Comments:

65	JT SEAL DMG	M	20.00	Slabs
66	SMALL PATCH	L	1.00	Slabs
73	SHRINKAGE CR	N	20.00	Slabs

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: AP TERM **Name:** TERMINAL APRON **Use:** APRON **Area:** 868,701 SqFt

Section: 4110 of 2 **From:** - **To:** - **Last Const.:** 1/1/2005

Surface: APC **Family:** CA653-PR-AP-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 13,317 SqFt **Length:** 930 Ft **Width:** 15 Ft

Slabs: 59 **Slab Length:** 15 Ft **Slab Width:** 15 Ft **Joint Length:** 915 Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1989 **Work Type:** New Construction - PCC **Code:** NC-PC **Is Major M&R:** True

Work Date: 1/1/2005 **Work Type:** Overlay - AC Structural **Code:** OL-AS **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 4 **Surveyed:** 1

Conditions: **PCI:** 49

Inspection Comments:

Sample Number: 97 **Type:** R **Area:** 3001.00 SqFt **PCI:** 49

Sample Comments:

47	JT REF. CR	M	195.00	Ft
48	L & T CR	L	109.00	Ft
52	RAVELING	L	300.00	SqFt
56	SWELLING	L	55.00	SqFt
57	WEATHERING	L	2701.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: RW 18-36 **Name:** RUNWAY 18-36 **Use:** RUNWAY **Area:** 1,050,000 SqFt

Section: 6105 of 6 **From:** - **To:** - **Last Const.:** 1/1/2023

Surface: AC **Family:** CA653-PR-RW-AC **Zone:** **Category:** **Rank:** P

Area: 607,550 SqFt **Length:** 6,076 Ft **Width:** 100 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1960	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1976	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1993	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 10/1/2012	**Work Type:** Patching - AC	**Code:** PA-AC	**Is Major M&R:** False
Work Date: 1/1/2023	**Work Type:** Complete Reconstruction - AC	**Code:** CR-AC	**Is Major M&R:** True

Last Insp. Date: 1/14/2019 **TotalSamples:** 114 **Surveyed:** 30

Conditions: **PCI:** 46 **NOTE: *** Pre-Construction PCI *****

Inspection Comments:

Sample Number: 308 **Type:** R **Area:** 5000.00 SqFt **PCI:** 47

Sample Comments:

48	L & T CR	L	317.00	Ft
48	L & T CR	M	138.00	Ft
50	PATCHING	L	750.00	SqFt
52	RAVELING	L	2500.00	SqFt
53	RUTTING	L	50.00	SqFt
57	WEATHERING	M	1750.00	SqFt

Sample Number: 311 **Type:** R **Area:** 5000.00 SqFt **PCI:** 50

Sample Comments:

48	L & T CR	L	280.00	Ft
48	L & T CR	M	13.00	Ft
50	PATCHING	L	750.00	SqFt
52	RAVELING	L	3000.00	SqFt
52	RAVELING	M	900.00	SqFt
56	SWELLING	L	45.00	SqFt

Sample Number: 315 **Type:** R **Area:** 5000.00 SqFt **PCI:** 58

Sample Comments:

48	L & T CR	L	103.00	Ft
48	L & T CR	M	136.00	Ft
50	PATCHING	L	750.00	SqFt
52	RAVELING	L	3000.00	SqFt
57	WEATHERING	M	1250.00	SqFt

Sample Number: 320 **Type:** R **Area:** 5000.00 SqFt **PCI:** 37

Sample Comments:

41	ALLIGATOR CR	L	310.00	SqFt
48	L & T CR	L	205.00	Ft
48	L & T CR	M	25.00	Ft
50	PATCHING	L	750.00	SqFt
52	RAVELING	L	1500.00	SqFt
56	SWELLING	L	35.00	SqFt
57	WEATHERING	M	2750.00	SqFt

Sample Number: 324 **Type:** R **Area:** 5000.00 SqFt **PCI:** 32

Sample Comments:

41	ALLIGATOR CR	L	472.00	SqFt
48	L & T CR	L	320.00	Ft
48	L & T CR	M	35.00	Ft
50	PATCHING	L	750.00	SqFt

52	RAVELING	L	1500.00	SqFt
56	SWELLING	L	25.00	SqFt
57	WEATHERING	M	2750.00	SqFt

Sample Number: 329 **Type:** R **Area:** 5000.00 SqFt **PCI:** 47

Sample Comments:

41	ALLIGATOR CR	L	154.00	SqFt
48	L & T CR	L	133.00	Ft
48	L & T CR	M	50.00	Ft
50	PATCHING	L	750.00	SqFt
56	SWELLING	L	25.00	SqFt
57	WEATHERING	M	4250.00	SqFt

Sample Number: 333 **Type:** R **Area:** 5000.00 SqFt **PCI:** 36

Sample Comments:

41	ALLIGATOR CR	L	364.00	SqFt
48	L & T CR	L	77.00	Ft
48	L & T CR	M	122.00	Ft
50	PATCHING	L	750.00	SqFt
52	RAVELING	L	100.00	SqFt
56	SWELLING	L	35.00	SqFt
57	WEATHERING	M	4150.00	SqFt

Sample Number: 334 **Type:** R **Area:** 5000.00 SqFt **PCI:** 51

Sample Comments:

41	ALLIGATOR CR	L	50.00	SqFt
48	L & T CR	L	209.00	Ft
48	L & T CR	M	50.00	Ft
50	PATCHING	L	750.00	SqFt
52	RAVELING	L	300.00	SqFt
56	SWELLING	L	60.00	SqFt
57	WEATHERING	M	3950.00	SqFt

Sample Number: 336 **Type:** R **Area:** 5000.00 SqFt **PCI:** 46

Sample Comments:

41	ALLIGATOR CR	L	100.00	SqFt
48	L & T CR	L	246.00	Ft
48	L & T CR	M	10.00	Ft
50	PATCHING	L	750.00	SqFt
52	RAVELING	L	500.00	SqFt
56	SWELLING	L	12.00	SqFt
57	WEATHERING	M	3750.00	SqFt

Sample Number: 339 **Type:** R **Area:** 5000.00 SqFt **PCI:** 46

Sample Comments:

41	ALLIGATOR CR	L	100.00	SqFt
48	L & T CR	L	150.00	Ft
48	L & T CR	M	20.00	Ft
50	PATCHING	L	750.00	SqFt
52	RAVELING	L	500.00	SqFt
56	SWELLING	L	25.00	SqFt
57	WEATHERING	M	3750.00	SqFt

Sample Number: 340 **Type:** R **Area:** 5000.00 SqFt **PCI:** 57

Sample Comments:

48	L & T CR	L	251.00	Ft
48	L & T CR	M	100.00	Ft
50	PATCHING	L	750.00	SqFt
52	RAVELING	L	1034.00	SqFt
56	SWELLING	L	40.00	SqFt
57	WEATHERING	M	3216.00	SqFt

Sample Number: 341 **Type:** R **Area:** 5000.00 SqFt **PCI:** 52

Sample Comments:

41	ALLIGATOR CR	L	50.00	SqFt
48	L & T CR	L	169.00	Ft
48	L & T CR	M	40.00	Ft
50	PATCHING	L	750.00	SqFt

52	RAVELING	L	1100.00	SqFt
56	SWELLING	L	15.00	SqFt
57	WEATHERING	M	3150.00	SqFt

Sample Number: 342 **Type:** R **Area:** 5000.00 SqFt **PCI:** 44

Sample Comments:

41	ALLIGATOR CR	L	100.00	SqFt
48	L & T CR	L	148.00	Ft
48	L & T CR	M	100.00	Ft
50	PATCHING	L	750.00	SqFt
52	RAVELING	L	600.00	SqFt
56	SWELLING	L	61.00	SqFt
57	WEATHERING	M	3650.00	SqFt

Sample Number: 344 **Type:** R **Area:** 5000.00 SqFt **PCI:** 42

Sample Comments:

41	ALLIGATOR CR	L	110.00	SqFt
48	L & T CR	L	230.00	Ft
48	L & T CR	M	66.00	Ft
50	PATCHING	L	750.00	SqFt
52	RAVELING	L	500.00	SqFt
56	SWELLING	L	85.00	SqFt
57	WEATHERING	M	3750.00	SqFt

Sample Number: 346 **Type:** R **Area:** 5000.00 SqFt **PCI:** 49

Sample Comments:

41	ALLIGATOR CR	L	40.00	SqFt
48	L & T CR	L	292.00	Ft
48	L & T CR	M	100.00	Ft
50	PATCHING	L	750.00	SqFt
52	RAVELING	L	300.00	SqFt
56	SWELLING	L	70.00	SqFt
57	WEATHERING	M	3950.00	SqFt

Sample Number: 351 **Type:** R **Area:** 5000.00 SqFt **PCI:** 46

Sample Comments:

41	ALLIGATOR CR	L	100.00	SqFt
48	L & T CR	L	56.00	Ft
48	L & T CR	M	144.00	Ft
50	PATCHING	L	750.00	SqFt
52	RAVELING	L	1000.00	SqFt
56	SWELLING	L	120.00	SqFt
57	WEATHERING	M	3250.00	SqFt

Sample Number: 357 **Type:** R **Area:** 5000.00 SqFt **PCI:** 50

Sample Comments:

41	ALLIGATOR CR	L	98.00	SqFt
48	L & T CR	L	450.00	Ft
50	PATCHING	L	750.00	SqFt
52	RAVELING	L	500.00	SqFt
57	WEATHERING	M	3750.00	SqFt

Sample Number: 364 **Type:** R **Area:** 5000.00 SqFt **PCI:** 41

Sample Comments:

41	ALLIGATOR CR	L	184.00	SqFt
48	L & T CR	L	680.00	Ft
48	L & T CR	M	20.00	Ft
50	PATCHING	L	750.00	SqFt
52	RAVELING	L	600.00	SqFt
57	WEATHERING	M	3650.00	SqFt

Sample Number: 369 **Type:** R **Area:** 5000.00 SqFt **PCI:** 51

Sample Comments:

41	ALLIGATOR CR	L	30.00	SqFt
48	L & T CR	L	450.00	Ft
48	L & T CR	M	29.00	Ft
50	PATCHING	L	750.00	SqFt
52	RAVELING	L	490.00	SqFt

56	SWELLING	L	25.00	SqFt
57	WEATHERING	M	3760.00	SqFt

Sample Number: 373 **Type:** R **Area:** 5000.00 SqFt **PCI:** 48

Sample Comments:

41	ALLIGATOR CR	L	75.00	SqFt
48	L & T CR	L	304.00	Ft
48	L & T CR	M	50.00	Ft
50	PATCHING	L	750.00	SqFt
52	RAVELING	L	500.00	SqFt
56	SWELLING	L	17.00	SqFt
57	WEATHERING	M	3750.00	SqFt

Sample Number: 378 **Type:** R **Area:** 5000.00 SqFt **PCI:** 48

Sample Comments:

41	ALLIGATOR CR	L	65.00	SqFt
48	L & T CR	L	414.00	Ft
48	L & T CR	M	24.00	Ft
50	PATCHING	L	750.00	SqFt
52	RAVELING	L	500.00	SqFt
56	SWELLING	L	6.00	SqFt
57	WEATHERING	M	3750.00	SqFt

Sample Number: 383 **Type:** R **Area:** 5000.00 SqFt **PCI:** 37

Sample Comments:

41	ALLIGATOR CR	L	250.00	SqFt
48	L & T CR	L	200.00	Ft
48	L & T CR	M	66.00	Ft
50	PATCHING	L	750.00	SqFt
52	RAVELING	L	750.00	SqFt
56	SWELLING	L	24.00	SqFt
57	WEATHERING	M	3500.00	SqFt

Sample Number: 387 **Type:** R **Area:** 5000.00 SqFt **PCI:** 42

Sample Comments:

41	ALLIGATOR CR	L	120.00	SqFt
48	L & T CR	L	550.00	Ft
48	L & T CR	M	44.00	Ft
50	PATCHING	L	750.00	SqFt
52	RAVELING	L	600.00	SqFt
56	SWELLING	L	60.00	SqFt
57	WEATHERING	M	3650.00	SqFt

Sample Number: 392 **Type:** R **Area:** 5000.00 SqFt **PCI:** 38

Sample Comments:

41	ALLIGATOR CR	L	230.00	SqFt
48	L & T CR	L	304.00	Ft
48	L & T CR	M	100.00	Ft
50	PATCHING	L	750.00	SqFt
52	RAVELING	L	500.00	SqFt
56	SWELLING	L	10.00	SqFt
57	WEATHERING	M	3750.00	SqFt

Sample Number: 397 **Type:** R **Area:** 5000.00 SqFt **PCI:** 46

Sample Comments:

41	ALLIGATOR CR	L	65.00	SqFt
48	L & T CR	L	411.00	Ft
48	L & T CR	M	80.00	Ft
50	PATCHING	L	750.00	SqFt
52	RAVELING	L	500.00	SqFt
57	WEATHERING	M	3750.00	SqFt

Sample Number: 401 **Type:** R **Area:** 5000.00 SqFt **PCI:** 43

Sample Comments:

41	ALLIGATOR CR	L	110.00	SqFt
48	L & T CR	L	325.00	Ft
48	L & T CR	M	85.00	Ft
50	PATCHING	L	750.00	SqFt

52	RAVELING	L	800.00	SqFt
56	SWELLING	L	38.00	SqFt
57	WEATHERING	M	3450.00	SqFt

Sample Number: 406 **Type:** R **Area:** 5000.00 SqFt **PCI:** 41

Sample Comments:

41	ALLIGATOR CR	L	175.00	SqFt
48	L & T CR	L	246.00	Ft
48	L & T CR	M	120.00	Ft
50	PATCHING	L	750.00	SqFt
52	RAVELING	L	550.00	SqFt
56	SWELLING	L	18.00	SqFt
57	WEATHERING	M	3700.00	SqFt

Sample Number: 410 **Type:** R **Area:** 5000.00 SqFt **PCI:** 58

Sample Comments:

48	L & T CR	L	300.00	Ft
48	L & T CR	M	82.00	Ft
50	PATCHING	L	750.00	SqFt
52	RAVELING	L	1200.00	SqFt
57	WEATHERING	M	3050.00	SqFt

Sample Number: 411 **Type:** R **Area:** 5000.00 SqFt **PCI:** 35

Sample Comments:

41	ALLIGATOR CR	L	200.00	SqFt
48	L & T CR	L	182.00	Ft
48	L & T CR	M	35.00	Ft
48	L & T CR	H	72.00	Ft
50	PATCHING	L	750.00	SqFt
52	RAVELING	L	1000.00	SqFt
56	SWELLING	L	27.00	SqFt
57	WEATHERING	M	3250.00	SqFt

Sample Number: 413 **Type:** R **Area:** 5000.00 SqFt **PCI:** 52

Sample Comments:

41	ALLIGATOR CR	L	30.00	SqFt
48	L & T CR	L	236.00	Ft
48	L & T CR	M	78.00	Ft
50	PATCHING	L	750.00	SqFt
52	RAVELING	L	500.00	SqFt
56	SWELLING	L	25.00	SqFt
57	WEATHERING	M	3750.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: RW 18-36 **Name:** RUNWAY 18-36 **Use:** RUNWAY **Area:** 1,050,000 SqFt

Section: 6110 of 6 **From:** - **To:** - **Last Const.:** 1/1/2023

Surface: AC **Family:** CA653-PR-RW-AC **Zone:** **Category:** **Rank:** P

Area: 303,775 SqFt **Length:** 12,151 Ft **Width:** 25 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1960	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1976	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1993	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 10/1/2012	**Work Type:** Surface Treatment - Seal Coat	**Code:** ST-SC	**Is Major M&R:** False
Work Date: 1/1/2023	**Work Type:** Complete Reconstruction - AC	**Code:** CR-AC	**Is Major M&R:** True

Last Insp. Date: 1/14/2019 **TotalSamples:** 58 **Surveyed:** 14

Conditions: **PCI:** 64 **NOTE: *** Pre-Construction PCI *****

Inspection Comments:

Sample Number: 104 **Type:** R **Area:** 5000.00 SqFt **PCI:** 69

Sample Comments:

48	L & T CR	L	334.00	Ft
52	RAVELING	L	500.00	SqFt
56	SWELLING	L	20.00	SqFt
57	WEATHERING	M	4500.00	SqFt

Sample Number: 112 **Type:** R **Area:** 5000.00 SqFt **PCI:** 64

Sample Comments:

48	L & T CR	L	244.00	Ft
48	L & T CR	M	76.00	Ft
52	RAVELING	L	1250.00	SqFt
56	SWELLING	L	45.00	SqFt
57	WEATHERING	M	3750.00	SqFt

Sample Number: 144 **Type:** R **Area:** 5000.00 SqFt **PCI:** 66

Sample Comments:

48	L & T CR	L	311.00	Ft
48	L & T CR	M	60.00	Ft
56	SWELLING	L	60.00	SqFt
57	WEATHERING	M	5000.00	SqFt

Sample Number: 156 **Type:** R **Area:** 5000.00 SqFt **PCI:** 66

Sample Comments:

48	L & T CR	L	585.00	Ft
56	SWELLING	L	60.00	SqFt
57	WEATHERING	M	5000.00	SqFt

Sample Number: 168 **Type:** R **Area:** 5000.00 SqFt **PCI:** 73

Sample Comments:

48	L & T CR	L	393.00	Ft
56	SWELLING	L	10.00	SqFt
57	WEATHERING	M	5000.00	SqFt

Sample Number: 208 **Type:** R **Area:** 5000.00 SqFt **PCI:** 63

Sample Comments:

48	L & T CR	L	251.00	Ft
48	L & T CR	M	100.00	Ft
52	RAVELING	L	750.00	SqFt
56	SWELLING	L	35.00	SqFt
57	WEATHERING	M	4250.00	SqFt

Sample Number: 524 **Type:** R **Area:** 5000.00 SqFt **PCI:** 66

Sample Comments:

48	L & T CR	L	224.00	Ft
48	L & T CR	M	80.00	Ft
56	SWELLING	L	75.00	SqFt
57	WEATHERING	M	5000.00	SqFt

Sample Number: 536 **Type:** R **Area:** 5000.00 SqFt **PCI:** 55

Sample Comments:

48	L & T CR	L	96.00	Ft
48	L & T CR	M	360.00	Ft
56	SWELLING	L	190.00	SqFt
57	WEATHERING	M	5000.00	SqFt

Sample Number: 540 **Type:** R **Area:** 5000.00 SqFt **PCI:** 68

Sample Comments:

48	L & T CR	L	144.00	Ft
48	L & T CR	M	130.00	Ft
56	SWELLING	L	20.00	SqFt
57	WEATHERING	M	5000.00	SqFt

Sample Number: 544 **Type:** R **Area:** 5000.00 SqFt **PCI:** 59

Sample Comments:

48	L & T CR	L	22.00	Ft
48	L & T CR	M	210.00	Ft
48	L & T CR	H	50.00	Ft
56	SWELLING	L	50.00	SqFt
57	WEATHERING	M	5000.00	SqFt

Sample Number: 548 **Type:** R **Area:** 5000.00 SqFt **PCI:** 55

Sample Comments:

48	L & T CR	L	135.00	Ft
48	L & T CR	M	233.00	Ft
48	L & T CR	H	25.00	Ft
56	SWELLING	L	160.00	SqFt
57	WEATHERING	M	5000.00	SqFt

Sample Number: 588 **Type:** R **Area:** 5000.00 SqFt **PCI:** 60

Sample Comments:

48	L & T CR	L	302.00	Ft
48	L & T CR	M	156.00	Ft
52	RAVELING	L	328.00	SqFt
56	SWELLING	L	64.00	SqFt
57	WEATHERING	M	4672.00	SqFt

Sample Number: 600 **Type:** R **Area:** 5000.00 SqFt **PCI:** 64

Sample Comments:

48	L & T CR	L	157.00	Ft
48	L & T CR	M	105.00	Ft
52	RAVELING	L	975.00	SqFt
56	SWELLING	L	26.00	SqFt
57	WEATHERING	M	4025.00	SqFt

Sample Number: 612 **Type:** R **Area:** 5000.00 SqFt **PCI:** 63

Sample Comments:

48	L & T CR	L	278.00	Ft
48	L & T CR	M	130.00	Ft
48	L & T CR	H	30.00	Ft
56	SWELLING	L	12.00	SqFt
57	WEATHERING	M	5000.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: RW 18-36 **Name:** RUNWAY 18-36 **Use:** RUNWAY **Area:** 1,050,000 SqFt

Section: 6125 of 6 **From:** - **To:** - **Last Const.:** 1/1/2023

Surface: AAC **Family:** CA653-PR-RW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 63,750 SqFt **Length:** 638 Ft **Width:** 100 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 10/1/2012 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Work Date: 1/1/2023 **Work Type:** Mill and Overlay **Code:** ML-OVL **Is Major M&R:** True

Last Insp. Date: 1/14/2019 **TotalSamples:** 13 **Surveyed:** 3

Conditions: **PCI:** 78 **NOTE: *** Pre-Construction PCI *****

Inspection Comments:

Sample Number: 289 **Type:** R **Area:** 5000.00 SqFt **PCI:** 81

Sample Comments:

48	L & T CR	L	184.00	Ft
56	SWELLING	L	28.00	SqFt
57	WEATHERING	L	5000.00	SqFt

Sample Number: 294 **Type:** R **Area:** 5000.00 SqFt **PCI:** 76

Sample Comments:

48	L & T CR	L	129.00	Ft
48	L & T CR	M	50.00	Ft
56	SWELLING	L	35.00	SqFt
57	WEATHERING	L	5000.00	SqFt

Sample Number: 299 **Type:** R **Area:** 5000.00 SqFt **PCI:** 77

Sample Comments:

48	L & T CR	L	274.00	Ft
56	SWELLING	L	25.00	SqFt
57	WEATHERING	L	5000.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: RW 18-36 **Name:** RUNWAY 18-36 **Use:** RUNWAY **Area:** 1,050,000 SqFt

Section: 6130 of 6 **From:** - **To:** - **Last Const.:** 1/1/2023

Surface: AAC **Family:** CA653-PR-RW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 31,875 SqFt **Length:** 1,275 Ft **Width:** 25 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 10/1/2012 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Work Date: 1/1/2023 **Work Type:** Mill and Overlay **Code:** ML-OVL **Is Major M&R:** True

Last Insp. Date: 1/14/2019 **TotalSamples:** 6 **Surveyed:** 2

Conditions: **PCI:** 88 **NOTE: *** Pre-Construction PCI *****

Inspection Comments:

Sample Number: 088 **Type:** R **Area:** 5575.00 SqFt **PCI:** 89

Sample Comments:

48	L & T CR	L	73.00	Ft
57	WEATHERING	L	5575.00	SqFt

Sample Number: 496 **Type:** R **Area:** 5000.00 SqFt **PCI:** 86

Sample Comments:

48	L & T CR	L	85.00	Ft
56	SWELLING	L	35.00	SqFt
57	WEATHERING	L	5000.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: RW 18-36 **Name:** RUNWAY 18-36 **Use:** RUNWAY **Area:** 1,050,000 SqFt

Section: 6155 of 6 **From:** - **To:** - **Last Const.:** 1/1/2023

Surface: AAC **Family:** CA653-PR-RW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 28,700 SqFt **Length:** 287 Ft **Width:** 100 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 10/1/2012 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Work Date: 1/1/2023 **Work Type:** Mill and Overlay **Code:** ML-OVL **Is Major M&R:** True

Last Insp. Date: 1/14/2019 **TotalSamples:** 7 **Surveyed:** 2

Conditions: **PCI:** 90 **NOTE: *** Pre-Construction PCI *****

Inspection Comments:

Sample Number: 423 **Type:** R **Area:** 5000.00 SqFt **PCI:** 91

Sample Comments:

48	L & T CR	L	9.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 426 **Type:** R **Area:** 5000.00 SqFt **PCI:** 89

Sample Comments:

48	L & T CR	L	55.00	Ft
57	WEATHERING	L	5000.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: RW 18-36 **Name:** RUNWAY 18-36 **Use:** RUNWAY **Area:** 1,050,000 SqFt

Section: 6160 of 6 **From:** - **To:** - **Last Const.:** 1/1/2023

Surface: AAC **Family:** CA653-PR-RW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 14,350 SqFt **Length:** 574 Ft **Width:** 25 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 10/1/2012 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Work Date: 1/1/2023 **Work Type:** Mill and Overlay **Code:** ML-OVL **Is Major M&R:** True

Last Insp. Date: 1/14/2019 **TotalSamples:** 4 **Surveyed:** 1

Conditions: **PCI:** 90 **NOTE: *** Pre-Construction PCI *****

Inspection Comments:

Sample Number: 624 **Type:** R **Area:** 4600.00 SqFt **PCI:** 90

Sample Comments:

48	L & T CR	L	24.00	Ft
57	WEATHERING	L	4600.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: RW 9-27 **Name:** RUNWAY 9-27 **Use:** RUNWAY **Area:** 1,200,000 SqFt

Section: 6205 of 2 **From:** - **To:** - **Last Const.:** 1/1/2015

Surface: AC **Family:** CA653-PR-RW-AC **Zone:** **Category:** **Rank:** P

Area: 400,000 SqFt **Length:** 8,050 Ft **Width:** 100 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1980 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Work Date: 1/1/1992 **Work Type:** OVERLAY **Code:** IMPORTED **Is Major M&R:** True

Work Date: 1/1/2015 **Work Type:** Complete Reconstruction - AC **Code:** CR-AC **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 80 **Surveyed:** 16

Conditions: **PCI:** 88

Inspection Comments:

Sample Number: 302 **Type:** R **Area:** 5000.00 SqFt **PCI:** 94

Sample Comments:

57	WEATHERING	L	5000.00	SqFt

Sample Number: 307 **Type:** R **Area:** 5000.00 SqFt **PCI:** 90

Sample Comments:

48	L & T CR	L	26.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 312 **Type:** R **Area:** 5000.00 SqFt **PCI:** 86

Sample Comments:

48	L & T CR	L	62.00	Ft
57	WEATHERING	L	4750.00	SqFt
57	WEATHERING	M	250.00	SqFt

Sample Number: 317 **Type:** R **Area:** 5000.00 SqFt **PCI:** 94

Sample Comments:

57	WEATHERING	L	5000.00	SqFt

Sample Number: 322 **Type:** R **Area:** 5000.00 SqFt **PCI:** 86

Sample Comments:

48	L & T CR	L	36.00	Ft
57	WEATHERING	L	4750.00	SqFt
57	WEATHERING	M	250.00	SqFt

Sample Number: 327 **Type:** R **Area:** 5000.00 SqFt **PCI:** 86

Sample Comments:

48	L & T CR	L	134.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 332 **Type:** R **Area:** 5000.00 SqFt **PCI:** 81

Sample Comments:

48	L & T CR	L	167.00	Ft
57	WEATHERING	L	4750.00	SqFt
57	WEATHERING	M	250.00	SqFt

Sample Number: 337 **Type:** R **Area:** 5000.00 SqFt **PCI:** 83

Sample Comments:

48	L & T CR	L	128.00	Ft
57	WEATHERING	L	4750.00	SqFt
57	WEATHERING	M	250.00	SqFt

Sample Number: 342 **Type:** R **Area:** 5000.00 SqFt **PCI:** 91

Sample Comments:

57	WEATHERING	L	4750.00	SqFt
57	WEATHERING	M	250.00	SqFt

Sample Number: 347 **Type:** R **Area:** 5000.00 SqFt **PCI:** 86

Sample Comments:

48	L & T CR	L	29.00	Ft
57	WEATHERING	L	4750.00	SqFt
57	WEATHERING	M	250.00	SqFt

Sample Number: 352 **Type:** R **Area:** 5000.00 SqFt **PCI:** 91

Sample Comments:

57	WEATHERING	L	4750.00	SqFt
57	WEATHERING	M	250.00	SqFt

Sample Number: 357 **Type:** R **Area:** 5000.00 SqFt **PCI:** 83

Sample Comments:

48	L & T CR	L	19.00	Ft
52	RAVELING	L	50.00	SqFt
57	WEATHERING	L	4455.00	SqFt
57	WEATHERING	M	495.00	SqFt

Sample Number: 362 **Type:** R **Area:** 5000.00 SqFt **PCI:** 91

Sample Comments:

57	WEATHERING	L	4750.00	SqFt
57	WEATHERING	M	250.00	SqFt

Sample Number: 367 **Type:** R **Area:** 5000.00 SqFt **PCI:** 87

Sample Comments:

48	L & T CR	L	18.00	Ft
57	WEATHERING	L	4750.00	SqFt
57	WEATHERING	M	250.00	SqFt

Sample Number: 372 **Type:** R **Area:** 5000.00 SqFt **PCI:** 92

Sample Comments:

48	L & T CR	L	3.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 377 **Type:** R **Area:** 5000.00 SqFt **PCI:** 84

Sample Comments:

48	L & T CR	L	95.00	Ft
48	L & T CR	M	5.00	Ft
57	WEATHERING	L	5000.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: RW 9-27 **Name:** RUNWAY 9-27 **Use:** RUNWAY **Area:** 1,200,000 SqFt

Section: 6210 of 2 **From:** - **To:** - **Last Const.:** 1/1/2015

Surface: AC **Family:** CA653-PR-RW-AC **Zone:** **Category:** **Rank:** P

Area: 800,000 SqFt **Length:** 16,100 Ft **Width:** 25 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1980 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Work Date: 1/1/1992 **Work Type:** OVERLAY **Code:** IMPORTED **Is Major M&R:** True

Work Date: 1/1/2015 **Work Type:** Complete Reconstruction - AC **Code:** CR-AC **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 160 **Surveyed:** 20

Conditions: **PCI:** 90

Inspection Comments:

Sample Number: 104 **Type:** R **Area:** 5000.00 SqFt **PCI:** 87

Sample Comments:

48	L & T CR	L	115.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 113 **Type:** R **Area:** 5000.00 SqFt **PCI:** 82

Sample Comments:

48	L & T CR	L	206.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 120 **Type:** R **Area:** 5000.00 SqFt **PCI:** 90

Sample Comments:

48	L & T CR	L	15.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 128 **Type:** R **Area:** 5000.00 SqFt **PCI:** 82

Sample Comments:

48	L & T CR	L	106.00	Ft
48	L & T CR	M	14.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 136 **Type:** R **Area:** 5000.00 SqFt **PCI:** 88

Sample Comments:

48	L & T CR	L	100.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 147 **Type:** R **Area:** 5000.00 SqFt **PCI:** 94

Sample Comments:

57	WEATHERING	L	5000.00	SqFt

Sample Number: 155 **Type:** R **Area:** 5000.00 SqFt **PCI:** 89

Sample Comments:

48	L & T CR	L	53.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 163 **Type:** R **Area:** 5000.00 SqFt **PCI:** 90

Sample Comments:

48	L & T CR	L	26.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 171 **Type:** R **Area:** 5000.00 SqFt **PCI:** 92

Sample Comments:

48	L & T CR	L	4.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 179 **Type:** R **Area:** 5000.00 SqFt **PCI:** 94

Sample Comments:

57	WEATHERING	L	5000.00	SqFt

Sample Number: 500 **Type:** R **Area:** 5000.00 SqFt **PCI:** 76

Sample Comments:

48	L & T CR	L	351.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 508 **Type:** R **Area:** 5000.00 SqFt **PCI:** 94

Sample Comments:

57	WEATHERING	L	5000.00	SqFt

Sample Number: 516 **Type:** R **Area:** 5000.00 SqFt **PCI:** 94

Sample Comments:

57	WEATHERING	L	5000.00	SqFt

Sample Number: 524 **Type:** R **Area:** 5000.00 SqFt **PCI:** 91

Sample Comments:

48	L & T CR	L	7.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 533 **Type:** R **Area:** 5000.00 SqFt **PCI:** 94

Sample Comments:

57	WEATHERING	L	5000.00	SqFt

Sample Number: 543 **Type:** R **Area:** 5000.00 SqFt **PCI:** 92

Sample Comments:

48	L & T CR	L	5.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 551 **Type:** R **Area:** 5000.00 SqFt **PCI:** 94

Sample Comments:

57	WEATHERING	L	5000.00	SqFt

Sample Number: 559 **Type:** R **Area:** 5000.00 SqFt **PCI:** 88

Sample Comments:

48	L & T CR	L	95.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 566 **Type:** R **Area:** 5000.00 SqFt **PCI:** 91

Sample Comments:

48	L & T CR	L	12.00	Ft
57	WEATHERING	L	5000.00	SqFt

Sample Number: 575 **Type:** R **Area:** 5000.00 SqFt **PCI:** 89

Sample Comments:

48	L & T CR	L	50.00	Ft
57	WEATHERING	L	5000.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TL AP S **Name:** TAXILANE SOUTH RAMP **Use:** TAXIWAY **Area:** 6,963 SqFt

Section: 3205 of 1 **From:** - **To:** - **Last Const.:** 1/1/1994

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 6,963 SqFt **Length:** 150 Ft **Width:** 38 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1994 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 1 **Surveyed:** 1

Conditions: **PCI:** 65

Inspection Comments:

Sample Number: 100 **Type:** R **Area:** 6963.00 SqFt **PCI:** 65

Sample Comments:

48	L & T CR	L	356.00	Ft
48	L & T CR	M	89.00	Ft
52	RAVELING	L	348.00	SqFt
57	WEATHERING	M	6615.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TL T-HANG **Name:** TAXILANE T-HANGAR **Use:** TAXIWAY **Area:** 125,875 SqFt

Section: 3105 of 3 **From:** - **To:** - **Last Const.:** 1/1/1998

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 46,227 SqFt **Length:** 2,330 Ft **Width:** 20 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1998 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 12 **Surveyed:** 2

Conditions: **PCI:** 62

Inspection Comments:

Sample Number: 153 **Type:** R **Area:** 6178.00 SqFt **PCI:** 60

Sample Comments:

41	ALLIGATOR CR	L	12.00	SqFt
48	L & T CR	L	332.00	Ft
48	L & T CR	M	12.00	Ft
52	RAVELING	L	618.00	SqFt
57	WEATHERING	M	5560.00	SqFt

Sample Number: 201 **Type:** R **Area:** 4000.00 SqFt **PCI:** 65

Sample Comments:

48	L & T CR	L	66.00	Ft
48	L & T CR	M	100.00	Ft
52	RAVELING	L	400.00	SqFt
57	WEATHERING	M	3600.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TL T-HANG **Name:** TAXILANE T-HANGAR **Use:** TAXIWAY **Area:** 125,875 SqFt

Section: 3110 of 3 **From:** - **To:** - **Last Const.:** 1/1/1985

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 16,646 SqFt **Length:** 485 Ft **Width:** 35 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1985 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 4 **Surveyed:** 2

Conditions: **PCI:** 52

Inspection Comments:

Sample Number: 105 **Type:** R **Area:** 4273.00 SqFt **PCI:** 55

Sample Comments:

48	L & T CR	L	223.00	Ft
48	L & T CR	M	223.00	Ft
52	RAVELING	L	4073.00	SqFt
52	RAVELING	M	200.00	SqFt

Sample Number: 108 **Type:** R **Area:** 5372.00 SqFt **PCI:** 49

Sample Comments:

48	L & T CR	L	374.00	Ft
48	L & T CR	M	63.00	Ft
50	PATCHING	H	84.00	SqFt
52	RAVELING	L	4759.00	SqFt
52	RAVELING	M	529.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TL T-HANG **Name:** TAXILANE T-HANGAR **Use:** TAXIWAY **Area:** 125,875 SqFt

Section: 3115 of 3 **From:** - **To:** - **Last Const.:** 1/1/1985

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 63,002 SqFt **Length:** 750 Ft **Width:** 25 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1985 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 13 **Surveyed:** 3

Conditions: **PCI:** 46

Inspection Comments:

Sample Number: 253 **Type:** R **Area:** 5000.00 SqFt **PCI:** 44

Sample Comments:

43	BLOCK CR	L	4500.00	SqFt
43	BLOCK CR	M	500.00	SqFt
52	RAVELING	L	4750.00	SqFt
52	RAVELING	M	250.00	SqFt

Sample Number: 452 **Type:** R **Area:** 5180.00 SqFt **PCI:** 44

Sample Comments:

43	BLOCK CR	L	4662.00	SqFt
43	BLOCK CR	M	518.00	SqFt
52	RAVELING	L	5180.00	SqFt
54	SHOVING	L	19.00	SqFt

Sample Number: 651 **Type:** R **Area:** 6390.00 SqFt **PCI:** 49

Sample Comments:

48	L & T CR	L	506.00	Ft
48	L & T CR	M	200.00	Ft
52	RAVELING	L	6007.00	SqFt
52	RAVELING	M	383.00	SqFt
54	SHOVING	L	70.00	SqFt
54	SHOVING	M	10.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW A **Name:** TAXIWAY A **Use:** TAXIWAY **Area:** 562,900 SqFt

Section: 103 of 4 **From:** - **To:** - **Last Const.:** 1/1/2023

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 79,944 SqFt **Length:** 660 Ft **Width:** 125 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 10/1/2012 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Work Date: 1/1/2023 **Work Type:** Mill and Overlay **Code:** ML-OVL **Is Major M&R:** True

Last Insp. Date: 1/14/2019 **TotalSamples:** 12 **Surveyed:** 2

Conditions: **PCI:** 84 **NOTE: *** Pre-Construction PCI *****

Inspection Comments:

Sample Number: 302 **Type:** R **Area:** 5979.00 SqFt **PCI:** 89

Sample Comments:

48	L & T CR	L	34.00	Ft
56	SWELLING	L	8.00	SqFt
57	WEATHERING	L	5979.00	SqFt

Sample Number: 307 **Type:** R **Area:** 4790.00 SqFt **PCI:** 77

Sample Comments:

48	L & T CR	L	196.00	Ft
56	SWELLING	L	93.00	SqFt
57	WEATHERING	L	4790.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW A **Name:** TAXIWAY A **Use:** TAXIWAY **Area:** 562,900 SqFt

Section: 105 of 4 **From:** - **To:** - **Last Const.:** 1/1/2023

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 243,781 SqFt **Length:** 3,190 Ft **Width:** 75 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1961	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1971	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1993	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2005	**Work Type:** Surface Reconstruction - AC	**Code:** SR-AC	**Is Major M&R:** True
Work Date: 1/1/2023	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 1/14/2019 **TotalSamples:** 120 **Surveyed:** 11

Conditions: **PCI:** 62 **NOTE: *** Pre-Construction PCI *****

Inspection Comments:

Sample Number: 101 **Type:** R **Area:** 6693.00 SqFt **PCI:** 63

Sample Comments:

48	L & T CR	L	111.00	Ft
48	L & T CR	M	50.00	Ft
50	PATCHING	L	12.00	SqFt
52	RAVELING	L	670.00	SqFt
57	WEATHERING	M	6011.00	SqFt

Sample Number: 115 **Type:** R **Area:** 3750.00 SqFt **PCI:** 64

Sample Comments:

48	L & T CR	L	68.00	Ft
48	L & T CR	M	50.00	Ft
52	RAVELING	L	375.00	SqFt
56	SWELLING	L	10.00	SqFt
57	WEATHERING	M	3375.00	SqFt

Sample Number: 129 **Type:** R **Area:** 3750.00 SqFt **PCI:** 65

Sample Comments:

48	L & T CR	L	184.00	Ft
48	L & T CR	M	110.00	Ft
52	RAVELING	L	375.00	SqFt
57	WEATHERING	M	3375.00	SqFt

Sample Number: 143 **Type:** R **Area:** 3750.00 SqFt **PCI:** 63

Sample Comments:

42	BLEEDING	N	2.00	SqFt
48	L & T CR	L	170.00	Ft
48	L & T CR	M	73.00	Ft
52	RAVELING	L	500.00	SqFt
56	SWELLING	L	25.00	SqFt
57	WEATHERING	M	3250.00	SqFt

Sample Number: 156 **Type:** R **Area:** 3750.00 SqFt **PCI:** 63

Sample Comments:

48	L & T CR	L	159.00	Ft
48	L & T CR	M	100.00	Ft
52	RAVELING	L	375.00	SqFt
56	SWELLING	L	18.00	SqFt
57	WEATHERING	M	3375.00	SqFt

Sample Number: 166 **Type:** R **Area:** 3750.00 SqFt **PCI:** 64

Sample Comments:

48	L & T CR	L	131.00	Ft
48	L & T CR	M	50.00	Ft
52	RAVELING	L	375.00	SqFt
56	SWELLING	L	15.00	SqFt
57	WEATHERING	M	3375.00	SqFt

Sample Number: 171 **Type:** R **Area:** 3750.00 SqFt **PCI:** 63

Sample Comments:

48	L & T CR	L	165.00	Ft
48	L & T CR	M	50.00	Ft
52	RAVELING	L	375.00	SqFt
56	SWELLING	L	18.00	SqFt
57	WEATHERING	M	3375.00	SqFt

Sample Number: 185 **Type:** R **Area:** 3750.00 SqFt **PCI:** 58

Sample Comments:

48	L & T CR	L	436.00	Ft
48	L & T CR	M	50.00	Ft
52	RAVELING	L	375.00	SqFt
56	SWELLING	L	12.00	SqFt
57	WEATHERING	M	3375.00	SqFt

Sample Number: 199 **Type:** R **Area:** 3750.00 SqFt **PCI:** 63

Sample Comments:

48	L & T CR	L	205.00	Ft
48	L & T CR	M	50.00	Ft
52	RAVELING	L	375.00	SqFt
56	SWELLING	L	20.00	SqFt
57	WEATHERING	M	3375.00	SqFt

Sample Number: 212 **Type:** R **Area:** 3750.00 SqFt **PCI:** 57

Sample Comments:

48	L & T CR	L	225.00	Ft
48	L & T CR	M	150.00	Ft
52	RAVELING	L	375.00	SqFt
56	SWELLING	L	106.00	SqFt
57	WEATHERING	M	3375.00	SqFt

Sample Number: 218 **Type:** R **Area:** 4650.00 SqFt **PCI:** 58

Sample Comments:

45	DEPRESSION	L	25.00	SqFt
48	L & T CR	L	160.00	Ft
48	L & T CR	M	177.00	Ft
52	RAVELING	L	750.00	SqFt
52	RAVELING	M	4.00	SqFt
56	SWELLING	L	35.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW A **Name:** TAXIWAY A **Use:** TAXIWAY **Area:** 562,900 SqFt

Section: 106 of 4 **From:** - **To:** - **Last Const.:** 1/1/2005

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 215,250 SqFt **Length:** 2,870 Ft **Width:** 75 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1961	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1971	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1993	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2005	**Work Type:** Surface Reconstruction - AC	**Code:** SR-AC	**Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 57 **Surveyed:** 6

Conditions: **PCI:** 61

Inspection Comments:

Sample Number: 164 **Type:** R **Area:** 3750.00 SqFt **PCI:** 68

Sample Comments:

48	L & T CR	L	126.00	Ft
48	L & T CR	M	50.00	Ft
52	RAVELING	L	375.00	SqFt
56	SWELLING	L	52.00	SqFt
57	WEATHERING	L	3375.00	SqFt

Sample Number: 169 **Type:** R **Area:** 3750.00 SqFt **PCI:** 63

Sample Comments:

48	L & T CR	L	125.00	Ft
48	L & T CR	M	100.00	Ft
52	RAVELING	L	750.00	SqFt
56	SWELLING	L	45.00	SqFt
57	WEATHERING	M	3000.00	SqFt

Sample Number: 183 **Type:** R **Area:** 3750.00 SqFt **PCI:** 58

Sample Comments:

48	L & T CR	L	291.00	Ft
48	L & T CR	M	177.00	Ft
52	RAVELING	L	375.00	SqFt
56	SWELLING	L	23.00	SqFt
57	WEATHERING	L	3375.00	SqFt

Sample Number: 197 **Type:** R **Area:** 3750.00 SqFt **PCI:** 70

Sample Comments:

48	L & T CR	L	125.00	Ft
48	L & T CR	M	50.00	Ft
52	RAVELING	L	375.00	SqFt
56	SWELLING	L	30.00	SqFt
57	WEATHERING	L	3375.00	SqFt

Sample Number: 210 **Type:** R **Area:** 3750.00 SqFt **PCI:** 56

Sample Comments:

48	L & T CR	L	160.00	Ft
48	L & T CR	M	171.00	Ft
52	RAVELING	L	375.00	SqFt
56	SWELLING	L	125.00	SqFt
57	WEATHERING	M	3375.00	SqFt

Sample Number: 216 **Type:** R **Area:** 4650.00 SqFt **PCI:** 52

Sample Comments:

48	L & T CR	L	267.00	Ft
48	L & T CR	M	200.00	Ft

52	RAVELING	L	463.00	SqFt
52	RAVELING	M	20.00	SqFt
56	SWELLING	L	75.00	SqFt
57	WEATHERING	L	4167.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW A **Name:** TAXIWAY A **Use:** TAXIWAY **Area:** 562,900 SqFt

Section: 107 of 4 **From:** - **To:** - **Last Const.:** 10/1/2012

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 23,925 SqFt **Length:** 320 Ft **Width:** 75 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 10/1/2012 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 6 **Surveyed:** 1

Conditions: **PCI:** 67

Inspection Comments:

Sample Number: 219 **Type:** R **Area:** 3750.00 SqFt **PCI:** 67

Sample Comments:

48	L & T CR	L	169.00	Ft
48	L & T CR	M	100.00	Ft
56	SWELLING	L	350.00	SqFt
57	WEATHERING	L	3750.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW A1 **Name:** TAXIWAY A1 **Use:** TAXIWAY **Area:** 40,291 SqFt

Section: 110 of 1 **From:** - **To:** - **Last Const.:** 10/1/2012

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 40,291 SqFt **Length:** 295 Ft **Width:** 100 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 10/1/2012 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 9 **Surveyed:** 1

Conditions: **PCI:** 64

Inspection Comments:

Sample Number: 102 **Type:** R **Area:** 5000.00 SqFt **PCI:** 64

Sample Comments:

48	L & T CR	L	51.00	Ft
48	L & T CR	M	200.00	Ft
56	SWELLING	L	363.00	SqFt
57	WEATHERING	L	5000.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW A10 **Name:** TAXIWAY A10 **Use:** TAXIWAY **Area:** 27,376 SqFt

Section: 175 of 2 **From:** - **To:** - **Last Const.:** 12/25/1999

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 4,954 SqFt **Length:** 100 Ft **Width:** 50 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 12/25/1999 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 1 **Surveyed:** 1

Conditions: **PCI:** 70

Inspection Comments:

Sample Number: 104 **Type:** R **Area:** 4954.00 SqFt **PCI:** 70

Sample Comments:

48	L & T CR	L	63.00	Ft
48	L & T CR	M	62.00	Ft
57	WEATHERING	M	4954.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW A11 **Name:** TAXIWAY A11 **Use:** TAXIWAY **Area:** 24,154 SqFt

Section: 180 of 1 **From:** - **To:** - **Last Const.:** 1/1/2023

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 24,154 SqFt **Length:** 356 Ft **Width:** 55 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1961	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1993	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2005	**Work Type:** Surface Reconstruction - AC	**Code:** SR-AC	**Is Major M&R:** True
Work Date: 1/1/2023	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 1/14/2019 **TotalSamples:** 6 **Surveyed:** 1

Conditions: **PCI:** 65 **NOTE: *** Pre-Construction PCI ***

Inspection Comments:

Sample Number: 106 **Type:** R **Area:** 5163.00 SqFt **PCI:** 65

Sample Comments:

48	L & T CR	L	147.00	Ft
48	L & T CR	M	20.00	Ft
52	RAVELING	L	250.00	SqFt
57	WEATHERING	M	4913.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW A12 **Name:** TAXIWAY A12 **Use:** TAXIWAY **Area:** 43,156 SqFt

Section: 185 of 1 **From:** - **To:** - **Last Const.:** 1/1/2023

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 43,156 SqFt **Length:** 295 Ft **Width:** 100 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1980	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1992	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2005	**Work Type:** Surface Reconstruction - AC	**Code:** SR-AC	**Is Major M&R:** True
Work Date: 1/1/2023	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 1/14/2019 **TotalSamples:** 10 **Surveyed:** 4

Conditions: **PCI:** 63 **NOTE: *** Pre-Construction PCI *****

Inspection Comments:

Sample Number: 130 **Type:** R **Area:** 4900.00 SqFt **PCI:** 58

Sample Comments:

42	BLEEDING	N	56.00	SqFt
48	L & T CR	L	70.00	Ft
52	RAVELING	L	200.00	SqFt
53	RUTTING	L	42.00	SqFt
56	SWELLING	L	23.00	SqFt
57	WEATHERING	M	4700.00	SqFt

Sample Number: 131 **Type:** R **Area:** 4900.00 SqFt **PCI:** 67

Sample Comments:

42	BLEEDING	N	25.00	SqFt
48	L & T CR	L	25.00	Ft
48	L & T CR	M	116.00	Ft
52	RAVELING	L	200.00	SqFt
57	WEATHERING	M	4700.00	SqFt

Sample Number: 132 **Type:** R **Area:** 4900.00 SqFt **PCI:** 67

Sample Comments:

48	L & T CR	L	66.00	Ft
48	L & T CR	M	54.00	Ft
52	RAVELING	L	200.00	SqFt
56	SWELLING	L	18.00	SqFt
57	WEATHERING	M	2700.00	SqFt

Sample Number: 135 **Type:** R **Area:** 5693.00 SqFt **PCI:** 60

Sample Comments:

48	L & T CR	L	225.00	Ft
48	L & T CR	M	50.00	Ft
52	RAVELING	L	570.00	SqFt
56	SWELLING	L	118.00	SqFt
57	WEATHERING	M	5123.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW A2 **Name:** TAXIWAY A2 **Use:** TAXIWAY **Area:** 42,179 SqFt

Section: 115 of 1 **From:** - **To:** - **Last Const.:** 1/1/2005

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 42,179 SqFt **Length:** 295 Ft **Width:** 100 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1971	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1993	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2005	**Work Type:** Surface Reconstruction - AC	**Code:** SR-AC	**Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 9 **Surveyed:** 2

Conditions: **PCI:** 70

Inspection Comments:

Sample Number: 101 **Type:** R **Area:** 5125.00 SqFt **PCI:** 72

Sample Comments:

48	L & T CR	L	9.00	Ft
52	RAVELING	L	1025.00	SqFt
56	SWELLING	L	5.00	SqFt
57	WEATHERING	M	4100.00	SqFt

Sample Number: 104 **Type:** R **Area:** 5125.00 SqFt **PCI:** 68

Sample Comments:

48	L & T CR	L	114.00	Ft
52	RAVELING	L	1025.00	SqFt
56	SWELLING	L	60.00	SqFt
57	WEATHERING	M	4100.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW A3 **Name:** TAXIWAY A3 **Use:** TAXIWAY **Area:** 67,248 SqFt

Section: 125 of 2 **From:** - **To:** - **Last Const.:** 1/1/2005

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 32,329 SqFt **Length:** 295 Ft **Width:** 60 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1971	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1993	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2005	**Work Type:** Surface Reconstruction - AC	**Code:** SR-AC	**Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 8 **Surveyed:** 2

Conditions: **PCI:** 61

Inspection Comments:

Sample Number: 101 **Type:** R **Area:** 4103.00 SqFt **PCI:** 65

Sample Comments:

48	L & T CR	L	112.00	Ft
48	L & T CR	M	96.00	Ft
52	RAVELING	L	820.00	SqFt
56	SWELLING	L	40.00	SqFt
57	WEATHERING	L	3283.00	SqFt

Sample Number: 104 **Type:** R **Area:** 4932.00 SqFt **PCI:** 57

Sample Comments:

48	L & T CR	L	173.00	Ft
48	L & T CR	M	220.00	Ft
52	RAVELING	L	986.00	SqFt
56	SWELLING	L	65.00	SqFt
57	WEATHERING	L	3946.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW A3 **Name:** TAXIWAY A3 **Use:** TAXIWAY **Area:** 67,248 SqFt

Section: 130 of 2 **From:** - **To:** - **Last Const.:** 7/1/2005

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 34,919 SqFt **Length:** 350 Ft **Width:** 90 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 7/1/2005 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 8 **Surveyed:** 1

Conditions: **PCI:** 67

Inspection Comments:

Sample Number: 103 **Type:** R **Area:** 4500.00 SqFt **PCI:** 67

Sample Comments:

48	L & T CR	L	94.00	Ft
48	L & T CR	M	112.00	Ft
57	WEATHERING	L	4050.00	SqFt
57	WEATHERING	M	450.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW A4 **Name:** TAXIWAY A4 **Use:** TAXIWAY **Area:** 19,805 SqFt

Section: 140 of 1 **From:** - **To:** - **Last Const.:** 1/1/1985

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 19,805 SqFt **Length:** 520 Ft **Width:** 35 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1985 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 5 **Surveyed:** 1

Conditions: **PCI:** 54

Inspection Comments:

Sample Number: 101 **Type:** R **Area:** 3500.00 SqFt **PCI:** 54

Sample Comments:

48	L & T CR	L	89.00	Ft
48	L & T CR	M	190.00	Ft
52	RAVELING	L	3300.00	SqFt
52	RAVELING	M	200.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW A7 **Name:** TAXIWAY A7 **Use:** TAXIWAY **Area:** 72,118 SqFt

Section: 150 of 1 **From:** - **To:** - **Last Const.:** 1/1/2023

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 72,118 SqFt **Length:** 300 Ft **Width:** 110 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1961	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1971	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1993	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2005	**Work Type:** Surface Reconstruction - AC	**Code:** SR-AC	**Is Major M&R:** True
Work Date: 1/1/2023	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 1/14/2019 **TotalSamples:** 8 **Surveyed:** 2

Conditions: **PCI:** 61 **NOTE: *** Pre-Construction PCI *****

Inspection Comments:

Sample Number: 101 **Type:** R **Area:** 4186.00 SqFt **PCI:** 60

Sample Comments:

48	L & T CR	L	204.00	Ft
48	L & T CR	M	34.00	Ft
52	RAVELING	L	500.00	SqFt
56	SWELLING	L	110.00	SqFt
57	WEATHERING	M	3686.00	SqFt

Sample Number: 103 **Type:** R **Area:** 3751.00 SqFt **PCI:** 62

Sample Comments:

48	L & T CR	L	132.00	Ft
48	L & T CR	M	28.00	Ft
52	RAVELING	L	500.00	SqFt
56	SWELLING	L	52.00	SqFt
57	WEATHERING	M	3251.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW A8 **Name:** TAXIWAY A8 **Use:** TAXIWAY **Area:** 54,633 SqFt

Section: 155 of 2 **From:** - **To:** - **Last Const.:** 1/1/2023

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 43,518 SqFt **Length:** 330 Ft **Width:** 90 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1961	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1971	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1993	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2005	**Work Type:** Surface Reconstruction - AC	**Code:** SR-AC	**Is Major M&R:** True
Work Date: 1/1/2023	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 1/14/2019 **TotalSamples:** 8 **Surveyed:** 1

Conditions: **PCI:** 70 **NOTE: *** Pre-Construction PCI ***

Inspection Comments:

Sample Number: 102 **Type:** R **Area:** 3750.00 SqFt **PCI:** 70

Sample Comments:

48	L & T CR	L	60.00	Ft
52	RAVELING	L	750.00	SqFt
56	SWELLING	L	10.00	SqFt
57	WEATHERING	M	3000.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW A8 **Name:** TAXIWAY A8 **Use:** TAXIWAY **Area:** 54,633 SqFt

Section: 160 of 2 **From:** - **To:** - **Last Const.:** 1/1/2023

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 11,115 SqFt **Length:** 70 Ft **Width:** 105 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1961	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1971	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1993	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2005	**Work Type:** Surface Reconstruction - AC	**Code:** SR-AC	**Is Major M&R:** True
Work Date: 1/1/2010	**Work Type:** Overlay - AC Structural	**Code:** OL-AS	**Is Major M&R:** True
Work Date: 1/1/2023	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 1/14/2019 **TotalSamples:** 1 **Surveyed:** 1

Conditions: **PCI:** 90 **NOTE: *** Pre-Construction PCI *****

Inspection Comments:

Sample Number: 106 **Type:** R **Area:** 6575.00 SqFt **PCI:** 90

Sample Comments:

48	L & T CR	L	36.00	Ft
57	WEATHERING	L	6575.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW B **Name:** TAXIWAY B **Use:** TAXIWAY **Area:** 677,024 SqFt

Section: 203 of 4 **From:** - **To:** - **Last Const.:** 10/1/2012

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 50,342 SqFt **Length:** 290 Ft **Width:** 130 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 10/1/2012 **Work Type:** New Construction - AC **Code:** NC-AC **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 10 **Surveyed:** 1

Conditions: **PCI:** 74

Inspection Comments:

Sample Number: 211 **Type:** R **Area:** 5200.00 SqFt **PCI:** 74

Sample Comments:

48	L & T CR	L	173.00	Ft
48	L & T CR	M	40.00	Ft
56	SWELLING	L	110.00	SqFt
57	WEATHERING	L	5200.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW B **Name:** TAXIWAY B **Use:** TAXIWAY **Area:** 677,024 SqFt

Section: 205 of 4 **From:** - **To:** - **Last Const.:** 1/1/2005

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 581,353 SqFt **Length:** 7,865 Ft **Width:** 75 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1980 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Work Date: 1/1/1992 **Work Type:** OVERLAY **Code:** IMPORTED **Is Major M&R:** True

Work Date: 1/1/2005 **Work Type:** Surface Reconstruction - AC **Code:** SR-AC **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 156 **Surveyed:** 13

Conditions: **PCI:** 50

Inspection Comments:

Sample Number: 102 **Type:** R **Area:** 4444.00 SqFt **PCI:** 59

Sample Comments:

48	L & T CR	L	482.00	Ft
48	L & T CR	M	100.00	Ft
52	RAVELING	L	889.00	SqFt
56	SWELLING	L	12.00	SqFt
57	WEATHERING	M	3555.00	SqFt

Sample Number: 109 **Type:** R **Area:** 3750.00 SqFt **PCI:** 50

Sample Comments:

48	L & T CR	L	394.00	Ft
48	L & T CR	M	200.00	Ft
52	RAVELING	L	750.00	SqFt
56	SWELLING	L	100.00	SqFt
57	WEATHERING	M	3000.00	SqFt

Sample Number: 123 **Type:** R **Area:** 3750.00 SqFt **PCI:** 53

Sample Comments:

48	L & T CR	L	381.00	Ft
48	L & T CR	M	150.00	Ft
52	RAVELING	L	750.00	SqFt
56	SWELLING	L	157.00	SqFt
57	WEATHERING	M	3000.00	SqFt

Sample Number: 130 **Type:** R **Area:** 3750.00 SqFt **PCI:** 51

Sample Comments:

48	L & T CR	L	554.00	Ft
48	L & T CR	M	100.00	Ft
52	RAVELING	L	750.00	SqFt
56	SWELLING	L	119.00	SqFt
57	WEATHERING	M	3000.00	SqFt

Sample Number: 144 **Type:** R **Area:** 3500.00 SqFt **PCI:** 52

Sample Comments:

48	L & T CR	L	485.00	Ft
48	L & T CR	M	105.00	Ft
52	RAVELING	L	700.00	SqFt
56	SWELLING	L	75.00	SqFt
57	WEATHERING	M	2800.00	SqFt

Sample Number: 151 **Type:** R **Area:** 3750.00 SqFt **PCI:** 54

Sample Comments:

48	L & T CR	L	356.00	Ft
48	L & T CR	M	150.00	Ft
52	RAVELING	L	750.00	SqFt
56	SWELLING	L	60.00	SqFt

57	WEATHERING	M	3000.00	SqFt

Sample Number: 165 **Type:** R **Area:** 3750.00 SqFt **PCI:** 56

Sample Comments:

48	L & T CR	L	350.00	Ft
48	L & T CR	M	125.00	Ft
52	RAVELING	L	750.00	SqFt
56	SWELLING	L	142.00	SqFt
57	WEATHERING	M	3000.00	SqFt

Sample Number: 172 **Type:** R **Area:** 3750.00 SqFt **PCI:** 56

Sample Comments:

48	L & T CR	L	250.00	Ft
48	L & T CR	M	178.00	Ft
52	RAVELING	L	750.00	SqFt
56	SWELLING	L	52.00	SqFt
57	WEATHERING	M	3000.00	SqFt

Sample Number: 186 **Type:** R **Area:** 3750.00 SqFt **PCI:** 21

Sample Comments:

41	ALLIGATOR CR	L	248.00	SqFt
41	ALLIGATOR CR	M	225.00	SqFt
48	L & T CR	L	106.00	Ft
48	L & T CR	M	100.00	Ft
52	RAVELING	L	750.00	SqFt
56	SWELLING	L	115.00	SqFt
57	WEATHERING	M	3000.00	SqFt

Sample Number: 200 **Type:** R **Area:** 3750.00 SqFt **PCI:** 39

Sample Comments:

41	ALLIGATOR CR	L	186.00	SqFt
48	L & T CR	L	320.00	Ft
48	L & T CR	M	115.00	Ft
52	RAVELING	L	750.00	SqFt
56	SWELLING	L	100.00	SqFt
57	WEATHERING	M	3000.00	SqFt

Sample Number: 207 **Type:** R **Area:** 3651.00 SqFt **PCI:** 47

Sample Comments:

48	L & T CR	L	350.00	Ft
48	L & T CR	M	206.00	Ft
52	RAVELING	L	730.00	SqFt
56	SWELLING	L	450.00	SqFt
57	WEATHERING	M	2921.00	SqFt

Sample Number: 228 **Type:** R **Area:** 3750.00 SqFt **PCI:** 58

Sample Comments:

48	L & T CR	L	416.00	Ft
48	L & T CR	M	101.00	Ft
52	RAVELING	L	1500.00	SqFt
56	SWELLING	L	30.00	SqFt
57	WEATHERING	M	2250.00	SqFt

Sample Number: 249 **Type:** R **Area:** 3913.00 SqFt **PCI:** 55

Sample Comments:

48	L & T CR	L	319.00	Ft
48	L & T CR	M	200.00	Ft
52	RAVELING	L	783.00	SqFt
56	SWELLING	L	38.00	SqFt
57	WEATHERING	M	3130.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW B **Name:** TAXIWAY B **Use:** TAXIWAY **Area:** 677,024 SqFt

Section: 207 of 4 **From:** - **To:** - **Last Const.:** 1/1/2023

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 15,151 SqFt **Length:** 110 Ft **Width:** 130 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 10/1/2012 **Work Type:** New Construction - AC **Code:** NC-AC **Is Major M&R:** True

Work Date: 1/1/2023 **Work Type:** Mill and Overlay **Code:** ML-OVL **Is Major M&R:** True

Last Insp. Date: 1/14/2019 **TotalSamples:** 21 **Surveyed:** 3

Conditions: **PCI:** 83 **NOTE: *** Pre-Construction PCI *****

Inspection Comments:

Sample Number: 300 **Type:** R **Area:** 5761.00 SqFt **PCI:** 80

Sample Comments:

48	L & T CR	L	254.00	Ft
56	SWELLING	L	7.00	SqFt
57	WEATHERING	L	5761.00	SqFt

Sample Number: 307 **Type:** R **Area:** 7035.00 SqFt **PCI:** 84

Sample Comments:

48	L & T CR	L	158.00	Ft
56	SWELLING	L	65.00	SqFt
57	WEATHERING	L	7035.00	SqFt

Sample Number: 312 **Type:** R **Area:** 6500.00 SqFt **PCI:** 85

Sample Comments:

48	L & T CR	L	149.00	Ft
56	SWELLING	L	30.00	SqFt
57	WEATHERING	L	6500.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW B **Name:** TAXIWAY B **Use:** TAXIWAY **Area:** 677,024 SqFt

Section: 209 of 4 **From:** - **To:** - **Last Const.:** 1/1/2023

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 30,178 SqFt **Length:** 255 Ft **Width:** 100 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 10/1/2012 **Work Type:** New Construction - AC **Code:** NC-AC **Is Major M&R:** True

Work Date: 1/1/2023 **Work Type:** Mill and Overlay **Code:** ML-OVL **Is Major M&R:** True

Last Insp. Date: 1/14/2019 **TotalSamples:** 21 **Surveyed:** 3

Conditions: **PCI:** 83 **NOTE: *** Pre-Construction PCI ***

Inspection Comments:

Sample Number: 300 **Type:** R **Area:** 5761.00 SqFt **PCI:** 80

Sample Comments:

48	L & T CR	L	254.00	Ft
56	SWELLING	L	7.00	SqFt
57	WEATHERING	L	5761.00	SqFt

Sample Number: 307 **Type:** R **Area:** 7035.00 SqFt **PCI:** 84

Sample Comments:

48	L & T CR	L	158.00	Ft
56	SWELLING	L	65.00	SqFt
57	WEATHERING	L	7035.00	SqFt

Sample Number: 312 **Type:** R **Area:** 6500.00 SqFt **PCI:** 85

Sample Comments:

48	L & T CR	L	149.00	Ft
56	SWELLING	L	30.00	SqFt
57	WEATHERING	L	6500.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW B1 **Name:** TAXIWAY B1 **Use:** TAXIWAY **Area:** 51,074 SqFt

Section: 210 of 2 **From:** - **To:** - **Last Const.:** 1/1/2005

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 46,292 SqFt **Length:** 470 Ft **Width:** 90 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1980	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1992	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2005	**Work Type:** Surface Reconstruction - AC	**Code:** SR-AC	**Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 10 **Surveyed:** 2

Conditions: **PCI:** 54

Inspection Comments:

Sample Number: 104 **Type:** R **Area:** 4500.00 SqFt **PCI:** 53

Sample Comments:

48	L & T CR	L	250.00	Ft
48	L & T CR	M	309.00	Ft
52	RAVELING	L	900.00	SqFt
56	SWELLING	L	26.00	SqFt
57	WEATHERING	M	3600.00	SqFt

Sample Number: 108 **Type:** R **Area:** 5367.00 SqFt **PCI:** 55

Sample Comments:

48	L & T CR	L	400.00	Ft
48	L & T CR	M	326.00	Ft
52	RAVELING	L	1073.00	SqFt
56	SWELLING	L	13.00	SqFt
57	WEATHERING	L	4294.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW B1 **Name:** TAXIWAY B1 **Use:** TAXIWAY **Area:** 51,074 SqFt

Section: 215 of 2 **From:** - **To:** - **Last Const.:** 1/1/2015

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 4,782 SqFt **Length:** 135 Ft **Width:** 30 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1980	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1992	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2005	**Work Type:** Surface Reconstruction - AC	**Code:** SR-AC	**Is Major M&R:** True
Work Date: 1/1/2015	**Work Type:** Complete Reconstruction - AC	**Code:** CR-AC	**Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 1 **Surveyed:** 1

Conditions: **PCI:** 87

Inspection Comments:

Sample Number: 100 **Type:** R **Area:** 4782.00 SqFt **PCI:** 87

Sample Comments:

48	L & T CR	L	81.00	Ft
56	SWELLING	L	5.00	SqFt
57	WEATHERING	L	4782.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW B2 **Name:** TAXIWAY B2 **Use:** TAXIWAY **Area:** 49,156 SqFt

Section: 220 of 1 **From:** - **To:** - **Last Const.:** 1/1/2015

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 49,156 SqFt **Length:** 500 Ft **Width:** 90 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/2005 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Work Date: 1/1/2015 **Work Type:** Complete Reconstruction - AC **Code:** CR-AC **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 11 **Surveyed:** 2

Conditions: **PCI:** 87

Inspection Comments:

Sample Number: 102 **Type:** R **Area:** 4160.00 SqFt **PCI:** 84

Sample Comments:

48	L & T CR	L	76.00	Ft
56	SWELLING	L	55.00	SqFt
57	WEATHERING	L	4160.00	SqFt

Sample Number: 107 **Type:** R **Area:** 4603.00 SqFt **PCI:** 89

Sample Comments:

48	L & T CR	L	50.00	Ft
57	WEATHERING	L	4603.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW B3 **Name:** TAXIWAY B3 **Use:** TAXIWAY **Area:** 147,361 SqFt

Section: 230 of 2 **From:** - **To:** - **Last Const.:** 1/1/2015

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 63,794 SqFt **Length:** 500 Ft **Width:** 90 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1980	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1992	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2005	**Work Type:** Surface Reconstruction - AC	**Code:** SR-AC	**Is Major M&R:** True
Work Date: 1/1/2015	**Work Type:** Complete Reconstruction - AC	**Code:** CR-AC	**Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 12 **Surveyed:** 3

Conditions: **PCI:** 90

Inspection Comments:

Sample Number: 100 **Type:** R **Area:** 6868.00 SqFt **PCI:** 89

Sample Comments:

48	L & T CR	L	111.00	Ft
57	WEATHERING	L	6868.00	SqFt

Sample Number: 104 **Type:** R **Area:** 4500.00 SqFt **PCI:** 91

Sample Comments:

57	WEATHERING	L	4275.00	SqFt
57	WEATHERING	M	225.00	SqFt

Sample Number: 108 **Type:** R **Area:** 6754.00 SqFt **PCI:** 91

Sample Comments:

57	WEATHERING	L	6416.00	SqFt
57	WEATHERING	M	338.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW B3 **Name:** TAXIWAY B3 **Use:** TAXIWAY **Area:** 147,361 SqFt

Section: 235 of 2 **From:** - **To:** - **Last Const.:** 1/1/2007

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 83,567 SqFt **Length:** 600 Ft **Width:** 125 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/2007 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 14 **Surveyed:** 3

Conditions: **PCI:** 76

Inspection Comments:

Sample Number: 800 **Type:** R **Area:** 6060.00 SqFt **PCI:** 75

Sample Comments:

48	L & T CR	L	110.00	Ft
48	L & T CR	M	67.00	Ft
57	WEATHERING	L	5757.00	SqFt
57	WEATHERING	M	303.00	SqFt

Sample Number: 809 **Type:** R **Area:** 5922.00 SqFt **PCI:** 79

Sample Comments:

48	L & T CR	L	86.00	Ft
48	L & T CR	M	25.00	Ft
57	WEATHERING	L	5626.00	SqFt
57	WEATHERING	M	296.00	SqFt

Sample Number: 811 **Type:** R **Area:** 6877.00 SqFt **PCI:** 74

Sample Comments:

48	L & T CR	L	69.00	Ft
48	L & T CR	M	101.00	Ft
57	WEATHERING	L	6533.00	SqFt
57	WEATHERING	M	344.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW B4 **Name:** TAXIWAY B4 **Use:** TAXIWAY **Area:** 48,156 SqFt

Section: 240 of 1 **From:** - **To:** - **Last Const.:** 1/1/2007

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 48,156 SqFt **Length:** 400 Ft **Width:** 125 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/2007 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 7 **Surveyed:** 2

Conditions: **PCI:** 76

Inspection Comments:

Sample Number: 101 **Type:** R **Area:** 7025.00 SqFt **PCI:** 76

Sample Comments:

48	L & T CR	L	108.00	Ft
48	L & T CR	M	38.00	Ft
57	WEATHERING	L	5620.00	SqFt
57	WEATHERING	M	1405.00	SqFt

Sample Number: 103 **Type:** R **Area:** 7000.00 SqFt **PCI:** 75

Sample Comments:

48	L & T CR	L	51.00	Ft
48	L & T CR	M	60.00	Ft
52	RAVELING	L	350.00	SqFt
56	SWELLING	L	5.00	SqFt
57	WEATHERING	L	6650.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW B5 **Name:** TAXIWAY B5 **Use:** TAXIWAY **Area:** 24,545 SqFt

Section: 250 of 1 **From:** - **To:** - **Last Const.:** 1/1/2005

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 24,545 SqFt **Length:** 100 Ft **Width:** 100 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1989 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Work Date: 1/1/2005 **Work Type:** Surface Reconstruction - AC **Code:** SR-AC **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 5 **Surveyed:** 2

Conditions: **PCI:** 43

Inspection Comments:

Sample Number: 100 **Type:** R **Area:** 5000.00 SqFt **PCI:** 41

Sample Comments:

48	L & T CR	L	353.00	Ft
48	L & T CR	M	319.00	Ft
50	PATCHING	L	1.00	SqFt
52	RAVELING	L	1000.00	SqFt
53	RUTTING	L	90.00	SqFt
56	SWELLING	L	375.00	SqFt
57	WEATHERING	M	3999.00	SqFt

Sample Number: 101 **Type:** R **Area:** 5629.00 SqFt **PCI:** 44

Sample Comments:

41	ALLIGATOR CR	L	12.00	SqFt
48	L & T CR	L	325.00	Ft
48	L & T CR	M	278.00	Ft
52	RAVELING	L	1126.00	SqFt
56	SWELLING	L	290.00	SqFt
56	SWELLING	M	10.00	SqFt
57	WEATHERING	M	4503.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW B6 **Name:** TAXIWAY B6 **Use:** TAXIWAY **Area:** 80,022 SqFt

Section: 260 of 3 **From:** - **To:** - **Last Const.:** 1/1/2015

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 38,862 SqFt **Length:** 390 Ft **Width:** 90 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/2005 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Work Date: 1/1/2015 **Work Type:** Complete Reconstruction - AC **Code:** CR-AC **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 8 **Surveyed:** 1

Conditions: **PCI:** 84

Inspection Comments:

Sample Number: 102 **Type:** R **Area:** 4590.00 SqFt **PCI:** 84

Sample Comments:

48	L & T CR	L	39.00	Ft
48	L & T CR	M	11.00	Ft
57	WEATHERING	L	4590.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW B6 **Name:** TAXIWAY B6 **Use:** TAXIWAY **Area:** 80,022 SqFt

Section: 265 of 3 **From:** - **To:** - **Last Const.:** 1/1/2005

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 17,002 SqFt **Length:** 100 Ft **Width:** 150 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1980	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1992	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2005	**Work Type:** Surface Reconstruction - AC	**Code:** SR-AC	**Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 3 **Surveyed:** 1

Conditions: **PCI:** 59

Inspection Comments:

Sample Number: 109 **Type:** R **Area:** 6935.00 SqFt **PCI:** 59

Sample Comments:

48	L & T CR	L	424.00	Ft
48	L & T CR	M	150.00	Ft
53	RUTTING	L	72.00	SqFt
56	SWELLING	L	322.00	SqFt
57	WEATHERING	M	6935.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW B6 **Name:** TAXIWAY B6 **Use:** TAXIWAY **Area:** 80,022 SqFt

Section: 267 of 3 **From:** - **To:** - **Last Const.:** 1/1/2005

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 24,158 SqFt **Length:** 100 Ft **Width:** 75 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1989 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Work Date: 1/1/2005 **Work Type:** Surface Reconstruction - AC **Code:** SR-AC **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 5 **Surveyed:** 2

Conditions: **PCI:** 52

Inspection Comments:

Sample Number: 100 **Type:** R **Area:** 5000.00 SqFt **PCI:** 57

Sample Comments:

48	L & T CR	L	481.00	Ft
48	L & T CR	M	78.00	Ft
52	RAVELING	L	1000.00	SqFt
56	SWELLING	L	130.00	SqFt
57	WEATHERING	M	4000.00	SqFt

Sample Number: 102 **Type:** R **Area:** 5309.00 SqFt **PCI:** 46

Sample Comments:

43	BLOCK CR	L	270.00	SqFt
48	L & T CR	L	382.00	Ft
48	L & T CR	M	147.00	Ft
52	RAVELING	L	796.00	SqFt
56	SWELLING	L	215.00	SqFt
56	SWELLING	M	15.00	SqFt
57	WEATHERING	M	4513.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW B7 **Name:** TAXIWAY B7 **Use:** TAXIWAY **Area:** 119,964 SqFt

Section: 270 of 5 **From:** - **To:** - **Last Const.:** 1/1/2015

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 39,535 SqFt **Length:** 500 Ft **Width:** 90 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/2005 **Work Type:** New Construction - AC **Code:** NC-AC **Is Major M&R:** True

Work Date: 1/1/2015 **Work Type:** Complete Reconstruction - AC **Code:** CR-AC **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 8 **Surveyed:** 2

Conditions: **PCI:** 85

Inspection Comments:

Sample Number: 102 **Type:** R **Area:** 4572.00 SqFt **PCI:** 86

Sample Comments:

48	L & T CR	L	125.00	Ft
57	WEATHERING	L	4572.00	SqFt

Sample Number: 106 **Type:** R **Area:** 4572.00 SqFt **PCI:** 85

Sample Comments:

48	L & T CR	L	142.00	Ft
57	WEATHERING	L	4572.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW B7 **Name:** TAXIWAY B7 **Use:** TAXIWAY **Area:** 119,964 SqFt

Section: 271 of 5 **From:** - **To:** - **Last Const.:** 1/1/2015

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 23,946 SqFt **Length:** 500 Ft **Width:** 90 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1980	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1992	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2005	**Work Type:** Surface Reconstruction - AC	**Code:** SR-AC	**Is Major M&R:** True
Work Date: 1/1/2015	**Work Type:** Complete Reconstruction - AC	**Code:** CR-AC	**Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 4 **Surveyed:** 1

Conditions: **PCI:** 83

Inspection Comments:

Sample Number: 108 **Type:** R **Area:** 6469.00 SqFt **PCI:** 83

Sample Comments:

48	L & T CR	L	250.00	Ft
57	WEATHERING	L	6469.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW B7 **Name:** TAXIWAY B7 **Use:** TAXIWAY **Area:** 119,964 SqFt

Section: 273 of 5 **From:** - **To:** - **Last Const.:** 1/1/2005

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 38,359 SqFt **Length:** 312 Ft **Width:** 90 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1980 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Work Date: 1/1/1992 **Work Type:** OVERLAY **Code:** IMPORTED **Is Major M&R:** True

Work Date: 1/1/2005 **Work Type:** Surface Reconstruction - AC **Code:** SR-AC **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 8 **Surveyed:** 3

Conditions: **PCI:** 62

Inspection Comments:

Sample Number: 101 **Type:** R **Area:** 5072.00 SqFt **PCI:** 67

Sample Comments:

48	L & T CR	L	341.00	Ft
52	RAVELING	L	1522.00	SqFt
56	SWELLING	L	80.00	SqFt
57	WEATHERING	M	3550.00	SqFt

Sample Number: 105 **Type:** R **Area:** 4887.00 SqFt **PCI:** 61

Sample Comments:

45	DEPRESSION	L	20.00	SqFt
48	L & T CR	L	182.00	Ft
48	L & T CR	M	90.00	Ft
52	RAVELING	L	1222.00	SqFt
56	SWELLING	L	25.00	SqFt
57	WEATHERING	L	2199.00	SqFt
57	WEATHERING	M	1466.00	SqFt

Sample Number: 106 **Type:** R **Area:** 4850.00 SqFt **PCI:** 60

Sample Comments:

45	DEPRESSION	L	20.00	SqFt
48	L & T CR	L	257.00	Ft
48	L & T CR	M	80.00	Ft
52	RAVELING	L	1453.00	SqFt
52	RAVELING	H	7.00	SqFt
57	WEATHERING	M	3390.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW B7 **Name:** TAXIWAY B7 **Use:** TAXIWAY **Area:** 119,964 SqFt

Section: 275 of 5 **From:** - **To:** - **Last Const.:** 1/2/1992

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 9,455 SqFt **Length:** 150 Ft **Width:** 60 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1961 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Work Date: 1/2/1992 **Work Type:** Overlay - AC Structural **Code:** OL-AS **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 3 **Surveyed:** 1

Conditions: **PCI:** 53

Inspection Comments:

Sample Number: 101 **Type:** R **Area:** 3007.00 SqFt **PCI:** 53

Sample Comments:

48	L & T CR	L	179.00	Ft
48	L & T CR	M	229.00	Ft
52	RAVELING	L	150.00	SqFt
57	WEATHERING	L	2857.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW B7 **Name:** TAXIWAY B7 **Use:** TAXIWAY **Area:** 119,964 SqFt

Section: 277 of 5 **From:** - **To:** - **Last Const.:** 1/1/1994

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 8,669 SqFt **Length:** 150 Ft **Width:** 60 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1961 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Work Date: 1/1/1994 **Work Type:** OVERLAY **Code:** IMPORTED **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 2 **Surveyed:** 1

Conditions: **PCI:** 69

Inspection Comments:

Sample Number: 103 **Type:** R **Area:** 4200.00 SqFt **PCI:** 69

Sample Comments:

48	L & T CR	L	19.00	Ft
48	L & T CR	M	100.00	Ft
52	RAVELING	L	1680.00	SqFt
57	WEATHERING	M	2520.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW B8 **Name:** TAXIWAY B8 **Use:** TAXIWAY **Area:** 125,168 SqFt

Section: 280 of 2 **From:** - **To:** - **Last Const.:** 7/1/2003

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 66,948 SqFt **Length:** 320 Ft **Width:** 130 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 7/1/2003 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 13 **Surveyed:** 2

Conditions: **PCI:** 66

Inspection Comments:

Sample Number: 301 **Type:** R **Area:** 5000.00 SqFt **PCI:** 65

Sample Comments:

48	L & T CR	L	262.00	Ft
48	L & T CR	M	35.00	Ft
52	RAVELING	L	499.00	SqFt
52	RAVELING	M	10.00	SqFt
57	WEATHERING	L	4491.00	SqFt

Sample Number: 400 **Type:** R **Area:** 4786.00 SqFt **PCI:** 67

Sample Comments:

48	L & T CR	L	329.00	Ft
48	L & T CR	M	50.00	Ft
52	RAVELING	L	718.00	SqFt
57	WEATHERING	L	4068.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW B8 **Name:** TAXIWAY B8 **Use:** TAXIWAY **Area:** 125,168 SqFt

Section: 285 of 2 **From:** - **To:** - **Last Const.:** 1/1/2003

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 58,220 SqFt **Length:** 380 Ft **Width:** 100 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1960	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1992	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2003	**Work Type:** Complete Reconstruction - AC	**Code:** CR-AC	**Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 11 **Surveyed:** 2

Conditions: **PCI:** 78

Inspection Comments:

Sample Number: 308 **Type:** R **Area:** 6189.00 SqFt **PCI:** 84

Sample Comments:

48	L & T CR	L	91.00	Ft
57	WEATHERING	L	5570.00	SqFt
57	WEATHERING	M	619.00	SqFt

Sample Number: 311 **Type:** R **Area:** 5039.00 SqFt **PCI:** 70

Sample Comments:

48	L & T CR	L	260.00	Ft
48	L & T CR	M	50.00	Ft
57	WEATHERING	L	4535.00	SqFt
57	WEATHERING	M	504.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW B9 **Name:** TAXIWAY B9 **Use:** TAXIWAY **Area:** 104,459 SqFt

Section: 290 of 2 **From:** - **To:** - **Last Const.:** 1/1/2015

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 20,199 SqFt **Length:** 77 Ft **Width:** 90 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1980	**Work Type:** New Construction - AC	**Code:** NC-AC	**Is Major M&R:** True
Work Date: 1/1/1992	**Work Type:** Overlay - AC Structural	**Code:** OL-AS	**Is Major M&R:** True
Work Date: 1/1/2005	**Work Type:** Surface Reconstruction - AC	**Code:** SR-AC	**Is Major M&R:** True
Work Date: 1/1/2015	**Work Type:** Complete Reconstruction - AC	**Code:** CR-AC	**Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 5 **Surveyed:** 1

Conditions: **PCI:** 83

Inspection Comments:

Sample Number: 96 **Type:** R **Area:** 3850.00 SqFt **PCI:** 83

Sample Comments:

48	L & T CR	L	111.00	Ft
57	WEATHERING	L	3800.00	SqFt
57	WEATHERING	M	50.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW B9 **Name:** TAXIWAY B9 **Use:** TAXIWAY **Area:** 104,459 SqFt

Section: 295 of 2 **From:** - **To:** - **Last Const.:** 1/1/2005

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 84,260 SqFt **Length:** 850 Ft **Width:** 90 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1980 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Work Date: 1/1/1992 **Work Type:** OVERLAY **Code:** IMPORTED **Is Major M&R:** True

Work Date: 1/1/2005 **Work Type:** Surface Reconstruction - AC **Code:** SR-AC **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 18 **Surveyed:** 3

Conditions: **PCI:** 55

Inspection Comments:

Sample Number: 104 **Type:** R **Area:** 5059.00 SqFt **PCI:** 55

Sample Comments:

42	BLEEDING	N	30.00	SqFt
48	L & T CR	L	426.00	Ft
48	L & T CR	M	73.00	Ft
52	RAVELING	L	2024.00	SqFt
56	SWELLING	L	123.00	SqFt
57	WEATHERING	M	3035.00	SqFt

Sample Number: 106 **Type:** R **Area:** 4350.00 SqFt **PCI:** 53

Sample Comments:

48	L & T CR	L	368.00	Ft
48	L & T CR	M	85.00	Ft
52	RAVELING	L	1740.00	SqFt
56	SWELLING	L	525.00	SqFt
57	WEATHERING	M	2610.00	SqFt

Sample Number: 112 **Type:** R **Area:** 4350.00 SqFt **PCI:** 58

Sample Comments:

48	L & T CR	L	223.00	Ft
48	L & T CR	M	200.00	Ft
52	RAVELING	L	870.00	SqFt
56	SWELLING	L	30.00	SqFt
57	WEATHERING	L	3480.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW C **Name:** TAXIWAY C **Use:** TAXIWAY **Area:** 318,249 SqFt

Section: 303 of 5 **From:** - **To:** - **Last Const.:** 1/1/2023

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 37,868 SqFt **Length:** 270 Ft **Width:** 100 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 10/1/2012 **Work Type:** New Construction - AC **Code:** NC-AC **Is Major M&R:** True

Work Date: 1/1/2023 **Work Type:** Mill and Overlay **Code:** ML-OVL **Is Major M&R:** True

Last Insp. Date: 1/14/2019 **TotalSamples:** 16 **Surveyed:** 3

Conditions: **PCI:** 84 **NOTE: *** Pre-Construction PCI ***

Inspection Comments:

Sample Number: 301 **Type:** R **Area:** 6500.00 SqFt **PCI:** 84

Sample Comments:

48	L & T CR	L	195.00	Ft
56	SWELLING	L	7.00	SqFt
57	WEATHERING	L	6500.00	SqFt

Sample Number: 306 **Type:** R **Area:** 6558.00 SqFt **PCI:** 89

Sample Comments:

48	L & T CR	L	100.00	Ft
57	WEATHERING	L	6558.00	SqFt

Sample Number: 312 **Type:** R **Area:** 5040.00 SqFt **PCI:** 79

Sample Comments:

48	L & T CR	L	235.00	Ft
56	SWELLING	L	35.00	SqFt
57	WEATHERING	L	5040.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW C **Name:** TAXIWAY C **Use:** TAXIWAY **Area:** 318,249 SqFt

Section: 305 of 5 **From:** - **To:** - **Last Const.:** 1/1/2023

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 53,314 SqFt **Length:** 415 Ft **Width:** 100 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 10/1/2012 **Work Type:** New Construction - AC **Code:** NC-AC **Is Major M&R:** True

Work Date: 1/1/2023 **Work Type:** Mill and Overlay **Code:** ML-OVL **Is Major M&R:** True

Last Insp. Date: 1/14/2019 **TotalSamples:** 16 **Surveyed:** 3

Conditions: **PCI:** 84 **NOTE: *** Pre-Construction PCI ***

Inspection Comments:

Sample Number: 301 **Type:** R **Area:** 6500.00 SqFt **PCI:** 84

Sample Comments:

48	L & T CR	L	195.00	Ft
56	SWELLING	L	7.00	SqFt
57	WEATHERING	L	6500.00	SqFt

Sample Number: 306 **Type:** R **Area:** 6558.00 SqFt **PCI:** 89

Sample Comments:

48	L & T CR	L	100.00	Ft
57	WEATHERING	L	6558.00	SqFt

Sample Number: 312 **Type:** R **Area:** 5040.00 SqFt **PCI:** 79

Sample Comments:

48	L & T CR	L	235.00	Ft
56	SWELLING	L	35.00	SqFt
57	WEATHERING	L	5040.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW C **Name:** TAXIWAY C **Use:** TAXIWAY **Area:** 318,249 SqFt

Section: 307 of 5 **From:** - **To:** - **Last Const.:** 1/1/2005

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 10,756 SqFt **Length:** 100 Ft **Width:** 125 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1961	**Work Type:** BUILT	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1985	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/1992	**Work Type:** OVERLAY	**Code:** IMPORTED	**Is Major M&R:** True
Work Date: 1/1/2005	**Work Type:** Overlay - AC Structural	**Code:** OL-AS	**Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 2 **Surveyed:** 1

Conditions: **PCI:** 65

Inspection Comments:

Sample Number: 101 **Type:** R **Area:** 5040.00 SqFt **PCI:** 65

Sample Comments:

48	L & T CR	L	326.00	Ft
48	L & T CR	M	125.00	Ft
52	RAVELING	L	756.00	SqFt
56	SWELLING	L	25.00	SqFt
57	WEATHERING	L	4284.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW C **Name:** TAXIWAY C **Use:** TAXIWAY **Area:** 318,249 SqFt

Section: 310 of 5 **From:** - **To:** - **Last Const.:** 1/1/1992

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 160,476 SqFt **Length:** 1,960 Ft **Width:** 75 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1961 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Work Date: 1/1/1985 **Work Type:** OVERLAY **Code:** IMPORTED **Is Major M&R:** True

Work Date: 1/1/1992 **Work Type:** OVERLAY **Code:** IMPORTED **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 40 **Surveyed:** 4

Conditions: **PCI:** 51

Inspection Comments:

Sample Number: 108 **Type:** R **Area:** 4031.00 SqFt **PCI:** 53

Sample Comments:

43	BLOCK CR	L	350.00	SqFt
48	L & T CR	L	310.00	Ft
48	L & T CR	M	225.00	Ft
52	RAVELING	L	1612.00	SqFt
57	WEATHERING	L	2419.00	SqFt

Sample Number: 119 **Type:** R **Area:** 4025.00 SqFt **PCI:** 49

Sample Comments:

43	BLOCK CR	L	313.00	SqFt
48	L & T CR	L	615.00	Ft
48	L & T CR	M	150.00	Ft
52	RAVELING	L	1006.00	SqFt
56	SWELLING	L	20.00	SqFt
57	WEATHERING	L	3019.00	SqFt

Sample Number: 127 **Type:** R **Area:** 3750.00 SqFt **PCI:** 53

Sample Comments:

48	L & T CR	L	639.00	Ft
48	L & T CR	M	150.00	Ft
52	RAVELING	L	1125.00	SqFt
56	SWELLING	L	5.00	SqFt
57	WEATHERING	L	2625.00	SqFt

Sample Number: 135 **Type:** R **Area:** 4016.00 SqFt **PCI:** 51

Sample Comments:

48	L & T CR	L	737.00	Ft
48	L & T CR	M	100.00	Ft
52	RAVELING	L	1205.00	SqFt
56	SWELLING	L	27.00	SqFt
57	WEATHERING	L	2811.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW C **Name:** TAXIWAY C **Use:** TAXIWAY **Area:** 318,249 SqFt

Section: 315 of 5 **From:** - **To:** - **Last Const.:** 1/1/2003

Surface: AAC **Family:** CA653-PR-TW-AAC-APC **Zone:** **Category:** **Rank:** P

Area: 55,835 SqFt **Length:** 650 Ft **Width:** 75 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/15/1960	**Work Type:** New Construction - Initial	**Code:** NU-IN	**Is Major M&R:** True
Work Date: 3/1/1985	**Work Type:** Overlay - AC Structural	**Code:** OL-AS	**Is Major M&R:** True
Work Date: 7/24/1991	**Work Type:** Overlay - AC Structural	**Code:** OL-AS	**Is Major M&R:** True
Work Date: 1/1/2003	**Work Type:** Mill and Overlay	**Code:** ML-OVL	**Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 13 **Surveyed:** 2

Conditions: **PCI:** 69

Inspection Comments:

Sample Number: 143 **Type:** R **Area:** 3750.00 SqFt **PCI:** 67

Sample Comments:

48	L & T CR	L	183.00	Ft
48	L & T CR	M	100.00	Ft
52	RAVELING	L	938.00	SqFt
57	WEATHERING	L	2812.00	SqFt

Sample Number: 151 **Type:** R **Area:** 4219.00 SqFt **PCI:** 71

Sample Comments:

48	L & T CR	L	187.00	Ft
48	L & T CR	M	50.00	Ft
52	RAVELING	L	844.00	SqFt
57	WEATHERING	L	3375.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW D **Name:** TAXIWAY D **Use:** TAXIWAY **Area:** 43,767 SqFt

Section: 405 of 2 **From:** - **To:** - **Last Const.:** 7/1/2005

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 33,610 SqFt **Length:** 612 Ft **Width:** 50 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 7/1/2005 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 7 **Surveyed:** 1

Conditions: **PCI:** 69

Inspection Comments:

Sample Number: 405 **Type:** R **Area:** 5000.00 SqFt **PCI:** 69

Sample Comments:

48	L & T CR	L	208.00	Ft
48	L & T CR	M	100.00	Ft
57	WEATHERING	L	4500.00	SqFt
57	WEATHERING	M	500.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW D **Name:** TAXIWAY D **Use:** TAXIWAY **Area:** 43,767 SqFt

Section: 410 of 2 **From:** - **To:** - **Last Const.:** 1/1/1998

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 10,157 SqFt **Length:** 185 Ft **Width:** 50 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1998 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 2 **Surveyed:** 1

Conditions: **PCI:** 67

Inspection Comments:

Sample Number: 400 **Type:** R **Area:** 5157.00 SqFt **PCI:** 67

Sample Comments:

48	L & T CR	L	103.00	Ft
48	L & T CR	M	139.00	Ft
57	WEATHERING	L	4642.00	SqFt
57	WEATHERING	M	515.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW Z **Name:** TAXIWAY Z **Use:** TAXIWAY **Area:** 67,569 SqFt

Section: 2605 of 3 **From:** - **To:** - **Last Const.:** 1/1/1994

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 62,575 SqFt **Length:** 1,200 Ft **Width:** 50 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1994 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Work Date: 1/1/1994 **Work Type:** OVERLAY **Code:** IMPORTED **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 12 **Surveyed:** 3

Conditions: **PCI:** 73

Inspection Comments:

Sample Number: 101 **Type:** R **Area:** 5666.00 SqFt **PCI:** 71

Sample Comments:

48	L & T CR	L	109.00	Ft
48	L & T CR	M	30.00	Ft
52	RAVELING	L	1133.00	SqFt
57	WEATHERING	L	4533.00	SqFt

Sample Number: 105 **Type:** R **Area:** 5000.00 SqFt **PCI:** 71

Sample Comments:

48	L & T CR	L	87.00	Ft
48	L & T CR	M	50.00	Ft
52	RAVELING	L	1000.00	SqFt
57	WEATHERING	L	4000.00	SqFt

Sample Number: 109 **Type:** R **Area:** 5000.00 SqFt **PCI:** 76

Sample Comments:

48	L & T CR	L	85.00	Ft
52	RAVELING	L	1000.00	SqFt
57	WEATHERING	L	4000.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW Z **Name:** TAXIWAY Z **Use:** TAXIWAY **Area:** 67,569 SqFt

Section: 2610 of 3 **From:** - **To:** - **Last Const.:** 1/1/1994

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 2,379 SqFt **Length:** 90 Ft **Width:** 20 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1994 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Work Date: 1/1/1994 **Work Type:** OVERLAY **Code:** IMPORTED **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 1 **Surveyed:** 1

Conditions: **PCI:** 42

Inspection Comments:

Sample Number: 100 **Type:** R **Area:** 2379.00 SqFt **PCI:** 42

Sample Comments:

41	ALLIGATOR CR	L	13.00	SqFt
45	DEPRESSION	L	54.00	SqFt
48	L & T CR	L	100.00	Ft
48	L & T CR	M	105.00	Ft
52	RAVELING	L	2022.00	SqFt
52	RAVELING	M	357.00	SqFt

Network: TLH **Name:** TALLAHASSEE INTERNATIONAL AIRPORT

Branch: TW Z **Name:** TAXIWAY Z **Use:** TAXIWAY **Area:** 67,569 SqFt

Section: 2615 of 3 **From:** - **To:** - **Last Const.:** 1/1/1994

Surface: AC **Family:** CA653-PR-TW-AC **Zone:** **Category:** **Rank:** P

Area: 2,615 SqFt **Length:** 90 Ft **Width:** 40 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1994 **Work Type:** BUILT **Code:** IMPORTED **Is Major M&R:** True

Work Date: 1/1/1994 **Work Type:** OVERLAY **Code:** IMPORTED **Is Major M&R:** True

Last Insp. Date: 11/30/2021 **TotalSamples:** 1 **Surveyed:** 1

Conditions: **PCI:** 70

Inspection Comments:

Sample Number: 100 **Type:** R **Area:** 2615.00 SqFt **PCI:** 70

Sample Comments:

48	L & T CR	L	47.00	Ft
48	L & T CR	M	45.00	Ft
52	RAVELING	L	523.00	SqFt
57	WEATHERING	L	2092.00	SqFt

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